

APPENDIX D
Cultural Resources
Documentation

Final

670 MESQUIT PROJECT, CITY OF LOS ANGELES, CALIFORNIA

Cultural Resources Assessment Report

Prepared for
RCS VE LLC
319 Lafayette Street, Suite 133
New York, NY 10012

December 2021



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Prepared for:

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319 Lafayette Street, Suite 133
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Project Location:

Los Angeles (CA) USGS 7.5-minute Topographic Quad
Township 1 South and 2 South, Range 13 West, Unsectioned

Acreage: Approx. 5.45 acres

APN: 5164-016-009; 5164-016-010; a
portion of 5164-016-803; 5164-017-002;
5164-017-003; 5164-017-006; 5164-017-
008; and 5164-018-009

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EXECUTIVE SUMMARY

Environmental Science Associates (ESA) has been retained by the RCS VE LLC (Applicant) to conduct a cultural resources assessment for the 670 Mesquit Project (Project) in support of an Environmental Impact Report (EIR). The purpose of the 670 Mesquit Project Cultural Resources Assessment Report (Cultural Resources Assessment Report) is to identify and evaluate cultural resources on the Project Site that may be affected by the Project and to analyze potential impacts to cultural resources including archaeological and historic built environment resources that could result from the implementation of the Project.

The Project Site is currently developed with existing attached one- to four-story freezer, cold storage, and dry storage warehouses with associated office space, loading docks, and surface parking. The existing warehouses date from approximately 1908 through 2002. The primary business, Rancho Cold Storage, has operated on-site for more than 30 years. Other on-site businesses include Hidden Villa Ranch, Integrated Food Service, and Harvey's Produce. The Applicant proposes to construct a new mixed-use development totaling up to 1,792,103 square feet on an approximately 5.45-acre property at 670 Mesquit Street (Project Site) in the along the southeastern edge of the Artists-in-Residence District within the Central City North Community Plan area of the City of Los Angeles (City). New development would include creative office space (up to 944,055 square feet); a 236-room hotel; 308 multi-family residential housing units; an Arts District Central Market, a grocery store, and general retail uses totaling up to 136,152 square feet; restaurants totaling up to 89,576 square feet; studio/event/gallery space and a potential museum totaling up to 93,617 square feet; and a gym of up to 62,148 square feet. The Project would include up to six levels of below-grade parking spanning the buildings' footprints and would include at-grade and above-grade parking within Building 5. The Project proposes to construct two vehicular and pedestrian ramp connections to the 7th Street Bridge from Buildings 4 and 5 at southern end of the Project Site. The Project also would include new pedestrian access from the bridge leading to an open courtyard space between Buildings 4 and 5. The proposed ramp connections have been designed in accordance with the Secretary of the Interior's Standards (Standards) to reduce potential impacts to the 7th Street Bridge (P-19-175070), which is considered a historical resource under CEQA, and potential impacts are analyzed in this assessment report. The City of Los Angeles is the Lead Agency pursuant to the California Environmental Quality Act (CEQA).

A California Historical Resources Information System (CHRIS) records search for the Project Site was conducted on January 24, 2018, at the South Central Coastal Information Center (SCCIC) housed at California State University, Fullerton. The records search included a review of all previously recorded archaeological resources and historic architectural resources within the Project Site and a 0.5-mile radius of the Project Site. Of the 33 previous studies, nine (LA-3813, -4834, -8252, -10506, -10887, -11048, -11409, -11642, and -11785) overlap the Project Site. A

total of 16 cultural resources have been previously recorded in the 0.5-mile radius of which five are archaeological resources and 11 are historic architectural resources. The 11 historic architectural resources consist of nine buildings and two structures, none of which are located on the Project Site. One designated structure, the 6th Street Bridge, has been demolished since the time it was recorded. Of the remaining 10 historic architectural resources, two are designated historical resources (7th Street Bridge and the National Biscuit Company Building), and two are eligible historical resources (Engine Co. #17 and H.J. Heinz Co. Warehouse). ESA also reviewed Department of City Planning Reports, HCM designations and SurveyLA findings which resulted in the identification of two additional resources. The Downtown Los Angeles Industrial Historic District is included in SurveyLA and is located about a quarter mile from the Project Site. The Ford Motor Company Factory Building is located at 777 Santa Fe and 2046 E 7th Street and was identified as a known historical resource in the 2143 Violet Initial Study (Case Number: ENV-2017-436-EIR).

All the five previously recorded archaeological resources are historic-period archaeological sites. None of the five archaeological resources are located within the Project Site; however, two segments of the historic alignments of the Union Pacific Railroad, Southern Pacific Railroad, Los Angeles Division and the Burlington Northern Santa Fe Railroad (BNSF Railroad) are located adjacent to the Project Site and were evaluated and found to lack integrity to contribute to the eligibility of these previously identified resources for the National or California Registers. Three historic archaeological sites previously recorded outside of the Project Site consisting of historic refuse scatter, a city block with historic refuse, and a historic road have not been evaluated.

The Native American Heritage Commission (NAHC) was contacted on March 15, 2018, to request a search of the Sacred Lands File (SLF). The NAHC responded to the request in a letter dated March 16, 2018, indicating that Native American cultural resources are not known to be located within the Project Site; however, that the general area is sensitive for cultural resources.

A cultural resources survey of the Project Site was conducted in 2016 and 2018 to identify surface evidence of archaeological resources and to document any potential historic architectural resources. For ease of documentation, the Project Site was divided into eight (8) Cultural Survey Areas corresponding to assessor's parcel numbers (APN) and configurations of the buildings on site.

Survey Area #1 (689 S. Mesquit Street [APN 5164-018-009]) contains one utilitarian industrial structure completed in 1963. Survey Area #2, (2143 E. 7th Street [APN 5164-017-008]) contains a cold storage warehouse built in 2002. Survey Areas #3-5 (642 and 674 S. Mesquit Street [APNs 5164-017-003, 5164-017-002, and 5164-017-006]) contain a complex of 10 attached buildings, the earliest of which was originally constructed in 1888; the original buildings were replaced in 1908 after a fire, and with several later attached additions and alterations they currently have the appearance of one large cold storage and dry warehouse building complex (10 attached buildings). Survey Areas #6 and #7 (1494-1498 E 6th Street [APNs 5164-016-009 and 5164-016-010]) are currently undeveloped; buildings previously located on these parcels have been demolished. Survey Area #8 (APN 5164-016-803) consists of a former railroad right-of-way within the Project Site (not to be confused with the Railway

Properties adjacent to the Project Site). In total, the potential historic architectural resources identified on the Project Site which are over 45 years in age and were evaluated as a result of the survey included one building (1963 industrial structure in Survey Area #1) and one building complex (1908-1985 building complex in Surveys Areas #3-5). No surface evidence of archaeological sites was encountered. Surveyors also documented condition of the adjacent resources, including the BNSF Railroad (P-19-186804) and the 7th Street Bridge (P-19-175070). One historic architectural resource located within the Project Site was included in SurveyLA Central City North Survey but found ineligible for designation due to lack of integrity, the Los Angeles Ice & Cold Storage Company (Rancho Cold Storage), 674-686 S. Mesquit Street. The property was assigned an 7SQ status code: Not eligible; individual property assessed for significance in accordance with SurveyLA but does not meet eligibility standards (SurveyLA, 2016; HistoricPlacesLA.org). ESA conducted further intensive-level survey and research on the cold storage building complex including the Los Angeles Ice and Cold Storage Company. The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) and the former fruit juice factory (now cold storage facility) in Survey Area #5 (APN: 5164-017-006) are attached structures that appear as one large building.

The results of the SCCIC records research and land use research conducted for the Project, which included a historic map analysis, historic aerial review, geoarchaeological study, review and analysis of the geotechnical report, review and analysis of the prehistoric and historic context of the project site and vicinity, and archaeological resources survey did not identify any known archaeological resources within the Project Site. Although no known unique archaeological resources were identified within the Project Site, based on the history of the area and the nature of past and historic development of the Project Site, the Project has the potential to encounter unknown subsurface archaeological resources that could qualify as either historical resources or unique archaeological resources under CEQA. Recommendations including the retention of a qualified archaeologist, archaeological monitoring, as well as procedures to be followed in the event of the discovery of archaeological resources or human remains are provided in the Conclusions and Recommendations section at the close of this report.

As a result of the historical resources investigations, there were two potential resources over 45 years in age within the Project Site that were identified and evaluated in this study. The 1963 industrial building located in Survey Area #1 and the 1908-1985 building complex located in Survey Areas #3-5 were surveyed, evaluated, and found ineligible for listing in the National Register, California Register, or for local listing, and are therefore not considered historical resources under CEQA as summarized below and documented in this Cultural Resources Assessment Report.

The one-story utilitarian industrial L-shaped concrete building at 689 S. Mesquit Street in Survey Area #1 (APN 5461-018-009), was completed in 1963 when a Certificate of Occupancy was issued to the West Coast Quilting Company (presently Rancho Cold Storage) for a one-story, type III-B, 120-foot by 178-foot office and manufacturing facility with 41 parking spaces. Previous improvements were demolished in 1955 prior to the construction of the existing utilitarian industrial manufacturing building. The West Coast Quilting Company factory (Rancho

Cold Storage) building at 689 S. Mesquit Street. was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 and C/3/3 for its association with the Sub-Context Manufacturing for the Masses (1887-1965) as applied to the Factory Property Type (1887-1990). Originally constructed for the West Coast Quilting Company, the existing building currently does not retain any characteristic features that convey its original factory use. Furthermore, research results do not indicate that the West Coast Quilting Company was a historically significant manufacturer in Los Angeles. The original industrial manufacturing building was substantially altered when it was converted from a factory to a cold storage warehouse in or by 2006 and it does not currently have intact character-defining features and integrity to convey its original function as a manufacturing facility. The building is presently used by Rancho Cold Storage as a staging area for frozen and refrigerated goods. There is no manufacturing occurring in the building currently. It is a common example of a utilitarian industrial factory building that has some minor articulation of the front entry to the building in a Mid-Century Modern style. Substantially altered, it is not considered an excellent example of its property type, nor is the building a representative example of a particular style since it is largely utilitarian—save for a few decorative elements at its entry. Furthermore, it does not appear to be the work of a master architect or builder. Therefore, due to lack of significance and integrity, the existing utilitarian industrial building at 689 S. Mesquit Street in Survey Area #1 (APN 5461-018-009) appears to be ineligible as a historical resource under CEQA.

Within the Project Site, a complex of 10 attached existing buildings constructed between 1908 and 1985 with later alterations and additions are located in Survey Areas #3, #4, and #5 (APN 5164-017-003, 5164-017-002, 5164-017-006) now appearing as one large cold storage and dry storage warehouse. A Sanborn insurance map from 1888 depicts the original cold storage and ice manufacturing facilities at 674 S. Mesquit Street. After it was destroyed by fire, a new facility of 10 connected buildings was developed, beginning in 1908 with the construction of a new building in Survey Area #3 (APN 5164-017-003) at 674 to 678 S. Mesquit Street to replace the original 1888 facility at 674 S. Mesquit Street. The new storage building erected for the Los Angeles Ice and Cold Storage Company was designed by Hunt, Eager & Burns and built for a cost of \$15,000; the 80-foot by 96-foot building was four-stories tall and had a concrete foundation, blue brick front, brick cornices, iron stairways, wood columns, and a composition roof. Later updates of the Sanborn map show the development of a much larger facility extending into Survey Areas #4 and #5, the construction and expansion of which is documented in multiple building permits that are characterized by incremental additions and structural upgrades over the years. In 1933, a series of changes to the building's walls were undertaken to repair damage from the Long Beach Earthquake. In 1990, to comply with earthquake safety regulations (Division 88), the existing building complex underwent substantial modifications. The Division 88 compliant alterations included replacing unreinforced masonry with modern textured, concrete blocks. In its present condition, the exterior of the building complex is completely unrecognizable due to over \$700,000 worth of Division 88 modifications.

LADBS Permits for Survey Area #4 (APN: 5164-017-002) also begin in April of 1908. The earliest permit on file was for lifting the roof of an existing building by 10 feet, similar to work completed at the same time in Survey Area #3 to repair fire damage. In 1912, the building was further modified, raising the second floor 4 feet and 4 inches to make room for the addition of a

new floor between the first and second levels. In 1932, the building's structure and floor were strengthened to support greater loads. In the 1970s, a series of permits were filed to repair additional fire damage. The greatest alterations to structures in Survey Area #4 occurred between 1989 and 1994 when the building underwent structural upgrades in compliance with Division 88. The alterations replaced the original unreinforced masonry exterior with new textured concrete block. Like the buildings in Survey Area #3, these alterations completely rendered the existing buildings unrecognizable. Furthermore, the buildings in Survey Area #4 cannot be differentiated from the buildings in Survey Area #3 due to the new concrete block exterior walls and Division 88 modifications.

The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) is historically associated with the Los Angeles Ice & Cold Storage Co. and was evaluated under the Industrial Development Context (1850-1980) for its potential eligibility under Criteria A/1/1 in association with the Theme, Agricultural Roots (1850-1945) and its associated Sub-Theme, From Farm to Market (1900-1960), and under Criteria C/3/3 as an example of an associated Property Type, Cold Storage Warehouse (1900-1945). The existing building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) has been substantially altered after its period of significance and as a result of comprehensive Division 88 improvements and does not have sufficient integrity to meet the criteria of eligibility for listing as an example of a cold storage warehouse property type associated with the Los Angeles Ice and Cold Storage Company.

Improvements in Survey Area #5 (APN: 5164-017-006) began as early as 1909 with the construction of a single-family residence owned by the Atchison, Topeka, and Santa Fe (AT&SF) Railroad. However, the residence had been demolished by 1922 when a concentrated fruit juice factory was built by the Hyland Stanford Company on the site. The Hyland Stanford Company made significant contributions to the fruit processing industry through multiple patents registered in the company's name. Throughout the 20th century, the facilities were modified by subsequent owners including South Coast Packing Co. and Rancho Cold Storage. The buildings in Survey Area #5 were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold-storage facility that was constructed in 1985, which is attached to the warehouse complex in Survey Areas #3 and #4 discussed above. The former fruit juice factory in Survey Area #5 (APN: 5164-017-006) associated with the Hyland Stanford Company was found ineligible as a historical resource under CEQA because the factory was destroyed by fire and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use.

Therefore, due to its lack of historic and architectural significance and integrity to convey any significant associations under the Industrial Development Context, the entire building complex in Survey Areas #3, #4, and #5 (APN 5164-017-003, 5164-017-002, 5164-017-006) was found ineligible as a historical resource under CEQA.

The 7th Street Bridge (P-19-175070), immediately adjacent to the Project Site at its southern end, was previously determined eligible for listing in the National Register by consensus through the Section 106 process, is listed in the California Register, and was locally listed as a Historic-

Cultural Monument (HCM) by the City of Los Angeles in 2007 and, therefore, is considered a historical resource under CEQA.

In addition, one building adjacent to the Project Site on the west was identified as being over 45 years in age and was included in this analysis. The structure at 2101 E. 7th Street, is a three-story brick utilitarian industrial building constructed in 1910 for the Bailey Schmitz Co., a mattress batting manufacturer who owned the building until 1983. The building has been seismically upgraded and substantially renovated for adaptive reused as a multi-family live-work residence. The Bailey Schmitz Co. building was not identified as an individually eligible resource or as a contributor to a potentially eligible historic district by SurveyLA. Furthermore, the records search results did not identify any previous evaluations of the building. It is not located within a known historic district. The building was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 and C/3/3 for its association with the Sub-Context Manufacturing for the Masses (1887-1965) as applied to a factory or manufacturing plant (1887-1980). The building does not appear to be historically significant under Criteria A/1/1 for its association with early 20th century manufacturing in Los Angeles, nor does it appear eligible under Criteria C/3/3/ as an example of a factory or manufacturing plant. Substantial alterations to convert the building from manufacturing to residential use have materially impaired its historic significance and architectural integrity, and therefore it is not considered a historical resource under CEQA.

As summarized above and described in greater detail in this report, the buildings on the Project Site and the adjacent structure at structure at 2101 E. 7th Street do not qualify as historical resources under CEQA Guidelines Section 15064.5(a)(1) or (2) and do not warrant consideration under CEQA Guidelines Section 15064.5(a)(3). Accordingly, the Project would not directly cause a substantial adverse change in the significance of an historical resource on the Project Site or at 2101 E. 7th Street, as defined in Section 15064.5.

While the proposed Project has been designed to follow the Secretary of the Interior's Standards, the integrity and significance of the 7th Street Bridge could still be affected by removal/alteration of some of the character-defining features of the 7th Street Bridge that are necessary for implementation of the Project. Moreover, there is a potential for structural damage to the 7th Street Bridge due to construction activities. Therefore, potential impacts on the 7th Street Bridge are analyzed in this Cultural Resources Assessment Report. As a result of the analysis, it was determined that the Project would not cause a substantial adverse change in the significance of the Bridge and after construction of the Project, the 7th Street Bridge would remain largely intact and continue to convey its historic significance. However, there is a potential for structural damage during construction. Therefore, pursuant to CEQA, although the Project, once constructed, would not materially impair the eligibility of the 7th Street Bridge as a historical resource, the potential for structural damage to the 7th Street Bridge during construction is considered a potentially significant indirect impact and mitigation measures are required to reduce impacts to less than significant.

Indirect impacts to resources within a quarter-mile radius of the Project Site were also analyzed to determine if the Project would result in substantial adverse changes to their integrity or their

immediate surroundings that would detract from their significance as historical resources. Four eligible or designated known historical resources and a historic district were identified within the Project vicinity, as follows: Engine Co. #17 at 708 S. Santa Fe Avenue (3S); the H. J. Heinz Co. Warehouse at 712 S. Santa Fe Avenue (3S); the Ford Motor Company Factory at 777 Santa Fe Avenue and 2046 E. 7th Street, which was recently determined eligible for the National Register; the National Biscuit Company Building at 1820 E. Industrial Street (5S1, HCM 888); and the Downtown Los Angeles Industrial Historic District. However, all these historical resources are approximately one to two blocks southwest or two to three blocks northwest of the Project Site, therefore, none of these resources would be materially impacted and primary views to and from these resources would not be obstructed or altered by the Project such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired.

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670 MESQUIT PROJECT

Cultural Resources Assessment Report

Introduction, Project Location, and Project Description

Introduction

Environmental Science Associates (ESA) has been retained by the RCS VE LLC (Applicant) to conduct a cultural resources assessment for the 670 Mesquit Project (Project) in support of an Environmental Impact Report (EIR). The City of Los Angeles (City) is the lead agency pursuant to the California Environmental Quality Act (CEQA).

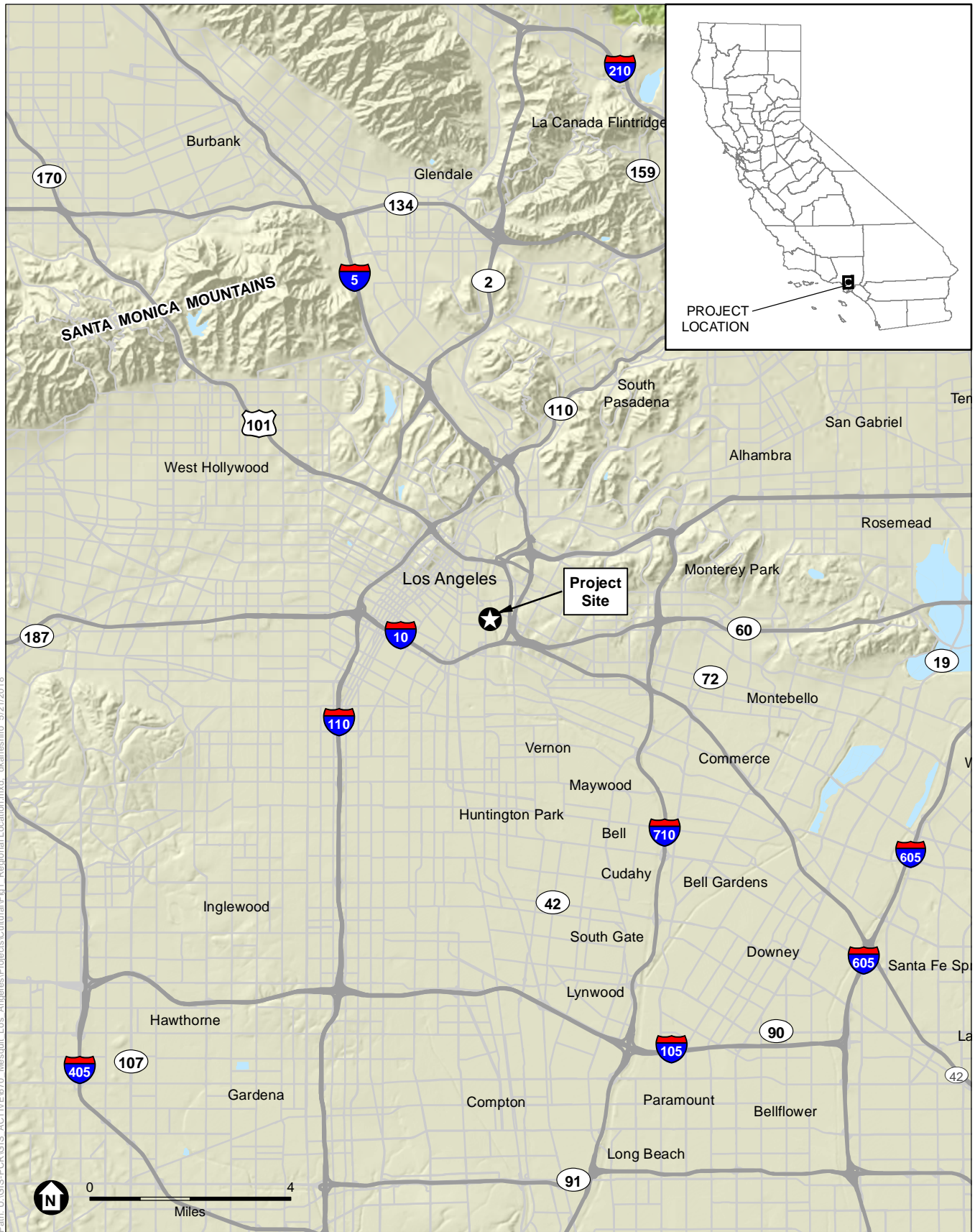
ESA personnel involved in the preparation of this report are as follows: Monica Strauss, M.A., R.P.A. and Margarita Jerabek, Ph.D., project directors; Sara Dietler, B.A., project manager; Vanessa Ortiz, M.A., RPA, Archaeologist and contributing author; Chris Taylor, M.H.A., and Ashley Brown, M.A., Architectural Historians and contributing authors; Henry Chodsky, B.A., Archaeologist (surveyor); and Jessie Lee, GIS specialist. Resumes of key personnel are included in **Appendix A**.

Project Location and Surrounding Uses

The Project Site is located within the Central City North Community Plan area within the City (**Figure 1**), in an unsectioned portion of Township 1 South and 2 South, Range 13 West on the Los Angeles USGS 7.5-minute topographic quadrangle (**Figure 2**). The Project Site is primarily bounded on the north by former 6th Street Viaduct right-of-way, on the south by 7th Street Bridge, on the east by the railroad right-of way, and on the west by a Los Angeles Department of Water and Power (LADWP) facility, Mesquit Street, and the warehouse facility on the west side of Mesquit Street north of the 7th Street Bridge (**Figure 3**). The majority of the Project Site is on the east side of Mesquit Street, with additional parcels in the southern portion of the Project Site located on the west side of Mesquit Street at 7th Street.

The Project Site (**Figure 4**) comprises eight assessor parcel numbers (APNs). For ease of reference, these are referred to herein as Cultural Survey Areas (Survey Areas), as follows:

- APN 5164-018-009 (**Survey Area #1**);
- APN 5164-017-008 (**Survey Area #2**);
- APN 5164-017-003 (**Survey Area #3**);
- APN 5164-017-002 (**Survey Area #4**);
- APN 5164-017-006 (**Survey Area #5**);
- APN 5164-016-009 (**Survey Area #6**);
- APN 5164-016-010 (**Survey Area #7**); and
- A portion of 5164-016-803 (**Survey Area #8**).

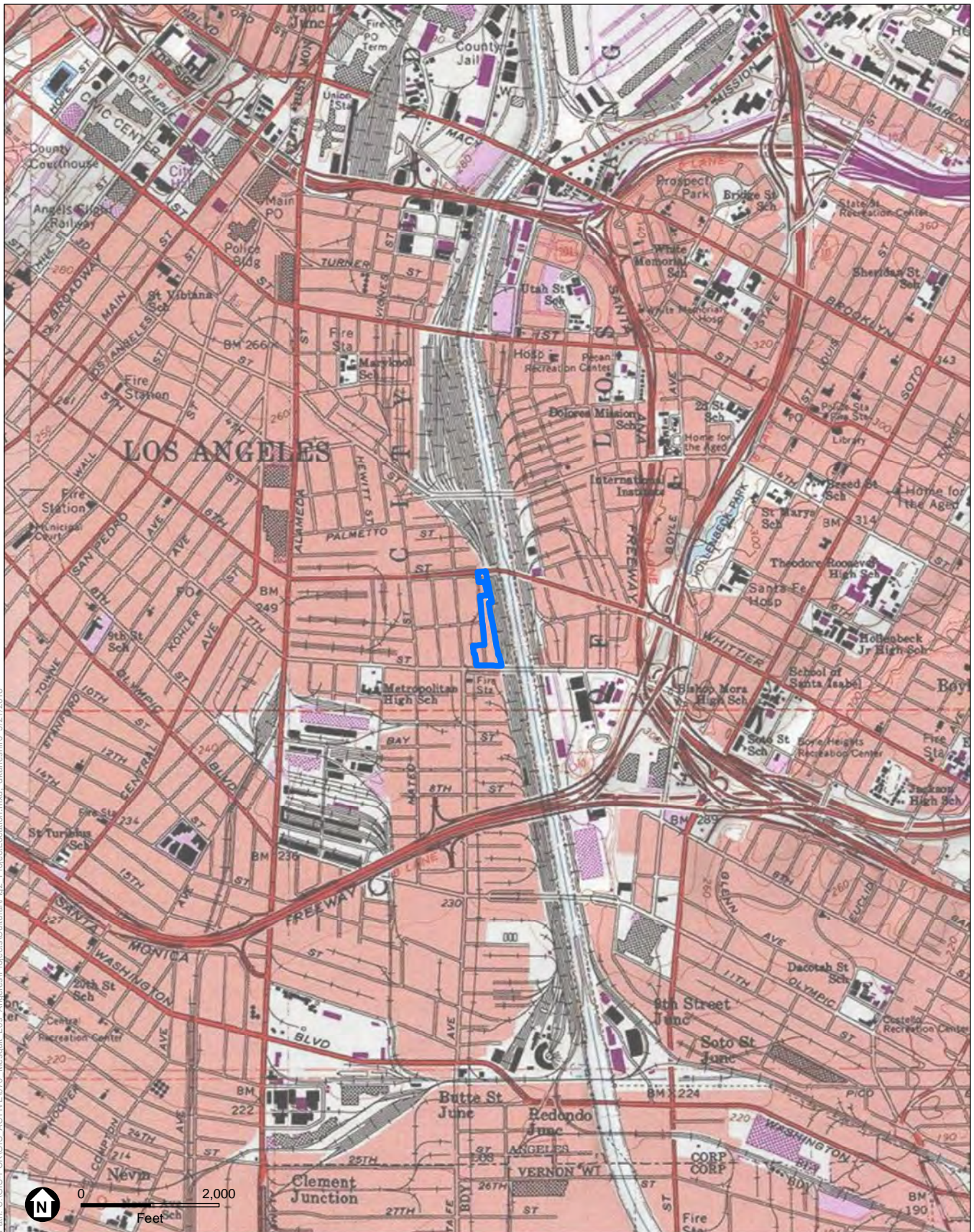


SOURCE: ESRI

670 Mesquit

Figure 1
Regional Location

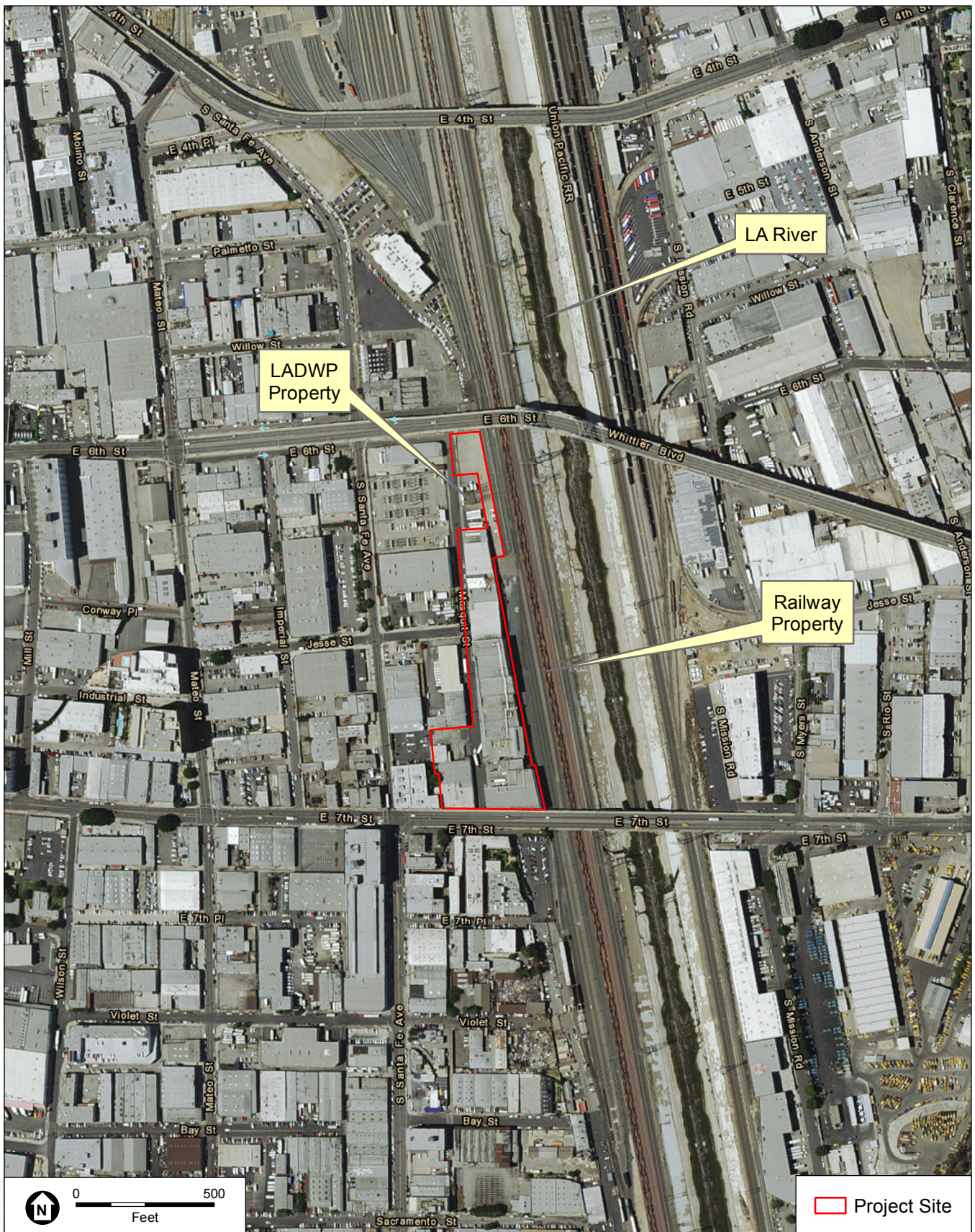




SOURCE: USGS Topographic Series (Los Angeles Quad).

670 Mesquit

Figure 2
Project Location



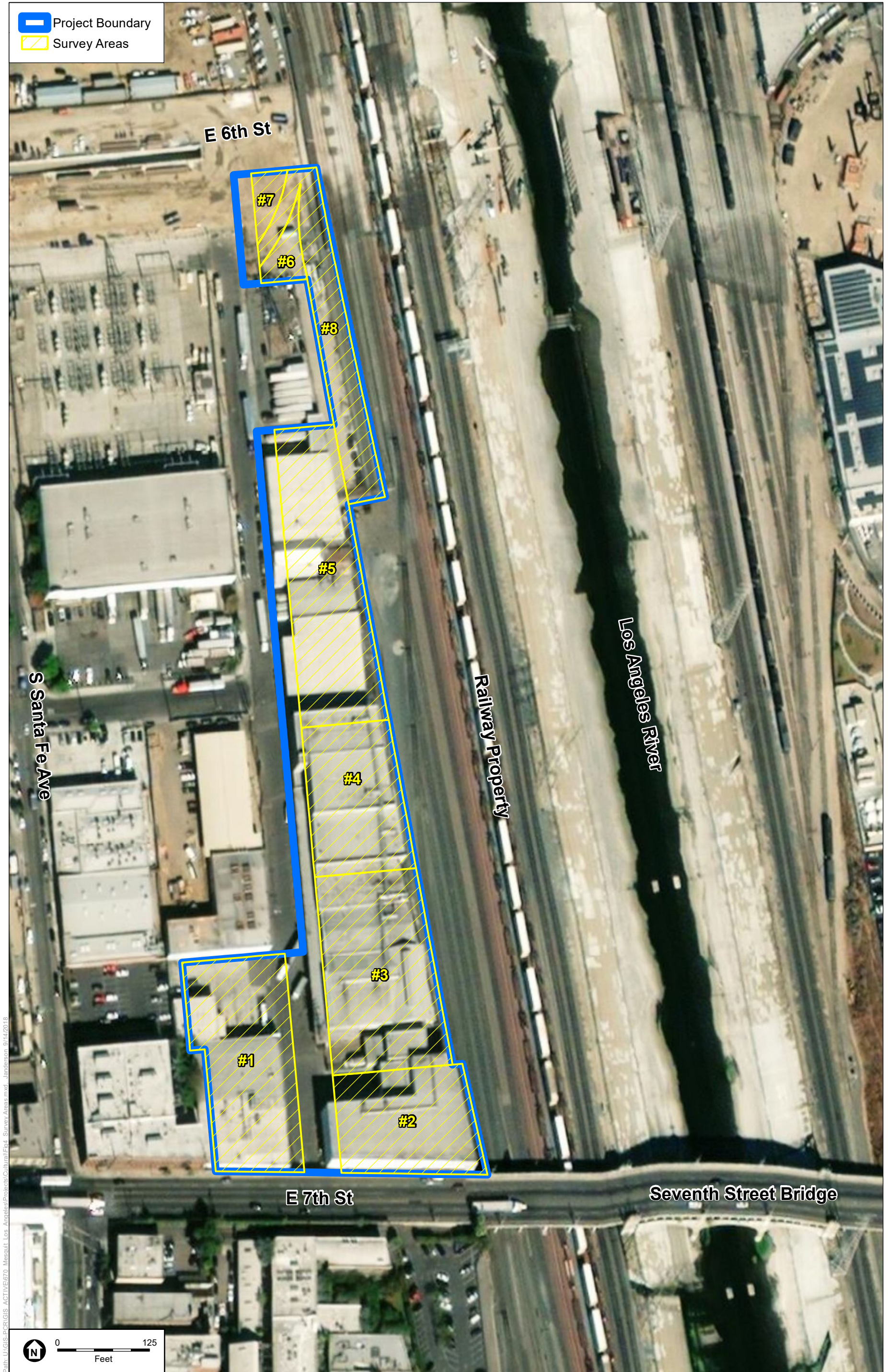
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SOURCE: Google Map, 2015 (Aerial)

670 Mesquit Street Project

Figure 3
Aerial Photograph with Surrounding Land Uses





SOURCE: DigitalGlobe, 2016 (Aerial), ESA, 2018.

670 Mesquit Street

Figure 4
 Aerial Photograph with Survey Area
 and Project Boundary

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Access. Access to the Project Site is provided by a network of regional transportation facilities that provide access to the greater metropolitan area. Regional access is provided by the Santa Monica Freeway (I-10), approximately 0.40 miles to the south; the Hollywood Freeway (US-101) and Golden State Freeway (I-5), approximately 0.40 miles to the east; and the Pomona Freeway (SR-60), approximately 0.50 miles to the southeast. Local access is provided by 6th and 7th Streets, with direct access provided by Jesse Street and Mesquit Street.

Surrounding Uses. To the north, the Project Site is bordered by the 6th Street ROW and the site of construction of the Sixth Street Viaduct Replacement project, the centerpiece of which is a new multi-modal bridge known as the Ribbon of Light Bridge. The Sixth Street Viaduct Replacement project is led by the City's Bureau of Engineering, which also proposes approximately 12 acres of open space and recreational amenities—known as the Sixth Street Park, Arts, River, and Connectivity (PARC) Improvements—under and adjacent to the Ribbon of Light Bridge. Metro owns several vacant parcels north of 6th Street.

The Project Site is bordered on the east by freight and passenger rail lines and rail yards (Railway Properties) owned and operated by National Railroad Passenger Corporation (Amtrak), BNSF Railway Company, and the Los Angeles County Metropolitan Transportation Authority (Metro). The Los Angeles River is located to the east of the Railway Properties, approximately 200 feet from the Project Site's eastern property line.

The elevated 7th Street Bridge forms the southern boundary of the Project Site. The 7th Street Bridge provides access between the greater Downtown area and the Arts District and Boyle Heights, east of the Los Angeles River. The 7th Street Bridge has previously been determined eligible for the National Register of Historic Places (National Register) by consensus through the Section 106 process, is listed in the California Register, and is a City-designated HCM No. 904. The area south of the 7th Street Bridge comprises a mix of uses in converted industrial and other buildings, including live/workspaces, restaurants, and coffee shops. There are warehouses and light manufacturing uses located throughout this area as well.

The west side of Mesquit Street, adjacent to the Project Site, is developed with the Los Angeles Department of Water and Power River Switching Station (LADWP Property), warehouses, an under-construction creative office building at the northwest corner of the intersection of Jesse Street and Mesquit Street (640 S. Santa Fe Avenue), and surface parking. Other land uses to the west include existing creative office space; restaurants; and commercial uses; the newly constructed AMP Lofts live/work and commercial development, the recently completed adaptive reuse of the Ford Motor Factory located a block from the Project Site to house Warner Music Group's new corporate campus, which includes office space, recording studios, and performance spaces; and various low-rise industrial and warehouse uses similar to those on the Project Site. A three-story multi-family residential building (Artist Lofts DTLA at 688 S. Santa Fe Avenue) occupies the northeast corner of S. Santa Fe Avenue and 7th Street and directly abuts the Project Site.

Several creative office, creative space, and mixed-use projects are currently proposed or developed farther to the west, beyond S. Santa Fe Avenue. These include the proposed 6 AM

project at 6th and Alameda Streets, which would include more than 2.8 million square feet of development in two 58-story towers including apartments, condominiums, a hotel, offices, retail and restaurant uses, a school, and art space; a proposed mixed-use development located 7th and Alameda Streets; and the completed redevelopment of the Los Angeles 30-acre Terminal Market property as the mixed-use ROW DTLA project.

Project Description

The Applicant proposes to construct a new mixed-use development (Project) totaling up to 1,792,103 square feet of floor area on an approximately 5.45-acre property at 670 Mesquit Street in the City. Project implementation would require the removal of all existing on-site uses, including cold storage warehouses that contain freezers, coolers, dry storage, and office space totaling up to 205,393 square feet of floor area. New development would include creative office space (up to 944,055 square feet); a 236-room hotel; 308 multi-family residential housing units; an Arts District Central Market, a grocery store, and general retail uses totaling up to 136,152 square feet; restaurants totaling up to 89,576 square feet; studio/event/gallery space and a potential museum totaling up to 93,617 square feet; and a gym of up to 62,148 square feet. Buildings would range between 126 feet to 378 feet tall.

The Project would provide open space for use by Project residents, hotel guests, employees, and visitors totaling up to 141,876 square feet. Proposed open space features include at-grade landscaped areas, pedestrian passageways and walkways, viewing platforms, and above-grade landscaped terraces and pool decks.

The Project may include a Deck Concept (Project with the Deck Concept) that would involve construction of a 132,000 square foot Deck that would extend over a portion of the freight and passenger rail lines and rail yards (Railway Properties) east of the Project Site. The Deck in the Project with the Deck Concept would serve as a multi-modal connection between the 7th Street Bridge and the Project Site's Northern Landscaped Area. The Deck could also provide access directly to the Los Angeles River. If the Deck is constructed, the vehicular and pedestrian ramp connecting and providing access between the Project Site and the 7th Street Bridge could be moved east, above the Railway Properties, from Building 4, and could connect to the 7th Street Bridge in a different location than otherwise proposed if the Deck is not constructed.

The Project would include up to six levels of below-grade parking that span the buildings' footprints, and it also would include at-grade and above-grade parking within Building 5.

Construction would include up to 531,319 cubic yards of grading (cut), all of which would be exported from the Project Site, with excavations ranging from 61 to 68 feet below ground surface (bgs) for the lowest subterranean parking level and 71 to 75 feet bgs in isolated areas to accommodate elevator pits.

Project construction is anticipated to commence as early as 2021 and to be completed as early as 2026—if constructed in a single phase—or as late as 2040, if built in separate phases over time. In the event that construction is phased, construction of below-grade parking may also be phased.

Proposed Buildings and Associated Uses

Building 1 is the northernmost proposed building on the east side of Mesquit Street. It would comprise up to 466,554 square feet of floor area and would be up to 378 feet in height. Proposed uses in Building 1 would include multi-family residential dwellings, retail spaces, and a hotel.

Building 2 would be located south of Building 1. It would comprise up to 331,517 square feet of floor area and would be a maximum of 294 feet in height. Proposed uses in Building 2 include retail, restaurants, a gym, and offices.

Building 3 would be located south of Building 2. It would comprise up to 239,936 square feet of floor area and would be a maximum of 210 feet in height. Uses in Building 3 include retail space, restaurant, studio/event/gallery and potential museum space, offices, the Arts District Central market, and a grocery store.

Building 4 would be located at the southern edge of the Project Site on the east side of Mesquit Street and abutting the 7th Street Bridge. Building 4 would comprise up to 70,519 square feet of floor area and would be a maximum of 126 feet in height, with a vehicular and pedestrian ramp connection to the 7th Street Bridge. Proposed uses in Building 4 include retail space and restaurants.

Building 5 would be located at the southern edge of the Project Site on the west side of Mesquit Street and abutting the 7th Street Bridge. Building 5 would comprise up to 683,577 square feet of floor area and would be a maximum of 378 feet in height. Building 5 would include a one-way outbound vehicular connection via a ramp from the parking structure to the 7th Street Bridge. A heliport is proposed atop Building 5. Proposed uses in Building 5 are primarily office but would include space for retail uses.

Regulatory Framework

Numerous laws and regulations require federal, state, and local agencies to consider the effects a project may have on cultural resources. These laws and regulations stipulate a process for compliance, define the responsibilities of the various agencies proposing the action, and prescribe the relationship among other involved agencies.

Federal

National Historic Preservation Act

The principal federal law addressing historic properties is the National Historic Preservation Act (NHPA) of 1966, as amended (54 United States Code of Laws [USC] 300101 et seq.), and its implementing regulations (36 Code of Federal Regulations [CFR] Part 800). Section 106 requires a federal agency with jurisdiction over a proposed federal action (referred to as an “undertaking” under the NHPA) to take into account the effects of the undertaking on historic properties, and to provide the Advisory Council on Historic Preservation (ACHP) an opportunity to comment on the undertaking.

The term “historic properties” refers to “any prehistoric or historic district, site, building, structure, or object included in, or eligible for inclusion in, the National Register” (36 CFR Part 800.16(l)(1)). The implementing regulations (36 CFR Part 800) describe the process for identifying and evaluating historic properties, for assessing the potential adverse effects of federal undertakings on historic properties, and seeking to develop measures to avoid, minimize, or mitigate adverse effects. The Section 106 process does not require the preservation of historic properties; instead, it is a procedural requirement mandating that federal agencies take into account effects to historic properties from an undertaking prior to approval.

National Register of Historic Places

The National Register of Historic Places (National Register) was established by the NHPA of 1966, as “an authoritative guide to be used by federal, State, and local governments, private groups and citizens to identify the Nation’s historic resources and indicate what properties should be considered for protection from destruction or impairment” (36 CFR 60.2) (U.S. Department of the Interior, 2002). The National Register recognizes a broad range of cultural resources that are significant at the national, state, and local levels and can include districts, buildings, structures, objects, prehistoric archaeological sites, historic-period archaeological sites, traditional cultural properties, and cultural landscapes. As noted above, a resource that is listed in or eligible for listing in the National Register is considered an “historic property” under Section 106 of the NHPA.

Significance Criteria

To be eligible for listing in the National Register, a property must be significant in American history, architecture, archaeology, engineering, or culture. Properties of potential significance must meet one or more of the following four established criteria:

- A. Are associated with events that have made a significant contribution to the broad patterns of our history;
- B. Are associated with the lives of persons significant in our past;
- C. Embody the distinctive characteristics of a type, period, or method of construction or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- D. Have yielded, or may be likely to yield, information important in prehistory or history.

Integrity

In addition to meeting one or more of the significance criteria, a property must have integrity. Integrity is defined as “the ability of a property to convey its significance” (U.S. Department of the Interior, 2002). The National Register recognizes seven aspects of a property that, in various combinations, define integrity, as follows (National Park Service, 1900, rev. 1991):

1. **Location:** The place where the historic property was constructed or the place where the historic event occurred. Except in rare cases, the relationship between a property and its historic associations is destroyed if the property is moved.

2. **Design:** The combination of elements that create the form, plan, space, structure, and style of a property. Design includes such elements as organization of space, proportion, scale, technology, ornamentation, and materials.
3. **Setting:** The physical environment of a historic property. Setting involves *how*, not just *where*, a property is situated and its relationship to surrounding features and open space. Setting often reflects the basic physical conditions under which a property was built and the functions it was intended to serve. The physical features that constitute the setting of a historic property can be natural, including such elements as topographic features, and vegetation, or manmade, such as relationships between buildings and other features or open space.
4. **Materials:** The physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property. A property must retain the key *exterior* materials dating from the period of its historic significance. If the property has been rehabilitated, the historic materials and significant features must have been preserved. The property must also be an actual historic resource, not a recreation; a recent structure fabricated to look historic is not eligible. Likewise, a property whose historic features and materials have been lost and then reconstructed is usually not eligible.
5. **Workmanship:** The physical evidence of the crafts of a particular culture or people during any given period in history or prehistory. It is the evidence of artisans' labor and skill in constructing or altering a building, structure, object, or site. Workmanship is important because it can furnish evidence of the technology of a craft, illustrate the aesthetic principles of a historic or prehistoric period, and reveal individual, local, regional, or national applications of both technological practices and aesthetic principles.
6. **Feeling:** A property's expression of the aesthetic or historic sense of a particular period. It results from the presence of physical features that, taken together, convey the property's historic character. For example, a rural historic district retaining original design, materials, workmanship, and setting will relate the feeling of agricultural life in the 19th century.
7. **Association:** The direct link between an important historic event or person and a historic property. A property retains association if it is the place where the event or activity occurred, and it is sufficiently intact to convey that relationship to an observer. Like feeling, association requires the presence of physical features that convey a property's historic character.

To retain historic integrity a property must possess several, and usually most, of these seven aspects. Thus, the retention of the specific aspects of integrity is paramount for a property to convey its significance. Determining which aspects of integrity are most important to a particular property requires knowing why, where, and when the property is significant.

Character-Defining Features

Character refers to those visual aspects and physical features that contribute to the appearance of every historic building. Character-defining features include the overall shape of the building, materials, craftsmanship, decorative details, interior spaces and features, as well as various aspects of its site and environment.

The Secretary of the Interior’s Preservation Brief #17, “Standards for Historic Preservation Projects,” (National Park Service, 1988) sets forth two goals: the preservation of historic materials and the preservation of a building’s distinguishing character. The term “character” refers to all those visual aspects and physical features that comprise the appearance of an historic building. Character-defining elements or features – those that are essential to communicating the building’s overall character – include the overall shape of the building, its materials, craftsmanship, decorative details, interior spaces and features, and aspects of its site and environment. The three-step approach recommended in the Preservation Brief to identify the visual character is summarized below:

- Identify the overall visual aspects including:
 - Shape
 - Openings
 - Roof and related features
 - Projections
 - Trim
 - Setting
- Visual character at close range including:
 - Materials
 - Craft Details
- Identify the visual character of interior spaces, features, and finishes
 - Individually Important Spaces
 - Related Spaces
 - Interior Features
 - Surface Materials and Finishes
 - Interior Exposed Structure
 - Fragility of a Building or Structures Visual Character

Criteria Considerations

Certain properties are not normally considered eligible for the National Register, including religious properties, moved properties, birthplaces or graves, cemeteries, reconstructed properties, commemorative properties, and properties that have achieved significance within the past 50 years. However, such properties can qualify for eligibility if they are integral parts of districts that meet one or more of the significance criteria A-D listed above, possess integrity, and fall within one of the following categories of Criteria Consideration (U.S. Department of the Interior, 2002):

- A. A religious property deriving primary significance from architectural or artistic distinction or historical importance; or

- B. A building or structure removed from its original location but which is primarily significant for architectural value, or which is the surviving structure most importantly associated with a historic person or event; or
- C. A birthplace or grave of a historical figure of outstanding importance if there is no appropriate site or building associated with his or her productive life; or
- D. A cemetery that derives its primary importance from graves of persons of transcendent importance, from age, from distinctive design features, or from association with historic events; or
- E. A reconstructed building when accurately executed in a suitable environment and presented in a dignified manner as part of a restoration master plan, and when no other building or structure with the same association has survived; or
- F. A property primarily commemorative in intent if design, age, tradition, or symbolic value has invested it with its own exceptional significance; or
- G. A property achieving significance within the past 50 years if it is of exceptional importance.

State

California Environmental Quality Act (CEQA)

CEQA is the principal statute governing environmental review of projects occurring in the state and is codified at Public Resources Code (PRC) Section 21000 et seq. CEQA requires lead agencies to determine if a proposed project would have a significant effect on the environment, including significant effects on historical or unique archaeological resources. Under CEQA (Section 21084.1), a project that may cause a substantial adverse change in the significance of an historical resource is a project that may have a significant effect on the environment.

The CEQA Guidelines (Title 14 California Code of Regulations [CCR] Section 15064.5) recognize that historical resources include: (1) a resource listed in, or determined to be eligible by the State Historical Resources Commission, for listing in the California Register of Historical Resources (California Register); (2) a resource included in a local register of historical resources, as defined in PRC Section 5020.1(k) or identified as significant in a historical resource survey meeting the requirements of PRC Section 5024.1(g); and (3) any object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be historically significant or significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California by the lead agency, provided the lead agency's determination is supported by substantial evidence in light of the whole record. The fact that a resource does not meet the three criteria outlined above does not preclude the lead agency from determining that the resource may be an historical resource as defined in PRC Sections 5020.1(j) or 5024.1.

If a lead agency determines that an archaeological site is a historical resource, the provisions of Section 21084.1 of CEQA and Section 15064.5 of the CEQA Guidelines apply. If an archaeological site does not meet the criteria for a historical resource contained in the CEQA Guidelines, then the site may be treated in accordance with the provisions of Section 21083, which is as a unique archaeological resource. As defined in Section 21083.2 of CEQA a "unique" archaeological resource is an archaeological artifact, object, or site, about which it can be clearly

demonstrated that without merely adding to the current body of knowledge, there is a high probability that it meets any of the following criteria:

- Contains information needed to answer important scientific research questions and there is a demonstrable public interest in that information;
- Has a special and particular quality such as being the oldest of its type or the best available example of its type; or,
- Is directly associated with a scientifically recognized important prehistoric or historic event or person.

If an archaeological site meets the criteria for a unique archaeological resource as defined in Section 21083.2, then the site is to be treated in accordance with the provisions of Section 21083.2, which state that if the lead agency determines that a project would have a significant effect on unique archaeological resources, the lead agency may require reasonable efforts be made to permit any or all of these resources to be preserved in place (Section 21083.1(a)). If preservation in place is not feasible, recommendations shall be required. The CEQA Guidelines note that if an archaeological resource is neither a unique archaeological nor a historical resource, the effects of the project on those resources shall not be considered a significant effect on the environment (CEQA Guidelines Section 15064.5(c)(4)).

A significant effect under CEQA would occur if a project results in a substantial adverse change in the significance of a historical resource as defined in CEQA Guidelines Section 15064.5(a). Substantial adverse change is defined as “physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of a historical resource would be materially impaired” (CEQA Guidelines Section 15064.5(b)(1)). According to CEQA Guidelines Section 15064.5(b)(2), the significance of a historical resource is materially impaired when a project demolishes or materially alters in an adverse manner those physical characteristics that:

- A. Convey its historical significance and that justify its inclusion in, or eligibility for, inclusion in the California Register; or
- B. Account for its inclusion in a local register of historical resources pursuant to section 5020.1(k) of the Public Resources Code or its identification in a historical resources survey meeting the requirements of section 5024.1(g) of the Public Resources Code, unless the public agency reviewing the effects of the project establishes by a preponderance of evidence that the resource is not historically or culturally significant; or
- C. Convey its historical significance and that justify its eligibility for inclusion in the California Register as determined by a Lead Agency for purposes of CEQA.

In general, a project that complies with the Secretary of the Interior’s Standards for the Treatment of Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring, and Reconstructing Historic Buildings or the Secretary of the Interior’s Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings (Grimmer, 2017) shall be considered as mitigated to a level of less than significant impact level on the historical resource (CEQA Guidelines Section 15064.5(b)(3)). The Standards for Rehabilitation are applicable to most renovation and adaptive reuse projects involving continuation of existing use or changes in use.

Standards 1 through 7 govern the use, repair and preservation of historic properties. Standard 8 considers the treatment of significant archaeological resources. Standard 9 governs new additions, exterior alterations, or related new construction, and requires that the new work be differentiated from the old, and that it shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment. Standard 10 governs new additions and adjacent or related new construction and requires that new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired. The Standards were developed as a means to evaluate and approve work for federal grants for historic buildings and then for the federal rehabilitation tax credit (see 36 Code of Federal Regulations (“CFR”) Section 67.7). Under CEQA, a proposed development must be evaluated to determine how it may impact the potential eligibility of a structure(s) or a site for designation as a historical resource. Similarly, the Los Angeles Cultural Heritage Ordinance, outlined below, provides that compliance with the Standards is part of the process for review and approval by the Cultural Heritage Commission of proposed alterations to City Monuments (see Los Angeles Administrative Code Section 22.171.14.a.1). Therefore, the Standards are used for regulatory approvals for designated resources but not for resource evaluations. Similarly, CEQA recognizes the value of the Standards by using them to demonstrate that a project may be approved without an EIR. In effect, CEQA has a “safe harbor” by providing either a categorical exemption or a negative declaration for a project which meets the Standards (see State CEQA Guidelines Section 15331 and 15064.5(b)(3)). Thus, a Project that generally follows the intent of the Standards and reduces impacts to the extent feasible would not have a significant unavoidable impact on historical resources, unless the Project would demolish, destroy, relocate, or alter a historical resource such that its eligibility for listing on a register of historical resources would be lost (i.e., no longer eligible for listing as a historical resource).

California Register of Historical Resources

The California Register is “an authoritative listing and guide to be used by State and local agencies, private groups, and citizens in identifying the existing historical resources of the State and to indicate which resources deserve to be protected, to the extent prudent and feasible, from substantial adverse change” (PRC Section 5024.1[a]). The criteria for eligibility for the California Register are based upon National Register criteria (PRC Section 5024.1[b]). Certain resources are determined by the statute to be automatically included in the California Register, including California properties formally determined eligible for, or listed in, the National Register.

To be eligible for the California Register, a prehistoric or historic-period property must be significant at the local, state, and/or federal level under one or more of the following four criteria:

1. Associated with events that have made a significant contribution to the broad patterns of California’s history and cultural heritage;
2. Associated with the lives of persons important in our past;
3. Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values; or
4. Has yielded, or may be likely to yield, information important in prehistory or history.

A resource eligible for the California Register must meet one of the criteria of significance described above, and retain enough of its historic character or appearance (integrity) to be recognizable as a historical resource and to convey the reason for its significance. It is possible that a historic resource might not retain sufficient integrity to meet the criteria for listing in the National Register, but might still be eligible for listing in the California Register.

Additionally, the California Register consists of resources that are listed automatically and those that must be nominated through an application and public hearing process. The California Register automatically includes the following:

- California properties listed on the National Register and those formally determined eligible for the National Register;
- California Registered Historical Landmarks from No. 770 onward; and,
- Those California Points of Historical Interest that have been evaluated by the OHP and have been recommended to the State Historical Commission for inclusion on the California Register.

Other resources that may be nominated to the California Register include:

- Historical resources that, on the basis of a survey meeting the OHP's requirements, have been assigned California Historical Resource Status Code categories 1 (Properties listed in the National Register or the California Register), 2 (Properties determined eligible for listing in the National Register or the California Register), or 3 (Appears eligible for National Register or California Register through Survey Evaluation) (California Office of Historic Preservation, 2004);
- Individual historical resources;
- Historical resources contributing to historic districts; and,
- Historical resources designated or listed as local landmarks, or designated under any local ordinance, such as an historic preservation overlay zone.

Whereas the National Register Criteria Consideration G states that a property of less than 50 years in age can be considered to have achieved significance and be eligible for listing only if it can be proven to be of exceptional importance, California Register criteria state only that for a resource to achieve significance within the past 50 years, sufficient time must have passed to understand its historical importance (i.e., sufficient time obtain a scholarly perspective on the events or individuals associated with the property) (California Code of Regulations Section 4852). Guidance provided by the California Office of Historic Preservation (OHP) states that formal evaluations of resources more than 45 years of age may be submitted to OHP for filing, in recognition of the fact that there is often a five-year lag between initial resource identification and the finalization of planning approvals, by which time a resource would be 50 years of age.

California Health and Safety Code Section 7050.5

California Health and Safety Code Section 7050.5 requires that in the event human remains are discovered, the County Coroner be contacted to determine the nature of the remains. In the event the remains are determined to be Native American in origin, the Coroner is required to contact the NAHC within 24 hours to relinquish jurisdiction.

Local

City of Los Angeles General Plan

The City's General Plan (adopted 2001) states as its objective, to "protect the City's archaeological and paleontological resources for historical, cultural, research, and/or educational purposes" by continuing "to identify and protect significant archaeological and paleontological resources known to exist or that are identified during land development, demolition, or property modification activities."

In addition, the City will:

...continue to protect historic and cultural sites and/or resources potentially affected by proposed land development, demolition, or property modification activities...The City's environmental guidelines require the applicant to secure services of a bona fide archaeologist to monitor excavations or other subsurface activities associated with a development project in which all or a portion is deemed to be of archaeological significance. Discovery of archaeological materials may temporarily halt the project until the site has been assessed, potential impacts evaluated and, if deemed appropriate, the resources protected, documented and/or removed (City of Los Angeles, 2001).

In addition to the National Register and the California Register, three additional types of historic designations may apply at a local level:

1. Historic-Cultural Monument
2. Designation by the Community Redevelopment Agency as being of cultural or historical significance within a designated redevelopment area
3. Classification by the City Council as an Historic Preservation Overlay Zone

The City's Cultural Heritage Ordinance states that an HCM designation is reserved for those resources that have a special aesthetic, architectural, or engineering interest or value of a historic nature and meet one of the following criteria (City of Los Angeles Department of City Planning, 2009). A historical or cultural monument is any site, building, or structure of particular historical or cultural significance to the City, such as historic structures or sites:

- In which the broad cultural, political, economic, or social history of the nation, state, or community is reflected or exemplified; or
- Which are identified with historic personages or with important events in the main currents of national, state, or local history; or
- Which embody the distinguishing characteristics of an architectural-type specimen, inherently valuable for a study of a period, style, or method of construction; or
- Which are a notable work of a master builder, designer, or architect whose individual genius influenced his or her age.

In addition, the Los Angeles Municipal Code (LAMC) Section 91.106.4.5 states that the Los Angeles Department of Building and Safety (LADBS) “shall not alter or remove a building or structure of historical, archaeological or architectural consequence if such building or structure has been officially designated” by a federal, state, or local authority. Further, the LADBS shall not issue a building or structure permit to demolish for which the original building permit was issued more than 45 years ago, nor will they issue a permit prior to the date of submittal of the application for demolition pre-inspection or where information submitted with the application indicates that the building or structure is more than 45 years old based on the date the application is submitted. At least 30 days prior to issuance of the demolition of building or structure permit, written notices of the demolition pre-inspection application to abutting property owners and the Council District office are sent. Additionally, any interested individual may apply for a proposed designation of a Monument. Upon the determination by the Planning Director that the application is complete, or upon initiation by City Council, the Cultural Heritage Commission or the Director, no permit for the demolition, substantial alteration or removal shall be issued; and the site, building or structure regardless of whether a permit exists, shall not be demolished, pending final determination by the Commission and City Council whether the proposed site, building or object or structure shall be designated a Monument (Section 22.171.12). Also, if the property has been previously identified in a survey or has been nominated for designation and it is determined by the City that a project is subject to CEQA review, the City may require preparation of a historical resources assessment report and CEQA impacts analysis, pursuant to CEQA 15064.5, prior to issuance of a demolition permit. Once the process pursuant to Section 91.106.4.5.1 is completed, the Building Department will then be able to issue the applicable permits.

City of Los Angeles Cultural Heritage Ordinance

The City enacted a Cultural Heritage Ordinance in April 1962, which defines Historic-Cultural Monuments. According to the Cultural Heritage Ordinance, Historic-Cultural Monuments are sites, buildings, or structures of particular historic or cultural significance to the City in which the broad cultural, political, or social history of the nation, state, or City is reflected or exemplified, including sites and buildings associated with important personages, or which embody certain distinguishing architectural characteristics and are associated with a notable architect. These HCMs are regulated by the City’s Cultural Heritage Commission and the City Council.

Los Angeles Cultural Heritage Ordinance Eligibility Criteria

The Los Angeles City Council adopted the Cultural Heritage Ordinance in 1967 and amended it in 2007 (Los Angeles Administrative Code, Chapter 9, Division 22, Article 1, Section 22.171.7). The Cultural Heritage Ordinance establishes criteria for designating a local historical resource as an HCM. An HCM is any site (including significant trees or other plant life located on the site), building, or structure of particular historic or cultural significance to the City, including historic structures or sites:

1. In which the broad cultural, economic or social history of the nation, State or community is reflected or exemplified; or
2. Which is identified with historic personages or with important events in the main currents of national, State or local history; or

3. Which embodies the distinguishing characteristics of an architectural type specimen, inherently valuable for a study of a period, style or method of construction; or
4. Which is a notable work of a master builder, designer, or architect whose individual genius influenced his or her age.

SurveyLA Eligibility Standards

SurveyLA was a citywide survey that identified and documented significant historic resources representing important themes in the City's history. The survey and resource evaluations were completed by consultant teams under contract to the City and the supervision of the Office of Historic Resources (OHR). The program was managed by the OHR, which maintains a website for SurveyLA. The field surveys covered the period from approximately 1850 to 1980 and included individual resources such as buildings, structures, objects, natural features and cultural landscapes as well as areas and districts (archaeological resources will be included in a future survey phase). Significant resources reflected important themes in the City's growth and development in various areas including architecture, city planning, social history, ethnic heritage, politics, industry, transportation, commerce, entertainment, and others. Field surveys, conducted from 2010-2017, were completed in three phases by Community Plan Area. All tools and methods developed for SurveyLA met state and federal professional standards for survey work.

The City's citywide Historic Context Statement (HCS) was designed for use by SurveyLA field surveyors and by all agencies, organizations, and professionals completing historic resources surveys in the City. The context statement was organized using the Multiple Property Documentation format developed by the National Park Service for use in nominating properties related by theme to the National Register. This format provided a consistent framework for evaluating historic resources. It was adapted for local use to evaluate the eligibility of properties for city, state, and federal designation programs and to facilitate environmental review processes. The HCS used Eligibility Standards to identify the character-defining, associative features, and aspects of integrity a property should retain to be a significant example of a type within a defined theme. Eligibility Standards also indicated the general geographic location, area of significance, applicable criteria, and period of significance associated with that type. These Eligibility Standards are guidelines based on knowledge of known significant examples of property types; properties do not need to meet all of them in order to be eligible. Moreover, there are many variables to consider in assessing integrity depending on why a resource is significant.

Environmental Setting

This section summarizes the environmental setting including the natural, prehistoric, ethnographic, and historic contexts for the Project Site. The natural setting briefly summarizes the geologic, hydrologic, and ecological history of the Project vicinity and Project Site. The prehistoric setting summarizes the history of human settlement in the Project area and region through 1769. The ethnographic setting summarizes the history of Native American settlement in the area and the region since 1769.

The historic setting is divided into several subsections or themes that reflect the Project Site's location; the history and nature of development on the Project Site; and the history and nature of adjacent built resources that the Project could affect. Accordingly, the historic setting addresses 1) 18th century Spanish settlement and establishment of the pueblo and associated infrastructure that ultimately made development of present-day Los Angeles possible, 2) the agricultural roots of Los Angeles, 3) development and evolution of the ice and cold storage industries, 4) the history of the Los Angeles Ice and Cold Storage Company, which first developed and occupied the Project Site in the late 19th century, and 5) the subsequent occupancy and modification of Project Site buildings. The prehistoric and historic settings for the Project provide a context for the land use history and archaeological sensitivity assessment completed as part of this study.

Finally, the historic setting presents the history of the 7th Street Bridge (P-19-175070), located adjacent to the Project Site to the south and to which the Project proposes to connect via ramps to provide Project Site access; and of the railroad industry in Los Angeles, as the BNSF Railroad (P-19-186804) is located adjacent the Project Site to the east and is proposed as the potential site of a proposed Deck extending over the Railway Properties.

Natural Setting

The Project is located in a relatively flat area of the western Los Angeles Basin. The Los Angeles Basin is formed by the Santa Monica Mountains to the northwest, the San Gabriel Mountains to the north, and the San Bernardino and San Jacinto Mountains to the east. The Los Angeles Basin was formed by alluvial and fluvial deposits derived from these surrounding mountains and the Los Angeles River. Prior to urban development and channelization of the Los Angeles River, the Project Site, which is located 200 feet west of the Los Angeles River Channel, was likely covered with marshes, thickets, dense woodland, and grassland. The floodplain forest of the Los Angeles Basin formed one of the most biologically rich habitats in southern California. Willow, cottonwood and sycamore, and dense underbrush of alder, hackberry, and shrubs once lined the Los Angeles River as it passed near present-day Downtown Los Angeles. Although historically most of the Los Angeles River was dry for at least part of the year, shallow bedrock in the Elysian Park area north of Downtown forced much of the river's underground water to the surface. This allowed for a steady year-round flow of water through the area that later became known as Downtown Los Angeles. However, as previously discussed, the Project Site is within a completely developed area. A geotechnical study prepared in support of the Project indicates that the Project Site is likely underlain by historic disturbance/fill of up to five to six feet in depth, overlying alluvium (Twining, 2018).

Prehistoric Setting

The earliest evidence of occupation in the Los Angeles area dates to at least 9,000 years before present (B.P.) and is associated with a period known as the Millingstone Cultural Horizon (Wallace 1955; Warren 1968). Departing from the subsistence strategies of their nomadic big-game hunting predecessors, Millingstone populations established more permanent settlements. These settlements were located primarily on the coast and in the vicinity of estuaries, lagoons, lakes, streams, and marshes where a variety of resources including seeds, fish, shellfish, small

mammals, and birds were exploited. Early Millingstone occupations are typically identified by the presence of handstones (manos) and milling stones (metates), while those Millingstone occupations dating later than 5,000 years B.P. contain a mortar and pestle complex as well, signifying the exploitation of acorns in the region.

Although many aspects of Millingstone culture persisted, by 3,500 years B.P. a number of socioeconomic changes occurred (Erlandson 1994; Wallace 1955; Warren 1968). These changes are associated with the period known as the Intermediate Horizon (Wallace 1955). Increased populations in the region necessitated the intensification of existing terrestrial and marine resources (Erlandson 1994). This was accomplished in part through the use of the circular shell fishhook on the coast, and more abundant and diverse hunting equipment. Evidence for shifts in settlement patterns has been noted at a variety of locations at this time and is seen by many researchers as reflecting increasingly territorial and sedentary populations. The Intermediate Horizon marks a period in which specialization in labor emerged, trading networks became an increasingly important means by which both utilitarian and non-utilitarian materials were acquired, and travel routes were extended. Archaeological evidence suggests that the margins of numerous rivers, marshes, and swamps within the Los Angeles River Drainage served as ideal locations for prehistoric settlement during this period. These well-watered areas contained a rich collection of resources and are likely to have been among the more heavily trafficked travel routes.

The Late Prehistoric period, spanning from approximately 1,500 years B.P. to the mission era, is the period associated with the florescence of the contemporary Native American group known as the *Gabrielino* (Wallace 1955). Coming ashore near Malibu Lagoon or Mugu Lagoon in October of 1542, *Juan Rodriguez Cabrillo* was the first European to make contact with the *Gabrielino* Indians. Occupying the southern Channel Islands and adjacent mainland areas of Los Angeles and Orange Counties, the *Gabrielino* are reported to have been second only to their Chumash neighbors in terms of population size, regional influence, and degree of sedentism (Bean and Smith 1978). The *Gabrielino* are estimated to have numbered around 5,000 in the pre-contact period (Kroeber 1925) and maps produced by early explorers indicate that at least 26 *Gabrielino* villages were within proximity to known Los Angeles River courses, while an additional 18 villages were reasonably close to the river (Gumprecht 2001). Subsistence consisted of hunting, fishing, and gathering. Small terrestrial game was hunted with deadfalls, rabbit drives, and by burning undergrowth, while larger game such as deer were hunted using bows and arrows. Fish were taken by hook and line, nets, traps, spears, and poison (Bean and Smith 1978; Reid 1939 [1852]). The primary plant resources were the acorn, gathered in the fall and processed with mortars and pestles, and various seeds that were harvested in late spring and summer and ground with manos and metates. The seeds included chia and other sages, various grasses, and islay or holly leafed-cherry (Reid 1939 [1852]).

Ethnographic Setting

The Project Site is located in a region traditionally occupied by the Gabrielino people. The term “Gabrielino” is a general term that refers to those Native Americans who were administered by the Spanish at the Mission San Gabriel Arcángel. Prior to European colonization, the Gabrielino occupied a diverse area that included the watersheds of the Los Angeles, San Gabriel, and Santa Ana rivers; the Los Angeles Basin; and San Clemente, San Nicolas, and Santa Catalina Islands (Kroeber, 1925). Their neighbors included the Chumash to the north, the Juañeno to the south, and the Serrano and Cahuilla to the east. The Gabrielino are reported to have been second only to the Chumash in terms of population size and regional influence (Bean and Smith, 1978). The Gabrielino language is part of the Takic branch of the Uto-Aztecan language family.

Community populations generally ranged from 50 to 100 inhabitants, although larger settlements may have existed. The Gabrielino are estimated to have had a population numbering around 5,000 in the pre-contact period, which ended in 1769 A.D. (Kroeber, 1925). Villages are reported to have been the most abundant in the San Fernando Valley, the Glendale Narrows area north of Downtown, and around the Los Angeles River’s coastal outlets (Gumprecht, 2001). The village of *Yaanga* was thought to be located southwest of what is presently Los Angeles Union Station, approximately 1.3 miles northwest of the Project Site (Morris et al., 2016). Of the approximately 100 known Gabrielino villages, *Yaanga* was one of the largest villages and leaders from other Gabrielino villages would regularly converge at *Yaanga* to hold councils (Rasmussen, 1997). The Gabrielino leaders would meet beneath the branches of a large sycamore tree known as the council tree, or *El Aliso*, which served as a regional landmark and meeting place. The 400-year-old tree died and was cut down in 1892 during the Downtown Los Angeles industrial expansion (Rasmussen, 1997). Recent research indicates that *El Aliso* was located south of what is presently the Metropolitan Transit Authority’s headquarters within the median of the Hollywood Freeway, located approximately 1.2 miles north of the Project Site (Rasmussen, 1997).

Based on baptismal records, *Yaanga* appears to have been occupied until at least 1813. But by the early 1820s, *Yaanga*’s Gabrielino residents were displaced from the original village site to what is presently the block north of Los Angeles Street and 1st Street, approximately 1.3 miles northwest of the Project Site (Morris et al., 2016). By 1836, the displaced Gabrielino community was known as *Rancho de los Pablinos*, and Los Angeles residents began complaining about the Gabrielino bathing in the *zanjas* (irrigation ditches) (Morris et al., 2016). Because of the complaints, the Gabrielino were once again displaced to the east near what is presently the intersection of Alameda Street and Commercial Street, approximately 1.22 miles northwest of the Project Site. Between 1845 and 1847, they were then moved to the east side of the river to a settlement that was known as *Pueblito*, and by 1847, the Gabrielino from *Yaanga* were displaced once again and left without a space in which to form a new community. As a result, the Gabrielino dispersed throughout Los Angeles.

Historic Setting

The *Gabrielino* were virtually ignored between the time of Cabrillo's visit (as described above, *Juan Rodriguez Cabrillo* was the first European to make contact with the *Gabrielino* Indians) and the Spanish Period, which began in 1769 when Gaspar de Portola and a small Spanish contingent began their exploratory journey along the California coast from San Diego to Monterey. Passing through the Los Angeles area, they reached the San Gabriel Valley on August 2, 1769, and traveled west through a pass between two hills where they encountered the Los Angeles River and camped on its east bank near the present-day North Broadway Bridge and the entrance to Elysian Park. This location has been designated California Historic Landmark Number 655 and is known as the Portola Trail Campsite. Father Crespi (a member of Portola's party) indicated in his diaries that on that day they "entered a spacious valley, well grown with cottonwoods and alders, among which ran a beautiful river. This plain where the river runs is very extensive and...is the most suitable site for a large settlement" (The River Project, 2001). The river was named *El Rio y Valle de Nuestra Señora la Reina de Los Angeles de la Porciuncula*.

Missions were established in the years that followed the Portola expedition, the fourth being the Mission San Gabriel Archangel founded in 1771 approximately 7.5 miles east of the Project Site. By the early 1800s, the majority of the surviving *Gabrielino* population had entered the mission system. The *Gabrielino* inhabiting Los Angeles County were under the jurisdiction of either Mission San Gabriel or Mission San Fernando. Mission life offered the Indians security in a time when their traditional trade and political alliances were failing, and epidemics and subsistence instabilities were increasing (Jackson 1999).

On September 4, 1781, 12 years after Crespi's initial visit, the *Pueblo de la Reina de Los Angeles* was established not far from the site where Portola and his men camped. Watered by the river's ample flow and the area's rich soils, the original pueblo occupied 28 square miles and consisted of a central square surrounded by 12 houses and a series of 36 agricultural fields occupying 250 acres, plotted to the east between the town and the river (Gumprecht 2001).

An irrigation system that would carry water from the river to the fields and the pueblo was the communities' first priority and was constructed almost immediately, as further discussed below (See *Zanja* Irrigation System).

By 1786, the flourishing pueblo attained self-sufficiency and funding by the Spanish government ceased (Gumprecht 2001). Fed by a steady supply of water and an expanding irrigation system, agriculture and ranching grew, and by the early 1800s the pueblo produced 47 cultigens. Among the most popular were grapes used for the production of wine (Gumprecht 2001). Vineyards blanketed the landscape between present-day San Pedro Street and the Los Angeles River. By 1830 an estimated 100,000 vines were being cultivated at 26 Los Angeles vineyards. Over 8,300 acres of land were being irrigated by the *zanjas* during the 1880s (Gumprecht 2001).

Alta California became a state of Mexico when Mexico won its independence from Spain in 1821. Independence and the removal of economic restrictions attracted settlers to Los Angeles, and it slowly grew in size and expanded to the south and west. The population nearly doubled during this period, increasing from 650 to 1,250 between 1822 and 1845 (Weber 1982:226). Until

1832, Los Angeles was essentially a military post, with all able-bodied males listed on the muster rolls and required to perform guard duty and field duty whenever circumstances required (Los Angeles County 1963). The Mexican Congress elevated Los Angeles from pueblo to city status in 1835, declaring it the new state capital (Robinson 1979:238–239).

The authority of the California missions gradually declined, culminating with their secularization in 1834. Although the Mexican government directed that each mission's lands, livestock, and equipment be divided among its converts, the majority of these holdings quickly fell into non-Indigenous hands. Mission buildings were abandoned and quickly fell into decay. If mission life was difficult for Native Americans, secularization was typically worse. After two generations of dependence on the missions, they were suddenly disenfranchised. After secularization, "nearly all of the Gabrielinos went north while those of San Diego, San Luis, and San Juan overran this county, filling the Angeles and surrounding ranchos with more servants than were required" (Reid 1977 [1851]:104). Upon his 1852 visit to Los Angeles, John Russell Bartlett wrote,

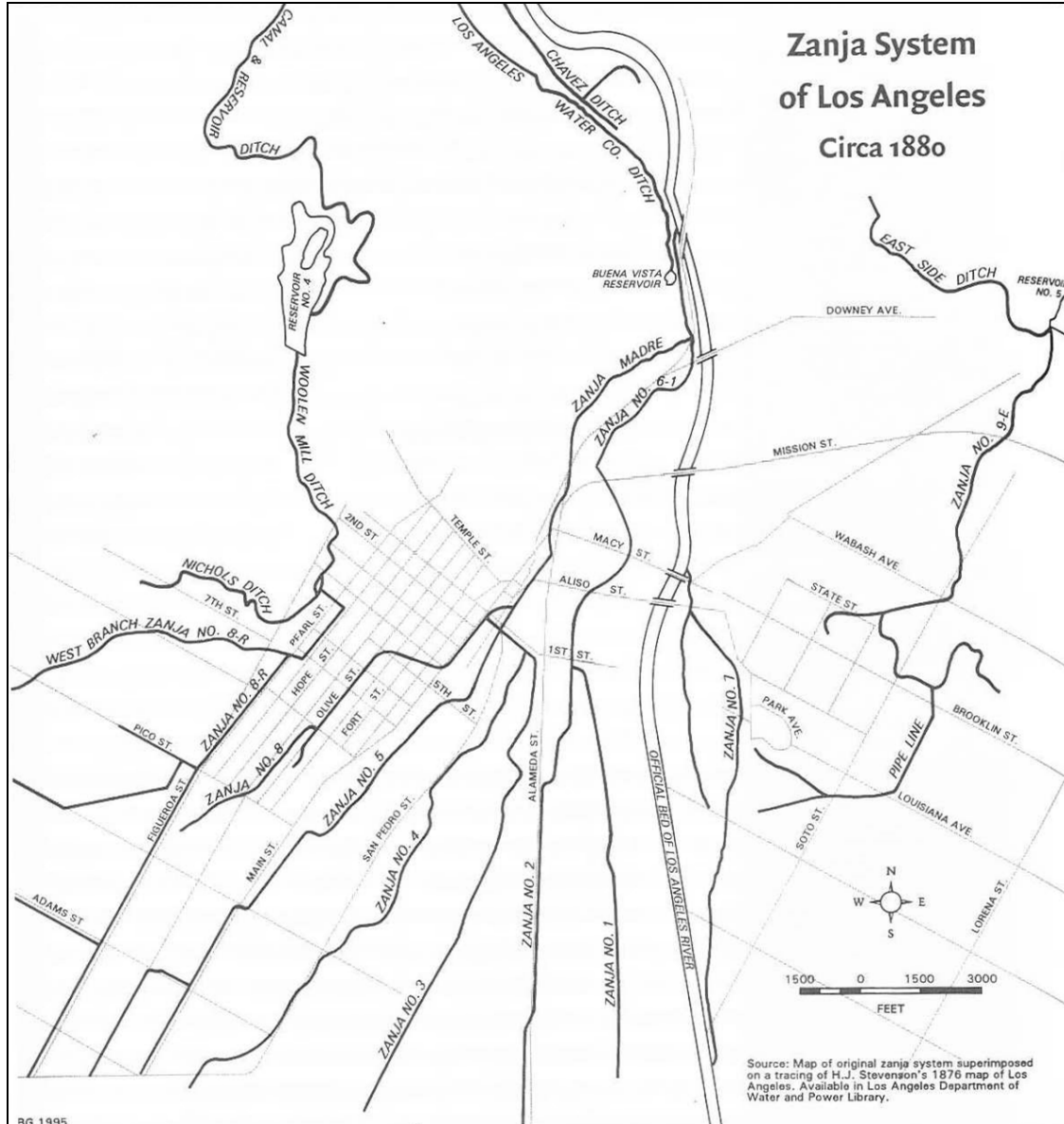
I saw more Indians about this place than in any part of California I had yet visited. They were chiefly mission Indians, i.e., those who had been connected with the missions and had derived their support from them until the suppression of those establishments. They are a miserable, squalid-looking set, squatting or lying about the corners of the streets with no occupation. They have no means of obtaining a living, as their lands are taken from them, and the missions for which they labored and which provided after a sort for many thousands of them, are abolished (as cited in Sugranes 1909:77).

The first party of U.S. immigrants arrived in Los Angeles in 1841, although surreptitious commerce had previously been conducted between Alta California and residents of the United States and its territories. Included in this first wave of immigrants were William Workman and John Rowland, who soon became influential landowners. As the possibility of a takeover of California by the United States loomed large, the Mexican government increased the number of land grants in an effort to keep the land in the hands of upper-class *Californios* like the Domínguez, Lugo, and Sepúlveda families (Wilkman and Wilkman 2006:14–17). Governor Pío Pico and his predecessors made more than 600 rancho grants between 1833 and 1846, putting most of the state's lands into private ownership for the first time (Gumprecht 2001). Having been established as a pueblo, property within Los Angeles could not be dispersed by the governor, and this task instead fell under the city council's jurisdiction (Robinson 1979).

For the pueblo of Los Angeles, the *zanjas*, or publicly owned irrigation ditches, sustained the area and enabled ranching and cultivation of the Los Angeles River's fertile floodplains. The *zanjas* consisted of gravity systems, which resulted in the irrigation of lands that lay at lower elevations from the water's source. The main ditch – the *Zanja Madre* (Mother Ditch) – was constructed in 1781 and carried water from the Los Angeles River south to the agricultural lands surrounding the pueblo. As the pueblo grew and more water was diverted from the river, the supply began to dwindle (Gumprecht, 2001). By the mid-19th century, City officials established a system of water use fees and rules to govern the *zanjas*. While the *zanjas* worked well for irrigation, the water was frequently unsuitable for domestic purposes. As Southern California grew, the Los Angeles River became an inadequate supply of water; water supply reservoirs came into use and the *zanja* system was gradually abandoned and, in some cases, dismantled (Gumprecht, 2001). By 1902, the Los

Angeles municipal government took back jurisdiction of its own water needs and purchased the existing water system, which consisted of seven reservoirs and 337 miles of pipe. **Figure 5** depicts the locations of the *zanjas* in 1880. The *zanja* has been observed as an above-ground decorative open trench, cement pipe, brick conduit, and wrought iron pipe. Past construction activities that have uncovered the system have recorded components of the resource just below the surface of sidewalks and pavement and up to 15 feet bgs in depth. Another map, produced in 1887 by V. J. Rowan and Theo. G. Koeberle (**Figures 6 and 7**) shows the alignment of *Zanja* No. 1 in relation to the Project Site. According to historic maps of Los Angeles, one branch of the *zanja* Conduit System (*Zanja* No. 1) is mapped as running north/south through downtown and located 0.16 miles west of the Project Site and would not be encountered by work at the Project Site.

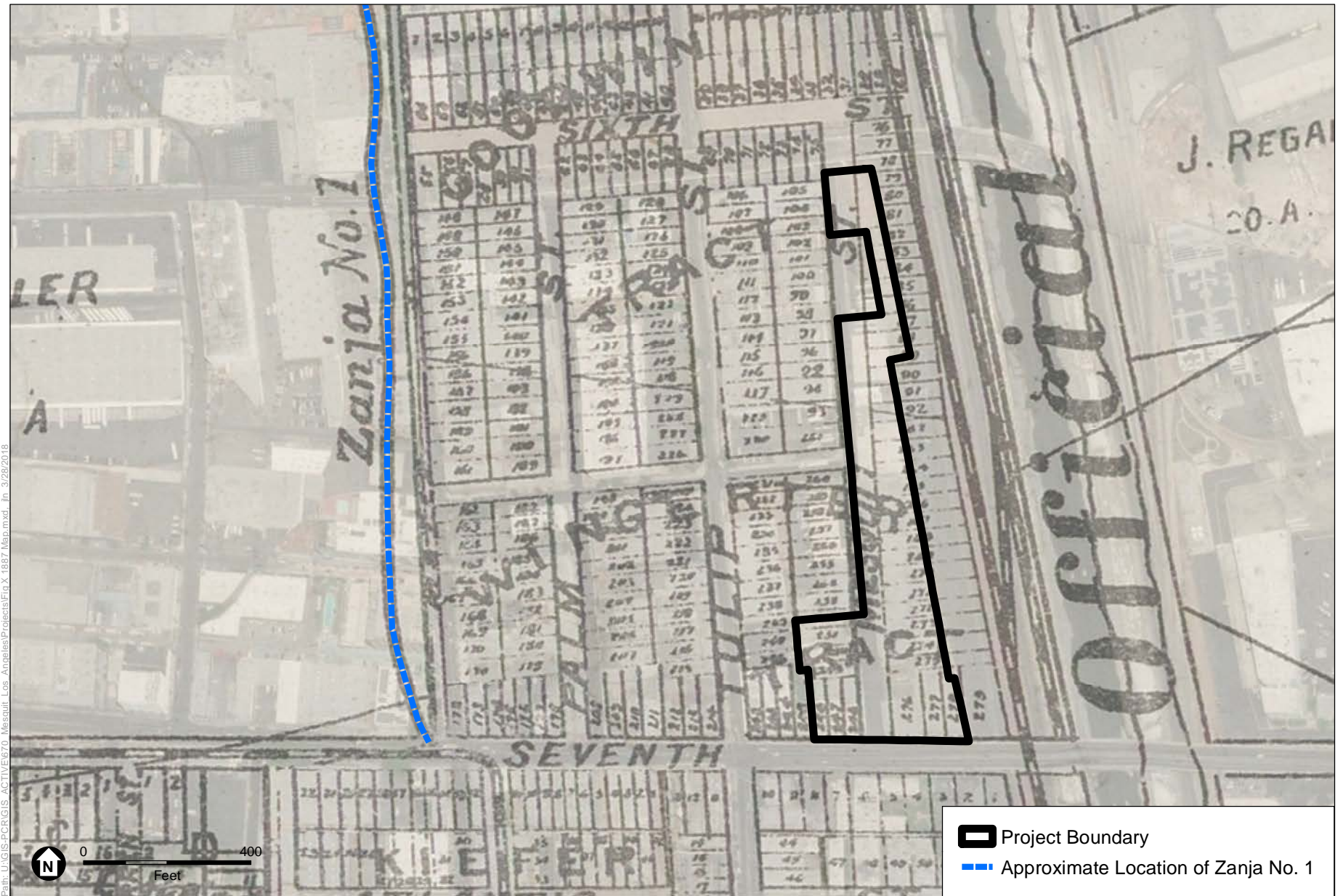
A constant struggle to bring water to the residents of the pueblo necessitated the construction of Echo Park Reservoir, the Silver Lake Reservoir, and the further expansion of the *Zanja* irrigation ditches in order to provide enough water to keep up with the continued expansion and growth of the city. When these measures proved insufficient, a more permanent solution to Los Angeles' water shortage was sought. Under the direction of City engineer William Mulholland, the Los Angeles Bureau of Water Works and Supply constructed the 238-mile-long Los Angeles Aqueduct. This 5-year project, completed in 1913, employed the labor of more than 5,000 men and brought millions of gallons of water into the San Fernando (now Van Norman) Reservoir (Gumprecht 2001). Now able to offer water and sewer service at a grand scale, many smaller cities were voluntarily incorporated by Los Angeles (Robinson 1979:244).



SOURCE: Gumprecht 2001

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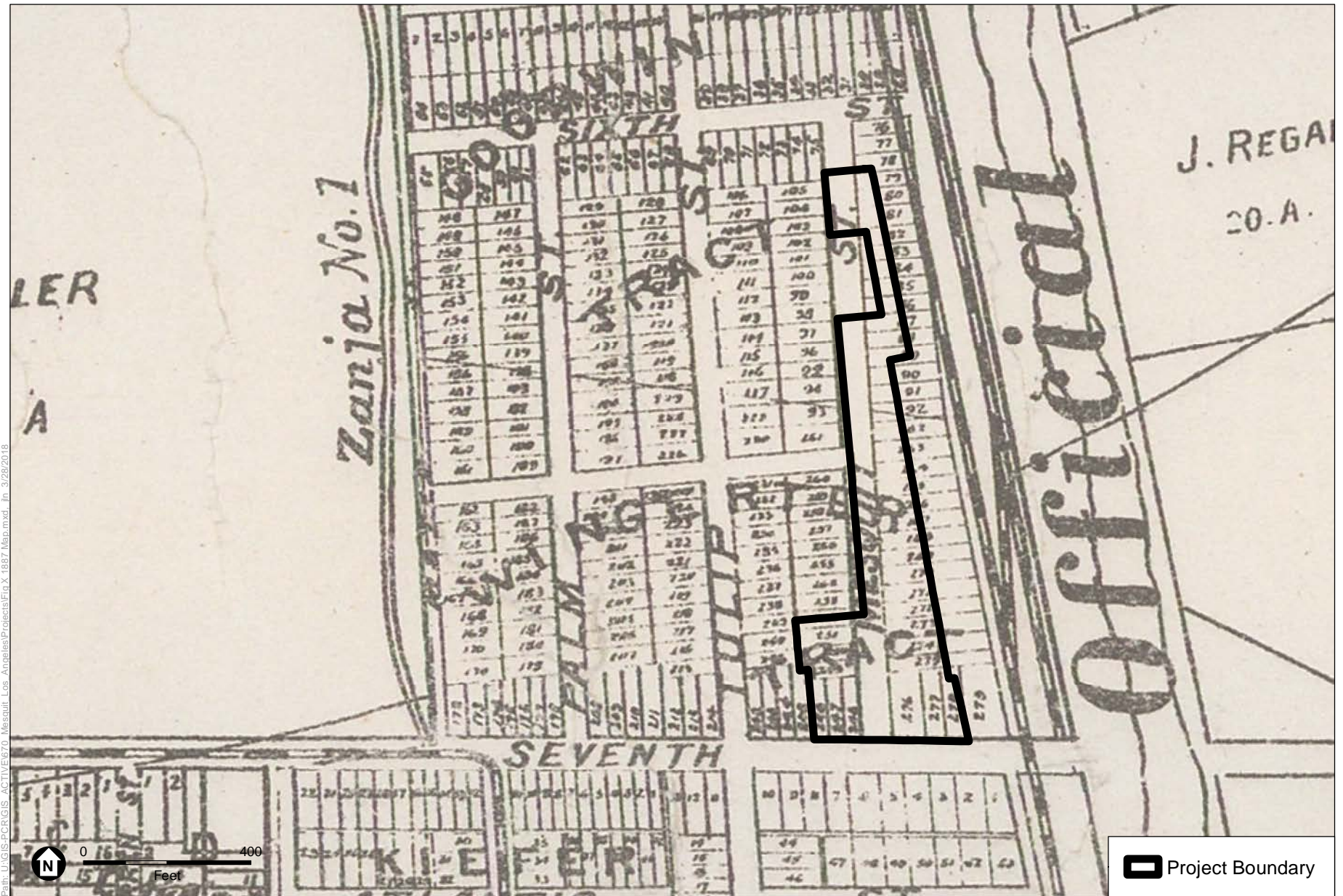
Figure 5
Los Angeles Zanja Conduit System in 1880



SOURCE: 1887 County Records Office

670 Mesquit

Figure 7
 Rowan and Koeberle Map and Zanja No. 1 in relation to Project Site on Aerial



SOURCE: 1887 County Records Office

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Figure 6
Rowan and Koeberle Map and Zanja No. 1 in relation to Project Site

The Population Boom of the 1880s

Los Angeles's exponential growth during the 1920s is well documented; however, it was a population boom 40 years earlier that saw Los Angeles become a major American city. While the state and city experienced steady expansion through the mid-19th century, the 1880s were a period of unprecedented population growth, leading up to "The Great Boom of 1886-87" (Lewis Publishing Company, 1889).

When Los Angeles was connected to The Southern Pacific's transcontinental railroad via San Francisco on September 5, 1876, the City began to experience significant population growth. The City would experience its greatest growth in the 1880s when two additional direct rail connections to the East Coast were constructed. The Southern Pacific completed its second transcontinental railway, the Sunset Route from Los Angeles to New Orleans, in 1883 (Orsi, 2005). In 1885, the Santa Fe Railroad completed a competing transcontinental railway to San Diego, with connecting service to Los Angeles (Mullaly and Petty, 2002). Resulting fare wars led to an unprecedented real estate boom, as well as affordable cross-country fares for immigrants. Despite a subsequent collapse of the real estate market, the population of Los Angeles increased 350 percent in the decade between 1880 and 1890 (Dinkelspiel, 2008).

The population boom of the 1880s drove the demand for real estate in Los Angeles. Farmland south and east of the City began to be replaced by residential and commercial development. Large tracts of agricultural land, now far more valuable for residential development, were subdivided and sold (Gumprecht, 2001).

Historic Context – Industrial Development, 1850-1980

SurveyLA is a comprehensive Citywide survey for identifying, recording, and evaluating historic properties and districts in Los Angeles. In order to evaluate identified resources, the City has developed historic contexts as a framework that describes important themes in the City's history, identifies representative property types, and establishes standard registration requirements. Pertinent themes and sub-themes developed in SurveyLA Citywide Historic Context Statement (HCS) are utilized in accordance with the City's survey methods to provide a framework to evaluate the eligibility of identified resources within the Project Site which include the Los Angeles Ice and Cold Storage Company (Los Angeles Cold Storage Company) Warehouse Complex constructed in sections between 1908 and 1985 located in Survey Areas #3, #4, and #5, and the West Coast Quilting Company Factory constructed in 1963 located in Survey Area #1. The contexts and themes that are applicable to evaluation of the Los Angeles Ice and Cold Storage Company buildings on the Project Site in Survey Areas #3, #4, and #5 include the Context, Industrial Development, 1850-1980 and the Theme, Agricultural Roots, 1850-1945 and its associated Sub-Theme, From Farm to Market, 1900-1960, and associated Property Type, Cold Storage Warehouse, 1900-1945. The contexts and themes that are applicable to the West Coast Quilting Company Factory Building in Survey Area #1 is the Context, Industrial Development, 1850-1980, Sub-Context Manufacturing for the Masses (1887-1980). These contexts and themes are summarized below.

Theme: Agricultural Roots, 1850-1945

Industry and commerce in Los Angeles have their roots in agriculture, and the earliest industries were related to processing agricultural produce. The California missions produced a variety of agricultural products for domestic consumption, including grain, livestock, citrus, and wine. During and after secularization, the Californios engaged in international trade of hides, beef, and tallow from cattle raised on their extensive ranchos. Los Angeles County was known as the “Queen of the Cow Counties” to Northern California during the Gold Rush because the county had the largest herds in the region (Glass 1922). Severe drought, devastating floods, and the breakup of the ranchos brought an end to the cattle industry in the 1860s. In the latter half of the 19th century, Americans began to settle in and around the Los Angeles pueblo (centered north of present-day Downtown Los Angeles), setting up vineyards along the Los Angeles River and dry-farming in outlying regions.

In large part because of this agricultural heritage, the earliest industries in this part of the City were related to processing agricultural produce. Flour mills were established in the 1870s and 80s to process local grain. Packing houses opened along rail alignments to prepare citrus and deciduous fruits for shipment, and during the late 19th century, several local wineries fermented Los Angeles grapes.

The railroads provided an efficient means to transport goods throughout the region and to outside markets. The rise of manufacturing in Los Angeles began slowly in the late 19th century, fueled by an emerging consumer market created by waves of immigrants. The earliest industries generally consisted of small-scale operations for food processing and the building industry. The rapid population growth during the 1880s required industrial support in the form of building materials, produce, and consumer goods (SurveyLA, 2018).

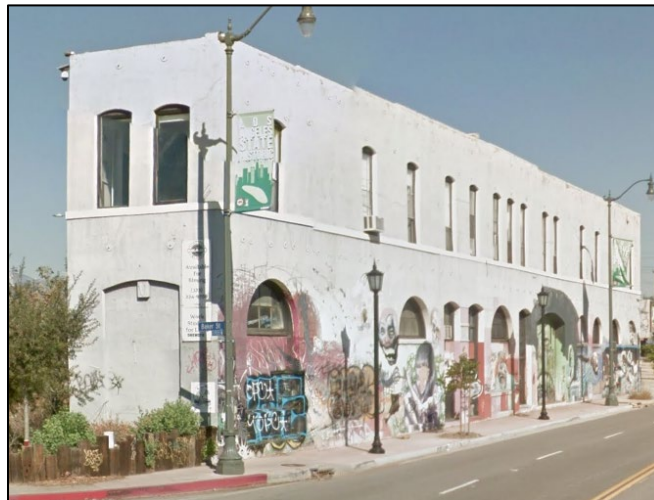
Subtheme: From Farm to Market, 1900-1960

From Farm to Market, 1900-1960, describes the historical context of this particular period in relation to the larger history of agricultural development in the area. The value of the cold storage facilities as storage places for perishable consumer goods would quickly be realized by the growing public. Cold storage facilities allowed local farmers to produce a surplus, turning their subsistence level operations into profitable businesses. The foundations of the thriving ice business and cold storage facilities of the early 20th century were laid over the course of the previous century, during which time the processes for harvesting (later manufacturing), transporting, storing, and distributing ice were developed (SurveyLA, 2018).

With the development of railroads came the possibility of shipping food long distances; ice was necessary for shipping, especially in Southern California where agriculture was abundant and demand was high. Around the turn of the 20th century, emerging refrigeration technologies prompted companies to build ice plants and cold storage warehouses. Despite the high construction cost, centralized cold storage and ice manufacturing plants were vital links in the food supply chain. Additionally, the production of ice was necessary to keep food fresh in those establishments, as well as in the iceboxes of private homes.

The Cold Storage Warehouse Property Type, 1900-1945, is an industrial property associated with the City's agricultural roots, during a distinct period of early twentieth-century development. Prevalent between 1900 and 1945, the Cold Storage Warehouse property type represented a link between the collection of agricultural goods from farms, fisheries, and ranches and their distribution to produce markets and food processors (SurveyLA, 2018). Because of the City's rich agricultural heritage as described above, the Cold Storage Warehouse Property Type was constructed in the industrial areas of Los Angeles adjacent to rail-lines, produce markets, and other manufacturing industries to facilitate the movement of perishable goods within the City and to other parts of the United States. In the early 20th century, many of these cold storage warehouses focused on the production of ice and provided cold storage on the side. Cold storage warehouses were examples of vernacular architecture, and the structures were purely functional, utilitarian designs devoid of excess decoration. The early cold storage warehouses were constructed of brick and were usually multiple stories plus a basement (SurveyLA, 2018). The cold storage warehouse was an expression of the technology employed in its construction; the designers made no attempt to design in a particular style. Other features of the early cold storage warehouse included interior space divided into rooms dedicated to specific goods, and loading docks, an essential component that enabled distribution (SurveyLA, 2018).

The California Ice Company Building (Historic Cultural Monument Number 872) (**Figure 8**), 1635-1637 N. Spring Street, constructed in 1890, is a two-story warehouse that exhibits Victorian Flatiron architectural features (Fischer, 2006). Another early example of this property type is the 1905 Los Angeles Ice and Cold Storage Company Building (**Figure 9**), 364 4th Street.



670 Mesquit Street / D170431.00

SOURCE: Google Maps

Figure 8
California Ice Company, 1635-1637 N. Spring Street
(Current Street View)



670 Mesquit Street / D170431.00

SOURCE: USC Digital Library

Figure 9
Los Angeles Ice and Cold Storage Company Building,
364 4th Street, constructed 1905

As ice manufacturing technology improved by the 1920s and 1930s and refrigerators became ubiquitous in households, cold storage warehouse plants began to devote greater space to cold storage. These second-generation cold storage warehouses were built of reinforced concrete, with multiple stories plus a basement, and were windowless walls insulated with corkboard with a plaster finish. As these cold storage warehouses grew in size, technology played a role in the computer organization of inventory, and interior spaces became larger, with taller ceilings to accommodate a wide variety of goods, divided by temperature of the room, instead of product type (SurveyLA, 2018).

After World War II, the cold storage industry continued to expand in the suburbs and industrial district established outside of Downtown Los Angeles. The changes in distribution and availability of forklifts, wooden pallets, and metal shelving encouraged the development of sprawling, single-story, high-volume cold storage facilities. Some facilities originally constructed with low- ceilings would have the ceilings raised to provide additional storage room to utilize modern technology. Because the cold storage warehouses were moving outside of the confines of dense urban areas, they could be built on larger parcels and have sprawling footprints. Often their designs were tailored for the convenience of large trucks fronting a wide street and adjacent to freeways with generously sized loading docks, truck bays, and automobile turnaround space (SurveyLA, 2018).

The 19th century was the period during which "...ice went from a luxury to a necessity for the vast majority of Americans" (Rees, 2013). The American commercial ice industry emerged in New England in the first half of the 19th century. There, natural ice was harvested during cold New England winters and shipped around the country and even the globe. The impediments to this

process in the early years of the commercial ice industry were numerous. Geographical distance, differing climates, seasonal availability, high prices, and lack of demand all made entering the ice business a risky endeavor. However, as more and more Americans became aware of the possibilities (primarily culinary) created by commercially available ice, demand – and therefore profits – increased, setting the wheels in motion for ice to become big business (Rees, 2013).

Around the turn of the 20th century, emerging refrigeration technologies prompted companies to build ice plants and cold storage warehouses. The first artificial ice plant was erected in New Orleans in 1866, and the first dedicated cold storage building employing mechanical refrigeration was constructed in Boston in 1881. In 1904, there were 620 cold storage warehouses nationwide; by 1925, there were more than 1,700. Such structures represented enormous investments. In addition to the high cost of equipment, construction, and operation, warehouse and plant owners faced large insurance premiums, usually two or three times that of typical warehouses. This was due to the dangers of using ammonia as a chemical refrigerant, which could cause fires or explosions if not handled properly (Rees, 2013).

Despite the high construction cost, centralized cold storage and ice manufacturing plants were vital links in the food supply chain. They accepted the delivery of bulk shipments (usually by ship or train) that were then stored and redistributed to local markets and grocers. Additionally, the production of ice was necessary to keep food fresh in those establishments as well as in the iceboxes of private homes. With the development of railroads came the possibility of shipping food long distances from its point of origin. Because of this, ice was a necessary component of the preservation of food shipping, especially in the warm climate of Southern California where agriculture was abundant and demand was therefore high (Avitable, 2014: 19).

By the first decade of the 20th century, ice was a common commodity. In his book *The Ice Crop*, published in 1908, author Theron L. Hiles writes of the ubiquitous nature of ice in American culture at that time:

The demand for ice creams and cooled drinks, together with the growing taste for luxuries, in our cities and towns, has stimulated the retailing of ice until, at this time, there is hardly a town or village where ice privileges exist, that does not support a representative of the ice trade, and there are few large towns in the South which are not furnished with one or more artificial ice factories...It is safe to say that, at this time, the users of ice, directly or indirectly, now include nearly the entire population of the United States (Hiles, 1908: 8).

Ice played a key role in American economics and industry during the early 20th century. As Hiles reports in 1908, the ice business of the day represented a \$28 million industry, providing approximately 20 million tons of ice to consumers each year and employing 90,000 workers (Hiles, 1908: 8).

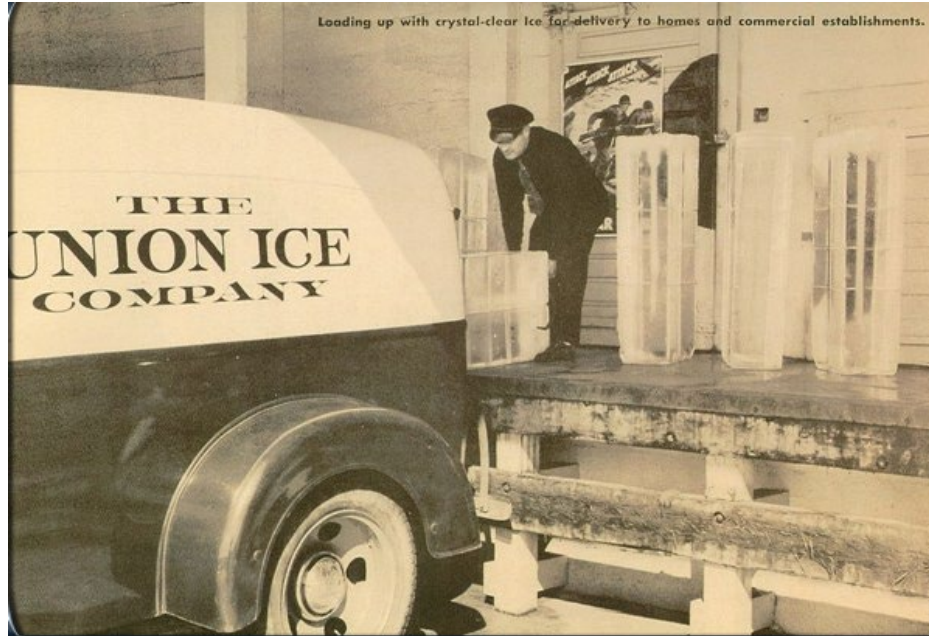
Two types of artificial systems were used to manufacture ice during the early 20th century, compression and absorption systems, as detailed in “Making Ice by Machinery” article published in the *New York Times* on June 1, 1902. Those two systems either produced ice in the shape of a can or plate dependent upon the type of manufacturing technique the ice production facility employed.

Following World War I, an important innovation transformed the ice production plant as electricity replaced steam as the primary source of ice plant power. As compared to a steam condenser, electric motors cost less to purchase and operate, occupied less space, and required less labor to maintain and operate. In addition, a factory powered by electricity did not require a railroad siding for coal delivery (McVarish et al., 2007:8).

By the 1930s, a second innovation, the automatic ice machine, revolutionized the ice industry and ultimately led to its eventual decline. These machines were able to produce ice rapidly, including large slabs of ice as well as small flakes or cylinders. They also occupied a small floor area, required less labor, and produced ice in smaller sizes more economically than it could be made by freezing and crushing large slabs of ice (McVarish et al., 2007: 8)

Another factor that led to the gradual decline of the commercial icehouse was the increased use of home refrigerators after World War II. By 1944, almost 70 percent of American households had mechanical refrigerators. As ice production decreased in importance, many ice houses changed their emphasis to cold storage. Even though the demand for ice decreased, the demand for cold storage warehouse space actually increased. During World War II, Americans adopted the convenience of frozen foods, which had been previously shunned. With their increased popularity, frozen foods needed to be distributed, increasing the demand for cold storage warehouse space. The cold storage warehouse property type reflected this change in focus through its construction compared to pre-war buildings. Generally, the newer cold storage warehouses were fireproof structures built of concrete with a number of different types of insulation, but the “board type” of insulation, composed of cork, glass, or rock fibers, became the most common. As mobile refrigeration improved and truck transportation gained ascendancy over rail transport, however, centralized cold storage warehouses were abandoned in favor of larger suburban locations (Avitable, 2014: 20-21).

Over the course of the first half of the 20th century, technology in the ice industry changed rapidly and had a profound impact on how (and how much) ice was bought and sold. As early as 1909 the Los Angeles Times was reporting on the decreased market of the ice business due to precooling of citrus and railroad cars. This precooling process reduced the need for ice to be packed into the cars, cutting the amount of ice shipped with fruit in transit from hundreds of tons a day to only a few tons per day. Changes also came to the distribution of ice to local consumers. In the early years of the 20th century, ice was delivered by horse drawn wagon. By 1926, virtually all of the 400,000 tons of ice consumed in the City of Los Angeles was delivered by truck (Los Angeles Times, 1926). Ice continued to be delivered to individual residences until after the end of World War II. Ice blocks were loaded from ice production facilities into the backs of insulated trucks and delivered around the city (**Figure 10**).



SOURCE: The Union Ice Company

670 Mesquit Street/ #170431.00

Figure 10
Placing ice in the truck from a loading dock c.1942

After World War II, in-home refrigeration became common, eliminating the need to have ice delivered to the door. Refrigeration also enabled railroad cars and truck trailers to carry cargo without having to be loaded down with tons of ice. The ice industry was forced to adapt its business model toward a mechanically refrigerated world.

During the 1960s, the growing use of automatic refrigerated vending machines, automatic ice vendors, and ice machines in restaurants reduced demand for commercial ice plants. However, the demand for cold storage warehouse space continued to grow as Americans embraced TV dinners and frozen foods. The rise of mass media in popular culture during the 1960s and the popularity of TV led to the acceptance of TV dinners that were cheap, convenient and suited the modern lifestyle. Other frozen foods like vegetables, meat, and fruit could be purchased cheaply and kept longer than fresh foods. The introduction of the microwave during the late 1960s and its proliferation in American households in the 1980s prolonged the demand for cold storage warehouses into the late 20th century (SurveyLA 2018).

Sub-Context: Manufacturing for the Masses, 1887-1965, Industrial-Manufacturing- Factory “factory” Property Type, 1887-1980

With the development of a deep-water port in San Pedro, which connected Los Angeles with markets abroad, Los Angeles became the prime connection to overseas markets for Southern California. Civic boosters actively courted eastern and Midwestern manufacturers, enticing them to come west with promises of all-year production capabilities, an abundance of cheap energy, and willing workers who were unaffiliated with unions. With an abundance of cheap electricity from hydropower, the 20th century saw the proliferation of electricity and refrigeration, and a

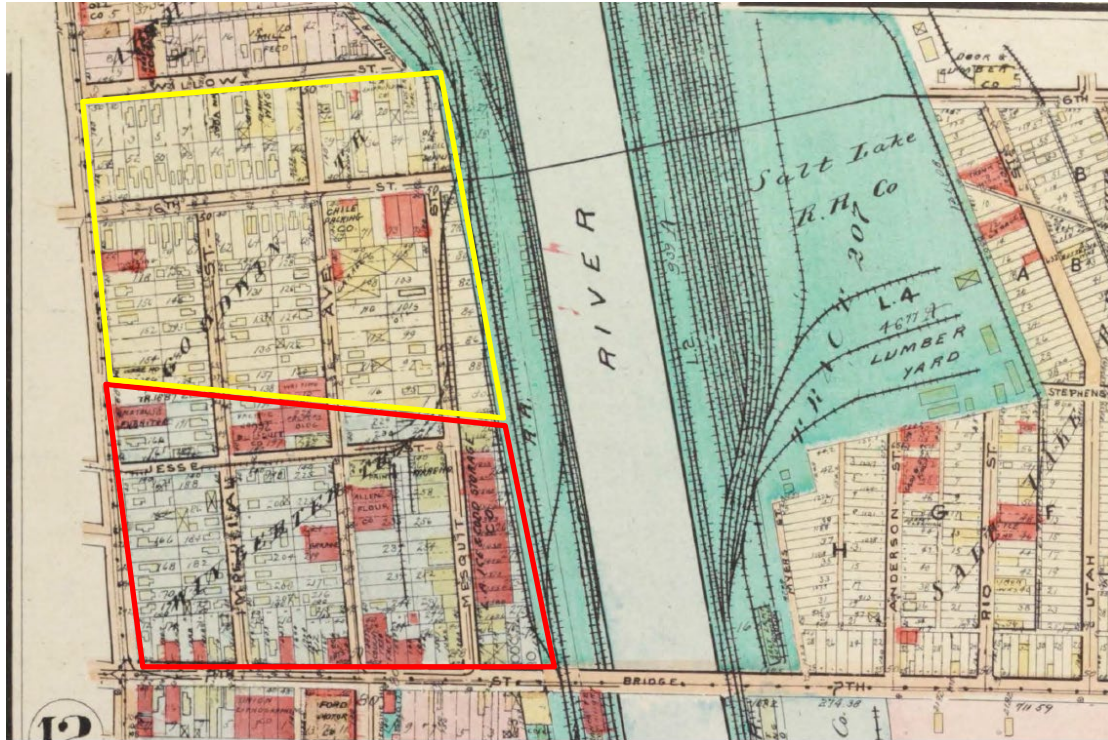
wide variety of new industries. The population booms of the 1920s and the post-WWII era, combined with a surge in the variety and novelty of consumer goods, produced an incredible inventory of new factories in Los Angeles making all manner of goods.

The Industrial-Manufacturing-Factory property type or “factory”, 1887-1980, is a general representative property type associated with the Sub-Context: Manufacturing for the Masses, 1887-1965, and is intended to cover utilitarian factory buildings that were not part of Los Angeles’s major industries but were nonetheless important to the City’s economic history. As the name indicates, utilitarian buildings are designed to be functional, and are usually constructed with inexpensive materials and have limited applied design. Their era of construction or location often dictates the materials utilized to construct these buildings. The term “factory” refers to an industrial building or small group of industrial buildings or manufacturing plant organized around a manufacturing process. This property type can include a single workshop, a large plant, or a complex of related buildings. A factory resource may be significant in the area of Industry, under a subtheme pertaining to a specific area of manufacturing that was important to the economy of Los Angeles. Factory property types include a wide range of buildings that may be specifically associated with food processing, automobile production, apparel, textiles, aircraft, or aerospace production. Property types can include a single workshop, a large plant, or a complex of related buildings. In general, intact factory buildings from the first half of the 20th century represent a brief but dramatic transition of Los Angeles from an agricultural town into a top-ranking industrial powerhouse. These property types may be historically significant if they are associated with well-known or demonstrably influential manufacturing enterprises or activities that represent the importance of manufacturing in the industrial, economic, or social history of Los Angeles. These property types are often designed in prevalent architectural styles of the period and may also be significant examples of an architectural style from the period of significance and/or the work of noted architects (SurveyLA, 2018).

History of the Project Site

Early maps from the 1850s to the 1870s do not depict the Project Site, likely due to the fact that it was undeveloped. Gumprecht (2001: 59) depicts the Project Site and the land to south of the City center, between the Elysian Hills and the river, as agricultural land used for wine grapes, vegetables, fruit and nut groves, and pastureland in the 1850’s. Later maps dating from 1880 to 1890, as well as photographs of the Project Site and vicinity, indicate that it was developed with cold storage warehouses by the 1880’s. By 1887, Rowan and Koeberle indicates that the Project Site had been subdivided for development.

A majority of the Project Site is located in the Wingerter Tracts, named after the first owner of the property, on the west side of the Los Angeles River. Additionally, portions of the Project Site extend north into the Goodwin Tract. Research of the Goodwin Tract did not reveal any specific information on who subdivided the area. Although it is not clear when the tracts were first subdivided, sales of the lots in the area are documented as early as 1893. The Baist Real Estate map below (**Figure 11**) from 1921 indicates that these two tracts were developed with a combination of large industrial buildings and smaller single- and multi-family residential buildings. The Project Site itself is shown as developed with the Los Angeles Ice and Cold Storage.



670 Mesquit Street / D170431.00

SOURCE: University of Southern California Library

Figure 11
Wingerter Tract (Red Box) and Goodwin Tract (Yellow Box) west of the Los Angeles River. (Plate 24 of the Baist Real Estate Atlas, 1921)

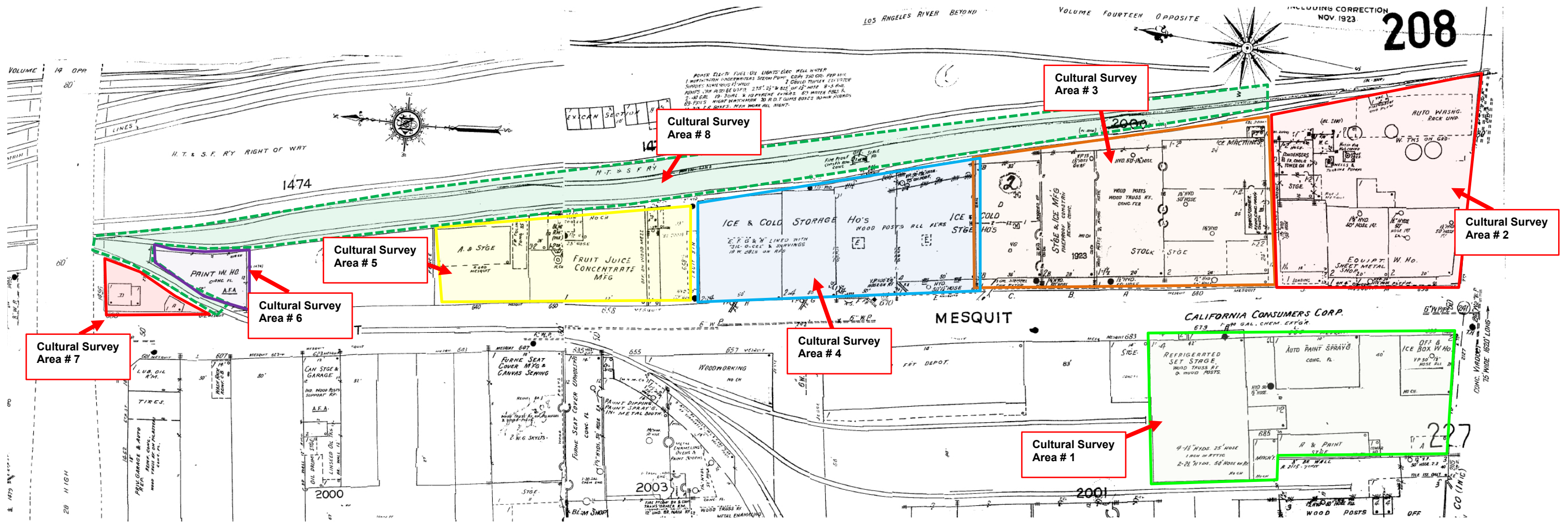
Development of the Project Site in the Wingerter and Goodwin Tracts occurred sporadically over time. The earliest building constructed on the Project Site was the Los Angeles Ice and Cold Storage Company (Los Angeles Cold Storage Company) Building located in Survey Areas #3, 4, and 5. The cold storage warehouse was originally designed by local architects Ezra Kysor and Octavius Morgan in 1888. In 1889, a year after its establishment, the Los Angeles Cold Storage Company located on the Project Site was handling over 750,000 pounds of pork products from the east coast each month, costing over \$10,000 in freight bills (Los Angeles Herald, 1889). The company urged local farmers to produce these products and take advantage of the storage facilities available. The Los Angeles Cold Storage Company used one of the two artificial systems to manufacture ice, as the 1888 Sanborn map notates rooms dedicated to the engines, ammonia condenser, condenser and filter, steam condenser, and freezing. (Sanborn Maps are available in **Appendix B**). Rebuilt after it was destroyed by fire, the warehouse was redesigned by architect Sumner P. Hunt in 1908. The Project Site also includes the former West Coast Quilting Company Factory constructed in 1963, located in Survey Area #1. The most recent building constructed on the Project Site is a cold storage warehouse built in 2002 located in Survey Area #2, according to a Certificate of Occupancy (COA) on file with the Los Angeles Department of Building and Safety (LADBS). Aerial images from 1994 and 2002 confirm the construction date presented in the COA.

The following section presents a detailed historical background and construction history associated with each of the eight Survey Areas within the Project Site (**Figure 12**) as well as two areas adjacent to the Project Site (the 7th Street Bridge and the Railway Properties).

Survey Area #1 (APN 5461-018-009)

Survey Area #1 (APN 5461-018-009) was originally developed with industrial buildings as early as 1907, as indicated by the building permits summarized below and listed in Table C-1 in **Appendix C** of this report. Between 1907 and 1955, a series of buildings were constructed, modified, and subsequently demolished at 679, 681, 683, 685-687, and 689 Mesquit Street for Union Well Supply and Los Angeles Ice and Cold Storage Company. The subject parcel is currently improved with an existing concrete industrial manufacturing building at 689 Mesquit Street, that was completed in 1963 for the West Coast Quilting Company and remodeled for use as cold storage warehouse in or by 2006.

In 1963, construction of the building currently within the Survey Area #1, 689 S. Mesquit Street, was completed for the West Coast Quilting Company, which used the factory for offices and manufacturing purposes. A Certificate of Occupancy (No. LA 4024-61) was issued June 20, 1963, to the West Coast Quilting Company, for a one-story, type III-B, 120-foot by 178-foot office and manufacturing facility with 41 parking spaces. Further research on the history of the West Coast Quilting Company did not reveal any specific information about the company or the products they produced. On June 2, 2006, a building permit was issued to owner Frank H. Gallo and applicant Stuart Baltz, for removal of the existing roof materials and installation of a new spray polyurethane foam and elastomeric coating roofing system, providing insulation supporting the building's current use as a cold storage warehouse. The building was likely modified for use as a cold storage warehouse at or by this time (2006).



SOURCE: EDR 2018

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Figure 12 Sanborn Fire Insurance Map from 1951 depicting the growth of the cold storage facilities, then operated by the California Consumers Corp.

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Survey Area #2 (APN: 5164-017-008)

Permits for building improvements and alterations/repair at 690-694 S. Mesquit Street and 2135 E 7th Street were issued to owner Los Angeles Ice Cold Storage Co, for a one-story oil house shed in 1908, to strengthen roof trusses and repair fire damage in the storeroom in 1912, to replace wood with cement floor in bottling works in 1913, construct steel tower in 1915, erect boiler room and cover transformers in 1918. However, by 1923, the previous improvements had been removed and a new reinforced concrete two-story plus basement ice tank and storage building was erected, designed by architect John E. Kunsh for the same owner. In 1924 the building was used for distilled water and the loading platform was rebuilt. In 1926, a wood platform was replaced with a concrete platform and a concrete retaining wall was constructed. In 1928 a sign was erected for the Los Angeles Ice and Cold Storage Company. In 1943, two buildings at 2135-2155 E 7th Street (APN 5164-017-008) were used by Lockheed Aircraft for aircraft manufacturing, and further alterations occurred in 1955 (LADBS Online Building Records summary for APN 5164-017-008). In 1979, a permit for a poster panel sign at 697 Mesquit designed by architect Kevin Kelly was issued to owners Arnold Rubin and Fred Kort. However, sometime before 1990 all of the improvements were demolished. In July 1990 the parcel was vacant, and a certificate of occupancy issued for storage of trucks, and in November its use changed to an auto impound yard. An Application for Inspection of New Building and Certificate of Occupancy, No. 30800300219, issued November 9, 1990, for an Auto Impound Yard–Use of Land, indicates the unimproved lot was being used for parking in the early 1990s (1990LA66998). Application for Inspection of New Building and for Certificate of Occupancy, No. 29100300298, Storage of Trucks for Truck Driving School, was issued July 2, 1990. On July 25, 1991, the land use was changed to storage of trucks for a truck driving school (Certificate of Occupancy, 7/25/1991, No. 1990LA59043). In 2000-2001, LADBS building records indicate the lot was undergoing grading and compaction, likely in preparation for construction of a new building. Survey Area #2 (APN: 5164-017-008) is currently improved with a cold storage warehouse constructed in 2002, according to a Certificate of Occupancy (COA) on file with the LADBS (Table C-2 in Appendix C of this report). (Certificate of Occupancy, 690 S. Mesquit Street, Permit No. 99010-10000-03361/0016-10000-17409, New 1-story, Type III-N, 192' x 148' reinforced concrete tilt-up cold storage building, S1 occupancy, issued 3/06/2002). Aerial images from 1994 and 2002 (**Figure 13**) confirm the construction date presented in the COA. The building was not evaluated further because it does not meet the age threshold for consideration as a historical resource.



SOURCE: EDR, 2018

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Figure 13

Aerial image of the Project Site (Survey Area 2), 1994 (left) showing vacant lot; Aerial image of the Project Site (Survey Area 2) 2002 (right) showing existing improvement.

Survey Areas #3, #4, and #5 (APN: 5164-017-003, 5164-017-002, 5164-017-006)

Originally known as the Los Angeles Cold Storage Company, the Los Angeles Ice and Cold Storage Company was under construction in 1887 and began operation in May of 1888 (Los Angeles County Assessor; Los Angeles Herald, 1888). In 1887-1888, the company constructed a \$75,000 ice and cold storage plant designed by Kysor, Morgan, and Walls on 7th Street (the Project Site) in Los Angeles (Los Angeles Herald, 1888). The original cold storage warehouse was located within Survey Area #3 at 674 S. Mesquit Street (APN 5164-017-003).

The opening of the Los Angeles Ice and Cold Storage Company's ice manufacturing facility was announced with great fanfare, "Contractors and consumers of ice will do well to wait for the opening of the works. At the same time there will be rooms ready for the cold storage of meat, poultry and fruit" (Los Angeles Herald, 1888). The owners of the new ice manufacturing and cold storage facilities advertised their facility as the largest and most complete ice and cold storage plant in the world. While their competitors supplied customers with natural ice, Los Angeles Cold Storage Company's manufactured ice was cheaper and superior in purity. Furthermore, manufacturing their own ice was less expensive than gathering it from distant frozen lakes and shipping it to Los Angeles.

In November of 1888, the Los Angeles Herald published an article describing the new ice and cold storage facilities in detail. The first room visited by the Herald reporter was kept at a chilly 35 degrees. "The department in which the scribe was standing was devoted to the storage of meat, some of which had been there two weeks, and yet looked and smelt as fresh as the day when it left the slaughter-house" (Los Angeles Herald, 1888). In addition to the cold storage rooms, the writer was shown the engine rooms in the basement where machines manufactured ice. "The

process takes about four days, and at the end of that time each can contains 170 pounds of ice which is then ready for consumption” (Los Angeles Herald, 1889)

In 1901, Los Angeles Cold Storage Company looked to expand its business by supplying private residences and businesses with cold air. The company petitioned the City of Los Angeles to install pipes throughout the city. “The company proposes to sell refrigeration to all comers and lay two 3-inch pipes in the streets” (Los Angeles Herald, 1901). In August of 1901, the board of public works conditionally approved the company’s petition. Once the plan was approved, the Los Angeles Cold Storage Company began construction on a new refrigeration plant near Los Angeles Street, between 4th and 1st Streets. (Los Angeles Herald, 1901).

Earlier that year, the Los Angeles Cold Storage Company agreed to a centralized distribution system in cooperation with the Union Ice Company and the National Ice Company (Los Angeles Herald, 1901). An “ice trust” was formed by the companies, previously in tight competition but opting to work cooperatively instead by the turn of the century. In 1904, the delicate balance maintained by the ice and cold storage companies within the ice trust was upset by the emergence of a new company, the Home Ice and Cold Storage Company (Home Ice). Home Ice sparked an “ice war” in an attempt to kill the ice trust (**Figure 14**). However, their efforts failed and in less than a year, Home Ice was absorbed into the ice trust (Los Angeles Times 1905).

The construction permits for the original cold storage warehouse are not available because LADBS records begin in 1905. However, a Sanborn insurance map from 1888 depicts the original cold storage and ice manufacturing facilities after completion (**Figure 15**). After being destroyed by fire, the original cold storage and ice manufacturing facilities were replaced in 1908 with a new building at 674 to 678 Mesquit Street approximately 80 feet by 96 feet in size that was designed by the architectural firm of Hunt, Eager & Burns and built for an estimated cost of \$15,000. There is one single family residence to the north of the original cold storage and ice manufacturing facility, that is depicted on the 1888 Sanborn. In 1894 an addition residence is depicted to the north of the first one. Both residences are gone by 1909. Later updates of the Sanborn map show a much larger facility extending into Survey Areas #4 and #5, the construction and expansion of which is documented in multiple building permits (**Figure 15**). All building permits for Survey Areas #3, #4 and #5 are summarized below and included in Tables C-3, C-4 and C-5 in Appendix C of this report.

Over \$40,000 worth of Gurney Refrigerators to be given away.

THEY ARE THE BEST. They save your ice, that saves your money, and while this beautiful gift is free, we sell you ice for less than you have ever bought it in this city before. This is a home enterprise. Encourage it with your patronage. We will save you money. You cannot afford to lose this opportunity.

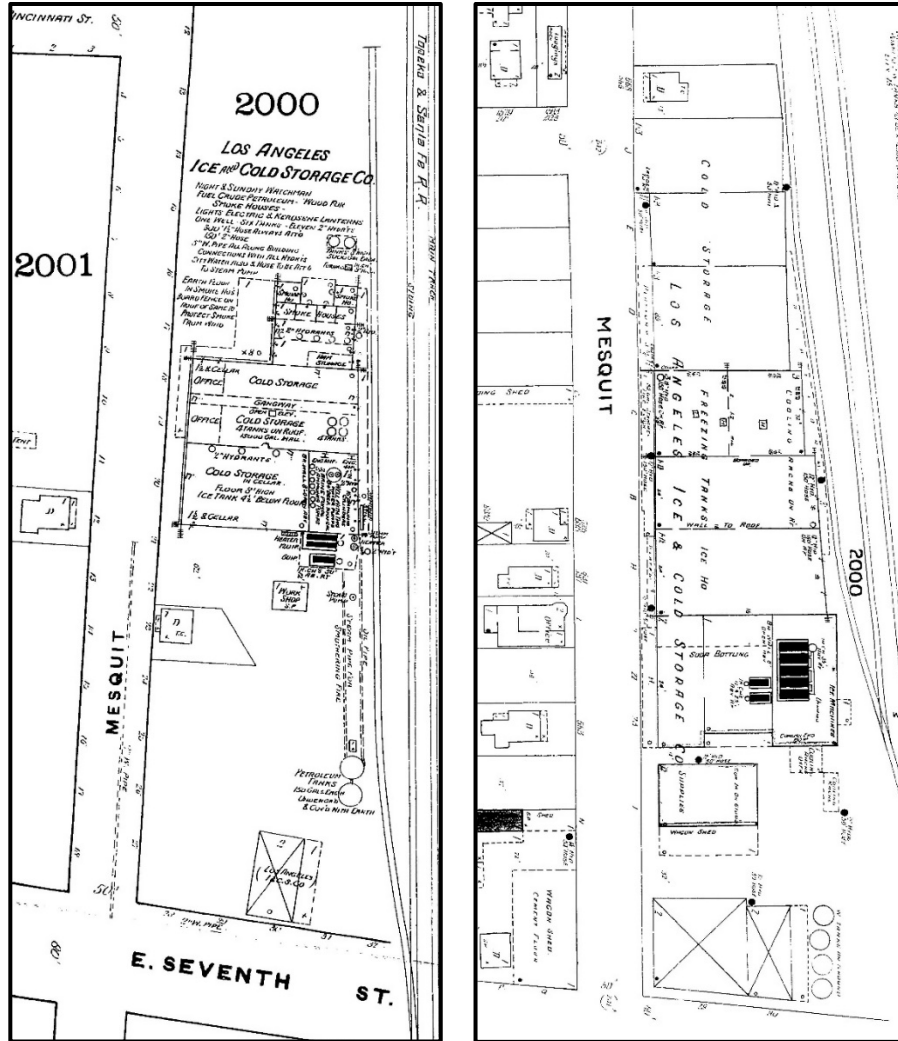
Call at Room 435, New Chamber of Commerce Building, see our samples, and make your selection before they are all gone.

HOME ICE AND COLD STORAGE COMPANY
CONTRACT DEPARTMENT. 540 BRADBURY BLOCK.
PHONES—Main 650; Home Phone 2130.

670 Mesquit Street / D170431.00

SOURCE: Los Angeles Times

Figure 14
Advertisement of the Home Ice and Cold Storage Company and their attempt to topple the "Ice Trust" in the Los Angeles Times, April 24, 1904

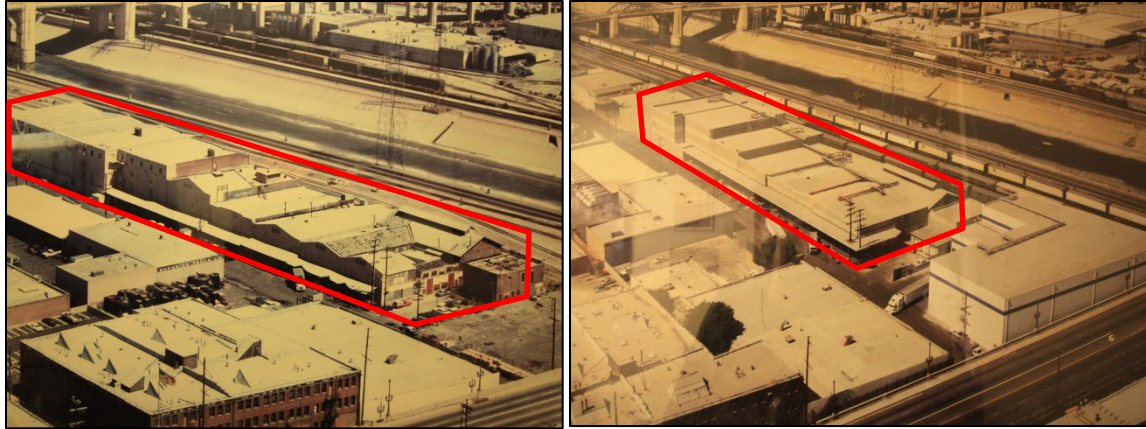


670 Mesquit / D170431.00

SOURCE: Los Angeles Public Library

Figure 15
 (Left) The Los Angeles Ice and Cold Storage Company c. 1888;
 (Right) The Los Angeles Ice and Cold Storage Company's new
 building c. 1908

Expansion and alteration of the ice and cold storage plant for the Los Angeles Ice and Cold Storage Company is documented in numerous building permits that are characterized by incremental additions and structural upgrades over the years. In 1927, new concrete platforms were constructed to replace old wooden platforms and a new concrete floor was installed. In 1933, a series of repairs to the building's walls were undertaken due to damage from the Long Beach Earthquake. The biggest change to the area came in 1952 when one of the smaller cold storage warehouses was demolished. Then in 1954, new offices and restrooms were added to the existing building. To comply with earthquake safety regulations (Division 88), the existing building underwent significant modifications in 1990. The Division 88 compliant alterations included replacing unreinforced masonry originally chosen by Hunt, Eager, and Burns in 1908, with modern textured, concrete blocks. In its present condition, the 1908 building's exterior is completely unrecognizable due to over \$700,000 worth of Division 88 modifications (**Figure 16**).



SOURCE: Rancho Cold Storage

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Figure 16

Aerial view of the former Los Angeles Ice and Cold Storage Company, prior to the Division 88 alterations, circa 1970s; Aerial view of the former Los Angeles Ice and Cold Storage Company, after the Division 88 alterations, circa 2003

Improvements in Survey Area #3 (APN: 5164-017-003) are documented in building permits reproduced in Table C-3 in Appendix C of this report. On January 2, 1908, a permit (No. 9) was issued for construction of a new storage building for the Los Angeles Ice and Cold Storage Company at 674 to 678 Mesquit Street designed by Hunt, Eager & Burns architect, and constructed for a cost of \$15,000; the 80-foot by 96-foot building was four-stories tall and had a concrete foundation, blue brick front, brick cornices, iron stairways, wood columns, and a composition roof. On April 11, 1908, a permit (No. 1984) was issued to the Los Angeles Ice and Cold Storage Company to raise the ceiling and roof ten feet and replace burned portions of the one-story wood-frame 55-foot by 110-foot ice storage building at 672-674 Mesquit Street for a cost of \$3,000. On July 22, 1927, a permit (No. 20768) was issued to the Los Angeles Ice and Cold Storage Company to replace old wood platforms with concrete platforms and construct a steel canopy covered with galvanized iron for the existing 78-foot by 94-foot three-story cold storage building at 678 S. Mesquit Street for a cost of \$1,500. On July 22, 1927, a permit (No. 20769) was issued to the Los Angeles Ice and Cold Storage Company for a new concrete floor for the existing three-story, three-room, 50-foot by 110-foot cold storage building at 680 Mesquit Street for a cost of \$800. On March 28, 1933, a permit (No. 4113) was issued to the California Consumers Corp. to “tear down then replace fire walls facing Mesquit St. and one section of fire wall on east end – wall will extend 2 feet above roof line – quake damage” of the present factory at 680 Mesquit Street at a cost of \$1,000, completed by Nance Construction Company, contractor. On April 4, 1933, a permit (No. 4605) was issued to California Consumers Corp. to demolish and replace 30 feet of present front wall at the south end of the building and install new posts under the ends of two trusses for the one-story brick storage building at 680 Mesquit Street for a cost of \$500. On August 29, 1944, a permit (No. 6394) was issued to California Consumers Corp. for the re-roof of the building at 680 Mesquit Street by Owen Roofing Co. On March 6, 1951, a permit (No. 2182) was issued to California Consumers Corp. to replace a portion of the cracked masonry wall of the 133-foot by 160-foot one-story ice manufacturing, storage, and office

building at 682 Mesquit Street, by engineer Geo. J. Fosdyk, and G. O. Gartz Construction Co. In July 1952, a demolition permit (No. 38819) was issued to California Consumers Corp. for El Rancho Lumber Company to remove the wood frame and corrugated iron 45-foot by 75-foot warehouse at 678 S. Mesquit Street, for a cost of \$270. On February 2, 1954, and permit (No. 79024) was issued to California Consumers Corp. and contractor G. O. Gartz Construction Co. to close two doorways in a masonry wall, resurface floor, add office partitions and ceiling, add toilet rooms, and add air conditioning for a 63-foot by 145-foot brick warehouse at 674 Mesquit Street for a cost of \$6,200. On July 15, 1965, a permit (No. 99824) was issued to Rancho Cold Storage c/o Harley R. Tulpis for parapet correction for a one-story cold storage building at 684 S. Mesquit Street, for a cost of \$300. Also on July 15, 1965, a second permit (No. 99827) was issued to Rancho Cold Storage for parapet correction for a cold storage building at 678-82 S. Mesquit Street, Building "A". On July 15, 1965, a third permit (No. 99828) was issued to Rancho Cold Storage for parapet correction for a cold storage building at 676 S. Mesquit Street. On August 18, 1967, a permit (No. 51570) was issued to Jack Levine for a parapet correction for the existing water-cooling tower at 684 S. Mesquit Street, a 30-foot by 55-foot one-story brick building. On February 21, 1990, a permit (No. 4582) was issued to Don Gallo for full compliance Division 88, RCIIB (structural reinforcement) for the cold storage/office at 678-682 Mesquit Street, and Building A, 164-foot by 150-foot, by engineer Merdad Houriani, for a cost of \$280,000. On April 3, 1990, a permit (No. 5425) was issued to Don Gallo for demolition by hand wrecking of a 35-foot by 55-foot unreinforced masonry (URM) warehouse at 684 Mesquit Street for a cost of \$7,700. On April 10, 1990, permit (No. 54366) was issued to Don Gallo to construct a new 35-foot by 55-foot one-story concrete-block cold storage building for a cost of \$100,000 at 684 Mesquit Street by engineer Ining Lu and architect Frank Gallo. On August 17, 1990, a permit (No. 61752) was issued to Don Gallo to change the roof system to Kim truss for the cold storage building at 684 Mesquit Street by engineer Ining Lu and architect Frank Gallo, owner-builder. On January 15, 1991, a permit was issued to Don Gallo for full compliance with Division 88 (change of engineer) for a cost of \$280,000, for a 164-foot by 150-foot warehouse/office at 682 Mesquit and Building A by engineer Chester Schultz and architect Frank Gallo. On May 2, 1991, a permit (No. 12329) was issued to Don Gallo for change of roof design (85-foot by 48-foot) for the 164-foot by 150-foot URM cold office/storage at 682 Mesquit Street Building A by engineer Chester Schultz, architect Frank Gallo, and contractor Gangi Builders for a cost of \$30,000. On October 29, 1992, a permit (No. 20055) was issued to Rancho Cold Storage, Inc. to replace the east URM wall at 682 Mesquit Street with a new concrete block wall and replace south wood wall with steel frame and wood infill for a cost of \$35,000 by engineer Chester Shultz, architect Frank Gallo and contractor Gangi Builders.

LADBS Permits for Survey Area #4 (APN: 5164-017-002) begin in April of 1908 (Table C-4 in Appendix C of this report). The earliest permit on file was for lifting the roof of an existing building by 10 feet, similar to work completed at the same time in Survey Area #3. In 1912, the building was further modified, raising the second floor 4 feet and 4 inches to make room for the addition of a new floor between the first and second levels. In 1932, the building's structure and floor were strengthened to support greater loads. In the 1970s, a series of permits were filed to repair fire damage. The greatest alterations to structures in Survey Area #4 occurred between 1989 and 1994 when the building underwent structural upgrades in compliance with Division 88. The alterations replaced the original unreinforced masonry exterior with new textured concrete

block. Like the buildings in Survey Area #3, these alterations completely rendered the existing buildings unrecognizable. Furthermore, the buildings in Survey Area #4 cannot be differentiated from the buildings in Survey Area #3 due to the new concrete block exterior walls and Division 88 modifications.

Improvements in Survey Area #5 (APN: 5164-017-006) began as early as 1909 with the construction of a single-family residence owned by the Atchison, Topeka, and Santa Fe (AT&SF) Railroad (Table C-5 in Appendix C of this report). The residence had been demolished by 1922 when a concentrated fruit juice factory was built by the Hyland Stanford Company on the site. The Hyland Stanford Company made significant contributions to the fruit processing industry through multiple patents registered in the company's name. Throughout the 20th century, the facilities were modified by subsequent owners including South Coast Packing Co and Rancho Cold Storage; the buildings were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold -storage facility that was constructed in 1985.

The buildings in Survey Area #3, #4, and #5 are examples of utilitarian cold storage warehouse facilities that are substantially renovated and no longer retain their original appearance. The firm of Kysor, Morgan, and Walls designed the original warehouses for the Los Angeles Ice and Cold Storage Company in 1887-1888. However, the warehouses were destroyed in a fire and the facility was reconstructed, therefore the existing improvements are not representative of this firm's work. After the original Los Angeles Ice and Cold Storage facilities were destroyed by fire in 1908, the company hired the firm of Hunt, Eager, and Burns to design their new facility. However, the concrete materials and construction techniques of the existing buildings on the Project Site are from (or have alterations from) the 1950s and 1960s, and as such their construction methods and appearance are characteristic of the modern post-World War II period. However, substantial alterations in 1990 including demolition of unreinforced masonry sections, full compliance Division 88 structural reinforcement improvements, and new construction have resulted in renovation of the entire complex.

Kysor, Morgan and Walls, Architects: Ezra Kysor was born in Cattargus, New York, in 1835 and moved west shortly after the American Civil War. After spending some time in Virginia City, Nevada, Kysor settled in Los Angeles in 1868. He partnered with E. J. Weston before forming a partnership with Octavius Morgan in 1876 (Withey et al., 1956). Kysor and Morgan designed many notable structures throughout Los Angeles prior to Kysor's retirement in 1887, including Saint Vibiana's Roman Catholic Cathedral in 1876, as well as several business blocks along Main Street in Downtown Los Angeles (Withey et al., 1956). When Kysor retired, J.A. Walls joined the firm and the name of Kysor was retained, forming Kysor, Morgan and Walls (later becoming Morgan and Walls and then Morgan, Walls, and Morgan in 1910) (Withey et al., 1956). The firm is responsible for Downtown landmarks, including the I.N. Van Nuys Building (1911) listed on the National Register of Historic Places. Octavius Morgan was once one of the oldest members of the American Institute of Architects on the Pacific Coast and a charter member and former president of the Southern California Architect's Association. Octavius Morgan is considered a notable architect due to his numerous contributions to the field of architecture, the development of California, and the architecture of Los Angeles.

Hunt, Eager and Burns Architects: Sumner P. Hunt (1865-1938), a well-known civic leader as well as a successful architect for fifty years, was born in Brooklyn, New York. After relocating to Los Angeles in 1889, he worked for the firm of Caulkin & Haas before establishing his own office in 1895 with Abraham Wesley Eager. The architectural firm of Hunt, Eager and Burns succeeded Hunt and Eager, which operated from 1899 until 1907. Silas Reese Burns, Jr., (1855-1940) joined them in 1907 (Pacific Coast Architecture Database). During the early period, Hunt's firm was responsible for the design of many notable residences in Southern California (The Southern California Institute of Architects, 1938). Mr. Hunt was also involved in the planning and design of many schools including a new building for Virgil Avenue Junior High; Louis Pastor Junior High; reconstruction of David Starr Junior High School; and Academic Hall (Balch Hall), Scripps College (Withey et al., 1956). Several of the firm's other notable works include the Los Angeles Tennis Club, the Auto Club of Southern California, the Southwest Museum, the Highland Park Ebell Club, the Ebell Club of Los Angeles, the Wilshire Ebell Theater and Club. One of his most valuable contributions was the support of seismic standards to resist earthquake damage, and he was on a number of committees, such as the American Institute Committee on Earthquake Hazards. A Fellow of the American Institute of Architects (AIA), Hunt was a strong proponent of the restoration of California missions and was one of the founders of the California Landmarks Club (The Southern California Institute of Architects, 1938).

Survey Areas #6 and #7 (APN: 5164-016-009 and APN: 5164-016-010)

Survey Areas #6 and #7 are small (3,708 square feet and 2,881 square feet, respectively) vacant portions of land adjacent to Survey Area #8 (Figure 17). According to aerial photographs and Sanborn maps, buildings were built here between 1923 and 1928. These buildings serviced the AT&SF Railroad and had a track running between the two buildings servicing parts of Mesquit and Jesse Streets. A concrete paint warehouse was located in Survey Area #6, and a single-family residence and automobile garage were located in Survey Area #7 as depicted on the 1951 Sanborn map (Figure 12). Survey Areas #6 and #7 are currently undeveloped and have remained so since the railroad, warehouse and residential buildings were demolished between 1964 and 1977.

Survey Area #8 (APN: 5164-016-803)

Survey Area #8 (see Figure 4 and Figure 17) is to the west of the adjacent Railway Properties and within the Project Site itself. It is a portion of the linear former railroad right-of way that spans from 6th Street to 7th Street. The first available map showing development on Survey Area #8 is an 1890 Sanborn Fire Insurance Map, which indicates that Survey Area #8 was a railroad right-of-way for the AT&SF Railroad. The 1906 Sanborn map indicates the AT&SF line was expanded and tracks were added. Between 1983 and 1989, the tracks closest to the Project Site were reconfigured and some removed. According to the Los Angeles County Assessor's online Assessor Portal, Parcel Detail for Assessor Identification Number (AIN) 5164-0170803, Survey Area #8 is a vacant State assessed tax exempt parcel. It is presently in use for railroad storage. The Railway Properties (adjacent to Project Site) is separate from Survey Area #8 (within Project Site) and according to the County Assessors (see Figure 4) Survey Area #8 is a parcel of private land.



SOURCE: EDR 2016

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Figure 17
1938 Aerial Photo of Railroad Improvements (later demolished). Location of Survey Areas #6, #7, and #8 (outlined in red)

Adjacent 7th Street Bridge (P-19-175070)

The 7th Street Bridge (Seventh Street Viaduct, Bridge 53C-1321) is located adjacent and outside of the Project Site, on at its southern boundary.

The 7th Street Bridge was originally constructed between 1908 and 1910, partly in response to the popular ideals of the City Beautiful Movement. This national movement dating from the 1890s through the 1920s was inspired by the “White City” of the 1893 World’s Columbian Exposition in Chicago, which advocated an aesthetic inspired by the grand formalized architecture of western European cities for application to public works projects, public spaces, and buildings in American cities. The White City, an oversized model city set that combined buildings, open space, and water features in a highly ornamented Neoclassical architectural style, presented an appealing, aspirational contrast to the industrial, utilitarian nature of American cities, particularly those on the East Coast (JRP Historical Consulting Corp., 2002: 6). In the years following the exposition, City Beautiful plans were developed for the civic centers of San Francisco, Chicago, Denver, and Kansas City. Likewise, in 1903, the City of Los Angeles founded a Municipal Art Commission (Commission), which adopted the City Beautiful principles for local implementation and became, for several decades going forward, the leading proponents of city beautification (JRP Historical Consulting Corp., 2002: 8). The Commission’s goal was to, “work for the gradual elimination of ugliness from the conspicuous parts of our city,” (Los Angeles Times, 1903), henceforth combining the utilitarian with the beautiful.

Part of the Commission’s self-defined purview was the design of bridges crossing the Los Angeles River, and it believed the bridges should combine “artistic effect” with function (JRP Historical Consulting Corp., 2002: 8). The few bridges then in existence that crossed the river in

the downtown area were Victorian era in age, and of metal or wooden trestle or truss construction. They were meant to accommodate trolleys and trains and were increasingly inadequate for the traffic generated by the growth of downtown Los Angeles. They were also prone to destruction by the periodic floods. The Commission objected to their utilitarian appearance; Homer Hamlin, then the City Engineer with the Los Angeles Bureau of Engineering, wrote in 1909 that metal truss or girder bridges were as follows:

[They are] inherently unsightly. It is now the policy of the Board of Public Works to recommend cheap wooden bridges only in the outlying districts and occasionally for more important crossings where a temporary bridge can serve purposes until funds are available for a more permanent structure... The aesthetic side is taken care of by adopting the arch form and by special treatments of the concrete surfaces. On each side is built an ornamental stone balustrade with lighting posts over the piers and other architectural ornamentation is employed in keeping with the character of the structure. (JRP Historical Consulting Corp., 2002: 9).

In 1907, the Commission enlisted a well-known Chicago-based landscape architect, Charles Mulford Robinson, to create a plan entitled the “Los Angeles: The City Beautiful”. The plan laid the foundation for a number of City improvements including, ultimately, all the monumental bridges across the Los Angeles River, City Hall, and Union Station (JRP Historical Consulting Corp., 2002: 9). To be constructed of concrete, not metal or wood, the new bridges were meant to withstand flooding and signal the rise of Los Angeles as a growing metropolis. Hamlin wrote that a bridge was so “monumental a structure that we should not be satisfied merely with durability and strength, but should demand that to these be added fitness, grace and beauty” (National Park Service, 2000: 23). City Engineer Hamlin was appointed to the Commission in 1911, which shortly thereafter became an advisory board to the City with nearly sole authority to approve the designs of public buildings and infrastructure.

Initially, three bridges were planned by the Commission over the Los Angeles River, as follows: the Main Street Bridge (1910), the 7th Street Bridge (1908-1910), and the North Broadway Bridge (also known as the Buena Vista Bridge, 1911). The new bridges would incorporate City Beautiful concepts and reflect Neoclassical and Beaux Arts architectural styles and symbolic surface decoration. When it was brought forth for review in 1911, the design of the North Broadway Bridge was the subject of some debate between the Bureau of Engineering, which advocated for either lions or pillars as decoration (“something bold and massive, suggestive of western strength, vigor, and courage”) and the Native Sons and Daughters of the Golden West, which advocated for bears. The Commission ultimately prevailed, with a compromise in the form of an architectural group of columns as surface decoration, which, the Commission stated, “in our judgment ... will better adapt itself to the architectural character of the bridge, and the effect of such treatment will be more monumental.” (JRP Historical Consulting Corp., 2002: 11).

The primary impetus for the construction of the 7th Street Bridge was the rapidly growing population of the City and the need to connect the City with the Boyle Heights neighborhood across the river east of downtown. The City needed a transportation and bridge system that could move people more efficiently between home, work, commercial centers, and recreational

destinations. To solve the City's transportation needs, electric railcar systems were developed. In 1895, the Pasadena & Los Angeles Railway and the Los Angeles Pacific Railway merged to create the Pasadena & Pacific Railway. This was the first electric interurban line, connecting Los Angeles and Pasadena (Southern California Railway Museum, 2020). Utility and real estate mogul Henry Huntington expanded on this by purchasing the Los Angeles Railway in 1898 and forming the Pacific Electric Railway Company in 1901. Huntington purchased rights-of-way and began constructing lines to surrounding areas, with the first being service to Long Beach in 1902 (Southern California Railway Museum, 2020).

By 1911, a series of mergers, including one with Edward H. Harriman of Southern Pacific Railroad allowed Huntington to control virtually all electric rail passenger service in the Los Angeles area. Following the merger, Huntington was the largest owner of interurban railway passenger service in the world (Southern California Railway Museum, 2020) and his Los Angeles Railway Company dominated transit in Los Angeles. Between 1908 and 1910, the Los Angeles Railway Company supplied the City with \$38,480 for the construction of the 7th Street Bridge (California Railroad Commission, Engineering Department, 1920). The 7th Street Bridge was completed in 1910 at the cost of approximately \$115,000 (Railroad Commission of the State of California 1920). The 7th Street Bridge was constructed as a reinforced concrete, closed-spandrel, arch bridge over the Los Angeles River. Designing bridges that could withstand the then-unchannelized river's seasonal vagaries was a major engineering challenge; "unpredictable flooding and shifting sands and gravel made fixing the locations of firm foundations, piers, and abutments a very difficult task. River scour, the rock and debris collected and pushed down the river channel by raging waters, was a threat recognized by bridge engineers" (National Park Service, 2000: 30). The 7th Street Bridge foundations ultimately had to be sunk 10 feet below the original specifications in response to riverbed scour and the consequences of upstream aggregate mining, which was aggressively practiced by city contractors until it was eventually outlawed.

By 1911, the Buena Vista Viaduct, 7th Street Bridge, and Main Street Bridge were in operation, and Huntington's railways were an essential link between downtown Los Angeles and outlying areas (National Park Service, 2000: 9). The AT&SF rail tracks were at grade along the western bank of the Los Angeles River, running perpendicular to 7th Street and interrupting the flow of traffic across the Bridge. Likewise, the Salt Lake Railroad lines on the east bank, one of which served as the entrance to the freight yard to the north and was heavily used by freight cars, was also at grade on the eastern riverbank. The original design of the 7th Street Bridge's made no attempt to grade-separate the road from the rail lines on either side of the river (as seen in **Figure 18**). The 7th Street Bridge was the third of, ultimately, 15 bridges built by the City across the Los Angeles River between 1910 and 1934; it was the last one constructed without any grade separation for rail and automobiles (Railroad Commission of the State of California, 1920).



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SOURCE: Railroad Commission of the State of California 1920

Figure 18

Original 7th Street Bridge, 1916, before the railroad tracks were depressed, and upper deck constructed.

By the 1920s, the increasing population and popularity of the automobile were choking the City's roads. Residents demanded that additional roads and bridges be built, prompting the City to develop the "Major Traffic Street Plan of 1924," undertaken by three nationally known city planners: Harland Bartholomew, Charles H. Cheney, and Frederick Law Olmstead, Jr. (National Park Service 2000, 13-14). Once the plan was approved, voters approved the construction of additional bridges over the Los Angeles River, including a new automobile deck for the 7th Street Bridge that also allowed for additional height for trains passing under the bridge. 7th Street was, by then, the principal crosstown street in downtown Los Angeles, east and west of the business center, and rush-hour traffic across the 7th Street Bridge and tracks was nearly the equivalent of that at Broadway and 7th Street in the heart of the city's business district (Railroad Commission of the State of California, 1920).

To better handle streetcar and automobile traffic during rush hour, the California Railroad Commission and the City considered two options in 1918 for redesigning the 7th Street Bridge: to depress the railroad tracks on either side of the bridge and elevate the bridge over the tracks, or to elevate the railroad tracks over the bridge (Railroad Commission of the State of California 1920). The City chose the first option, to depress the tracks (as shown in **Figure 19**), as this was the cheaper option and avoided the creation of a steep grade for trains approaching and departing the 7th Street Bridge (National Park Service 2000, 10).



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SOURCE: USC Special Collections

Figure 19
A train about to pass under the 7th Street Bridge (prior to construction of second deck), 1927

The chosen plan also called for the extension of the 7th Street Bridge with additional bents and decking to allow it to pass over the rail lines on either bank of the river. A suggestion was made to extend 7th Street Bridge farther into Boyle Heights on its eastern side, but the Railroad Commission decided to end the bridge short of that, touching down at grade along the western riverbank, so as not to preclude the use of suitable “lowlands” along the river for industrial purposes. Once the 7th Street Bridge was extended and the rail lines depressed and widened, the AT&SF could now handle six tracks under the bridge, in contrast to its original four-track capacity over the bridge (Railroad Commission of the State of California, 1920).

In 1927, the 7th Street Bridge was retrofitted and a second, upper deck was added above the original trolley deck, to accommodate heavier automobile traffic. A usable concrete arch bridge originally completed in 1908-1910 already existed at the 7th Street crossing in 1927 when the viaduct was designed by Merrill Butler, City of Los Angeles. City engineers elected to retain that arch bridge and use it as a base upon which to pour concrete piers to support the girder spans for the longer viaduct, which accounts for its resulting double-decked appearance (Snyder, John W., Stephen Mikesell, and Pierzinski, 1986). Construction of the second deck required excavating the trenches of the old deck, installing new support bents, attaching shear walls to abutments, filling in the historic railings on the original lower deck, and extending the approaches passing over the railroad tracks (National Park Service, 2000) (**Figure 20**).



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SOURCE: USC Special Collections

Figure 20
Construction of the second upper deck on the 7th Street Bridge, 1927

The 7th Street Bridge has undergone extensive seismic structural retrofits and earthquake repairs at different times. In 1981-1983, deformed threaded prestressing steel bar seismic restrainers and structural steel bearing plates were added to existing expansion joints to strengthen 7th Street Bridge (City of Los Angeles 1981-83). The 1994 and 1995 plans for extensive seismic repairs to 7th Street Bridge included the following elements: piers; abutments 2 and 3; the bent columns; the east and west approach abutments, including shear walls and foundations; longitudinal and transverse concrete restrainers at piers 1 and 2; seismic restrainers at bents 2, 5, 11, 14, 17 and 20; and seismic restrainers at the east and west abutments (City of Los Angeles September and October 1994, and October 1995). Plans showing structural damage and spalling from the 1994 earthquake and proposed repairs were completed in October 1994 (City of Los Angeles October 1994), and a bridge lighting rehabilitation program was undertaken in 1995 (City of Los Angeles 1995). During the 1995 seismic retrofitting, the original historic railings on the upper deck, which were previously altered and filled in, were reconstructed as replacements in-kind to match their 1927 appearance. Also, during the same period of time, the bridge lighting was rehabilitated, and sidewalks and stairs were replaced.

Today, a large number of the railings on the deck have been replaced in-kind in accordance with the Secretary of Interior Standards (**Figure 21**) There are approximately 1,475 linear feet of this railing on the south side of the 7th Street Bridge and another 1,222 linear feet of railing on the north side, for a total of 2,697 total linear feet of railing. The 7th Street Bridge currently comprises both the original circa 1910 closed spandrel arch construction and the 1927 “simply supported reinforced concrete bridge built on top of the earlier span” (National Park Service, 2000: 26).



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SOURCE: ESA, 2018

Figure 21

New railings cast from original mold to match the appearance of the railings in 1927

Adjacent Railway Properties

The Railway Properties consist of several lines of active trackage located adjacent to the Project Site (see **Figure 4**). The BNSF uses the railroad lines along the Los Angeles River primarily for freight trains, while the other tracks are utilized by Amtrak and Metro for passenger trains. The Railway Properties (adjacent to Project Site) is separate from Survey Area # 8 (within Project Site). The Railway Properties includes the active BNSF Railroad, which is active trackage, located adjacent to the Project Site and further described below in the Archival Research section of this report.

In 1859, the Kansas legislature chartered the Atchison, Topeka Railroad Company, soon known as the AT&SF. Within California, the AT&SF began construction in 1881 under the original California Southern Railroad Company, which was acquired by the AT&SF in 1885. In 1885 the California Southern Line was extended from San Bernardino to Barstow to meet up with the Atlantic & Pacific Railroad, which “was jointly owned by the Santa Fe and the St. Louis & San Francisco Railroad Companies” (Holterhoff Jr., 1914: 5-6). Once completed, AT&SF provided service from Kansas City to Los Angeles. In 1886, the AT&SF expanded their operation in the Los Angeles and was granted a 50-foot right-of-way by the City to build a flood embankment near the current 1st Street Bridge. AT&SF laid additional tracks and by 1890, Santa Fe Avenue was created (McAvoy, et al., 2003: Section 8, 1). In 1893, to better cater to passenger traffic, the AT&SF constructed the Moorish Revival-style La Grande Railroad Station at the corner of East 2nd Street and Santa Fe Avenue, just south of the 1st Street Bridge (**Figure 22**). In 1906, to better outfit the growing demand of freight trains, the AT&SF built its first freight depot. The AT&SF ceased operation in 1996 when it merged with the Burlington Northern Railroad, and formed the BNSF Railroad (Switzer, 1994: 1 and 10). In 2001, Sci-Arc school of architecture moved to the

Santa Fe Freight Depot designed by Harrison Albright and rehabilitated the building. In 2006, the Santa Fe Freight Depot was listed on the National Register of Historic Places (NRHP #05001488).



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SOURCE: Security Pacific National Bank Collection, Los Angeles Public Library

Figure 22

La Grande Railroad Station and Santa Fe Railyard, located on the corner of 2nd and Santa Fe, 1924

Adjacent Bailey Schmitz Co. Building

One building adjacent to the Project Site on the west, 2101 E. 7th Street, is a three-story brick utilitarian industrial building constructed in 1910 for the Bailey Schmitz Co., a mattress batting manufacturer who owned the building until 1983. Assessor records indicate that the unreinforced masonry Bailey Schmitz Co. building was constructed in 1910. According to Davison's 1919 Mattress Directory, the Bailey Schmitz Co., Inc., was a manufacturer of mattress batting and felt in Los Angeles at 7th Street and Santa Fe Avenue (Davidson, 1918). Sanborn maps from 1950, 1951, and 1955 show the building continued to be occupied by Bailey Schmitz Co., Inc., mattress manufacturer, through the 1950s. Review of City building permits indicate the building was substantially altered over the years with numerous permits for additions, alterations, and repairs on file. The Bailey Schmitz Co. continued to own the building until 1983.

Archival Research

SCCIC Records Search

Methods

A records search for the Project Site was conducted on January 24, 2018, at the California Historical Resources Information System (CHRIS) South Central Coastal Information Center (SCCIC) housed at California State University, Fullerton. The records search included a review of all previously recorded archaeological resources and historic architectural resources within the Project Site and a 0.5-mile radius of the Project Site. The records search also included a review of California Points of Historical Interest, California Historical Landmarks, the California Register, the National Register, the California State Historic Resources Inventory listings, the Caltrans Statewide Historic Bridge Inventory, and the List of Los Angeles Historic-Cultural Monuments. The data gleaned from this record search was used to supplement the evaluation of historic resources and assessment of archaeological sensitivity.

Results

The records search results indicate that 33 cultural resources studies have been conducted within a 0.5-mile radius of the Project Site (**Table 1**). Approximately 75 percent of the 0.5-mile records search radius has been included in previous cultural resources studies. Of the 33 previous studies, eight (LA-4834, -8252, -10506, -10887, -11048, -11409, -11642, and -11785) overlap the Project Site.

TABLE 1
PREVIOUS CULTURAL RESOURCES INVESTIGATIONS

Author	SCCIC# (LA-)	Title	Year
Anonymous	LA-02966	Draft Stage I Environmental Site Assessment Eastside Extension (from Whittier Boulevard and Atlantic Boulevard Intersection to Union Station Area) Metro Red Line Los Angeles, California	1993
Anonymous	LA-09844	Draft: Los Angeles Eastside Corridor, Revised Cultural Resources Technical Report, Final Supplemental Environmental Impact Statement/Final Subsequent Environmental Impact Report	2001
*Ashkar, Shahira	LA-04834	Cultural Resources Inventory Report for Williams Communications, Inc. Proposed Fiber Optic Cable System Installation Project, Los Angeles to Anaheim, Los Angeles and Orange Counties	1999
Ashkar, Shahira	LA-04835	Cultural Resources Inventory Report for Williams Communications, Inc. Proposed Fiber Optic Cable System Installation Project, Los Angeles to Riverside, Los Angeles and Riverside Counties	1999
Bonner, Wayne H. and Sarah A. Williams	LA-08733	Cultural Resources Records Search Results and Site Visit for Sprint Nextel Telecommunications Facility Candidate Ca8283e (van Wyck) 601 South Santa Fe Avenue, Los Angeles, Los Angeles County, California	2006
Bray, Madeleine, Michael Vader, and Monica Strauss	LA-12665	Final Archaeological Resources Monitoring Report for the Los Angeles Department of Water and Power La Kretz Innovation Campus Project, Los Angeles County, California	2015
Brown, Joan C.	LA-02788	Archaeological Literature and Records Review, and Impact Analysis for the Eastside Corridor Alternatives Los Angeles, California	1992
Chasteen, Carrie	LA-10451	Finding of Effect - Sixth Street Viaduct Seismic Improvement Project	2008

Author	SCCIC# (LA-)	Title	Year
*Daly, Pam and Nancy Sikes	LA-11642	Westside Subway Extension Project, Historic Properties and Archaeological Resources Supplemental Survey Technical Reports	2012
Foster, John M. and Roberta S. Greenwood	LA-03923	Archaeological Investigations at Maintenance of Way Facility, South Santa Fe Avenue (CA-LAN-2563h)	1998
Fulton, Phil	LA-12381	Cultural Resources Assessment Class I Inventory, Verizon Wireless Services Metro Relo Facility City of Los Angeles, Los Angeles County, California	2013
Glenn, Brian and Patrick Maxon	LA-12586	Archaeological Survey Report for the Sixth Street Viaduct Improvement Project City of Los Angeles Los Angeles County, California	2008
Greenwood, Roberta S.	LA-03103	Cultural Resources Impact Mitigation Program Angeles Metro Red Line Segment 1	1993
*Greenwood, Roberta S., Scott Savastio, and Peter Messick	LA-10506	Cultural Resources Monitoring: North Outfall Sewer - East Central Interceptor Sewer Project	2004
Gust, Sherri	LA-13239	Extent of Zanja Madre	2017
*Horne, Melinda C.	LA-11409	Construction Phase Cultural Resources Monitoring and Treatment Plan for the City of Los Angeles North Outfall - East Central Interceptor Sewer Project	2000
McMorris, Christopher	LA-07425	City of Los Angeles Monumental Bridges 1900-1950: Historic Context and Evaluation Guidelines	2004
McMorris, Christopher	LA-07427	Caltrans Statewide Historic Bridge Inventory Update: Metal Truss, Movable, and Steel Arch Bridges	2004
Ohara, Cindy L.	LA-04074	Sixth Street Viaduct Over Los Angeles River Earthquake Damages - W.O. E6000000 Determination of Effect Report	1989
Perez, Don	LA-12349	Metro Relo Starkman/Ensite #11976, 544 Mateo Street, Los Angeles, Los Angeles County	2012
Ramirez, Robert S. and Robin D. Turner	LA-08635	A Phase I Cultural Resource Assessment and Paleontological Assessment for the Proposed Los Angeles Department of Water and Power Distribution Center #144 in the City of Los Angeles, Los Angeles County, California	2008
Richard Starzak	4448	Section 106 Documentation for the Metro Rail Red Line East Extension in the City and County of Los Angeles, California	1994
*Rogers, Leslie	11785	Final Environmental Impact Statement/Final Environmental Impact Report for the Westside Subway Extension	2012
Romani, John F. and Jerry Kleeb	03813	Assessment of the Archaeological Impact of the Proposed Widening and Realignment of Mulholland Scenic Parkway (w.o. 61710) Between Encino Hills Drive and Canoga Avenue (sub Purchase Order 071548)	1975
Smith, Francesca	10452	Historical Resources Evaluation Report - Sixth Street Viaduct Seismic Improvement Project	2007
*Snyder, John W., Stephen Mikesell, and Pierzinski	08252	Request for Determination of Eligibility for Inclusion in the National Register of Historic Places/Historic Bridges in California: Concrete Arch, Suspension, Steel Girder and Steel Arch	1986
*Speed, Lawrence	11048	American Recovery and Reinvestment Act (ARRA) Funded Security Enhancement Project (PRJ29112359) - Improved Access Controls, Station Hardening, CCTV Surveillance System, and Airborne Particle Detection at Los Angeles Station and Maintenance Yard, LA, CA	2009
Starzak, Richard	04625	Historic Property Survey Report for the Proposed Alameda Corridor From the Ports of Long Beach and Los Angeles to Downtown Los Angeles in Los Angeles County, California	1994
*Starzak, Richard, Alma Carlisle, Gail Miller, Catherine Barner, and Jessica Feldman	10887	Historic Property Survey Report for the North Outfall Sewer-East Central Interceptor Sewer, City of Los Angeles, County of Los Angeles, California	2001

Author	SCCIC# (LA-)	Title	Year
Tang, Bai "Tom"	10638	Preliminary Historical/ Archaeological Resources Study, Southern California Regional Rail Authority (SCRRA) River Subdivision Positive Train Control Project, City of Los Angeles, Los Angeles County, California	2010
Taniguchi, Christeen	08518	Historic Architectural Survey and Section 106 Compliance for a Proposed Wireless Telecommunications Service Facility Located on a Warehouse Building in the City of Los Angeles (Los Angeles County), California	2004
Wlodarski, Robert J.	02577	Results of a Records Search Phase Conducted for the Proposed Alameda Corridor Project, Los Angeles County, California)	1992
Wlodarski, Robert J.	02644	The Results of a Phase 1 Archaeological Study for the Proposed Alameda Transportation Corridor Project, Los Angeles County, California	1992

*Indicates study overlaps the Project Site

The records search results indicate that 16 cultural resources have been previously recorded within a 0.5-mile radius of the Project Site, including five archaeological sites and 11 historic architectural resources (**Table 2**). All of the five archaeological sites (P-19-004192 (historic refuse scatter), -004193 (historic road), -004460 (city block with historic refuse), -186112 (Union Pacific Railroad and Southern Pacific Railroad), and -186804 (BNSF Railroad) are historic-period archaeological sites. The 11 buildings and structures include the following: 19-175070 (7th Street Bridge), -188524 (the 6th Street Bridge, which has been demolished) -188537 (Michelin, Lumary's Tire Service Inc.), -188538 (Gasket Manufacturing Co. Warehouse), -188539 (Mrs. Lee's Pies), -188985 (Engine Co #17), -188986 (J.J. Heinz Co Warehouse), -189094 (Joe Toplitzky Warehouse), -189095 (industrial building), -189096 (Anna Broughton Warehouse), and HCM No. 888, National Biscuit Company Building. Of these 11 historic architectural resources, one structure has been demolished since the time it was recorded, 19-188524 (6th Street Bridge). Two are eligible historical resources, Engine Co. #17 (P-19-188985), H J Heinz Co. Warehouse (P-19-188986). Three are designated historical resources, as follows: the 7th Street Bridge (P-19-175070); the 6th Street Bridge (P-19-188524) (now demolished); and 1850 E. Industrial Street, the National Biscuit Company Building (HCM No. 888). None of the 16 existing resources are located within the Project Site, itself; however, several resources have been recorded adjacent to the Project Site, including historic archaeological resources—the BNSF Railroad (P-19-186804), and the Union Pacific Railroad and Southern Pacific Railroad, Los Angeles Division alignment (P-19-186112)—and a historic architectural resource, the 7th Street Bridge (P-19-175070), which are described in greater detail below. While these resources are not located on the Project Site, the Project does include the possibility of a Deck over the Railway Properties and a connection from the Project Site to the 7th Street Bridge, as is described in the Project with the Deck Concept.

**TABLE 2
PREVIOUSLY RECORDED CULTURAL RESOURCES**

P-Number (P-19-)	Permanent Trinomial (CA-LAN-)	Other Designation	Description	Date Recorded	Eligibility and Status Code	Distance From Project Site
004192	004192H		Historic-period refuse	2010	Unevaluated	
004193	004193H		Historic period road	2010	Unevaluated	
004460	004460H		Historic period trash deposits, railroad, and foundations	2014; 2016	Unevaluated	
*175070		7th Street Bridge (7th Street Viaduct, Bridge 53C-1321)	Historic period concrete arch, double-deck bridge, built 1908-1910; addition 1927	1986	Determined Eligible for National Register and listed in the California Register-OHP Property Number: 294422; (2S2); locally designated (5S1) HCM No. 904 (2008)	Adjacent
*186112		Union Pacific Railroad, Southern Pacific Railroad Los Angeles Division	Historic-period railroad	1999; 2002; 2009	6Z (ineligible)	
*186804		Burlington Northern & Santa Fe Railroad, Atchison Topeka & Santa Fe RR	Historic-period railroad	2002; 2007; 2011	6Z (ineligible)	
188524		6th Street Bridge	Historic period bridge (demolished)	1987; 2007; 2011	Determined Eligible for National Register; and Listed in the California Register-OHP Property Number: 114990 (2S2); locally designated (5S1) HCM No. 905 (2008)	
188537		Michelin, Lumary's Tire Service Inc, 600-602 Santa Fe Avenue (No. 110)	Historic period commercial building, built ca. 1946-1948	2007	6Z (ineligible)	
188538		1450 E. 6th Street (No. 115)	Historic period industrial building, built 1952	2007	6Z (ineligible)	

P-Number (P-19-)	Permanent Trinomial (CA-LAN-)	Other Designation	Description	Date Recorded	Eligibility and Status Code	Distance From Project Site
188539		Mrs. Lee's Pies, 605 South Santa Fe Avenue	Historic period industrial building, 1953	2007	6Z (ineligible)	
188985		Engine Co #17, 708 South Santa Fe Avenue	Historic period public utility building built 1927	1999	3S (eligible for National Register)	0.05 miles (250 feet) SW of Project Site and South of E. 7th Street. Oriented west facing S. Santa Fe Avenue and separated from Project Site by intervening development; no views of Project Site from West front façade
188986		H J Heinz Co, Warehouse, 712 South Santa Fe Avenue	Historic period industrial building, built 1912	1999	3S (eligible for National Register)	0.06 miles (300 feet) SW of Project Site and South of E. 7th Street; oriented facing West toward S. Santa Fe Avenue and separated from Project Site by intervening development; no views of the Project Site from West front façade of building, which faces Santa Fe Avenue
189094		Joe Toplitzky, Warehouse, 680-682 South Santa Fe Avenue	Historic period industrial building built 1922	1999	6Y1 (ineligible for National Register; not evaluated at the local level)	0.06 miles (300 feet) West of Project Site; oriented facing S. Santa Fed Avenue and separated from Project Site by intervening development; no views of Project Site from West front façade of building, which faces S. Santa Fe Avenue

P-Number (P-19-)	Permanent Trinomial (CA-LAN-)	Other Designation	Description	Date Recorded	Eligibility and Status Code	Distance From Project Site
189095		720 South Santa Fe Avenue	Historic period industrial building, built ca. 1916	1999	6Y1 (ineligible for National Register; not evaluated at the local level)	0.09 miles (450 feet) SW of Project Site; oriented facing S. Santa Fed Avenue and separated from Project Site by intervening development; no views of Project Site from West front façade of building, which faces S. Santa Fe Avenue
189096		Warehouse for Anna Broughton, 800-806 South Santa Fe Avenue	Historic period industrial building built 1922	1999	6Y1 (ineligible for National Register; not evaluated at the local level)	0.10 miles (544 feet) SW of Project Site; oriented facing S. Santa Fed Avenue and separated from Project Site by intervening development; no views of Project Site from West front façade of building, which faces S. Santa Fe Avenue.
N/A	N/A	National Biscuit Company Building, 1850 E Industrial Street	Historic period industrial building built 1925	Adopted 10/9/2007	HCM No. 888	0.20 mi (1,059 feet); separated from Project Site by intervening development; no views of Project Site

*Adjacent to the Project Site

Burlington Northern & Santa Fe Railroad, Atchison Topeka & Santa Fe Railroad (19-186804)

The Project Site is bordered on the east by the Railway Properties, which is in turn bordered on the east by the Los Angeles River. Within the Railway Properties, existing rail lines include the two tracks closest to the Los Angeles River, which are owned and operated by Metro for passenger travel. The four central tracks west of the Metro tracks are presently owned and operated by BNSF Railroad (19-186804) for freight travel, and the westernmost tracks are owned and operated by Amtrak and Metro for passenger travel. The segment ranges in width from 153 feet near 6th Street to 200 feet at its widest point at the approximate midpoint of the Project Site,

and 190 feet at 7th Street. There are nine tracks adjacent to the northern end of the Project Site, near 6th Street, expanding to eleven tracks closer to the 7th Street Bridge.

The BNSF Railroad right-of-way located within the Railway Properties, is historic in age and was recorded as 19-186804 in the CHRIS system. It was originally owned by the transcontinental AT&SF Railroad and then later became the BNSF Railroad. In 2007, a segment of the BNSF Railroad that overlaps with the portion of the BNSF Railroad tracks located adjacent to the Project Site was evaluated (Smith and Harper, 2007). The evaluation does not record or evaluate the full extent of the larger transcontinental BNSF Railroad; instead, smaller segments have been recorded on a project-by-project basis. The study found only that the segment is part of the larger BNSF Railroad system and characterized the appearance of the segment at the time of the survey as modestly recognizable with respect to its original appearance. However, the remaining tracks have been reconfigured and altered by maintenance for more than 100 years. The Smith and Harper evaluation also found that the landscape had also changed considerably in the last century and no longer retains integrity of setting. The evaluation assigned this segment of the BNSF Railroad a 6Z status, indicating that the segment is not eligible for listing as an historical resource at the national, state, or local levels under applicable criteria. Although it is noted that the entire railroad line is likely eligible for the National and California Registers, this segment of the BNSF Railroad does not retain sufficient integrity to qualify.

6th Street Bridge (Resource P-19-188524)

The 6th Street Bridge (1932) was demolished in 2016 but was originally located on 6th Street between Mateo Street and S. Boyle Avenue, adjacent to the Project Site's north boundary. It was previously determined eligible for the National Register and listed in the California Register. It was also designated Caltrans Bridge 53C1880 and HCM No. 905, the 6th Street Bridge (Viaduct) was an important engineering landmark in the City of Los Angeles, with an overall length of 3,500 feet it was the longest of a set of 14 historic Los Angeles River crossing structures. The concrete in the 6th Street Bridge had been subject to Alkali Silica Reaction (ASR) causing significant deterioration of the structure. A new bridge is expected to be complete in its place in 2022, extending nearly a mile from Boyle Heights over the Los Angeles River and into downtown.

7th Street Bridge (19-175070)

The 7th Street Bridge is located at the south end of the Project Site. It was originally constructed between 1908 and 1910 by Henry Huntington's Los Angeles Railway company. It was built as a reinforced concrete close-spandrel arch bridge of three 80-foot clear spans. The impetus for the construction of the 7th Street Bridge was the rapidly growing population of the City and the need for an electric railcar system, which the bridge originally was constructed to accommodate. The 7th Street Bridge's original design did not attempt to bypass the AT&SF and Salt Lake Railroad rail lines on either side of the Los Angeles River; at the time, the 7th Street Bridge deck was at the same grade as the rail lines, which intersected the approaches to the bridge on either side with railroad crossings. This conflict led to the 7th Street Bridge's redesign in 1918, when the railroad tracks on either side of the bridge were depressed and the bridge was extended over the tracks, and additional bents and decking were added to allow freight and

passenger traffic under the bridge and not disrupt traffic. In 1927, the 7th Street Bridge was retrofitted and a second, upper automobile deck was added above the original trolley deck, to accommodate heavier automobile traffic due to increasing transportation needs. Construction of the second deck required excavating the trenches of the old deck, installing new support bents, attaching shear walls to abutments, filling in the historic railings on the original lower deck, and extending the approaches passing over the railroad tracks (National Park Service, 2000). In 1981-83 and 1994-1995, the 7th Street Bridge underwent extensive seismic retrofitting and repairs, and in 1995 the railings on the upper deck that had been previously altered were replaced in-kind to their 1927 appearance and the bridge lighting fixtures were rehabilitated. Today, the 7th Street Bridge is a double-deck bridge with a lower reinforced concrete closed-spandrel arch bridge with Beaux Arts elements and an upper reinforced open-spandrel concrete bridge built atop the original bridge (**Figures 23-25**).

The 7th Street Bridge adjacent to the Project Site (also known as the Seventh Street Viaduct, Bridge 53C-1321) (19-1754070) was previously evaluated in 1986 as part of the Caltrans Statewide Historic Bridge Inventory conducted for the Federal Highway Administration and determined eligible for the National Register by consensus through the Section 106 process as an example of a concrete arch bridge included in a thematic group of significant bridges across the Los Angeles River (Caltrans, 2018). Because the bridge was determined eligible to the National Register, it was automatically placed on the California Register, as is standard procedure for all properties determined eligible to the National Register. Subsequently, it was assigned a CHR status code of 2S2, meaning that it is an “Individual property determined eligible for NR [the National Register] by a consensus through Section 106 process. Listed in the CR [California Register].” In 2007, the 7th Street Bridge was surveyed by the City of Los Angeles Office of Historic Resources; the following year, in 2008, the 7th Street Bridge was locally designated as City of Los Angeles HCM No. 904. Subsequently, it was assigned a CHR status code of 5S1, meaning that it is an “individual property that is listed or designated locally.”



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 23
Current photo of the 7th Street Bridge, north elevation,
view southeast



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 24
Area of potential connection to the 7th Street Bridge,
view northwest



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 25

A section of the 7th Street Bridge where the railings have been altered, view north

Union Pacific Railroad, Southern Pacific Railroad, Los Angeles Division (Resource P-19-186112)

The Project Site is bordered on the south by a former section of the Union Pacific Railroad and Southern Pacific Railroad, Los Angeles Division, which ran down 7th Street. The record for the Los Angeles Division was recorded based on mapping and only a section of the segment was examined. While it is no longer visible in this location, to the south of the project, it is possible that it is still extant under the pavement but unknown at this time. It was assigned a CHR status code of 6Z, meaning that the segment was “found ineligible for NR [the National Register], CR [the California Register] or Local designation through survey evaluation.” Therefore, the segment is not eligible for listing as an historical resource at the national, state, or local levels under applicable criteria.

City of Los Angeles Planning Reports, Historic-Cultural Monuments (HCM), and SurveyLA

Methods

Additional archival research included the review of Department of City Planning Reports, the List of City of Los Angeles HCM designations, and SurveyLA findings for the Project Site and within a 0.25-mile radius. For the purpose of analyzing potential indirect impacts, previously identified historical resources within 0.25 miles of the Project Site were located to determine if any are in the immediate vicinity of the Project Site, and in sufficient proximity that the Project could have the potential to result in indirect impacts affecting the eligibility of such resources for listing.

Results

The review of Department of City Planning Reports, HCM designations, and SurveyLA findings found one historic architectural resource within the Project Site, the Los Angeles Ice and Cold Storage Company (Rancho Cold Storage) located at 674-686 S. Mesquit Street, that was included in SurveyLA Central City North Survey but found ineligible for designation due to lack of integrity. It was identified as a rare example of an early-20th century cold storage facility in Los Angeles' primary industrial district. Research indicates that this property has operated as a cold storage facility since 1905. Historical aeriels suggest that the southeastern-most portion of the existing building—the portion with a gabled and monitor roof situated along the railroad tracks—may be the original 1905 building. However, this building has been substantially altered, and is now surrounded on three sides by later construction. Therefore, this property does not retain sufficient integrity to be eligible for listing. The property was assigned an 7SQ status code: Not eligible; individual property assessed for significance in accordance with SurveyLA but does not meet eligibility standards (SurveyLA, 2016; HistoricPlacesLA.org).

Within 0.25 miles of the Project Site, one known historic industrial building currently undergoing rehabilitation in the Project vicinity, and three designated HCM resources were identified, along with one potential historic district, the Los Angeles Industrial Historic District, recorded by SurveyLA in 2016. The identified resources are compiled in **Table 3**. The district is described briefly below.

TABLE 3
PREVIOUSLY IDENTIFIED HISTORICAL RESOURCES (HCM, SURVEYLA, CITY OF LA HRI)
WITHIN 0.25- MILE RADIUS OF THE PROJECT SITE

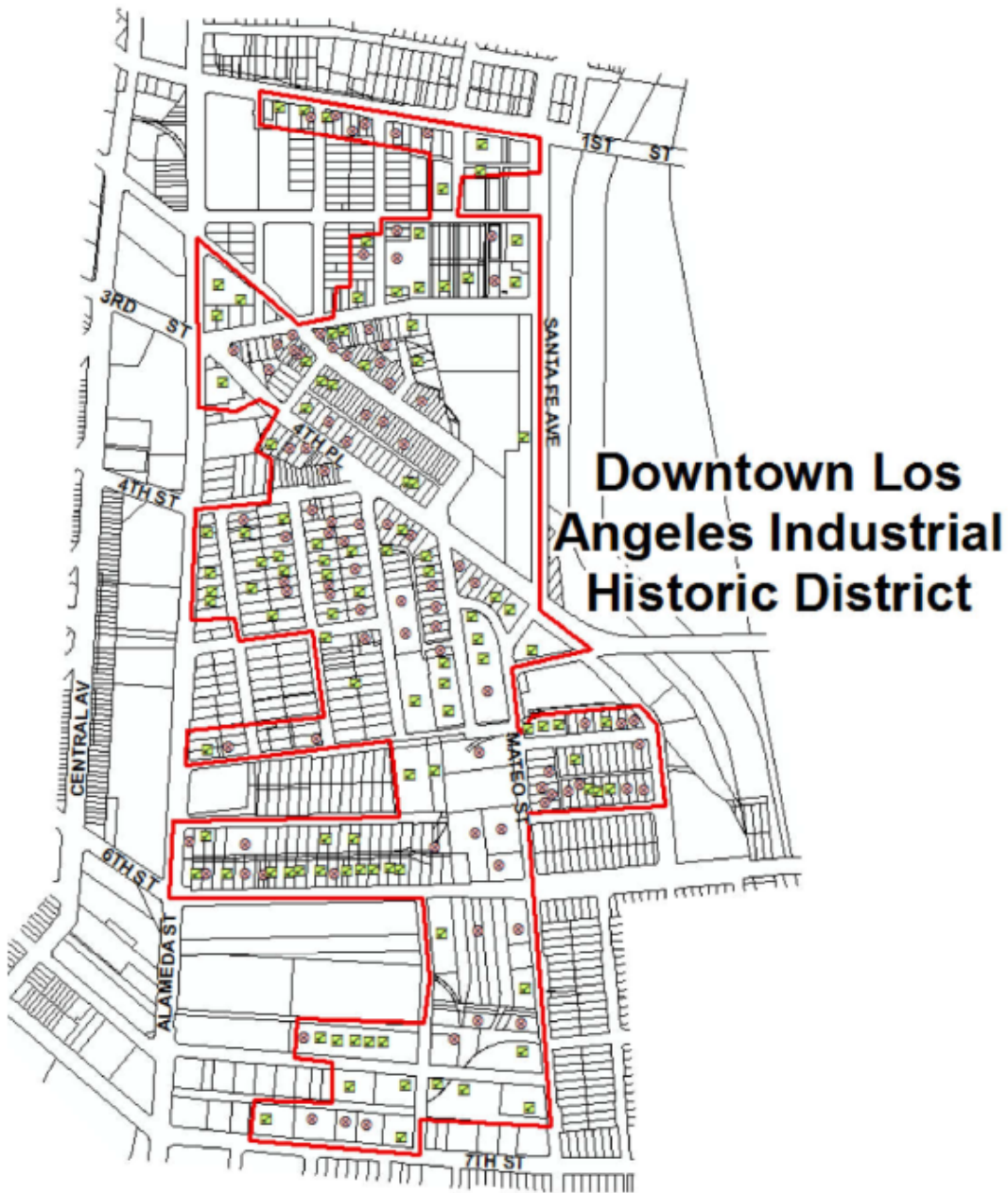
Name and Address	Description	CHR Status Code	Date Recorded	Distance from Project Site
Ford Motor Company Factory Building 777 Santa Fe Avenue and 2046 E. 7 th Street (7 th & Santa Fe)	Industrial Building; 1923; Beaux Arts Classicism style; recently rehabilitated for adaptive reuse as creative office for Warner Music Group. The Ford Motor Company Plant is noted in the SurveyLA Citywide Historic Context Statement, Industrial Development (1850-1980), pages 151-153, as the only extant automobile manufacturing-related resource left in Los Angeles	Identified as a known historical resource in the 2143 Violet Initial Study – Environmental Checklist (Case Number: ENV-2017-438-EIR) (City of Los Angeles Department of City Planning 2018, page B- 13). Determined eligible for the National Register.	2018	0.09 mi (475.2 feet); direct view from Ford Motor Company Factory Building northeast toward Project Site from north façade at southwest corner of S. Santa Fe Avenue and 7 th Street
National Biscuit Company Building 1820 E. Industrial Street	Industrial Building; 1925; Beaux Arts Classicism style; Eckel and Aldrich, architects	5S1, HCM 888	2014	0.20 mi (1,059 feet); separated from Project Site by intervening development; no views of Project Site
6 th Street Bridge, No. 53C1880	6 th Street Bridge over the Los Angeles River	5S1, HCM 905	2008	demolished
7 th Street Bridge, No. 53C1321	7 th Street Bridge over the Los Angeles River	5S1, HCM 904	2008	Adjacent to Project Site

Los Angeles Industrial Historic District (**Figure 26**), which does not include or overlap with the Project Site, has boundaries identified as Alameda Street to the west, portions of 1st and 4th Streets to the north, Santa Fe Avenue, Mateo Street, and the Los Angeles River to the east, and 6th and 7th Streets to the south. The majority of the district is located between 0.25 mile to 0.8-mile northwest of the Project Site and is separated from the Project Site by intervening industrial development. There are three contributing buildings within approximately 0.25 miles of the Project Site:

- 635 S. Mateo Street, Romanesque Revival style industrial building, built in 1929 approximately 0.20 mile (1,056 feet) to the northwest of the Project Site; no view of the Project Site.
- 1820 E. Industrial Street, National Biscuit Company, a Beaux Arts style industrial building, built in 1925 approximately 0.20 mile (1,056 feet) to the northwest of the Project Site; no view of the Project Site.
- 1855 E. Industrial Street, vernacular/Neoclassical industrial storage/warehouse building, built in 1924 approximately 0.20 mile (1,059 feet) to the northwest of the Project Site; no view of the Project Site.

Central City North

Historic Districts, Planning Districts, and Multi-Property Resources - 09/04/16



SOURCE: SurveyLA: Central City North, Historic Districts, Planning Districts, and Multiple-Property Resources, page 5, 9/04/2016

Figure 26
Map of Downtown Los Angeles Industrial Historic District

Additional Research

Methods

Site-specific research was conducted utilizing building permits, Sanborn Fire Insurance maps, City directories, historical photographs, California Index, Avery Index, Online Archive of California, Calisphere, California State Archives, California State Library, University of California Santa Barbara (UCSB) Frame Finder (aerial imagery), historic newspapers, and other published sources. Research was conducted at the Los Angeles Public Library, City of Los Angeles Department of Building and Safety, and Los Angeles County Assessor.

Results

Historic Maps and Aerial Photographs

Historic maps and aerial photographs were examined to provide historical information about land uses of the Project Site and to contribute to an assessment of the Project Site's archaeological sensitivity. This information was incorporated into the "History of the Project Site". Available topographic maps include the 1894 Los Angeles 15-minute quadrangle, 1896 Santa Monica 15-minute quadrangle; 1900 Pasadena and Los Angeles 15-minute quadrangles; 1928, 1953, 1966, 1972, 1981, 1994, 2012 Los Angeles 7.5-minute quadrangles. Sanborn Fire Insurance maps were available for the years 1890, 1894, 1900, 1906, 1950, 1953, 1954, 1959, 1960, 1967, and 1970. Historic aerial photographs were available for the years 1923, 1928, 1938, 1948, 1952, 1964, 1977, 1979, 1983, 1989, 1994, 2002, 2005, 2009, 2010, and 2012 (EDR, 2016).

Building Permits

As previously stated, the Project Site comprises eight assessor parcels (see Figure 4). To clearly describe the construction history of the Project Site, building permits are discussed in conjunction with the corresponding APNs. Building permits were obtained from the LADBS and are presented in Table 1 of **Appendix C** of this report.

Sacred Lands File Search

The NAHC maintains a confidential Sacred Lands File (SLF) that lists sites of traditional, cultural, or religious value to the Native American community. In some instances, these sites can qualify as historical resources or unique archaeological resources under CEQA. ESA contacted the NAHC on March 15, 2018, to request a search of the SLF. The NAHC responded to the request in a letter dated March 16, 2018. The results of the NAHC SLF search indicate that Native American cultural resources are not known to be located within the Project Site; however, the area is considered sensitive for cultural resources (**Appendix D**). Assembly Bill 52 (AB 52) consultation conducted for the Project is separately documented in the *670 Mesquit Project, City of Los Angeles, California Assembly Bill 52 Consultation Summary Report*, (ESA 2021), provided as Appendix N of the Draft EIR.

Geoarchaeological Review

The Project Site is situated in the Los Angeles Basin, a sedimentary basin approximately 50 miles long and 20 miles wide (Ingersoll and Rumelhart, 1999). The Los Angeles Basin is bounded on

the north and east by the Santa Monica Mountains and Puente, Elysian, and Repetto Hills (Yerkes et al., 1965). The Los Angeles Basin formed between 18 and 3 million years ago as a result of tectonic subsidence (Critelli et al., 1995). Continuous sedimentation into the Los Angeles Basin began during the middle Miocene around 13 million years, as thousands of feet of sediments were deposited in a marine environment (Yerkes et al., 1965). Deposition of terrestrial alluvial sediments commenced after sea level dropped during the Pleistocene.

Surface geology of the Project Site is mapped as Holocene-aged Quaternary alluvium (Dibblee and Ehrenspeck, 1989). The sediments consist primarily of well-sorted, unconsolidated silts and sands representing overbank flooding from the Los Angeles River. These fine-grained sediments are interbedded with coarser grained sand and gravels deposited within former channels. The Holocene-aged alluvium is underlain by older Pleistocene-aged alluvium. The older alluvium consists of gravel, sand and silt, and is weakly consolidated (dense), differentiating it from the younger alluvium. The depth at which younger alluvium transitions to older alluvium has not been determined in the Project Site. The older alluvium is underlain by marine and nonmarine bedrock of the Fernando Formation (Dibblee and Ehrenspeck, 1989). The top of the Fernando Formation has been found at 200-250 feet bgs in the vicinity of the Project Site (Yerkes et al., 1977).

Geotechnical analysis of the Project Site indicates that artificial fill (historic disturbance layer) appears to be shallow, to depths of 5-6 feet bgs recorded in borings at the site (Twining Consulting, 2018). Due to the age of development on the Project Site, it is unlikely that the artificial fill layer represents sediments brought on to the site but actually represents the depth of historic disturbance activities such as site preparation or demolition and rebuilding. Below the artificial fill is Holocene-aged younger Quaternary Alluvium, as mapped by Dibblee and Ehrenspeck (1989). Geotechnical analysis is not always able to identify a clear division between newer and older alluvium because of limited sample size (i.e., number of borings), potential historic disturbance (i.e., chaotic flood deposits) of the alluvial layers that may prevent clear stratification, interfingering of layers (i.e., old and new layers intermixed), and the fact that core samples were not dated, as that was beyond the scope of work for this geotechnical analysis. The depth at which younger alluvium transitions to older alluvium has not been determined in the Project Site. To the northwest and north of the Project area, along the US-110 (Harbor) Freeway and US-101 (Hollywood) Freeway, a study correlating well and boring logs found that the depths of the older alluvium are highly variable, ranging from 10 to 200 feet bgs (Yerkes et al., 1977). At greater depths, Pleistocene-aged older Quaternary Alluvium and the Pliocene-aged Fernando Formation underlie the surficial Holocene-aged Quaternary Alluvium. The nearest outcrops of older alluvium to the Project Site are just east of the Hollywood Freeway, roughly 0.7 miles away (Dibblee and Ehrenspeck, 1989).

Channel deposits are less likely to bury and preserve intact archaeological sites; thus, coarse-grained younger alluvium is considered to have a lower sensitivity for buried archaeological sites. As noted above, the depth of transition from younger to older alluvium is unknown. However, the general principle that fine-grained floodplain sediments are more amenable to site preservation than coarse-grained channel bed deposits may be applied equally to Pleistocene-aged older alluvium provided the deposits date to the time of human presence in southern California. Shifts

in sediment grain size reflects changes in fluvial competence or energy and suggests a transition from a channel bed (sand and gravel) environment to a floodplain (silt and sand) environment. The vertical accretion of overbank flood deposits would be generally favorable for in situ burial and preservation of any archaeological sites located on the floodplain, which includes the Project Site; thus, fine-grained younger alluvium, such as is found within the Project Site, is considered to have a higher sensitivity for buried archaeological sites.

Site Survey

Survey Methods

A cultural resources survey was conducted to identify surface evidence of archaeological resources and historic architectural resources within the Project Site that could be directly impacted by the proposed Project and to identify historic architectural resources adjacent the Project Site that could be subject to indirect impacts by the proposed Project.

A historic architectural resources survey aimed at identifying buildings and structures 45 years or older within the Project Sites was conducted on November 17, 2016, by Senior Architectural Historian Christian Taylor, M.H.P. A second survey was conducted on April 5, 2018, by architectural historians Margarita Jerabek, Ph.D., Christian Taylor, M.H.P., and Ashley Brown, M.A. to identify any buildings and structures adjacent the Project Site that could be subject to indirect impacts. Existing on-site buildings and structures, as well as the adjacent surroundings, were photographed to document changes and to update the existing conditions.

On March 16, 2018, ESA staff Henry Chodsky, B.A., conducted an archaeological survey that focused on identifying surface evidence of archaeological resources within the Project Site. Areas with visible ground surface were subject to pedestrian survey using transect intervals spaced no more than 15 meters (approximately 50 feet) apart. Access was not available to Survey Area #8, but this area was photographed from a distance. California Department of Parks and Recreation (DPR) 523 forms for BNSF Railroad (P-19-186804) and 7th Street Bridge (P-19-186804) are attached in **Appendix E**.

Survey Results

Project Site Survey Area #1 (689 S. Mesquit Street [APN 5164-018-009]) contains one utilitarian industrial structure completed in 1963. Project Site Survey Area #2, (2143 E. 7th Street [APN 5164-017-008]) contains a cold storage warehouse constructed in 2002. Project Site Survey Areas #3-5 (642 and 674 S. Mesquit Street [APNs 5164-017-003, 5164-017-002, and 5164-017-006]) contain a complex of 10 attached buildings, the earliest of which was originally constructed in 1888; the original buildings were rebuilt in 1908 after a fire, with several later attached additions and alterations so that they currently have the appearance of one large cold storage and dry warehouse building complex (10 attached buildings). Project Site Survey Areas #6 and #7 (1494-1498 E 6th Street [APNs 5164-016-009 and 5164-016-010]) are currently undeveloped; buildings formerly located on these parcels have been previously demolished. Survey Area #8 encompasses the northern portion of the parcel (APN 5164-016-803) and consists of a former railroad right-of-way within the Project Site (not to be confused with the Railway Properties adjacent to the

Project Site). Survey Area # 8 is undeveloped, and portions are in use as railroad storage. In total, the potential historic architectural resources which are over 45 years in age and were evaluated as a result of the survey included one building (the 1963 industrial structure located in Survey Area #1) and one building complex (the 1908-1985 building complex in Survey Areas #3-5). No surface evidence of archaeological sites was encountered. Surveyors also documented condition of the adjacent resources, including the BNSF Railroad (P-19-186804), the 7th Street Bridge (P-19-175070), and the Bailey Schmitz Co. Building located at 2101 E. 7th Street.

Resources Descriptions and Evaluations

The following section presents architectural descriptions, significance evaluations, and integrity analysis for the surveyed historic architectural resources on the Project Site, organized by Survey Area; and for the BNSF Railroad (P-19-186804), 7th Street Bridge (P-19-186804) and 2101 E. 7th Street Bailey Schmitz Co. Building that are located adjacent to the Project Site. As previously discussed, the Union Pacific Railroad, Southern Pacific Railroad, Los Angeles Division (P-19-186112) is no longer visible in this location and was previously found ineligible for listing as a historical resource due to lack of integrity.

Survey Area #1

Architectural Description

Survey Area #1, 689 S. Mesquit Street (APN 5164-018-009), is a rectangular lot improved with a utilitarian industrial structure completed in 1963. The building has an irregular L-shaped footprint with multiple access points along the north elevation, including a pedestrian entrance and vehicle loading bays (**Figure 27**). Exterior walls are constructed of a combination of concrete blocks and reinforced concrete. The subject building has a flat combination roof with parapets extending above the roofline at various heights. The main entrance is located at the northeast corner of the primary (north) elevation. The building occupies half of the lot, with the other half reserved for vehicular parking and loading.



SOURCE: ESA 2016

670 Mesquit / D170431.00

Figure 27
Primary (north) elevation, view south

Primary Elevation (North)

The primary (north) elevation is dominated by a sunken truck loading dock featuring three loading bays. Behind the loading dock is a large open warehouse used as a staging area for goods being shipped. To the left of the loading bays is the subject building's primary entrance. The main entry, which leads to the building's office spaces, exhibits some minor decorative articulation characteristic of the of Mid-Century Modern Style that was popular at the time of the building's construction in 1963. This portion of the façade features a decorative parapet wall, made up of stacked brick (painted over), extending from the ground to beyond the building's roofline. A flat porch canopy extends from the building's façade, supported by three large projecting L-shaped metal posts and beams (later alteration). The posts and beams connect the building's façade with a small landscape feature (concrete planter). Together, the porch, canopy, planter, posts, and beams create a small outdoor foyer or patio (**Figure 28**). The main entry consists of a fully glazed aluminum framed door with sidelights and transoms (**Figure 29**) and is fortified with metal security bars and screens (alteration). The main entry is the only part of the building that is articulated in a decorative manner, as it was obviously intended to serve as the main point of contact with the public; the rest of the building is articulated in a utilitarian manner.

On the right side of the primary (north) elevation is a protruding mass extending northward, giving the building its L-shaped footprint. While the majority of the building is constructed out of reinforced concrete, the protruding mass is made up of concrete blocks. This section of the building houses the main cold storage areas. Along the east side of the mass is a metal canopy constructed out of steel I-beams with a corrugated steel roof (alteration). The canopy provides shade and shelter for a concrete path leading to two industrial size openings, one of which leads to the interior of the concrete block mass, while the other opening leads to the interior of the reinforced concrete warehouse (**Figure 30**). A metal railing (alteration) separates the concrete path from the adjacent sunken loading bays. Fenestration on the mass includes three small industrial style windows with metal security bars and screens (alterations) (**Figure 31**).



670 Mesquit / D170431.00

SOURCE: ESA, 2016

Figure 28

View of the primary entrance on the north elevation, view south. Steel bents appear to be later additions.



670 Mesquit / D170431.00

SOURCE: ESA, 2016

Figure 29

Closer view of the entryway on the primary (north) elevation, view south



SOURCE: ESA, 2016

670 Mesquit / D170431.00

Figure 30
Concrete walkway adjacent to loading bays, metal canopy and railing, view south



SOURCE: ESA, 2016

670 Mesquit Street / D170431.00

Figure 31
View of the building's projecting mass, north elevation, view south

Secondary Elevation (East)

The east elevation features tilt-up concrete construction with decorative textured concrete closer to the building's main entry (north end of the east elevation). The building possesses few architectural features along the east façade, exhibiting its industrial utilitarian design (**Figure 32**). Secondary entrances to the building are located near the south end of the east elevation and include a pedestrian entry with a single metal door and a larger vehicle entry with a metal roll-up door. Fenestration along the east elevation consists of three moderate sized window openings with security screens (alteration). The window openings have been boarded up (alteration), most likely due to the building's conversion from a factory to a cold storage warehouse.



SOURCE: ESA, 2016

670 Mesquit Street / D170431.00

Figure 32
Secondary (east) elevation, view southwest

South Elevation

The building's south elevation does not contain any significant architectural features. It is clad with concrete and there are no entryways or fenestration (**Figure 33**).

West Elevation

The west elevations were not accessible during the site survey. This elevation adjoins the neighboring lot, which is private property and admission to that property was not granted during the survey.



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 33
Overview of the South Elevation, view east

Interior

Originally constructed as factory for a quilt manufacturing company, the building shows signs of alteration in the interior, converting it for its current use as a cold storage warehouse. The interior is dominated by a large open area where goods preparing to be shipped are staged. The open area has been divided by new interior partitions, creating office spaces and cold storage areas. The construction of these partitions is not documented in building permits. The ceiling of the interior has been insulated to accommodate the building's use as for cold storage.

Significance Evaluation

The one-story utilitarian industrial L-shaped concrete building located at 689 S. Mesquit Street in Survey Area #1 (APN 5461-018-009) was evaluated under the Industrial Development Context, 1850-1980, for potential eligibility under Criteria A/1/1 for its association with patterns of history under the Sub-Context Manufacturing for the Masses, 1887-1965, and under Criteria A/1/1 or C/3/3 as a representative example of the associated Industrial-Manufacturing-Factory “factory” Property Type, 1887-1980. The building was not identified as a potential resource in SurveyLA’s findings for the Central City North Community Plan Area. SurveyLA Registration Requirements for the Factory Property Type are included in **Appendix F**.

Broad Patterns of History

Originally constructed for the West Coast Quilting Company, the existing building currently does not retain any characteristic features that convey its original factory use. Furthermore, research results do not indicate that the West Coast Quilting Company was a historically significant manufacturer in Los Angeles. With regard to broad patterns of history, research on the West Coast Quilting Company, the initial owner and occupant of the building, did not reveal information identifying the company as significant to Los Angeles industrial history. The company did not produce any products that had a significant influence on 20th century economic or social history or technological innovation in Los Angeles, California, or the United States. The original industrial manufacturing building was substantially altered when it was converted from a factory to a cold storage warehouse in or by 2006 and it does not currently have intact character-defining features and integrity to convey its original function as a manufacturing facility. On June 2, 2006, a building permit was issued to owner Frank H. Gallo and applicant Stuart Baltz, for removal of the existing roof materials and installation of a new spray polyurethane foam and elastomeric coating roofing system, providing insulation supporting the building’s current use as a cold storage warehouse. The building was likely modified for use as a cold storage warehouse at or by this time (2006). Presently, the building is used by Rancho Cold Storage as a staging area for frozen and refrigerated goods. The building has been modified with new interior partitions and insulation materials to accommodate its current use as a cold storage warehouse. There is no manufacturing occurring in the building currently. Because the building located in Survey Area #1 no longer conveys its original function as a quilt manufacturing facility and does not appear to be significant in Los Angeles industrial history, it does not meet the requirements for significance under National Register Criterion A, California Register Criterion 1, and the HCM Criterion 1 for history.

Significant Persons

With regard to associations with important persons, based upon City directory research and research into the history of the West Coast Quilting Company that occupied the building in Survey Area #1, there does not appear to be any significant association of the property with the productive lives of and significant historic personages. According to the available historic directories, the building was owned and operated by the West Coast Quilting Company beginning with the date of its construction in 1963. The exact date ending the company’s association with the property is unclear, however, a City directory from 1987 shows the property operated by American Fiber Industries. Additionally, no significant individuals appear to have been

associated with these enterprises. Today the building is owned and operated by Rancho Cold Storage. Therefore, the building in Survey Area #1 does not meet the requirements for significance under National Register Criterion B, California Register Criterion 2, or the HCM Criterion 2 related to historic personages.

Architecture

The existing building in Survey Area #1 appears to be an altered example of a common utilitarian industrial factory building, although it does have a small portion of the building—the entry—that is articulated in a manner characteristic of the Mid-Century Modern Style that was popular at the time of its construction in 1963. It is not uncommon for utilitarian industrial buildings to employ a few stylistic flourishes at the primary entry, as this represents the most publicly visible portion of the building; however, the employment of a few stylistic flourishes does not mean that the building is representative of the style. The existing building has exterior concrete block and reinforced concrete walls, and a flat roof with parapets. The building occupies half of the lot; the remainder of the lot is paved with asphalt and is used for vehicular parking and loading. The primary entrance (north elevation) is dominated by a three-bay loading dock, and the building's primary entrance is at the corner to the left of the loading bays. The primary entrance provides access to the building's office space and exhibits some minor decorative articulation characteristic of the Mid-Century Modern style including a decorative wall of stacked brick (painted over) and a flat projecting canopy supported by three projecting L-shaped metal beams (later alteration) that along with a concrete planter creates a small outdoor entrance porch. The front entrance has a fully glazed aluminum framed door with sidelights and transoms, fortified with metal security bars and screens (alteration). The rest of the building is utilitarian in character with painted concrete walls, rectangular metal multi-light industrial windows and large rectangular warehouse door openings. As an example of the utilitarian Manufacturing-Industrial-Factory Property Type for "factory", the building predominantly exhibits ordinary features and materials. It is not a specifically distinct factory type like a daylight factory, but a simple concrete structure with a flat combination roof. Furthermore, the original design of the building has been compromised by being converted from a manufacturing facility into a cold storage warehouse. Interior spaces have been reconfigured with partition walls and insulation materials to accommodate the building for its present use. There is no manufacturing occurring in the building currently. The few Mid-Century Modern Style details located at the building's primary entrance reflect the widespread popularity of the style during the period in which the building was constructed (1963); however, the use of these details at the entry does not warrant a consideration of this otherwise utilitarian building as representative of the style. Furthermore, it does not possess any branding or company logos on the exterior or retain any distinctive equipment or building elements that reflect a particular kind of manufacturing process. The Modern style façade elements are not programmatic and do not denote that it was a manufacturing building. Although the building's architect is unknown, the utilitarian design of the building suggests that it was not designed by a master architect, or if it was, that it would not be a notable example of the architect's work. Finally, in its present condition today, the building does not retain adequate integrity to convey its historical use as a factory. Therefore, the building in Survey Area #1 does not meet the requirements for significance under National Register Criterion C, California Register Criterion 3, and the HCM Criterion 3 for architecture.

Data

While most often applied to archaeological districts and sites, Criterion D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criterion D/4, they themselves must be, or must have been, the principal source of the important information. The existing building in Survey Area #1 does not appear to yield significant information that would expand our current knowledge or theories of design, methods of construction, operation, or other information that is not already known. The building does not appear to satisfy National Register Criterion D or California Register Criterion 4, and the HCM Criterion 4 for data potential.

Therefore, due to lack of significance under all applicable criteria as discussed above, the existing utilitarian industrial building at 689 S. Mesquit St. in Survey Area #1 (APN 5461-018-009) appears to be ineligible as a historical resource under CEQA.

Integrity Analysis

A property must have both significance and integrity to be considered a historical resource under federal, state, and local evaluation guidelines and CEQA. As *National Register Bulletin 15* notes that “only after significance is fully established can you proceed to the issue of integrity” (National Park Service, 1991).

As a result of this investigation, the building in Survey Area #1 at 689 S. Mesquit Street (APN:5164-018-009) was not identified as significant under any of the applicable national, state, or local criteria, as discussed above, and is found ineligible as a historical resource. Accordingly, an integrity analysis for the buildings is not required due to lack of historic or architectural significance. Furthermore, the building has been substantially altered, converting it from its original use as a factory into a cold storage warehouse. Aside from its location, even if an integrity analysis were appropriate, which it is not, the building would not be found to possess sufficient integrity to be considered potentially eligible as a historical resource.

Survey Area #2

Significance Evaluation

Survey Area #2, 2143 E. 7th Street (APN: 5164-017-008), is improved with a cold storage warehouse constructed in 2002. As stated in the Regulatory Setting, the National Register requires that properties be at least 50 years of age to be eligible, while the California Register recommends evaluation for properties 45 years or older. Since this building was constructed in 2002, it does not meet the age thresholds for consideration as a historic resource under the National Register (50 years) or California Register (45 years), and therefore is not eligible for listing on either register. Also, as stated in the Regulatory Setting, pursuant to the National Park Service Criteria Consideration G, properties of less than 50 years of age may be eligible for listing on the National Register if it can be clearly established that they are of exceptional significance. However, the building in Survey Area 2 is a utilitarian cold storage warehouse and does not possess exceptional significance to qualify it under Criteria Consideration G. Therefore, the existing cold storage warehouse building in Survey Area #2, 2143 E. 7th Street (APN: 5164-

017-008) does not meet the age or significance requirements for designation and is found ineligible as a historical resource under CEQA.

Survey Areas #3, #4, and #5

Architectural Description

Encompassing an irregular lot spanning three parcels, Survey Areas #3, #4, and #5, 642 and 674 S. Mesquit Street (APNs:5164-017-003, 5164-017-002, and 5164-017-006), are occupied by a complex of 10 buildings constructed between 1908 and 1985 with later alterations and additions that now appear as a single building (**Figure 34**). Sections of the building range between one-, two-, and three-stories in height and form an irregular rectangular footprint. The building has been divided into multiple cold storage warehouses, which have been connected at various points in the site's history. The period during which an addition was constructed, and the function of each space dictate the associated outward appearance, including such features as wall heights and materials. Exterior walls are constructed of concrete blocks or reinforced concrete. The building has several roof types, including flat and arched roofs with parapets extending above the roofs at various heights.



— 670 Mesquit Street / D170431.00

SOURCE: Google Earth

Figure 34
Aerial view of the building, outlined in red

Primary Elevation (West)

The primary (west) elevation is an amalgamation of 10 buildings, and because of this the materials and roof height and type vary. According to LADBS permits and building plans provided by the property owner, a majority of the primary elevation was re-clad with textured concrete blocks (alteration) as part of a project to comply with the City of Los Angeles Division

88 ordinance. These alterations occurred through a series of projects dating from 1989 through 1994 and resulted in a uniform appearance across seven of the 10 buildings (**Figure 16**).

The primary elevation is dominated by a partial length loading dock, covered by a long flat wood canopy supported by metal posts (alteration). The building's main entrance, which leads to the company's office spaces, is recessed beneath the canopy, where a set of concrete steps lead to the elevated loading dock and doorway (**Figure 35**). The main entrance consists of a single wood door with partial glazing. Additional points of entry consisting of large openings with metal roll-up doors (alterations) can be found along the loading dock on the east elevation. These openings provide access to the various cold storage spaces throughout the complex (**Figure 36**).

At the north end of the east elevation, there is a series of structures that have been combined through a series of alterations and additions (**Figure 37**). These portions of the building are constructed out of a variety of materials including tilt-up concrete, plaster, metal paneling, concrete block, and poured-in-place concrete (**Figure 38**). Roof profiles along this portion of the building vary, including pitched, flat, and shed roof types. A pedestrian entrance provides access to the cold storage area at the north end of the east elevation. Additional points of entry along this portion of the building include multiple vehicle loading bays.



SOURCE: ESA, 2016

670 Mesquit Street / D170431.00

Figure 35
Primary (west) elevation, overview of the loading bays
and primary entry into the company offices, view
southeast



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 36
Primary (west) elevation, view of additional loading bays, view northeast



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 37
Northern section of the building's primary (west) elevation, view northeast



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 38

Closer view of the multiple attached structures that make up the northern section of the primary (west) elevation, view east

North Elevation

The north elevation features a tilt-up concrete structure built in 1985 (alteration). This portion of the building also has a concrete block addition that is not documented in the LADBS records but appears to be a newer addition (**Figure 39**). An elevated concrete walkway, accessed via four concrete steps, leads to secondary entrances along the north elevation. The secondary entrances consist of a pedestrian entry with a single wood door and a large industrial entrance with a metal roll-up door (**Figure 40**). Additional access points near the north elevation consist of multiple vehicle loading bays.

South Elevation

The south elevation features a combination of concrete block and poured-in-place concrete construction. The wooden canopy that dominates the east elevation, wraps around to the south façade (alteration), providing shelter to two additional industrial scale openings (**Figure 41**). Extending from the south elevation is a concrete block garage area where equipment maintenance is performed (alteration) (**Figure 42**). The south end of the building houses equipment to power the refrigeration needs of the cold storage facility (**Figure 43**). Due to the use of these spaces, the south elevation features multiple openings providing ventilation (**Figure 44**).



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 39
The building's north elevation, view east



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 40
Tilt-up concrete addition constructed in 1985, view southeast



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 41
The building's south elevation featuring wrap around canopy, view northeast



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 42
Concrete block garage attached to south elevation, view north



SOURCE: ESA, 2016

670 Mesquit Street / D170431.00

Figure 43
Interior of machine room



SOURCE: ESA, 2016

670 Mesquit Street / D170431.00

Figure 44
Ventilation along south elevation, view north

East Elevation (Rear)

The rear (east) elevation contains no specific architectural features. Historic Sanborn maps show the east side of the building fronting a rail spur; however, the rail spur is no longer present and any aspect of the building that interacted with the adjacent railyard has been altered. The east elevation is clad with a combination of concrete block and poured-in-place concrete (**Figure 45**).

A new concrete wall (alteration) and metal fencing (alteration) separate the property from the adjacent railyard. There is a single point of entry, featuring a metal roll-up door on the east elevation (alteration) (**Figure 46**).



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 45
Rear (east) elevation with concrete block exterior and new concrete wall to the east, view north



670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

Figure 46
Rear (east) elevation with poured-in-place concrete exterior and roll-up metal door, view north

Interior

The building's interior is where the most evidence of the building's construction history can be observed. Portions of the building's interior spaces contain the original wood framing and unreinforced masonry. Other sections of the building demonstrate more modern construction techniques that have evolved throughout the building's existence. In one section of the cold storage facility, columns made of poured-in-place concrete support heavy concrete ceilings. This area may be the result of a building permit from 1932 that describes efforts to increase the load of the upper floors.

Significance Evaluation

As previously discussed, one historic architectural resource located within the Project Site was included in SurveyLA Central City North Survey but found ineligible for designation due to lack of integrity: the Los Angeles Ice and Cold Storage Company (Rancho Cold Storage), 674-686 S. Mesquit Street. The property was assigned an 7SQ status code: Not eligible; individual property assessed for significance in accordance with SurveyLA but does not meet eligibility standards (SurveyLA, 2016; HistoricPlacesLA.org). ESA conducted further intensive-level survey and research on the cold storage building complex including the Los Angeles Ice and Cold Storage Company. The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) and the former fruit juice factory (now cold storage facility) in Survey Area #5 (APN: 5164-017-006) are attached structures that appear as one large building.

The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) associated with the Los Angeles Ice and Cold Storage Company was evaluated under the Industrial Development Context (1850-1980) for its potential eligibility under Criteria A/1/1 in association with the Theme, Agricultural Roots (1850-1945) and its associated Sub-Theme, From Farm to Market (1900-1960), and under Criteria C/3/3 as an example of an associated Property Type, Cold Storage Warehouse (1900-1945).

The former fruit juice factory in Survey Area #5 (APN: 5164-017-006) associated with the Hyland Stanford Company was found ineligible as a historical resource under CEQA because the factory was destroyed by fire and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use.

SurveyLA Registration Requirements for the associated themes and property types are included in **Appendix F**.

Broad Patterns of History

Within the Project Site, a complex of 10 connected existing buildings constructed between 1908 and 1985 with later alterations and additions are located in Survey Areas #3, #4, and #5 (APN 5164-017-003, 5164-017-002, 5164-017-006) now appearing as one large cold storage warehouse. Early maps of the Project vicinity show that the initial large parcels were subdivided and used for agriculture. Over time, this gave way to further subdivision of the agricultural fields for residential use. By 1888 there were several one-story, single-family dwellings on Cincinnati Street and Mesquit Street. A Sanborn insurance map from 1888 depicts the original cold storage

and ice manufacturing facilities at 674 S. Mesquit Street. After it was destroyed by fire, a new facility of 10 connected buildings was developed, beginning in 1908 with the construction of a new building in Survey Area #3 (APN 5164-017-003) at 674 to 678 S. Mesquit Street to replace the original 1888 facility at 674 S. Mesquit Street. The new storage building erected for the Los Angeles Ice and Cold Storage Company was designed by Hunt, Eager & Burns and built for a cost of \$15,000; the 80-foot by 96-foot building was four-stories tall and had a concrete foundation, blue brick front, brick cornices, iron stairways, wood columns, and a composition roof. Later updates of the Sanborn map show the development of a much larger facility extending into Survey Areas #4 and #5, the construction and expansion of which is documented in multiple building permits that are characterized by incremental additions and structural upgrades over the years. In 1933, a series of changes to the building's walls were undertaken to repair damage from the Long Beach Earthquake. In 1990, to comply with earthquake safety regulations (Division 88), the existing building complex underwent substantial modifications. The Division 88 compliant alterations included replacing unreinforced masonry with modern textured, concrete blocks. In its present condition, the exterior of the building complex is completely unrecognizable due to over \$700,000 worth of Division 88 modifications.

LADBS Permits for Survey Area #4 (APN: 5164-017-002) also begin in April of 1908. The earliest permit on file was for lifting the roof of an existing building by 10 feet, similar to work completed at the same time in Survey Area #3 to repair fire damage. In 1912, the building was further modified, raising the second floor 4 feet and 4 inches to make room for the addition of a new floor between the first and second levels. In 1932, the building's structure and floor were strengthened to support greater loads. In the 1970s, a series of permits were filed to repair additional fire damage. The greatest alterations to structures in Survey Area #4 occurred between 1989 and 1994 when the building underwent structural upgrades in compliance with Division 88. The alterations replaced the original unreinforced masonry exterior with new textured concrete block. Like the buildings in Survey Area #3, these alterations completely rendered the existing buildings unrecognizable. Furthermore, the buildings in Survey Area #4 cannot be differentiated from the buildings in Survey Area #3 due to the new concrete block exterior walls and Division 88 modifications.

Improvements in Survey Area #5 (APN: 5164-017-006) began as early as 1909 with the construction of a single-family residence owned by the Atchison, Topeka, and Santa Fe (AT&SF) Railroad. However, the residence had been demolished by 1922 when a concentrated fruit juice factory was built by the Hyland Stanford Company on the site. The Hyland Stanford Company made significant contributions to the fruit processing industry through multiple patents registered in the company's name. Throughout the 20th century, the facilities were modified by subsequent owners including South Coast Packing Co and Rancho Cold Storage. The buildings in Survey Area #5 were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold-storage facility that was constructed in 1985.

The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) associated with the Los Angeles Ice and Cold Storage Company was evaluated under the Industrial Development Context (1850-1980) for its potential eligibility under Criteria A/1/1 in association with the Theme, Agricultural Roots (1850-1945) and its associated Sub-

Theme, From Farm to Market (1900-1960). The proximity of the railroad facilitated the movement of agricultural products from farm to market, and made the area ideal for cold storage, and several facilities were built along the river close to the tracks. Review of the Los Angeles Ice and Cold Storage Company's history suggests a significant association with development of Los Angeles industry in relation to the City's Agricultural Roots (1850-1945) and Farm to Market (1900-1960) historic contexts, as well as historical significance as one of the City's earliest cold storage warehouses. The technology of cold storage provided a way for local farmers to produce a surplus in order to better turn a profit, as agricultural products could be harvested and stored and then later brought to market by rail over great distances. The original cold storage and ice manufacturing facility appeared to predate the surrounding subdivision, which was originally developed for residential use. Although the building was reconstructed in 1908, the use of the property for cold storage and ice manufacturing purposes represents the shifting trend of development in the area from primarily residential to industrial. The residential lots in the area gave way to increased industrial development in the early 20th century. Following a fire in 1908, the building was reconstructed and expanded by Hunt, Eager, and Burns, through alterations and additions to accommodate the increasing demand for cold storage services. The building complex located in Survey Areas #3 and #4 appears to satisfy significance requirements outlined under National Register under Criterion A (association with events that have made a significant contribution to broad patterns of our history), the California Register under Criterion 1 (association with events that have made a significant contribution to the broad patterns of California's history and cultural heritage) and the HCM Criterion 1, which requires that a proposed building reflect or exemplify the broad cultural, political, economic, or social history of the community. However, the existing building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) has been substantially altered after its period of significance as a result of comprehensive Division 88 improvements and does not have sufficient integrity to convey its significance for listing as an example of a Cold Storage Warehouse Property Type associated with the Los Angeles Ice and Cold Storage Company.

The former fruit juice factory in Survey Area #5 (APN: 5164-017-006) associated with the Hyland Stanford Company was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 for its association with patterns of history under the Sub-Context Manufacturing for the Masses (1887-1965). The former fruit juice factory associated with the Hyland Stanford Company was destroyed by fire and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use.

Therefore, due to its lack of integrity to convey any significant associations under the Industrial Development Context, the entire building complex in Survey Areas #3, #4, and #5 (APN 5164-017-003, 5164-017-002, 5164-017-006) does not meet the requirements for significance under National Register Criterion A, California Register Criterion 1, and the HCM Criterion 1 for history.

Significant Persons

Regarding associations with important persons, the building complex is not identified with any historic personages. The Project Site does not show any historical importance in association with various owners, and the occupancy history could not be fully established. Research regarding ownership history uncovered four owners, the Los Angeles Ice and Cold Storage Company, the

California Consumers Corp., Rancho Cold Storage, and the Hyland Stanford Company. No information was found linking persons involved with these companies to important events in history. Because no significant personages were identified in the research of the Project Site, it is not eligible for listing under the National Register Criterion B, California Register Criterion 2, or the HCM Criterion 2 for eligibility related to a historic personage.

Architecture

Regarding architecture, design, or construction, the building complex on Survey Areas #3, #4, and #5 is ineligible for designation under National Register Criterion C, California Criterion 3, and the HCM Criterion 3. The building complex is not an exceptional, distinctive, outstanding, or singular example of its type or style either individually or as a contributor to a district. Although the building complex in Survey Areas #3 and #4 was once an early example of the Cold Storage Warehouse Property Type, it is not architecturally distinctive and is a typical, altered example of a cold storage warehouse built in Southern California and the United States. The building complex in Survey Area #5, originally a fruit juice factory, was destroyed by fire and rebuilt as a cold storage facility in 1985. Furthermore, the building complex has experienced significant alterations, which will be discussed further in the integrity analysis to follow. Although the building was originally designed by the notable Los Angeles architecture firm of Hunt, Eager, and Burns, the building complex has experienced significant alterations since its initial date of construction (1908) and no longer conveys its original design. Therefore, the building complex does not appear to satisfy National Register Criterion C, California Register Criterion 3, or the HCM Criterion 3 for significance related to a distinctive type, method, or period of construction, or as a work of a master.

Data

While most often applied to archaeological districts and sites, Criterion D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criterion D/4, they themselves must be, or must have been, the principal source of the important information. The building complex in Survey Areas #3, #4 and #5 does not appear to yield significant information that would expand our current knowledge or theories of design, methods of construction, operation, or other information that is not already known. The buildings do not appear to satisfy National Register Criterion D, California Register Criterion 4, or the HCM Criterion 4 for data potential.

Integrity Analysis

As stated previously in the significance evaluation, the building complex located in Survey Areas #3 and #4 was found to have a significant association with the industrial development of Los Angeles under two SurveyLA Citywide Historic Context theme of Agricultural Roots (1850-1945) and its associated sub-theme, From Farm to Market (1900-1960) under Criterion A/1/1, and was evaluated and under Criteria A/1/1 as an example of a Cold Storage Warehouse (1900-1945) property type. The portion of the building complex in Survey Area #5 was the site of a former fruit juice factory and was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 for its association with patterns of history under the Sub-Context Manufacturing for the Masses (1887-1965); however, the factory was destroyed by fire

and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use. Initially constructed in 1908, the history of the building complex on Survey Areas #3 and 4 suggests a significant association with the development of Los Angeles industry in relation to the City’s Agricultural Roots historic context, as well as one of the City’s earliest cold storage warehouses. Under the Agricultural Roots theme and Farm to Market sub theme as expressed by the Cold Storage Warehouse Property Type, the following considerations are applied to integrity: because of the rarity of the type and uses that may have changed over time, there may be a greater degree of alteration or fewer extant features and the setting may have changed (surrounding buildings and land uses). While all seven aspects of integrity are important for an eligible resource to possess, the SurveyLA Citywide Historic Context Survey identified integrity of location, design, feeling, and association as the most important aspects for a resource significantly associated with the Agricultural Roots (1850-1945) theme (**Table 4**). An excerpt from SurveyLA outlining these guidelines is included in **Appendix F**.

**TABLE 4
INTEGRITY MATRIX**

Agricultural Roots (1850-1945)	Design	Feeling	Association	Location
Survey Areas #3 and, #4	No	No	Yes	Yes

SOURCE: ESA, 2018

According to the SurveyLA Citywide Historic Context Statement, the Cold Storage Warehouse Property Type is representative of a key link between the collection of agricultural goods from farms, fisheries, and ranches and their distribution to produce markets and food processors. Good to excellent examples of the property type should retain most of the essential character-defining features from the period of significance including utilitarian reinforced concrete construction of 1-6 stories, frontage on a wide street for efficient truck circulation, generously-sized loading docks on one or more sides with vehicle turnaround space, few or no window openings, may have “dummy” windows to maintain continuity with the streetscape, and the property setting should include produce markets and/or fish processors (within a block). The threshold of integrity is higher for the Cold Storage Warehouse Property Type than properties associated with the Agricultural Roots theme. To retain integrity, a Cold Storage Warehouse Property Type should retain integrity of location, setting, design, materials, feeling, and association. New refrigeration equipment may have replaced older equipment, as long as the new equipment does not significantly alter the appearance of the property.

**TABLE 5
INTEGRITY MATRIX**

Cold Storage Property Type (1900-1945)	Location	Setting	Design	Materials	Workmanship	Feeling	Association
Survey Areas #3 and #4	Yes	No	No	No	No	No	Yes

SOURCE: ESA, 2018

Location

The building complex has not been moved and therefore retains integrity of location.

Design

Originally constructed in 1908, the former Los Angeles Cold Storage Company Building was designed by the influential Los Angeles architecture firm of Hunt, Eager, and Burns. However, throughout the building's history alterations and additions have resulted in a significant loss of integrity with regard to its original design. The substantive alterations occurred between 1989 and 1994, when various sections of the building complex in Survey Areas #3 and #4 were structurally reinforced to comply with updates to the City of Los Angeles Building Code, Division 88 ordinance (Division 88) regarding earthquake safety and mandatory retrofits. As part of the efforts to strengthen the building complex's structural system, the series of projects result in the re-cladding of the exterior with textured concrete blocks. This alteration essentially combined what were originally multiple buildings into one structure. The facilities of the former fruit juice factory built by the Hyland Stanford Company in Survey Area #5 were modified by subsequent owners including South Coast Packing Co. and Rancho Cold Storage. The buildings in Survey Area #5 were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold-storage facility that was constructed in 1985. Because of these alterations, the building complex no longer conveys its original design and therefore its integrity of design has been lost.

Setting

The setting of Survey Areas #3, #4 and #5 has been altered over time. While the area remains an industrial region, infill development adjacent to the building complex has impaired its character. The adjacent Survey Area #2, 2143 E. 7th Street (APN: 5164-017-008), is improved with a cold storage warehouse constructed in 2002. As discussed in this report, 689 Mesquit Street in Survey Area #1 is a former manufacturing building constructed in 1963, which has since been converted into a cold storage warehouse. Survey Areas #6 and #7 (APNs 5164-016-009 and 5164-016-010) are small (3,708 square feet and 2,881 square feet, respectively) portions of land that were previously improved with railroad support facilities that were later demolished and the lots are now vacant. Survey Area #8 (APN: 5164-016-803) is an unimproved vacant lot currently used as a railroad storage yard. The railyard to the east of the Project Site once serviced the former Los Angeles Ice and Cold Storage Company. However, the Project Site has become isolated from the adjacent railroad tracks, refocusing on vehicular traffic along its west elevation. While the surroundings of the Project Site have evolved over time, the way that the building complex interacts with its setting has also changed. Therefore, the integrity of setting has been lost.

Workmanship

Although some of the interior spaces of the building complex in Survey Areas #3 and #4 exhibit portions of what may be the original structures, significant alterations to the exterior of the building complex, through Division 88 compliant structural reinforcement have resulted in a significant loss of integrity of workmanship. The exterior elevations have been re-clad with textured concrete blocks and no longer convey the workmanship associated with the early twentieth-century construction methods. Furthermore, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985. Therefore, the building complex no longer retains integrity of workmanship.

Materials

As mentioned above, the building complex has suffered significant alterations to its exterior in order to comply with Division 88. This includes the re-cladding of the original building complex with textured concrete blocks, which has resulted in the exterior of the complex being unrecognizable. The original materials used on the façade by Hunt, Eager, and Burns when the building was constructed in 1908 are no longer observable. Furthermore, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985. Therefore, the building complex no longer retains its integrity of materials.

Feeling

When it was constructed in 1908 for the Los Angeles Ice and Cold Storage Company, the building complex was multiple structures. These structures became conjoined over time through multiple additions and alterations. Despite continued use as a cold storage warehouse, significant alterations to the exterior of the building complex in Survey Areas #3 and #4 has resulted in a loss of feeling. In its present condition, the building complex no longer conveys itself as a cold storage warehouse constructed in 1908. Today the building complex appears modern and utilitarian in design. Exterior cladding consisting of textured concrete blocks has resulted in a significant change to the building's outward appearance. Furthermore, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985. Therefore, the building complex no longer retains integrity of feeling.

Association

Despite the alterations to the exterior of the building complex in Survey Areas #3 and #4, it continues to serve as a cold storage facility, as it has throughout its history. Therefore, the building complex in Survey Areas #3 and #4 retains integrity of association with its historic use. However, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985 and does not retain integrity of association with its historic use.

Summary

In summary, because of significant alterations to the exterior of the building complex and immediate setting, the cold storage facility's integrity has been substantially compromised over the years in terms of design, materials, workmanship, feeling, and setting. The cold storage facility only retains integrity of location and association. For the building complex to convey its historical significance under the historical theme of Agricultural Roots (1850-1945) and sub theme Farm to Market (1900-1960), a cold storage facility that is representative of a Cold Storage Warehouse Property Type should have integrity of location, setting, design, materials, feeling, and association. Because the cold storage facility in Survey Area #3 and #4 only retains integrity of location and association, it does not meet the integrity requirements for historical significance at the national, state, or local level under Criteria A/1/1. Furthermore, the cold storage facility lacks the integrity of setting, design, materials, and feeling necessary to meet the threshold of integrity for its property type significance under Criteria C/3/3. Therefore, the cold storage facility in Survey Areas #3 and #4 is found ineligible as a historical resource. In addition, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985 and no longer retains integrity of setting, design, materials,

workmanship, feeling or association. Therefore, the existing cold storage facility in Survey Area #5 is also found ineligible as a historical resource.

Survey Area #6 and #7

Survey Areas #6 and #7 (APNs 5164-016-009 and 5164-016-010) are small (3,708 square feet and 2,881 square feet, respectively) vacant portions of land adjacent to Survey Area #8. According to aerial photographs and Sanborn maps, buildings were built here between 1923 and 1928. These buildings serviced the AT&SF *Railroad* and had a track running between the two buildings servicing parts of Mesquit and Jesse Streets. However, Survey Areas #6 and #7 are currently undeveloped and have remained so since the railroad buildings were demolished between 1964 and 1977. There are no historic buildings or structures on this lot that qualify as a historical resource under CEQA.

Survey Area #8

As described above Survey Area #8 (APN: 5164-016-803) is partially undeveloped towards the north section of the Project Site, while the area adjacent to Parcels #3-4 is currently used for railroad storage (**Figure 47**). There are no historic buildings or structures on this lot that qualify as a historical resource under CEQA. As Survey Area #8 is currently undeveloped it was surveyed only for archaeological resources. No archaeological resources were observed during the survey of this parcel. Since no evidence of the former tracks were visible during the survey, an evaluation for historic resources was not necessary.



SOURCE: ESA, 2018

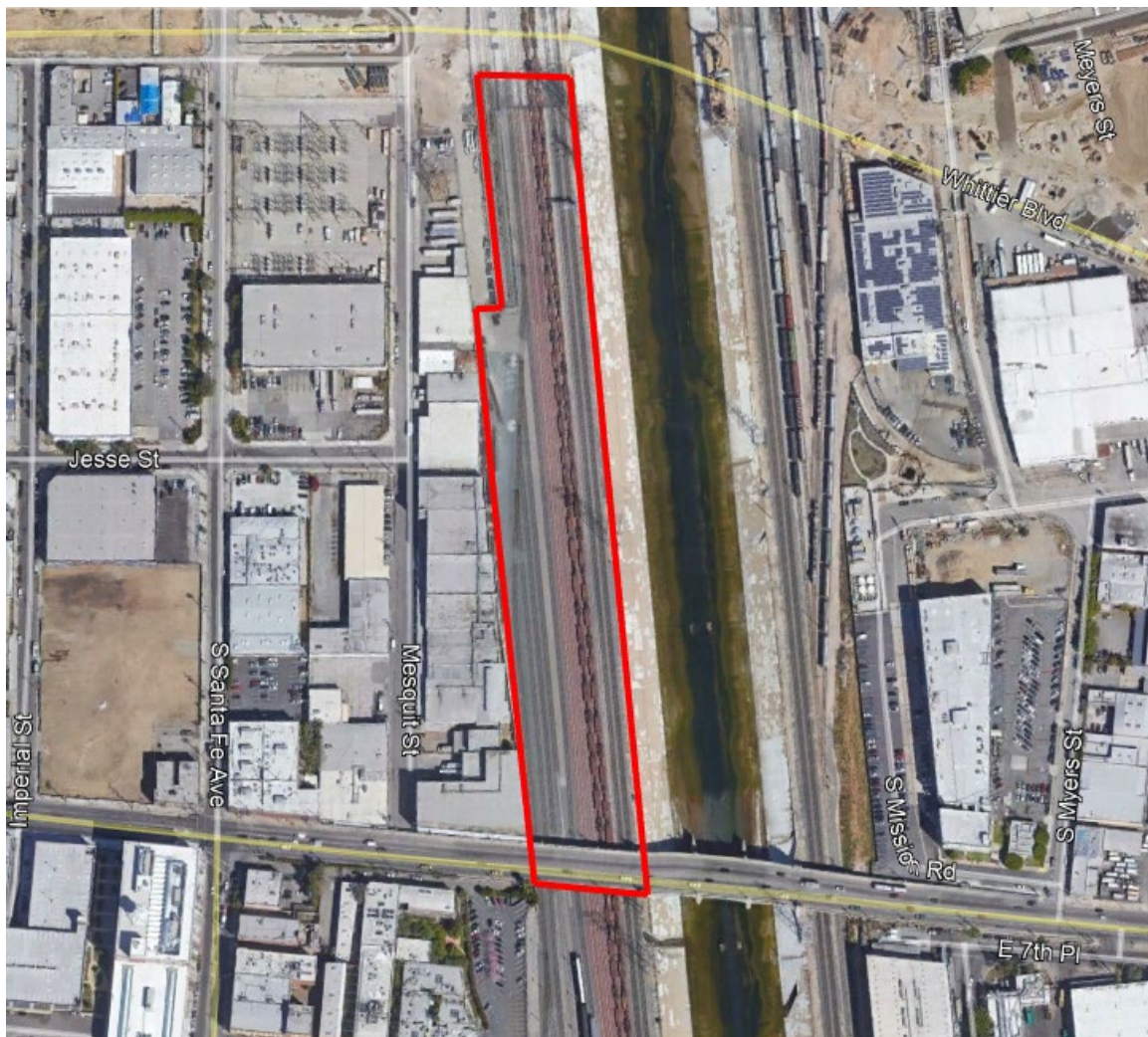
670 Mesquit Street / D170431.00

Figure 47
Ground visibility available, view south

Off Site Resources

Adjacent Railway Properties

The adjacent Railway Properties (**Figure 48**) is located adjacent to the east side of the Project Site and includes the BNSF Railroad (P-19-186804). The railways are bordered on the east by the channelized Los Angeles River and the Project Site/Mesquit Street on the west. Access to survey on foot was not available or permitted as the area is in active railroad, so the Railway Properties were visually inspected from the Project Site and the 7th Street Bridge. The area visually surveyed is 1,426 linear feet in length and ranges from 153 feet in width near 6th Street to 200 feet at its widest point. From the northern portion of the Project Site near the location of the future Ribbon of Light Bridge, there are seven tracks, which expand to eleven tracks near the 7th Street Bridge.



SOURCE: Google 2017

670 Mesquit Street / D170431.00

Figure 48
Adjacent Railway Properties

There are two additional spurs that stop 419 feet south of 6th Street (**Figure 49**). These two spurs are fenced off with chain link fence surrounded by a concrete foundation. All of the rails are extruded steel, which consists of the base, web, and ball. The standard gauge tracks (4 feet and 8.5 inches) consist of the rail atop wood ties (or sleepers), which sit on a relatively flat gravel or paved surface. There are small berms along the outer perimeter of the track. (**Figure 50**). The ballast bed (trackbed) is relatively flat with small berms on each track line. The two tracks closest to the Los Angeles River are utilized by Metro for passenger travel. The four tracks west of the first two tracks are presently utilized by BNSF for freight travel, and the remaining tracks are utilized by Amtrak and Metro for passenger travel. The four tracks currently utilized by BNSF have recently been updated with new wood ties (or sleepers), steel rails, steel spikes, and gravel Ballast beds (**Figure 51**).

The purpose of the survey was to verify that the resource was unchanged from the previous survey and evaluation (19-186804) by Smith and Harper (2007). ESA confirmed that this railway segment has undergone extensive upgrades and indeed does not retain integrity. The gravel bed and railroad tracks appear to be entirely replaced with new material and no evidence of historic period features or materials appears to be present. Because this resource railroad segment does not retain integrity, it does not qualify as a historical resource under CEQA. Furthermore, for the same reasons, it does not have enough integrity to contribute to the eligibility of the BNSF Railroad system as a whole. Therefore, this railway segment is individually ineligible and is non-contributing to the significance of the BNSF Railroad as a whole. As such, it does not qualify as a historical resource under CEQA, and it is not considered further in this report.



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 49
Railway, view northeast towards the 4th Street Bridge



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 50
View of the Railway alignment, view north



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 51
Railway, view southeast towards the 7th Street Bridge

7th Street Bridge

According to the Historic American Engineering Record (HAER) report prepared by the National Park Service for 15 historic City bridges built between 1910 and 1934, including the 7th Street Bridge, the bridges represent important engineering works that reflected the cutting edge of technology at the time of their construction (National Park Service, 2000: 26), and their significance is most clearly conveyed “when regarded as a set, a progression designed and constructed to serve the burgeoning transportation needs of the early twentieth century metropolis” (National Park Service, 2000: 23). The 7th Street Bridge (also known as the Seventh Street Viaduct, Bridge 53C-1321) (19-1754070) previously has been determined eligible for the National Register and is listed in the California Register as an individual example of a concrete arch bridge. In addition, the 7th Street Bridge is included in a thematic group of nationally significant bridges in California and is among the 12 significant bridges across the Los Angeles River, of which nine—including the 7th Street Bridge structure—are viaducts (Synder et al, 1986). In addition to their history and design, the visual and spatial relationships of the bridges to one another along the Los Angeles River is a character-defining aspect of the bridges.

Considered a minor example of the work of a significant designer, Merrill Butler, the 7th Street Bridge is significant under Criteria A and C for its significance in engineering history and design as an example of a double-decked bridge incorporating the original close-spandrel concrete-arch bridge (1908-1910), the bents and decking added in 1918 to accommodate railroad traffic under the bridge, and the concrete piers and girder spans of the longer viaduct built in 1927 to carry automobile traffic across the river (Snyder, John W., Stephen Mikesell, and Pierzinski, 1986). The Caltrans Statewide Historic Bridge Inventory also notes the 7th Street Bridge’s status as a property previously determined eligible for the National Register (Status Code 2S2) (Caltrans, 2018). The 7th Street Bridge was surveyed by OHR in 2007; the following year, in 2008, it was locally designated as a City of Los Angeles HCM (HCM No. 904). Subsequently, it was assigned a status code of 5S1.

Although ESA did not formally re-evaluate the 7th Street Bridge, a survey of its existing conditions was conducted to ensure that the bridge remained eligible for listing on the National Register, California Register, and as an HCM. Upon completion of the survey, ESA found that the 7th Street Bridge continues to convey its historical significance as an example of early twentieth century bridge construction and engineering. The 7th Street Bridge retains important features of its structural design, including its concrete bents, columns, and bracings. Although some features, such as its concrete railings, have been reconstructed due to seismic retrofitting in 1995, these important architectural features were replaced in-kind by using concrete molds. Furthermore, the effects of the seismic retrofitting were mitigated by HAER documentation completed at the time. Based on the survey results, the 7th Street Bridge remains eligible with a period of significance of 1910-1927, beginning with the completion of the bridge’s initial construction (1910) and ending with its expansion (1927).

Previous evaluations did not identify the 7th Street Bridge’s character-defining features; therefore, ESA conducted a survey of the bridge and compiled a list of character-defining features presented below in **Table 6**. This character-defining features analysis was prepared following the National

Park Service guidance provided in *Preservation Brief 17: Architectural Character-Identifying the Visual Aspects of Historic Buildings as an Aid to Preservation their Character* (National Park Service, 1988).



The 7th Street Bridge's character-defining features from its original 1908-1910 construction are as follows: the bridge abutments anchoring the bridge; its three large close-spandrel concrete arches above the river that are divided by large rectangular decorative columns; and the decorative concrete panel railing (recessed panel). Features from the 1918 redesign that extended the 7th Street Bridge over the tracks with additional bents and decking were subsequently altered by the 1927 retrofit, and the resulting existing bridge is primarily representative of its 1927 appearance. Character-defining features of the 7th Street Bridge, as it appeared in 1927, are as follows: the significant double-decked structure spanning the railroad right-of-way and the Los Angeles River; its horizontal massing interrupted by vertical elements formed by the large concrete piers and columns; the bents, shear walls and girder spans of the longer viaduct; the upper second deck with its decorative railings, light fixtures, and 7th Street Viaduct Plaque; and the support bents, abutments and shear walls that support the bridge and its approaches over the railroad tracks. Non-contributing features include the sidewalks, curbs, and street surface, which have all been replaced or reconfigured over time. Additionally, the metal picket guard fence with razor wire and concrete stairs do not appear original and are not character-defining.


Bailey Schmitz Co. Building – 2101 E. 7th Street

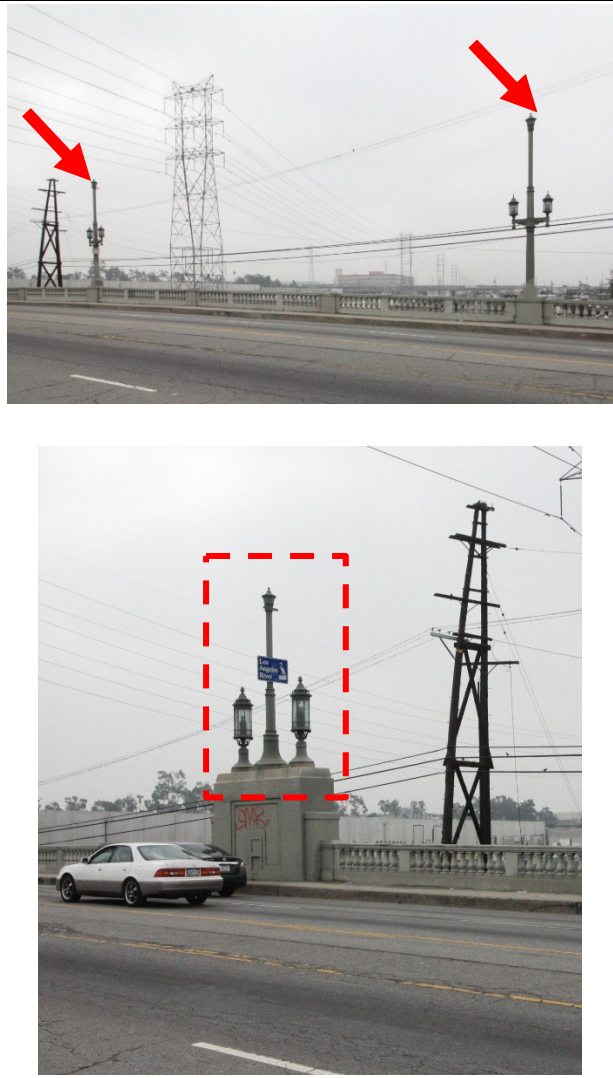
Architectural Description

The former Bailey Schmitz Co. building located at 2101 E. 7th Street is over 45 years in age and is within the Central City North Community Plan Area surveyed in 2016 by SurveyLA (**Figures 52-55**). The Bailey Schmitz Co. building was not identified as an individually eligible resource or as a contributor to a potentially eligible historic district by SurveyLA. Furthermore, the records search results did not identify any previous evaluations of the building. It is not located within a known historic district. Located directly adjacent to the Project Site at the intersection of 7th Street and South Santa Fe Avenue, the three-story utilitarian industrial brick building is currently used for artist's lofts. Assessor records indicate that the unreinforced masonry Bailey Schmitz Co. building was constructed in 1910, and building permits indicate the Bailey Schmitz Co. continued to own the building until 1983. Review of City building permits indicate the building was substantially altered over the years with numerous permits for additions, alterations and repairs on file. The Bailey Schmitz Co. building appears to have been seismically upgraded and renovated for its current multi-family live-work residential use. The fenestration has been replaced with new windows and doors; a new fire escape has been added to the rear east elevation; the brickwork along the ground-floor level on the south and west facades has been painted; and warehouse door openings have been closed (bricked over) on the south facade. Remnants of a painted sign on the building's cornice are cut off at the top due to a "parapet correction" in 1966 according to building permits (LADBS, 1966). The painted sign appears to read "Bailey Schmitz Co. Inc." Remnants of another painted sign on the south façade between the second and third stories indicates the building was once a wholesale warehouse.

**TABLE 6
CHARACTER-DEFINING FEATURES OF THE 7TH STREET BRIDGE, LOS ANGELES, CALIFORNIA**

Feature	Photograph	Character Defining Feature (Primary/Secondary/Tertiary)	Condition
<p>Overall Scale and Massing; The Seventh Street Bridge spans the Los Angeles River and includes a double-decker structure that was incorporated in the 1920s; The horizontal massing of the Bridge is interrupted by vertical decorative elements and decorative ornamentation along its north and south sides.</p>		Primary	<p>Overall, the Bridge is in fair condition. However, portions of the center structure may be deteriorating due to lack of use.</p>
<p>Concrete Structural Bents and Girder Spans</p>		Primary	<p>The Bridge's concrete bents appear to be in good condition.</p>

Feature	Photograph	Character Defining Feature (Primary/Secondary/Tertiary)	Condition
Decorative Concrete Railings		Primary	The original railings are no longer extant. In 1995 the railings were replaced as part of a project to seismically retrofit the Bridge.

Feature	Photograph	Character Defining Feature (Primary/Secondary/Tertiary)	Condition
Original Light Fixtures and their Locations		Primary	The Bridge's light fixtures appear to be in good working condition

Feature	Photograph	Character Defining Feature (Primary/Secondary/Tertiary)	Condition
7th Street Viaduct Plaque		Primary	Appears to be in good condition
Concrete Columns, Piers, Shear Walls, Girder Spans, Bents, Concrete Decks (structural), and Abutments		Primary	The concrete columns and structural supports appear to be in good condition. The decks are resurfaced.



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 52
Bailey Schmitz Co. Building and 7th Street Bridge, view east



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 53
South façade of Bailey Schmitz Co. Building along 7th Street, view to north



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 54
West façade of Bailey Schmitz Co. Building along
Santa Fe Avenue, view north



670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 55
Rear east elevation of Bailey Schmitz Co. Building,
view southwest from Project Site

Significance Evaluation

The building was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 and C/3/3 for its association with the Sub-Context Manufacturing for the Masses (1887-1965) as applied to a factory or manufacturing plant (1887-1980).

Broad Patterns of History

According to Davison's 1919 Mattress Directory, the Bailey Schmitz Co., Inc., was a manufacturer of mattress batting and felt in Los Angeles at 7th Street and Santa Fe Avenue (Davidson, 1918). Sanborn maps from 1950, 1951 and 1955 show the building continued to be occupied by Bailey Schmitz Co., Inc., mattress manufacturer, through the 1950s. However, no evidence was found to indicate that Bailey Schmitz Co., Inc., was associated with events that have made a significant contribution to the broad patterns of local or regional history or the cultural heritage of California or the United States. Furthermore, substantial alterations to convert the building from manufacturing to residential use have materially impaired its historic significance. The Bailey Schmitz Co. building does not appear to be historically significant under National Register Criterion A, California Register Criterion 1 or the HCM Criterion 1 for its association with early 20th century manufacturing in Los Angeles.

Significant Persons

The Bailey Schmitz Co. building is not associated with the lives of persons important to local, California or national history. The Bailey Schmitz Co. building does not appear to be eligible under National Register Criterion B, California Register Criterion 2 or the HCM Criterion 2.

Architecture

The Bailey Schmitz Co. Building does not appear eligible under Criteria C/3/3/ as an example of a factory or manufacturing plant. Substantial alterations to convert the building from manufacturing to residential use have materially impaired its historic significance and architectural integrity, and therefore it is not considered a historical resource under CEQA.

Data

While most often applied to archaeological districts and sites, Criterion D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criterion D/4, they themselves must be, or must have been, the principal source of the important information. The Bailey Schmitz Co. building does not appear to yield significant information that would expand our current knowledge or theories of design, methods of construction, operation, or other information that is not already known. The Bailey Schmitz Co. building does not appear to satisfy National Register Criterion D, California Register Criterion 4, or the HCM Criterion 4 for data potential.

Therefore, the Bailey Schmitz Co. building does not appear eligible under any of the applicable criteria and does not qualify as a historical resource under CEQA.

Conclusion Regarding Resources Identified and Evaluated

The currently extant buildings identified during the survey of the Project Site were evaluated for their potential eligibility to the National Register, California Register, and for local listing as part of this Report. Based upon the evaluation presented above, none of the buildings were found to be eligible for listing in the National Register, California Register, or locally designated as an HCM; therefore, they do not qualify as historical resources under CEQA Guidelines Section 15064.5(a)(1) or (2), and do not warrant consideration under CEQA Guidelines Section 15064.5(a)(3).

However, the 7th Street Bridge (P-19-175070), located adjacent to the Project Site, is considered a historical resource under CEQA. The 7th Street Bridge was previously determined eligible for the National Register as an example of a concrete arch bridge and is included in a thematic group of significant bridges across the Los Angeles River. The status of the 7th Street Bridge as a property previously determined eligible for the National Register and listed in the California Register is also noted in the Caltrans Statewide Historic Bridge Inventory, where it is assigned a status code 2S2 (Caltrans, 2018). The 7th Street Bridge was surveyed by the City of Los Angeles' OHR in 2007; the following year, in 2008, it was locally designated as a City of Los Angeles HCM (HCM No. 904). Subsequently, it was assigned a status code of 5S1 to reflect its designation at the local level.

Impacts Analysis

Direct Impacts

Resources Within the Project Site

No archaeological or historical architectural resources qualifying as historical resources under CEQA were identified within the Project Site and therefore the proposed Project would not result in a direct impact under CEQA.

Adjacent Resources

7th Street Bridge

As discussed above, the 7th Street Bridge (P-19-175070) is adjacent to the Project Site and previously has been determined eligible for the National Register, is listed on the California Register, and is a designated HCM; therefore, it is considered a historical resource pursuant to CEQA. As described in detail above, it is an individually significant under Criterion C as an example of a concrete arch bridge and as a minor example of the work of a significant bridge designer, Merrill Butler. It is also significant under Criteria A and C in both engineering history and design as an important example of a double-decked bridge incorporating the original closed-spandrel concrete-arch bridge (1908-1910), the bents and decking added in 1918 to accommodate railroad traffic under the bridge, and the concrete piers and girder spans of the longer viaduct built in 1927 to carry automobile traffic across the river (Snyder, John W., Stephen Mikesell, and Pierzinski, 1986).

The 7th Street Bridge's character-defining features analyzed and described in detail above include the following: its overall location and massing, including the length and tall scale of the bridge from the beginning of its on-ramps and spanning the railroad right-of-way and the Los Angeles River; the original abutments and three large closed-spandrel concrete arches divided by large rectangular decorative columns of the 1908-1910 Bridge; the concrete deck structure and decorative concrete panel railing; the double-decked structure of the 1927 viaduct; the horizontal massing interrupted by vertical elements; the large concrete piers and columns, concrete bents, concrete shear walls, concrete girder spans, upper second concrete deck and concrete viaduct; the decorative railings (replaced in-kind); the light fixtures; the 7th Street Viaduct Plaque; and the concrete support bents, abutments, and shear walls supporting the viaduct and its approaches over the railroad tracks. The 1990s seismic retrofit strengthened the structure of the 7th Street Bridge and retained the majority of its structural and decorative character-defining features, while other missing original elements—such as the railings—were reconstructed with in-kind replacements in conformance with the Standards in order to return the bridge to its 1927 appearance. Features of the 7th Street Bridge that are not character-defining include the sidewalks (replaced), curbs (replaced), street surface (replaced), metal picket guard fence with razor wire (non-original), and concrete stairs (non-original).

Proposed Bridge Alterations under the Project

The Project proposes to create new vehicular and pedestrian circulation connections to the existing 7th Street Bridge, which would alter certain parts of the bridge. The Project would include the construction of two new vehicular ramps that would provide access to the 7th Street Bridge from Buildings 4 and 5. The structural supports for the two ramps would be separate from the bridge structure and would be connected to the bridge by a cover plate that spans a seismic joint. The seismic joint would accommodate differential movements in longitudinal and transverse directions during thermal expansion and seismic events. Additional vehicular loads would be applied only to the new buildings so that the bridge's structural strength would remain intact. The Project also would include new pedestrian access from the 7th Street Bridge leading to an open courtyard space between Buildings 4 and 5. Detailed plans depicting the Project's proposed alterations and connections to the 7th Street Bridge are included in **Appendix G**, Bridge Connection Plans.

One of the two vehicular ramps with pedestrian access would be located at the southwestern corner of Building 5, to the north of the 7th Street Bridge. To provide for vehicular and pedestrian access to the Project from the 7th Street Bridge, proposed alterations are necessary that would affect character-defining features of the bridge. One historic light post would be removed and relocated or stored to make way for the ramp for Building 5. The 7th Street Viaduct Plaque would be temporarily removed and relocated on the bridge. To allow for the southwest exit ramp for Building 5, approximately 78 feet of railing would be removed on the north side of the 7th Street Bridge. As previously described, this railing is not original and was replaced in-kind and in conformance with the Standards in the 1990s as part of the seismic retrofit of the 7th Street Bridge. However, because the railing is a replacement-in-kind that restored the handrails to their 1927 appearance, the railing is considered a character-defining feature of the historic bridge, even though it is not original.

Other proposed changes associated with the connections to the 7th Street bridge at the southwest corner of Building 5 would involve removal of non-original, non-character-defining elements and construction of new elements that would be compatible in design and materials and would not adversely impact the integrity of the bridge. Non-original, non-character-defining sidewalk would be removed to allow for the southwest exit ramp for Building 5, and new compatible curb ramps and bollards would be added. The existing non-original north maintenance stairs would be removed and rebuilt approximately 60 feet east. The new concrete staircase would be utilitarian in design, and the concrete of the staircase would match the color and texture of the bridge. The new concrete staircase would not be visible to southward views of the 7th Street Bridge and would be minimally visible to northward views of the bridge. A sidewalk would be added to replace the area where the non-original stairs used to be. A non-original metal picket guard fence with razor wire would be removed and replaced with a similar fence of approximately 120 feet.

The second vehicular ramp would be located at the southeastern corner of Building 4. On the north side of the 7th Street Bridge, the Project would remove the outer ends of character-defining cantilevering concrete transverse beams and approximately 75 feet of character-defining railing that was previously replaced in-kind in conformance with Standards, as well as the non-original, non-character-defining sidewalk to allow for the ramp for Building 4. Modifications to three of the character-defining bridge bents would most likely be necessary to allow the new ramp to be attached. New curb ramps and bollards would be added, and a crosswalk would be installed on the 7th Street Bridge just north of the intersection of the ramp for Building 4. On the south side of the 7th Street Bridge, approximately 30 feet of character-defining railing that was previously replaced in-kind in conformance with the Standards as well as non-original, non-character-defining sidewalk would be removed to allow for a new crosswalk.

In addition to the modification to the 7th Street Bridge to facilitate vehicular access to Buildings 4 and 5, the bridge also would be altered to better accommodate pedestrian access to the Project Site. The Project would remove the outer ends of character-defining cantilevering transverse beams at the juncture of the new ramps and would also remove approximately 39 feet of character-defining railing that was previously replaced in-kind in conformance with Standards to allow for vehicular and pedestrian circulation to access the Project. In addition, the Project would entail the removal of non-original sidewalk on the north side of the 7th Street Bridge at the location of a new stair that provides access to a 30-foot-wide courtyard space located between Building 4 and 5.

It is anticipated that four new signal poles would be added and placed in the rail yard at-grade and not directly affixed to the 7th Street Bridge. The new signal poles would meet City standards and would be installed at non-contributing locations on land adjacent to the 7th Street Bridge and would not materially alter character-defining features of the bridge or block views of the bridge. The installation of new signals and poles would also be reversible in conformance with the Standards; because these elements are reversible, they could be removed in the future. Therefore, the integrity and eligibility of the 7th Street Bridge would not be affected by their installation.

As indicated above, the Project would require some removal of character-defining, in-kind replacement railing. With both the vehicular access and pedestrian improvements, the Project

would potentially remove approximately 192 feet or less of the 7th Street Bridge's existing character-defining, in-kind replacement railing along the north side near the Project Site. This includes approximately 78 feet to allow for the southwest exit ramp for Building 5; 75 feet for the entrance/exit ramp for Building 4; and 39 feet to better accommodate pedestrian access. On the south side of the 7th Street Bridge, approximately 30 feet of character-defining railing that was previously replaced in-kind in conformance with Standards and non-original sidewalk would be removed to allow for a new crosswalk. Therefore, a total of approximately 222 feet would be removed. As previously described, there are approximately 1,475 linear feet of this railing on the south side of the 7th Street Bridge and another 1,222 linear feet of railing on the north side, for a total of 2,697 total linear feet of railing. Therefore, if 222 feet or less of character-defining railing is removed on the north and south sides of the 7th Street Bridge as part of the Project, it would represent approximately 8.23% of the bridge's character-defining railing overall.

The Project would be restricted to the western portions of the 7th Street Bridge adjacent to the Project Site, where the new buildings would be connected to the bridge with two vehicular ramps. Since the concrete slab and seismic joint for connecting the new ramps to the 7th Street Bridge along with the structural framing supporting them would be separate structures, no longitudinal or main transverse beams would be demolished, and the bridge's structural integrity would remain intact. Non-character-defining sidewalks and character-defining, in-kind replacement railings would need to be removed, but the majority of the original significant character-defining structural features of the 7th Street Bridge would remain intact, including the abutments, concrete arches, columns, deck, and panel railings of the 1908-1910 bridge and the double-decker structure, piers, columns, bents, girders, decorative railings, and light fixtures of the 1927 Viaduct. Although alterations to the 7th Street Bridge would occur, these changes would largely be restricted to segments of decorative non-structural elements (character-defining, in-kind replacement railings), relatively few structural elements (a select number of bents for which modification is likely necessary for the installation of the ramp and the outer ends of character-defining cantilevering transverse beams), and non-original additions/alterations (e.g., sidewalks, curbs, maintenance stairs, metal fence).

Alterations to the 7th Street Bridge to support the Project would result in limited overall changes to its character-defining features; however, the Project would follow the Standards to the extent feasible. The design and construction of the 7th Street Bridge is fully documented in a HAER report, and based upon this documentation, several of the minor changes to the bridge proposed by the Project would be reversible should the Project be removed in the future, including the relocation of the light post and bridge plaque that could be returned to their original locations.

As discussed above, the Project would retain the majority of the 7th Street Bridge's character-defining features; however, modifications to the bridge would still occur with potential for unintended construction impacts to bridge fabric through alteration/removal of approximately 8.23% of the bridge's railing, relocation of the historic light post and plaque, and construction activity associated with removal/replacement of non-character-defining sidewalks/stairs as well as adjacent new construction. These modifications to the 7th Street Bridge that would occur under the Project are considered potentially significant; therefore, mitigation measures are required to

ensure that appropriate preservation treatment of affected bridge fabric during construction is undertaken in conformance with Standards and to reduce impacts to less than significant.

Due to the removal of historic materials and the alteration of features—including the alteration of features that have acquired historic significance over time—and the alteration of distinctive features, finishes and construction techniques that characterize the property, the Project and the Project with the Deck Concept would not fully conform with Standards 2, 4, and 5, respectively. However, the degree of alteration to the bridge is not such that it would lose its eligibility as a resource at the federal, state or local levels, or its ability to convey its significance; therefore, the minor alterations to the bridge do not constitute material impairment. With mitigation incorporated to enforce conformance with the Standards during construction, as specified in mitigation measures CUL-MM-1, CUL-MM-2, CUL-MM-3 and CUL-MM-4, the existing National Register eligibility of the 7th Street Bridge, its listing in the California Register, and its designation as a HCM would be retained since the bridge would still remain largely intact and would continue to convey its significance as an example of early twentieth century engineering.

To support conformance with Standards during construction and reduce potential impacts to less than significant, mitigation measures are presented below. Implementation of mitigation measure CUL-MM-1, *Standards Conformance Plan Review for the 7th Street Bridge*, would ensure that a Qualified Preservation Professional shall review the draft and final plans for the Project for conformance with the Standards; CUL-MM-2, *Reproduction of the 7th Street Bridge Railings*, would ensure that by taking molds of the railings and storing them, there would be the ability to reconstruct and reinstall the removed portions of the railing at a future point in time, if necessary. CUL-MM-3, *Construction Monitoring for the 7th Street Bridge*, would require that a Qualified Preservation Professional be retained to document existing conditions and provide preservation treatment recommendations including protective measures during construction. Implementation of CUL-MM-4, *Historic Structure Report for the 7th Street Bridge* would ensure baseline documentation of the historical resource occurs, including documenting in detail—in measured drawings and photographs—character defining elements of the bridge that would be altered by the Project.

After construction of the Project, the 7th Street Bridge would remain intact. However, the integrity and significance of the 7th Street Bridge could be materially impaired by removal of character-defining railing, modification of several of the bents to accommodate the installation of a ramp, removal of the outer ends of the transverse beams, and the removal and relocation of the light post and plaque. Moreover, there is potential for structural damage to the 7th Street Bridge due to construction activities. Therefore, impacts on the 7th Street Bridge are potentially significant, and mitigation measures are required to reduce impacts to less than significant.

Proposed Bridge Alterations under the Project with the Deck Concept

The conceptual plans for the Project also include an optional component called the Project with the Deck Concept. The Project with the Deck Concept includes a Deck that would be built east of the Project Site and over part of the adjacent Railway Properties. If the Deck is constructed, it may connect with the north side of the 7th Street Bridge. It is possible—depending on the final

configuration of the Deck as the Project with the Deck Concept is further developed—that the vehicular ramp proposed at the southeast corner of Building 4 could shift east to the Deck. If the Deck is developed, different segments of the 7th Street Bridge railing would be affected. The railings to be removed or altered would be dependent upon the final configuration of the Deck. However, it is important to note that both the Project and the Project with the Deck Concept would require some removal of the character-defining railings that were reconstructed in 1995 as replacements-in-kind as part of the seismic retrofit of the 7th Street Bridge.

As with the Project, the Project with the Deck Concept would require removal of a total of approximately 222 linear feet of the 7th Street Bridge's existing character-defining, in-kind replacement railing, as well as other minor alterations to the bridge described above for the Project. Although detailed design plans for the Deck have not been completed, it is anticipated that the Project with the Deck Concept may also require up to approximately 69 linear feet of additional demolition, including non-contributing sidewalk and in-kind replacement railing to the next bent. Therefore, the Project with the Deck Concept could require the removal of up to 291 linear feet of character-defining railing out of the total of 2,697 total linear feet of railing, or 10.79% of the 7th Street Bridge's character-defining railing overall.

Due to the removal of historic materials and the alteration of features—including the alteration of features that have acquired historic significance over time—and the alteration of distinctive features, finishes and construction techniques that characterize the property, the Project with the Deck Concept would not fully conform with Standards 2, 4, and 5, respectively. However, the degree of alteration to the bridge is not such that it would lose its eligibility as a resource at the federal, state or local levels, or its ability to convey its significance; therefore, the minor alterations to the bridge do not constitute material impairment. With mitigation incorporated to enforce conformance with the Standards during construction, as specified in mitigation measures CUL-MM-1, CUL-MM-2, CUL-MM-3 and CUL-MM-4, the existing National Register eligibility of the 7th Street Bridge, its listing in the California Register, and its designation as a HCM would be retained since the bridge would still remain largely intact and would continue to convey its significance as an example of early twentieth century engineering. Therefore, with mitigation, the Project with the Deck Concept would result in less-than-significant direct impacts to the 7th Street Bridge.

After construction of the Project with the Deck Concept, the 7th Street Bridge would remain intact. However, as with the Project, the integrity and significance of the 7th Street Bridge under the Project with the Deck Concept could be materially impaired by the minor modifications to it, including the removal of approximately 10.79% of the 7th Street Bridge's character-defining railing, the modification of several of the bents to accommodate the installation of a ramp, the removal of the outer ends of the transverse beams, and the removal and relocation of the light post and plaque. Moreover, there is also the potential for structural damage to the 7th Street Bridge under the Project with the Deck Concept due to construction activities. Therefore, impacts to the 7th Street Bridge are potentially significant, and mitigation measures are required to reduce impacts to less than significant.

Archaeological Resources

As a result of the SCCIC records research; land use research, including historic map analysis and historic aerial review; geoarchaeological study; review and analysis of the Preliminary Geotechnical Report prepared for the Project; review and analysis of the prehistoric and historic context of the project site and vicinity; and the archaeological resources survey conducted for the Project, no archaeological resources have been identified within the Project Site.

The earliest maps of the City of Los Angeles show the Project Site and vicinity were used as agricultural fields and vineyards as early as 1850. Prior to the settlement of the City, the area was open space with no development. Some of the buildings currently on the Project Site contain basements that extend at least one level below ground surface (bgs).

The construction of these buildings and their basements during the early 20th century likely destroyed any surficial or shallowly buried deposits. However, there are other portions of the Project Site where potentially sensitive subsurface remnants may remain that are associated with the residential dwellings that were constructed in the late 19th century with the residential single-family property owned by the railroad, or with early 20th century industrial development. Furthermore, deeply buried prehistoric archaeological resources may also remain. This includes portions of the Project Site that were never developed with buildings or where the existing buildings do not have basements, the portions of the Project Site below the foundations and/or basements, as well as off-site areas proposed for Project-related improvements, such as Mesquit Street and the Railway Properties to the east.

The Preliminary Geotechnical Report prepared for the Project indicates that the Project Site is underlain by five to six feet of artificial fill, which may contain historic period archaeological resources from past demolition on the Project Site. Project excavation would range from 61 to 68 feet bgs for the lowest subterranean parking level and 71 to 75 feet bgs in isolated areas to accommodate elevator pits. The excavation would impact both the artificial fill, as well as the native alluvial soils beneath, which have the potential to contain prehistoric and/or historic archaeological resources that could qualify as historical resources or unique archaeological resources under CEQA.

Due to the Project Site's proximity to the Los Angeles River (which is a known landmark for prehistoric habitation), soil matrices, past historic period uses, and only moderate past disturbances, Project grading and excavation may encounter unknown archaeological resources. As a result, Project construction has the potential to disturb, damage, or degrade archaeological resources that could be encountered during construction, thus resulting in a substantial adverse change in the significance of an archaeological resource qualifying as a historical resource or unique archaeological resources pursuant to CEQA Guidelines Section 15064.5. **Therefore, the Project could potentially cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5. Impacts would be significant and mitigation measures are required to ensure that impacts would be reduced to less-than-significant levels.**

Project with Deck Concept

Construction activities for the Project with the Deck Concept would be similar to the Project and would involve excavation and other ground-disturbing activities. The Deck would be supported by vertical columns that would be located between the existing railroad tracks. The Deck would use prefabricated steel or pre-cast concrete members to speed construction and minimize effects on railroad operations. Excavation depths for the Project with the Deck Concept would be the same as the Project. The foundations for the vertical columns would be drilled concrete piers, with one drilled pier below each vertical column. The piers would vary between approximately 30 to 50 feet in length. Deck construction and installation of the piers would be carried out in close coordination with the railyard authorities.

Given the relatively limited excavation required to install the piers that would support the Deck and the same general sensitivity for encountering unknown archaeological resources where excavation extends into native soil/sediment, impacts associated with archaeological resources would be essentially the same under the Project or the Project with the Deck Concept. As a result, Project with the Deck Concept construction has the potential to disturb, damage, or degrade archaeological resources that could be encountered during construction. **Therefore, the Project with the Deck Concept could potentially cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5. Impacts would be significant and mitigation measures are required to ensure that impacts are reduced to less-than-significant levels.**

Human Remains

Although no human remains were identified during the pedestrian survey of the Project Site, and no known human remains have been recorded within the Project Site or a 0.5-mile radius, the overall sensitivity of the Project Site with respect to archaeological resources is considered high as the Project Site is located in close proximity to water sources, which were used prehistorically for travel routes, as well as village or campsite locations due to the proximity of fresh water and could be the location of unknown burial sites. Therefore, the Project Site's sensitivity with respect to human remains is considered moderate.

The Project Site has been previously disturbed by the original construction of the existing land use. The review of historic aerial photographs and Sanborn Maps indicate that the Project Site has been developed from agricultural fields to industrial uses, as well as a single-family residence owned by the AT&SF Railroad formerly located in Survey Area # 5 on the Project Site.

As previously discussed, the Project Site is underlain by five to six feet of artificial fill. Given the relatively moderate degree of past disturbance within the Project Site, the depth of the proposed excavation would impact both the artificial fill layer, as well as the native soils beneath which have the potential to contain buried human remains. Human remains typically occur within the context of archaeological sites and have been found deeply buried in the vicinity of the Project Site.

Although unlikely, Project grading and excavation may encounter buried human remains. As a result, construction may disturb human remains, including those interred outside of dedicated cemeteries. However, with implementation of procedures codified in PRC Section 5097.98 and State Health and Safety Code Section 7050.5, impacts would be less than significant. No mitigation is required.

Project with the Deck Concept

The Project with the Deck Concept would grade and excavate to the same levels as under the Project. Therefore, grading and excavation for the Project with the Deck Concept may also have the potential to encounter buried human remains. As a result, construction may disturb human remains, including those interred outside of dedicated cemeteries. However, with the implementation of procedures codified in PRC Section 5097.98 and State Health and Safety Code Section 7050.5, impacts would be less than significant. No mitigation is required.

Indirect Impacts

The Project may result in potential indirect impacts to one adjacent historical resource, the 7th Street Bridge. No other known historic architectural resources were identified adjacent the Project Site that would be subject to potential indirect impacts. Potentially adverse indirect impacts may include:

- changes to important primary views that characterize a historical resource and its relationship with its historic setting and related resources;
- alteration of the setting of off-site resources with incompatible architecture;
- excavation or construction vibration that could damage the physical structure or materials of adjacent historic buildings.

7th Street Bridge

The 7th Street Bridge is the southernmost of what was once a series of four concrete bridges of similar style and age spanning the Los Angeles River, including the 1st Street (Bridge (1929), 4th Street Bridge (1931), and 6th Street Bridge (1932, now demolished) to the north. The existing bridges are included in a thematic group of 12 significant bridges across the Los Angeles River. In addition to their history and design, the visual and spatial relationships of the bridges along the Los Angeles River corridor are among their character-defining aspects. With the recent removal of the 6th Street Bridge and its future replacement with a new bridge, the immediate setting north of the 7th Street Bridge has been significantly altered and the previous visual and spatial relationships between these two historic bridges have been substantially changed. South of the 7th Street Bridge, the historic setting is altered by the I-10 and CA-60 freeway crossings over the Los Angeles River. Thus, the historic setting, spatial relationships, and primary views between the 7th Street Bridge and the 6th Street Bridge to the north have been lost. However, due to its location, scale and massing, the 7th Street Bridge is presently visually prominent within its immediate setting and views of the 7th Street Bridge along the Los Angeles River corridor are currently open and important.

Proposed Bridge Alterations under the Project

The Project would introduce new construction adjacent to the 7th Street Bridge that would be greater in density and taller in scale than the existing improvements on the Project Site. However, it would not obscure the existing primary views of the 7th Street Bridge along the Los Angeles River corridor from the north and south, where the bridge spans the Los Angeles River. Because of the open views along the Los Angeles River, the 7th Street Bridge would remain visually prominent within the surrounding setting. The character-defining concrete arches that span the Los Angeles River and the double-decked structure above, including the decorative columns, railings, and historic lighting fixtures of the bridge would remain visible, and the primary views of the 7th Street Bridge along the Los Angeles River corridor from north and south would be retained. These views from the Los Angeles River Corridor are considered the primary views of the 7th Street Bridge, as they are the only views in which the bridge can be observed comprehensively as an entity comprised of multiple structural elements that help it to convey its significance as a work of engineering. By contrast, the secondary views of the 7th Street Bridge are those views in which only some components of the bridge are readily visible, such as the view of the bridge as both motorists and pedestrians are traversing the 7th Street corridor from the east and west. From this vantage point, the deck, the decorative railings and the light posts are the most visible features of the 7th Street Bridge; however, views of these features are not prominent and alone, do not meaningfully convey the bridge's significance as a work of engineering. The views of the 7th Street Bridge from the east and the west would remain largely unobstructed after implementation of the Project. Other views of the 7th Street Bridge in the area are incidental in nature and are considered neither primary nor secondary views. The Project would potentially alter the 7th Street Bridge's existing setting with new construction adjacent to the bridge; however, the Project would not result in a substantial adverse change to the bridge's immediate surroundings such that the significance of the bridge would be materially impaired.

The existing structures on the Project Site were constructed outside the period of significance for the 7th Street Bridge (1910-1927). Accordingly, they are not part of the historic setting of the 7th Street Bridge and do not contribute to its eligibility. As such, demolition of the existing structures on the Project Site would not indirectly impact the eligibility of the 7th Street Bridge. The existing setting of the 7th Street Bridge consists of dense infill development on the banks of the Los Angeles River. Except for the Los Angeles River itself, the existing setting does not contribute to the significance of the 7th Street Bridge as an historical resource. Located on the Project Site, directly adjacent to the 7th Street Bridge, are two warehouses constructed outside of the bridge's period of significance (1910-1927). At 689 Mesquit Street (APN: 5164-018-009), the former West Coast Quilting Company factory was built in 1963. At 690-694 Mesquit Street is a new cold storage warehouse built in 2002. Furthermore, there is a large contemporary warehouse located at 2160 E. 7th Street, on the east side of the bridge, constructed in 1986. The removal of these ineligible non-historic buildings from the Project Site would not impact the 7th Street Bridge's setting or alter its eligibility. As discussed above under Archival Record Search for the BNSF Railroad, AT&SF Railroad (19-186804), the Railway Properties lacks integrity due to substantial alterations, and only the location of the railroad right-of-way can be considered to contribute to the historic setting of the 7th Street Bridge.

The historic setting of the 7th Street Bridge as it spans the Los Angeles River and its visual and spatial relationships with other historic bridges along the Los Angeles River corridor comprise the historic setting of the 7th Street Bridge during its period of significance. However, as previously stated, the setting of the 7th Street Bridge has been substantially compromised by the removal of the 6th Street Bridge and the construction of the I-10 and SR-60 crossings. As a result, the 7th Street Bridge is now an isolated resource. Furthermore, the Project Site and immediate surrounding setting is substantially altered and does not contribute to the eligibility of the 7th Street Bridge. Therefore, the introduction of new construction on the Project Site would not materially alter the immediate surrounding setting of the 7th Street Bridge or alter its eligibility as an historical resource under CEQA. Thus, the Project would have a less-than-significant indirect impact on the 7th Street Bridge because its integrity of setting has already been substantially compromised. Furthermore, the 7th Street Bridge would remain visually prominent within the Los Angeles River view corridor after Project completion, and the majority of its important character-defining features would be preserved. Although the Project would not fully conform to Standard 9 due to its substantial scale and massing, the Project has been designed to protect the integrity of the 7th Street Bridge. If the new construction would be removed in the future, the essential form and integrity of the 7th Street Bridge would be unimpaired; as such, the bridge would remain an eligible historical resource, and the Project would conform to Standard 10. **Therefore, pursuant to CEQA, since the Project would not materially impair the eligibility of the 7th Street Bridge as a historical resource, the Project would result in a less-than-significant indirect impact to the 7th Street Bridge.**

Proposed Bridge Alterations under the Project with the Deck Concept

Under the Project with the Deck Concept, as shown in Figure II-12 of Chapter II, *Project Description*, of the Draft EIR, the new construction would partially obstruct southern views of the western end of the 7th Street Bridge from the Los Angeles River corridor and the area of the now-demolished 6th Street Bridge, including the western approach, western abutments, and the western-most piers of the bridge that span the railroad right-of-way. However, there would be no additional obstruction of views of the character-defining bridge railings, deck, and lighting fixtures in regard to views from the north. The majority of the northern elevation of the 7th Street Bridge—including the more prominent features that span the river corridor and continue across to the eastern end of the 7th Street Bridge—would still be visible after construction of the Project. The views of the 7th Street Bridge from the south would be more affected under the Project with the Deck Concept as they are under the Project due to the construction of the Deck Concept. However, with implementation of the Project with the Deck Concept, the 7th Street Bridge would still be prominently visible within the urban setting and able to convey its significance as a work of architecture and engineering, as well as a transportation corridor.

Construction Vibration Impacts

For both the Project and the Project with the Deck Concept, there would be potential for temporary construction vibration and settlement effects on the 7th Street Bridge from onsite construction activities. As is common in similar urban development sites, vibration and settlement would be controlled through adherence to design values prescribed by the shoring engineer and geotechnical engineer with the intent to prevent damage to adjacent structures and through monitoring of associated construction activities. Although steps would be taken during

construction to help ensure design values are not exceeded, if exceedance were to occur and result in structural damage, such damage would likely be surficial and repairable based on industry practice and knowledge of construction activities in similar settings. Nonetheless, the potential for damage to the bridge due to construction-related vibration is considered a potentially significant impact. The 7th Street Bridge is a reinforced concrete, closed-spandrel, arch bridge constructed in 1910 that has undergone multiple improvements/retrofits, including retrofitting to accommodate heavier automobile traffic in 1927 and seismic retrofitting in 1990. Therefore, as discussed in the EIR in *Section IV.I Noise*, the 7th Street Bridge is considered a Category I structure and the significance threshold of 0.5 in/sec PPV for potential structural damage to a reinforced-concrete structure (the bridge) has been applied in the noise analysis for on-site construction (FTA, Transit Noise and Vibration Impact Assessment, page 186). As analyzed and summarized in Table IV.I-38, in *Section IV.I Noise* of the EIR, the estimated vibration velocity levels from construction equipment would exceed the 0.5 in/sec PPV threshold for the 7th Street Bridge (identified as Off Site Structure V6). Therefore, vibration impacts associated with structural damage from on-site construction activities would be potentially significant prior to the implementation of mitigation measures. With implementation of Mitigation Measure NOISE-MM-6: Construction Vibration (Except Shorting) the use of large bulldozers and loaded trucks shall be prohibited within 8 feet, use of jackhammers shall be prohibited within 5 feet, and the use of small bulldozers shall be prohibited within 1 foot of the 7th Street Bridge. With implementation of Mitigation Measure NOISE-MM-7: Construction Vibration (Shoring), the shoring system design and monitoring of excavation, grading, and shoring activities will be required to protect the 7th Street Bridge. Although structural damage may not occur, in the event it does, it would be repaired pursuant to Mitigation Measure NOISE-MM-8, which requires that the physical condition of the 7th Street Bridge be documented prior to the commencement of construction activity and that daily inspections occur when construction activities involving vibration-generating equipment such as bulldozers, jackhammers, loaded trucks, and drill rigs are used within 8 feet of the bridge. In the event that construction-related vibration occurs, the contractor shall arrange for inspection and repair as necessary. Implementation of vibration-related mitigation measures (Mitigation Measures NOISE-MM-6 through NOISE-MM-8) would provide protections from structural damage and provisions for repair, and impacts would be reduced to less-than-significant levels.

Therefore, with mitigation incorporated for potential vibration impacts, since the Project with the Bridge Concept would not materially impair the eligibility of the 7th Street Bridge as a historical resource, it would result in a less-than-significant indirect impact to the 7th Street Bridge pursuant to CEQA.

Historical Resources in the Project Vicinity

Indirect impacts to resources further away within a quarter-mile radius of the Project Site were analyzed to determine if the Project would result in adverse effects to their integrity or their immediate surroundings that would detract from their significance as historical resources. Four other eligible or designated known historical resources and a historic district were identified within the Project vicinity: Engine Co. #17-708 South Santa Fe Avenue (3S), H. J. Heinz Co. Warehouse – 712 South Santa Fe Avenue (3S), Ford Motor Company Factory-777 Santa Fe

Avenue and 2046 E. 7th Street, National Biscuit Company Building-1820 E. Industrial Street (5S1, HCM 888), and the Los Angeles Industrial Historic District. However, all are approximately one to two blocks southwest or two to three blocks northwest of the Project Site. Therefore, primary views to and from these resources would not be obstructed or altered by the Project such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired. Furthermore, the setting of the area has evolved over time with infill development. Due to the density of the surrounding urban environment, intervening development and substantial alterations of the setting, none of the identified resources would be adversely affected by the Project. The Project would not alter the setting or immediate surroundings that may contribute to the eligibility of any of the identified resources within the Project vicinity and would not reduce the integrity or significance of any of these historical resources. **Therefore, the Project would not result in a significant indirect impact to identified resources in the surrounding area.**

Similar to the Project, due to the density of the surrounding urban environment, intervening development and substantial alterations of the setting, the Project with the Deck Concept would not alter the setting or immediate surroundings to the extent that it would impair eligibility of the identified historical resources and historic district within a quarter-mile radius of the Project Site. As with the Project, the identified historical resources and historic district are all approximately one to two blocks southwest or two to three blocks northwest of the Project Site, therefore, primary views to and from these resources would not be obstructed or altered by the Project such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired. The setting of the area has evolved over time with infill development. Due to the density of the surrounding urban environment, intervening development and substantial alterations of the setting, none of the identified resources would be adversely impacted by the Project. For these reasons, the Project with the Deck Concept would not indirectly cause a substantial adverse change in the significance of an historical resource in the vicinity as defined in Section 15064.5. **Indirect impacts to historical architectural resources in the vicinity of the Project with the Deck Concept would be less than significant.**

For these reasons, the Project would not indirectly cause a substantial adverse change in the significance of an historical resource as defined in Section 15064.5. Indirect impacts on historical resources would be less than significant.

Cumulative Impacts

Impact Analysis

Historical Resources

A significant cumulative impact associated with the Project and related projects would occur if the impact would render a historical resource as no longer eligible for listing, and the Project's contribution to the impact would be cumulatively considerable. A list of six (6) related projects that are planned or under construction in the immediate vicinity of the Project Site that would potentially impact historical resources by demolition or alteration of a historical resource or by indirect impacts to the setting of a historical resource was compiled to support the analysis of cumulative impacts for the Project. These six (6) related projects were selected for further

analysis due to their close proximity to the Project Site, which makes them more likely to result in a significant cumulative impact. As the only identified historical resource that would be potentially impacted by the Project is the 7th Street Bridge, and since none of the related projects would directly affect the bridge or are close enough to result in construction vibration impacts, direct cumulative impacts to the 7th Street Bridge would not occur. Thus, the related projects are assessed for potential cumulative indirect effects due to proximate new construction or obstruction of important primary views to the 7th Street Bridge. The proximate related projects considered in this analysis are as follows:

- **Related Project 9:** 2051 E. 7th Street. New construction of a mixed-use building, including 15,000 square feet of retail space, 5,000 square feet of restaurant space and 320 condominium units. The new construction is replacing an ineligible one-story brick retail space and an ineligible two-story stucco commercial building that would be removed. The project is approximately 0.1 miles (540 feet) southwest of the Project Site.
- **Related Project 12:** 2030 E. 7th Street. New construction of a mixed-use development, including 243,583 square feet of office space and 40,000 square feet of retail space. The new construction is replacing a one-story warehouse and parking lot and is located approximately 0.15 miles (786 feet) southwest of the Project Site.
- **Related Project 20:** 2130 E. Violet Street. New construction of a mixed-use development, including 90,700 square feet of office space, 6,100 square feet of ground-floor retail, and a 274-space parking structure. The new construction is replacing a warehouse and vacant lot. It is located approximately 0.15 miles (814 feet) south of the Project Site.
- **Related Project 35:** 676 Mateo Street. New construction of a mixed-use development, including 172 live/work units, and 23,000 square feet of commercial space. The existing warehouse would be demolished. The new construction is located approximately 0.13 miles (707 feet) west of the Project Site.
- **Related Project 41:** 2143 E. Violet Street. New construction of a 13-story mixed-use development of live/work units and 288,000 square feet of commercial space. Corrugated metal warehouses have already been demolished on the site. It is .15 miles (750 feet) south of the Project Site.
- **Related Project 68:** 641 Imperial Street. New construction of a 12-story mixed-use development of 140 live/work lofts, approximately 7,000 square feet of ground-floor retail and art production space, 7,000 square feet of creative office space and four levels of underground parking. A one-story historic retail building will be demolished for the project. It is .12 miles (657 feet) northwest of the Project Site.

Related Project 9 and Related Project 12 are located further to the west of the 7th Street Bridge. Therefore, cumulatively the Project and Related Projects 9 and 12 would not alter the setting of the 7th Street Bridge or block any important views of the bridge, specifically the span of bridge over the Los Angeles River, which would still be observed from the north and the south along the Los Angeles River. Related Project 35 is located two blocks west of the Project Site, along the east side of Mateo Street. Related Project 68 is also located west of the Project Site at 641 Imperial Street. However, the settings of these related projects do not contribute to the historic setting of the 7th Street Bridge and construction of these related projects would not obstruct views of the bridge. Therefore, the Project and these four related projects would alter the setting of the 7th Street Bridge by introducing infill development; however, the setting has already been altered

with other examples of infill development constructed outside of the 7th Street Bridge's period of significance (1910-1927). Therefore, the Project and four related projects would not result in cumulative indirect impacts to the 7th Street Bridge.

Related Project 20 and Related Project 41 are located on the west bank of the Los Angeles River, to the south of the 7th Street Bridge and the Project Site. Related Projects 20 and 41 would potentially block views of the 7th Street Bridge's south elevation, along the bank of the river. The new construction on the Project Site, which is located north of the 7th Street Bridge, would not cumulatively contribute to that impact. Although the Project and related projects would alter the setting of the 7th Street Bridge, the setting has already been altered by infill development and no longer reflects the bridge's period of significance. Therefore, the Project and Related Projects 20 and 41 would not result in cumulative indirect impacts to the 7th Street Bridge.

In summary, while these related projects would alter the industrial area in the neighborhood surrounding the 7th Street Bridge, the area has already been altered by previous redevelopment and infill projects. While the 7th Street Bridge did contribute to industrial development in the area as an important transportation corridor, the bridge is designated as a historic structure for its significance in architecture and engineering not for its contributions to industrial history.

As discussed above, because the related projects would not have direct impacts on the 7th Street Bridge, no direct cumulative impacts related to historical resources would occur. Furthermore, the setting of the 7th Street Bridge has been substantially compromised; therefore, potential indirect cumulative impacts to the bridge as a result of these related projects would not be significant and would not affect the eligibility of the bridge. Primary views to and from identified resources in the project vicinity would not be obstructed or altered by the Project such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired. As previously evaluated, the Project would not result in indirect impacts to the 7th Street Bridge due to view obstruction or incompatible architecture. Therefore, Project impacts would be less than significant and would not be cumulatively considerable.

Project with Deck Concept

Overall impacts associated with historical resources are essentially the same under the Project or the Project with the Deck Concept. As with the Project, the only identified historical resource that would be directly impacted by the Project with the Deck Concept is the 7th Street Bridge, and since none of the related projects would directly affect the bridge or are close enough to result in construction vibration impacts, cumulative direct impacts would not occur. Similarly, the indirect cumulative impacts to 7th Street Bridge as a result of the Project with the Deck Concept and related projects would be less than significant. Primary views to and from identified resources in the project vicinity would not be obstructed or altered by the Project with the Deck Concept such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired.

For these reasons, the Project with the Deck Concept, considered together with the related projects, would not result in a cumulatively significant impact on historical resources.

Archaeological Resources and Human Remains

Many of the related projects, including the six related projects in the Project vicinity, would be expected to require grading and excavation that have the potential to encounter archaeological resources and human remains, although in some cases, these related projects are located in developed urban areas with sites that have been previously disturbed, which would reduce the likelihood of encountering archaeological resources and human remains. As discussed above, the Project has the potential to for inadvertent archaeological discovery and would be required to implement Mitigation Measures CUL-MM-5 through CUL-MM-7, which would reduce the Projects impacts on archaeological resources to a less-than-significant level. Similarly, as part of environmental review for the related projects, it is expected that mitigation measures would be imposed where necessary to reduce the potential for significant impacts on archaeological resources, as is required by the City. In addition, each related project would be required to comply with applicable regulatory requirements, such as CEQA Guidelines Section 15064.5 and PRC Section 21083.2, which address archaeological resources, and PRC Section 5097.98 and State Health and Safety Code Section 7050.5, which address human remains. Compliance with regulatory requirements and implementation of required mitigation measures for each individual development project would ensure that impacts to archaeological resources remain less than significant and reduce the potential for the individual related projects to contribute to cumulative impacts. As such, Project impacts to archaeological resources and human remains are not cumulatively considerable and cumulative impacts would be less than significant. **For these reasons, the Project, considered together with the related projects, would not cause a cumulatively significant substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5.**

Project with the Deck Concept

Overall impacts associated with archaeological resources are essentially the same under the Project or the Project with the Deck Concept. Thus, the conclusions regarding cumulative impact significance presented above also apply to the Project with the Deck Concept. **Therefore, the Project with the Deck Concept, considered together with the related projects, would not cause a cumulatively significant substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5.**

Conclusions, Mitigation Measures and Recommendations

This section provides conclusions, mitigation measures, and recommendations for Historic Architectural Resources and Archaeological Resources.

Historic Architectural Resources

As a result of its investigations, ESA concluded that the improvements on the Project Site do not qualify as historical resources pursuant to CEQA.

However, the 7th Street Bridge, to which two vehicular and pedestrian ramp connections from Buildings 4 and 5 in the southern end of the Project Site are proposed, is considered a historical resource. Although the design of the connections of the proposed ramps to the 7th Street Bridge, and any related modifications of the bridge's original architectural and structural design to permit these

connections, have not yet been finalized, the current conceptual plans would potentially have an impact on the bridge and could potentially alter its eligibility as a historical resource under CEQA. This is the case whether the Project or the Project with the Deck Concept is constructed.

Mitigation measures are provided below to ensure that the Project conforms to the Standards to the extent feasible. With implementation of the mitigation measures, potential Project impacts on the adjacent 7th Street Bridge would be less than significant. The mitigation measures include a Standards Conformance Plan Review for the 7th Street Bridge of the draft and final Project plans for compliance with CEQA and the Secretary of the Interior's Standards for Rehabilitation, Reproduction of Bridge Railings, Construction Monitoring for the 7th Street Bridge, and a Historic Structure Report for the 7th Street Bridge.

- **CUL-MM-1: Standards Conformance Plan Review for 7th Street Bridge.** The Project proposes new vehicular/pedestrian ramps that would connect to the 7th Street Bridge and would result in removal of character-defining features and materials. To reduce potential impacts, the Applicant shall retain a qualified preservation consultant, meeting the Secretary of the Interior's Professional Qualifications Standards for History, Architectural History, or Architecture, pursuant to 36 CFR 61 and who has at least 10 years of experience in design review and collaboration applying the Standards (Qualified Preservation Professional) to review the draft and final plans for the Project, to ensure conformance with the Secretary of the Interior's Standards for Rehabilitation (Standards).
 - Where the Project could impact the physical materials of the 7th Street Bridge, the Qualified Preservation Professional shall provide recommendations for appropriate protective measures and preservation treatment (repair or in-kind replacement) of the affected historic bridge fabric to be retained to ensure that historic features, materials and finishes are protected, and that the 7th Street Viaduct Plaque and the light post shall be protected during removal, storage, and relocation.
 - The Qualified Preservation Professional shall prepare a Plan Review Report, documenting conformance with the Standards, which shall be submitted as a draft to the City's Department of City Planning Office of Historic Resources, within 30 days of completion of the draft design plans, and shall make any recommendations necessary to bring the Project design for the alterations and additions to the 7th Street Bridge into conformance with the Standards.

Once the Project plans have been revised and are ready to be finalized, the Qualified Preservation Professional shall review the 90 percent construction plans and prepare a final report documenting conformance with the Standards, which shall be submitted to the City's Department of City Planning Office of Historic Resources, for final approval.

- **CUL-MM-2: Reproduction of the 7th Street Bridge Railings.** The 7th Street Bridge's existing concrete railings are not original but are reproductions of the originals. Prior to their removal, the Applicant shall prepare molds for the 7th Street Bridge's concrete railings. The molds will be stored on-site, ensuring that the railings could be reinstalled in the future if necessary and that the 7th Street Bridge can be returned to its current condition.
- **CUL-MM-3: Construction Monitoring for the 7th Street Bridge.** Prior to any demolition or construction activities that would affect the historic fabric of the 7th Street Bridge, including removal of steps, fencing, or other existing materials attached to or part of the bridge, removal of the bridge's concrete railings or light posts, or alteration of structural

members such as bents, a Qualified Preservation Professional shall be retained to document existing conditions and provide preservation treatment recommendations including protective measures and treatment recommendations.

- Prior to commencement of construction activities, the Qualified Preservation Professional shall document existing conditions at Project locations where alterations are to be made and meet with and provide preservation guidelines and instructions to the construction manager and team.
- During construction, the Qualified Preservation Professional shall monitor the Project, including demolition monitoring, preservation treatment oversight, and construction monitoring for Project components that would affect the character-defining features of the 7th Street Bridge such as any structural alterations of the bridge, removal/construction of pedestrian stairs, construction of vehicular ramps/intersections, removal of railings and relocation of the 7th Street Viaduct Plaque and light post and fixture, installation of new street signals, and if included, construction of the Deck. The Qualified Preservation Professional shall provide oversight and monitoring for the preparation of molds of the 7th Street Bridge’s existing concrete railings (see CUL-MM-2) and shall also provide preservation oversight and monitoring for the removal and relocation of the 7th Street Viaduct Plaque and the historic light post on the bridge’s railings that would be removed and relocated. The Qualified Preservation Professional shall document the existing conditions of the railing, 7th Street Viaduct Plaque and light post prior to their removal; monitor the railing mold-making process and appropriate storage of the molds for potential future use; monitor the process of removal of the 7th Street Viaduct Plaque and light post; review and document procedures for temporary storage of the 7th Street Viaduct Plaque and light post; monitor the 7th Street Viaduct Plaque and light post relocation and reinstallation process; and provide preservation treatment recommendations for repair of the 7th Street Viaduct Plaque and light post in conformance with the Standards. Monitoring intervals are to be determined based upon construction schedule and timing of Project activities that will affect the 7th Street Bridge. The monitoring visits shall be documented in a monitoring report for each visit. Once the majority of the construction activities affecting the 7th Street Bridge are completed, the Qualified Preservation Professional shall document the Project’s conformance with the Standards in a Substantial Completion Report that shall be submitted to the City’s Department of City Planning Office of Historic Resources for review and approval.
- **CUL-MM-4: Historic Structure Report for the 7th Street Bridge.** The 7th Street Bridge was previously documented in a Historic American Engineering Record report that summarized the history of the bridge and included copies of the historic bridge plans. The existing 7th Street Bridge exhibits several alterations from its original design, and under the Project will undergo additional alterations. To provide a baseline for the current Project and protect the integrity of the 7th Street Bridge under the current and future projects, a Historic Structures Report (HSR) shall be prepared by a Qualified Preservation Professional in accordance with guidelines set forth by the National Park Service in Preservation Brief No. 43: “The Preparation and Use of Historic Structure Reports” by Deborah Slaton (Slaton, 2005: 1). The HSR shall provide a summary of the 7th Street Bridge’s history and existing condition through available historic plans, current plans, and physical information. The HSR shall act as a guidance document for the current project and any future projects on the 7th Street Bridge. The HSR will include guidelines for the most appropriate approach to treatment for any currently proposed work including but not limited to: protective measures, rehabilitation, repair, in-kind replacement,

preservation treatment of materials/features, and maintenance. The HSR shall follow the three-part format and organization as outlined in Preservation Brief No. 43, including the following: Part 1 - the 7th Street Bridge's history, chronology, physical description, significance, and existing condition assessment; Part 2 – Treatment and Work Recommendations for the Project; and Part 3 – Supplemental Record of Work Performed including planning or technical studies or other investigations, records of physical work, construction documents, annotated drawings, construction monitoring logs, photographs, the Project plans showing the proposed alterations to the 7th Street Bridge, the Substantial Completion Report, and any other pertinent technical data or documentation. This report shall be reviewed by the City's Office of Historic Resources and Bureau of Engineering, to ensure that that the HSR meets the City's requirements. Once the Project is completed, the Applicant shall file the HSR with the City's Department of City Planning Office of Historic Resources and Bureau of Engineering, and the South Central Coastal Information Center (SCCIC).

Archaeological Resources

As a result of the SCCIC records research, land use research including historic map analysis, historic aerial review, geoarchaeological study, review and analysis of the geotechnical report, review and analysis of the prehistoric and historic context of the project site and vicinity, and archaeological resources survey conducted for the Project, no archaeological resources have been identified within the Project Site. The review of historic aerial photographs and Sanborn Maps indicate that the Project Site has been developed from agricultural fields to industrial uses. In addition, a single-family residence owned by the AT&SF Railroad was formerly present on the portion of the Project Site designated as Survey Area # 5 for purposes of this report. The earliest maps of the City of Los Angeles show the Project Site and vicinity were in use as agricultural fields and vineyards as early as 1850. Prior to the settlement of the City, the area was open space with no development. Some of the buildings currently extant on the Project Site contain basements that extend at least one level below ground surface. The construction of these buildings and their basements during the early 20th century likely destroyed any surficial or shallowly buried deposits. However, there are other portions of the Project Site where subsurface remnants associated with the residential dwellings that were constructed in the late 19th century, later early 20th century industrial development, or deeply buried prehistoric archaeological resources, may remain. This includes portions of the Project Site that were never developed with buildings or where the existing buildings do not have basements, the portions of the Project Site below the foundations and/or basements, as well as off-site areas proposed for Project-related improvements such as Mesquit Street and the Railway Properties to the east. The geotechnical report prepared for the Project (Twining Consulting, 2018) indicates that the Project Site is underlain by between 5 and 6 feet of historic fill, which likely represents an accumulation of matrix during the historic period from past demolition phases, and given the context is not likely representative of imported or engineered fill. Thus, this artificial fill may contain historic period archaeological resources. Given the relatively moderate degree of past disturbance within the Project Site (i.e., most original buildings still remain, with largely interior improvements undertaken over time and no redevelopment of the site since original building construction) there is potential to encounter intact buried archaeological resources. The majority of Project excavation is planned to range from approximately 61 to 68 feet bgs for the lowest subterranean

parking level and 71 to 75 bgs in isolated areas for elevator pits. Excavation would impact both the historic fill layer as well as the native soils beneath, which have the potential to contain prehistoric and/or historic archaeological resources that could qualify as historical resources or unique archaeological resources under CEQA. Due to the Project Site's proximity to the Los Angeles River (which is a known landmark for prehistoric habitation), soil matrices, past historic period uses, and only moderate past disturbances, the potential during ground disturbance to encounter intact buried archaeological resources that qualify as historical resources or unique archaeological resources under CEQA is considered high. The following recommendations are recommended to reduce impacts to such resources.

The following mitigation measures have been prescribed to reduce potentially significant impacts on archaeological resources and human remains due to inadvertent discovery:

- **CUL-MM-5:** Prior to the issuance of a demolition permit, the Applicant shall retain a qualified Archaeologist who meets the Secretary of the Interior's Professional Qualifications Standards for professional archaeology (qualified Archaeologist) to carry out and ensure proper implementation of mitigation measures that address archaeological resources. The Applicant shall submit a letter of retention to the City of Los Angeles Department of City Planning (City) no fewer than 15 days before construction activities commence to demonstrate to the City that the Applicant has retained a qualified Archaeologist who meets the Secretary of the Interior's Professional Qualifications Standards. The letter shall include a resume for the qualified Archaeologist. The letter shall also demonstrate that a Native American Monitor from the Gabrieleño Band of Mission Indians – Kizh Nation has been retained as required by Mitigation Measure TCR-MM-1.

The qualified Archaeologist shall oversee an archaeological monitor who has a bachelor's degree in a relevant field of study and either two months of archaeological construction monitoring experience or two months of supervised training with prehistoric or historic archaeological materials in a field or laboratory setting. The archaeological monitor shall be present during construction activities on the Project Site deemed by the qualified Archeologist to have the potential for encountering archeological resources, such as demolition, pavement removal, clearing/grubbing, drilling/auguring, potholing, grading, trenching, excavation, tree removal, or other ground disturbing activity associated with the Project. The activities to be monitored may also include off-site improvements in the vicinity of the Project Site, such as utilities, sidewalks, or road improvements. The archeological monitor and Native American Monitor shall have the authority to direct the pace of construction equipment activity in areas of higher sensitivity and to temporarily divert, redirect or halt ground disturbance activities to allow identification, evaluation, and potential recovery of archaeological resources in coordination with the qualified Archaeologist. Full-time monitoring may be reduced to part-time inspections, or ceased entirely, if determined appropriate by the qualified Archaeologist after consulting with Native American Monitor.

- **CUL-MM-6:** Prior to commencement of construction activities, a Sensitivity Training shall be given by the qualified Archaeologist and Native American Monitor for construction personnel. The training shall focus on how to identify archaeological resources and tribal cultural resources that may be encountered during construction activities, and the procedures to be followed in such an event. Within 5 days of completing the training, a list of those in attendance shall be provided by the qualified Archaeologist to the Applicant. Applicant shall maintain the documentation of this training, including the list of attendees, for inspection by the City upon its reasonable request.

- CUL-MM-7:** In the event that historic (e.g., bottles, foundations, refuse dumps/privies, railroads, etc.) or prehistoric (e.g., hearths, stone tools, shell and faunal bone remains, etc.) archaeological resources are unearthed, ground-disturbing activities shall be halted or diverted away from the vicinity of the find so that the find can be evaluated. An appropriate buffer area shall be established by the archaeological monitor and the Native American Monitor (in the case of prehistoric resources) in accordance with industry standards, reasonable assumptions regarding the potential for additional discoveries in the vicinity, and safety considerations for those making an evaluation and potential recovery of the discovery. This buffer area shall be established around the find where construction activities shall not be allowed to continue. Work shall be allowed to continue outside of the buffer area.

All resources unearthed by Project construction activities shall be evaluated by the qualified Archaeologist. If a resource is determined by the qualified Archaeologist to constitute a “historical resource” pursuant to CEQA Guidelines Section 15064.5(a) or a “unique archaeological resource” pursuant to Public Resources Code Section 21083.2(g), the qualified Archaeologist shall coordinate with the Applicant and the City to develop a formal treatment plan that would serve to reduce impacts to the resource. The treatment plan established for the resource shall be in accordance with CEQA Guidelines Section 15064.5(f) for historical resources and Public Resources Code Sections 21083.2(b) for unique archaeological resources. Preservation in place (i.e., avoidance) is the preferred manner of treatment. If in coordination with the City, it is determined that preservation in place is not feasible, appropriate treatment of the resource shall be developed by the qualified Archaeologist in coordination with the City and may include implementation of archaeological data recovery excavations to remove the resource along with subsequent laboratory processing and analysis. Any archaeological material collected shall be curated at a public, non-profit institution with a research interest in the materials, if such an institution agrees to accept the material. If no institution accepts the archaeological material, they shall be donated to a local school, Tribe, or historical society in the area for educational purposes.

(Note: In the event encountered resources appear to qualify as tribal cultural resource, separate and appropriate mitigation measures to address such resources would be required, as necessary.)

- CUL-MM-8:** Within 14 days of concluding the archaeological monitoring, the qualified Archaeologist shall prepare a memorandum stating that the archaeological monitoring requirement of the mitigation measure has been fulfilled and summarize the results of any archaeological finds. The memorandum shall be submitted to the Applicant and City. Following submittal of the memorandum, the qualified Archaeologist shall prepare a technical report that follows the format and content guidelines provided in California Office of Historic Preservation’s Archaeological Resource Management Reports (ARMR). The technical report shall include a description of resources unearthed, if any, treatment of the resources, results of the artifact processing, analysis, and research, and evaluation of the resources with respect to the California Register of Historical Resources and CEQA. Appropriate California Department of Parks and Recreation Site Forms (Site Forms) shall also be prepared and provided in an appendix to the report. The technical report shall be prepared under the supervision of the qualified Archaeologist and submitted to the City within 150 days of completion of the monitoring. The final draft of the report shall be submitted to the South Central Coastal Information Center.

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Los Angeles City of Los Angeles Building Department. Building Permits and Construction Drawings.

County of Los Angeles, Tax Assessor.

HistoricPlacesLA.org

Los Angeles Public Library, California Index.

Los Angeles Public Library Photo Database.

V. J. Rowan and Theo. G. Koeberle Map of the City of Los Angeles. 1887.

Sanborn Fire Insurance Company Maps – Santa Monica and Los Angeles, California.

State Lands Commission [SLC]. 1982. Grants of Land in California Made by Spanish or Mexican Authorities, www.slc.ca.gov/reports/grants_of_land/part_1.pdf, accessed February 8, 2012.

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Appendix A

Professional Qualifications



Monica Strauss, RPA

Director, Southern California
Cultural Resources Group

EDUCATION

M.A., Archaeology,
California State
University, Northridge

B.A., Anthropology,
California State
University, Northridge

AA, Humanities, Los
Angeles Pierce College

20 YEARS EXPERIENCE

SPECIALIZED EXPERIENCE

Treatment of Historic
and Prehistoric Human
Remains

Archaeological
Monitoring

Complex Shell Midden
Sites

Groundstone Analysis

PROFESSIONAL AFFILIATIONS

Register of Professional
Archaeologists (RPA),
#12805

Society for California
Archaeology (SCA)

Society for American
Archaeology (SAA)

QUALIFICATIONS

Exceeds Secretary of
Interior Standards

CA State BLM
Permitted

Monica has successfully completed dozens of cultural resources projects throughout California and the greater southwest, where she assists clients in navigating cultural resources compliance issues in the context of CEQA, NEPA, and Section 106. Monica has extensive experience with archaeological resources, historic buildings and infrastructure, landscapes, and Tribal resources, including Traditional Cultural Properties. Monica manages a staff of cultural resources specialists throughout the region who conduct Phase 1 archaeological/paleontological and historic architectural surveys, construction monitoring, Native American consultation, archaeological testing and treatment, historic resource significance evaluations, and large-scale data recovery programs. She maintains excellent relationships with agency staff and Tribal representatives. Additionally, Monica manages a general compliance monitoring team who support clients and agencies in ensuring the daily in-field compliance of overall project mitigation measures.

Relevant Experience

Orange County, Saddle Crest Homes Project EIR, Orange County, CA. Cultural Resources Project Director. The Saddle Crest project includes the development of 65 residential homes on an approximately 113.7-acre site. Monica managed the preparation of a Cultural Resources EIR section as well as a Phase 1 archaeological resources assessment. As part of the Phase 1 archaeological resources assessment, a literature review, a pedestrian survey, and Native American outreach were undertaken to meet CEQA compliance requirements.

Irvine Ranch Water District, Baker Treatment Plant, Orange County, CA. Cultural Resources Principal Investigator. ESA was retained by the Irvine Ranch Water District to provide environmental compliance services. In support of an EIR for the upgrade of the IRWD's Baker Treatment Plant near Lake Forest, ESA cultural resources staff conducted a Phase I Cultural Resources Assessment. Monica directed the archival research, a series of pedestrian surveys, and oversaw the preparation of Phase I Cultural resources Technical reports and the cultural resources section of the EIR.

Topock Compressor Station Remediation CEQA Services. Mohave County, AZ and San Bernardino County, CA. Cultural Resources Project Director. Monica is overseeing the preparation of cultural resources EIR sections and is providing project support to the California Department of Toxic Substances Control (DTSC), including facilitating Native American involvement. DTSC provides oversight of the site investigation and cleanup activities for the Pacific Gas and Electric Company (PG&E) Topock Gas Compressor Station, located in San Bernardino County, 15 miles southeast of Needles, California. Groundwater samples taken under and near the Station were found to be contaminated with hexavalent chromium and other chemicals as result of past disposal activities. Soils contamination is also present at the site, requiring investigation and cleanup. These activities are highly scrutinized by the regional Native American Tribes because the area has important cultural and religious significance. ESA is currently preparing an EIR for soil investigations and will be conducting CEQA

evaluations that tier off of the Program EIR for the Groundwater Remedy. Additional project-specific EIRs may be required for the final remedy, which is currently undergoing engineering design. ESA will provide these services as well as lead the Native American and public participation efforts.

Los Angeles Department of Water and Power, Path 46 Clearance Surveys, San Bernardino, CA. *Project Director.* ESA has been tasked by Los Angeles Department of Water and Power (LADWP) to conduct required surveys for the Path 46 Transmission Line Clearances Project. The project's objective is to restore required code clearances to the transmission conductors, which will be accomplished by grading the ground surface underneath the transmission lines to achieve required height consistency. The work is being conducted in compliance with BLM guidelines and federal laws and statutes. Biological, archaeological, and paleontological resource surveys are currently being conducted for the 77 proposed grading areas, staging areas, and roads. Reports will be written documenting the results of the surveys and providing recommendations on the areas for access, staging areas, and soil distribution that would have the least amount of impacts on natural resources. Monica is providing support to LADWP in their coordination with the BLM, including providing oversight of map preparation, field surveys, and preparation of pre-field research designs and post-field technical reports.

Ballona Wetlands Restoration EIR, Los Angeles County, CA. *Cultural Resources Project Director.* As part of the development of the restoration plan for the Ballona Wetlands, the ESA project team characterized existing conditions that included water and sediment sampling and analysis. The water and sediment quality sampling was performed to develop and evaluate potential restoration alternatives, and to develop a conceptual plan. The ESA project team compiled existing data on and conducted additional sampling for water and sediment to assess potential effects on the proposed wetland restoration habitat from the use of urban runoff and tidal in-flow from Ballona Creek. These data were used to complete a baseline report and restoration alternatives assessment. Monica is assisting the CSCC in fulfilling Army Corps of Engineers requirements under Section 106 of the National Historic Preservation Act. In addition, she is coordinating with Tribal members and is overseeing a team of resource specialists who are compiling cultural resources technical in preparation of the EIR's Cultural Resources section.

Los Angeles Department of Water and Power La Kretz Innovation Campus, Los Angeles County, CA. *Project Director.* The project involved the rehabilitation of the 61,000-square-foot building located at 518-524 Colyton Street, demolition of the building located at 537-551 Hewitt Street, and construction of an open space public plaza and surface parking lot, and involved compliance with Section 106 of the National Historic Preservation Act and consultation with the California State Historic Preservation Officer. ESA is providing archaeological monitoring and data recovery services and is assisting LADWP with meeting their requirements for Section 106 of the National Historic Preservation Act. Monica is providing oversight to archaeological monitors and crew conducting resource data recovery and laboratory analysis, and is providing guidance to LADWP on meeting Section 106 requirements.

Los Angeles Department of Water and Power Lone Pine Landfill Paleontological Resources Recovery, Inyo County, CA. *Cultural Resources Project Director.* At the request of LADWP, ESA responded to a discovery of large mammal bone at the Lone Pine Landfill in an area where borrow materials were being excavated.



ESA conducted geologic map research and recovered what was identified as a mammoth tusk. The tusk was stabilized, prepared for curation, and transported to a storage facility. Monica provided senior oversight of the paleontological resources recovery team and conducted paleontological resources sensitivity training and guidance to landfill staff in the event additional material are encountered.

City of Los Angeles Recreation and Parks, Hansen Dam Skate Park Project, Los Angeles County, CA. *Cultural Resources Principal Investigator.* ESA prepared a joint EA and IS/MND for the Los Angeles Department of Recreation and Parks in coordination with the U.S. Army Corps of Engineers (Corps) for a proposed skate park facility within the Hansen Dam Recreation Area. Monica managed a Phase I Cultural resources Study, coordinated with the Army Corps of Engineers and provided senior review for the EA/IS/MND cultural resources section.

Los Angeles Unified School District, Central Los Angeles High School #9. Los Angeles, CA. *Project Director.* ESA contributed to Data Recovery Report sections for Los Angeles Unified School District's Central High School #9, constructed in downtown Los Angeles. Between 2004 and 2009, Monica led a team of archaeological staff of ten who conducted archaeological monitoring and data recovery of archaeological materials in connection with the 19th century Los Angeles City Cemetery. She coordinated with the Los Angeles County Coroner and office of Vital Statistics to obtain disinterment permits and developed a mitigation plan incorporating components related to the future disposition of remains, artifact curation, and commemoration. She directed an extensive historical research effort to identify the human remains, and at the request of the client, participated in public outreach and coordination with media.

Bureau of Land Management, On-Call Cultural Resources Services, Riverside County, CA. *Project Manager.* ESA has been retained by the Bureau of Land Management under an on-call contract to provide cultural resource services including compliance monitoring for projects under Bureau of Land Management (BLM) jurisdiction. Monica managed a number of projects for the BLM (Palm Springs South Coast Field Office) providing a wide range of cultural resources services for solar projects and other projects taking place on BLM lands in compliance with Section 106 and specified BLM protocols. Services that she and her staff provide under this contract include compliance monitoring and peer review, Phase I archaeological resources surveys, resource evaluations, the preparation of reports, and Native American consultation. Projects completed under this contract include Dos Palmas Phase I Survey and Archaeological Monitoring, National Monument Phase I Survey, Windy Pointe Archaeological Monitoring, and Fast and the Furious Phase I Survey.



Margarita Jerabek, PhD

Historic Resources Director

EDUCATION

Ph.D., Art History,
University of California,
Los Angeles

M.A., Architectural
History, School of
Architecture, University
of Virginia

Certificate of Historic
Preservation, School of
Architecture, University
of Virginia

B.A., Art History, Oberlin
College

30 YEARS EXPERIENCE

AWARDS

2014 Preservation
Award, The Dunbar
Hotel, L.A. Conservancy

2014 Westside Prize, The
Dunbar Hotel, Westside
Urban Forum

2014 Design Award:
Tongva Park & Ken
Genser Square, Westside
Urban Forum

2012 California
Preservation Foundation
Award, RMS Queen Mary
Conservation Management
Plan, California
Preservation Foundation

PROFESSIONAL AFFILIATIONS

California Preservation
Foundation

Santa Monica Conservancy

Los Angeles Conservancy

Society of Architectural
Historians

National Trust for
Historic Preservation
Leadership Forum

American Institute of
Architects (AIA), National
Allied Member

Margarita Jerabek has 30 years of professional practice in the United States with an extensive background in historic preservation, architectural history, art history and decorative arts, and historical archaeology. She specializes in Visual Art and Culture, 19th-20th Century American Architecture, Modern and Contemporary Architecture, Architectural Theory and Criticism, Urbanism, and Cultural Landscape, and is a regional expert on Southern California architecture. Her qualifications and experience meet and exceed the Secretary of the Interior's Professional Qualification Standards in History, Archaeology, and Architectural History. Margarita has managed and conducted a wide range of technical studies in support of environmental compliance projects, developed preservation and conservation plans, and implemented preservation treatment projects for public and private clients in California and throughout the United States.

Relevant Experience

Margarita has prepared a broad range of environmental documentation and conducted preservation projects throughout the Los Angeles metropolitan area and Southern California. She provides expert assistance to public agencies and private clients in environmental review, from due diligence through planning/design review and permitting and when necessary, implements mitigation and preservation treatment measures on behalf of her clients. As primary investigator and author of hundreds of technical reports, plan review documents, preservation and conservation plans, HABS/HAER/HALS reports, construction monitoring reports, salvage reports and relocation plans, she is a highly experienced practitioner and expert in addressing historical resources issues while supporting and balancing project goals.

She is an expert in the evaluation, management and treatment of historic properties for compliance with Sections 106 and 110 of the NHPA, NEPA, Section 4(f) of the Department of Transportation Act, CEQA, and local ordinances and planning requirements. Margarita regularly performs assessments to ensure conformance with the Secretary of the Interior's Standards for the Treatment of Historic Properties, and assists clients with adaptive reuse/rehabilitation projects by providing preservation design and treatment consultation, agency coordination, legally defensible documentation, construction monitoring and conservation treatment.

Margarita is a regional expert on Southern California architecture. She has prepared a broad range of environmental documentation and conducted preservation projects throughout the Los Angeles metropolitan area as well as in Ventura, Orange, Riverside, San Bernardino and San Diego counties. Beyond her technical skill, she is a highly experienced project manager with broad national experience throughout the United States. She currently manages PCR's on-call preservation services with the City of Santa Monica, County of San Bernardino Department of Public Works, City of Hermosa Beach, Los Angeles Unified School District, and Long Beach Unified School District.



Sara Dietler

Archaeologist

EDUCATION

B.A., Anthropology,
San Diego State
University

19 YEARS EXPERIENCE

CERTIFICATIONS/ REGISTRATION

California BLM Permit,
Principal Investigator,
Statewide

Nevada BLM Permit,
Paleontology, Field
Agent, Statewide

PROFESSIONAL AFFILIATIONS

Society for American
Archaeology (SAA)

Society for California
Archaeology (SCA)

Sara is a senior archaeology and paleontology lead with 20 years of experience in cultural resources management in Southern California. As a senior project manager, she manages technical studies including archaeological and paleontological assessments and surveys, as well as monitoring and fossil salvage for many clients, including public agencies and private developers. She is a cross-trained paleontological monitor and supervisor, familiar with regulations and guidelines implementing the National Historic Preservation Act (NHPA), National Environmental Policy Act (NEPA), California Environmental Quality Act (CEQA), and the Society of Vertebrate Paleontology guidelines. She has extensive experience providing oversight for long-term monitoring projects throughout the Los Angeles Basin for archaeological, Native American, and paleontological monitoring compliance projects and provides streamlined management for these disciplines.

Relevant Experience

Los Angeles Unified School District (LAUSD) Central Los Angeles High School #9; Los Angeles, CA. *Senior Project Archaeologist & Project Manager.* Sara conducted on-site monitoring and investigation of archaeological sites exposed as a result of construction activities. During the data recovery phase in connection with a 19th century cemetery located on-site, she participated in locating of features, feature excavation, mapping, and client coordination. She organized background research on the cemetery, including genealogical, local libraries, city and county archives, other local cemetery records, internet, and local fraternal organizations. Sara advised on the lab methodology and setup and served as project manager. Sara was a contributing author and editor for the published monograph, which was published as part of a technical series, “Not Dead but Gone Before: The Archaeology of Los Angeles City Cemetery.”

Downtown Cesar Chavez Median Project, City of Los Angeles, CA. *Project Manager.* Sara assisted the City of Los Angeles Department of Public Works Bureau of Engineering with a Local Assistance Project requiring consultations with Caltrans cultural resources. Responsible for Caltrans coordination, serving as contributing author and report manager for required ASR, HPSR, and HRER prepared for the project.

Elysian/USC Water Recycling Project Initial Study/Environmental Assessment, Los Angeles, CA. *Project Manager.* Sara worked on the Initial Study/Mitigated Negative Declaration and an Environmental Assessment/Finding of No Significant Impact to construct recycled water pipelines for irrigation and other industrial uses serving Los Angeles Department of Water and Power customers in downtown Los Angeles, including Elysian Park. The U.S. Environmental Protection Agency is the federal lead agency.



Vanessa N. Ortiz, MA, RPA

Cultural Resources Specialist

EDUCATION

M.A., Anthropology
emphasis
Archaeology,
California State
University, Los
Angeles

B.A. Anthropology,
California State
University, Los
Angeles

7 YEARS EXPERIENCE

PROFESSIONAL AFFILIATIONS

Register of
Professional
Archaeologists
#30984230

Society for American
Archaeology

California Cultural
Resources
Preservation Alliance

Society for California
Archaeology

Lambda Alpha Honors
Society

Vanessa is an archaeologist with over seven years of documentation, records searches, survey, excavation, and monitoring experience. She is cross trained in archaeology and paleontology. She has worked extensively throughout California, with particular experience in the context of the Mojave and California Great Basin, prehistoric food processing sites, and historic artifacts.

Relevant Experience

City of Beverly Hills Metro Purple Line Extension, Beverly Hills, CA. Compliance Coordinator. ESA is retained by the City of Beverly Hills to conduct general compliance monitoring during the advanced utilities relocation phase of construction for the segment of the Metro Purple Line Extension Project located in the City of Beverly Hills. Vanessa oversees ESA monitors, prepare weekly reports and 3-week look-ahead projections based on estimated contractor planned activities. As needed, she issues violations in the event a non-compliance issue is identified. ESA's primary objective is to assist contractors in avoiding non-compliance issues through thorough observation and open communication.

Ballona Wetland Restoration, Playa Del Rey, CA. Archaeologist. As part of the development of the restoration plan for the Ballona Wetlands, the ESA project team characterized existing conditions that included water and sediment sampling and analysis. The water and sediment quality sampling was performed to develop and evaluate potential restoration alternatives, and to develop a conceptual plan. The ESA project team compiled existing data on and conducted additional sampling for water and sediment to assess potential effects on the proposed wetland restoration habitat from the use of urban runoff and tidal in-flow from Ballona Creek. These data were used to complete a baseline report and restoration alternatives assessment. Vanessa assisted in survey, data recovery and artifact analysis.

Los Angeles Department of Water and Power (LADWP), Path 46 Clearance Surveys, San Bernardino, CA. Archaeologist. ESA has been tasked by LADWP to conduct required surveys for the Path 46 Transmission Line Clearances Project. The project's objective is to restore required code clearances to the transmission conductors, which will be accomplished by grading the ground surface underneath the transmission lines to achieve required height consistency. The work is being conducted in compliance with BLM guidelines and federal laws and statutes. Biological, archaeological, and paleontological resource surveys are currently being conducted for the 77 proposed grading areas, staging areas, and roads. Pending reports will document results of the surveys and provide recommendations for minimally invasive access areas, staging areas, and soil distribution. Vanessa provided field surveys and documentation of archaeological sites for submission to the California State Parks.



Ashley Brown

Senior Architectural Historian

EDUCATION

M.A. Public History:
Historic Preservation,
Middle Tennessee
State University

B.A. History,
California University
of Pennsylvania

5 YEARS EXPERIENCE

RECOGNITIONS

Minong, Traditional
Cultural Property
National Register
Nomination – Agency:
Grand Portage Band
of Lake Superior
Chippewa

Grand Portage CCC
Indian Division Bridge
– Agency: National
Park Service and
Grand Portage Band
of Lake Superior
Chippewa

Ashley Brown is a senior architectural historian with more than five years of academic and professional experience preparing documentation to address the restoration, rehabilitation, and adaptive reuse of historic properties—including historic structures reports, preservation and interpretation plans, and National Register of Historic Places nominations. Ashley also has experience contributing to California Environmental Quality Act CEQA-level documents. She is adept at developing and implementing historic resources surveys to address architectural, building, and cemetery condition assessments utilizing such programs ArcGIS and Survey123. Ashley continues to expand her knowledge of Southern California history by conducting primary source research and developing historic contexts.

Relevant Experience

Los Angeles Department of Water and Power (LADWP), East Hollywood District Yard Cultural Resources Technical Report, Los Angeles, CA. *Architectural Historian and Report Author.* ESA prepared a Cultural Resources Assessment for the Los Angeles Department of Water and Power (LADWP) Distribution Yard No. 2, which was built by the Bureau of Power and Light in 1926. Ms. Brown evaluated the District Yard for architectural and historic significance at the local, state, and federal levels.

Maguire Properties, 755 Figueroa Street Cultural Resources Technical Report, Los Angeles, CA. *Cultural Resources Specialist.* Ms. Brown authored project specific historic context for the 755 Figueroa Street Cultural Resources Report and identified archaeological potential for Project site. The Report was used in a MND for two new residential tower units in downtown Los Angeles.

Sportsmen’s Lodge Hotel Historic Resources Assessment, Studio City, Los Angeles, CA. *Project Manager and Report Author.* Ms. Brown evaluated the Sportsmen’s Lodge Hotel, which was identified by SurveyLA as part of the Sportsmen’s Lodge Historic District for historic and architectural significance at the local, state, federal levels. The hotel was designed in the Mid-Century Modern style by James D. Barrington and was identified for its historical associations with the Sportsmen’s Lodge.

3600 Wilshire Boulevard Historic Resource Assessment and Impacts Analysis, Los Angeles, CA. *Project Manager and Report Author.* Ms. Brown evaluated 3600 Wilshire Boulevard, a Modern style office building designed by master architect Robert Tyler, of Welton Becket and Associates for significance at the local, state, federal levels. Included in this report was CEQA Impacts Analysis. This CEQA document was used to support a MND.



Christian Taylor

Senior Architectural Historian

EDUCATION

Master's Degree,
Historic Preservation,
University of Southern
California, Los Angeles

B.A., History, University
of Oklahoma, Norman

5 YEARS EXPERIENCE

Christian Taylor is a historic resources specialist with academic and professional experience in assessing historic structures and contributing to California Environmental Quality Act (CEQA)-level documents. Throughout the course of his career, Christian has developed an interest in Los Angeles' industrial, economic, and transportation related history. Christian continues to hone his skills in management of rehabilitation and restoration projects, preparation of historic contexts, the use of non-invasive material investigation methods and advanced methods of documentation, and historic resource assessments.

Christian has completed and co-authored a wide range of architectural investigations including historic resources assessment and impacts analysis reports for compliance with CEQA, character-defining features reports, plan reviews, investment tax credit applications, Section 106 significance evaluations, and HABS/HAER documentations. He has also performed extensive research, survey work, and prepared landmark and preliminary assessment reports as a part of ESA's On-Call Historic Preservation Contract with the City of Santa Monica.

Representative Experience

344 8th Street, Long Beach, CA. Architectural Historian. ESA prepared a historic resources analysis for the 344 8th Street project. This project included a physical inspection of a small corner store constructed in the early twentieth century. The building was recorded and evaluated on Department of Parks and Recreation (DPR) record forms based on relevant historic contexts surrounding its development. Recommendations for restoration treatments of the building were provided as a result of the investigation. Chris was responsible for conducting the site survey, archival research and preparing the DPR forms and restoration treatment recommendations.

929 E. 2nd Street IS/MND, Los Angeles, CA. Architectural Historian. ESA prepared an IS/MND for the 929 E. 2nd Street project. The project required a Historic Resources Assessment to evaluate the existing two-story industrial building for individual eligibility at the local, state, and national level. The results of the evaluation were that the former Challenge Creamery Association Building did not appear individually eligible under the applicable local, state, or national criteria. The building is located within the boundaries of a potential historic district identified by SurveyLA. The assessment of the property included a review of the potential district and its contributors. A district description was developed and the building was found eligible as a contributor. The proposed project was then reviewed for potential impacts to the district, nearby contributors and individual resources, and the contributor within the project area. Mitigation measures and project alterations were recommended to the client as a result of the investigations. Chris conducted the HRA and prepared the Historic Technical Report for the IS/MND.

Boething Tree Farms EIR, 23475 Long Valley Road, Los Angeles, CA.

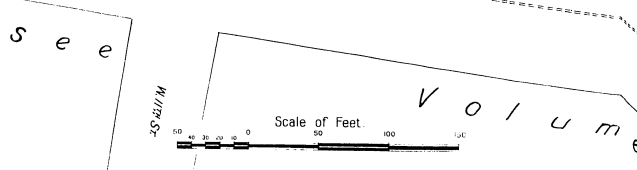
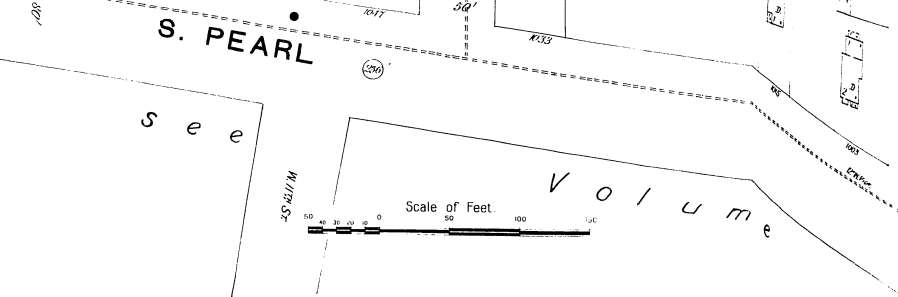
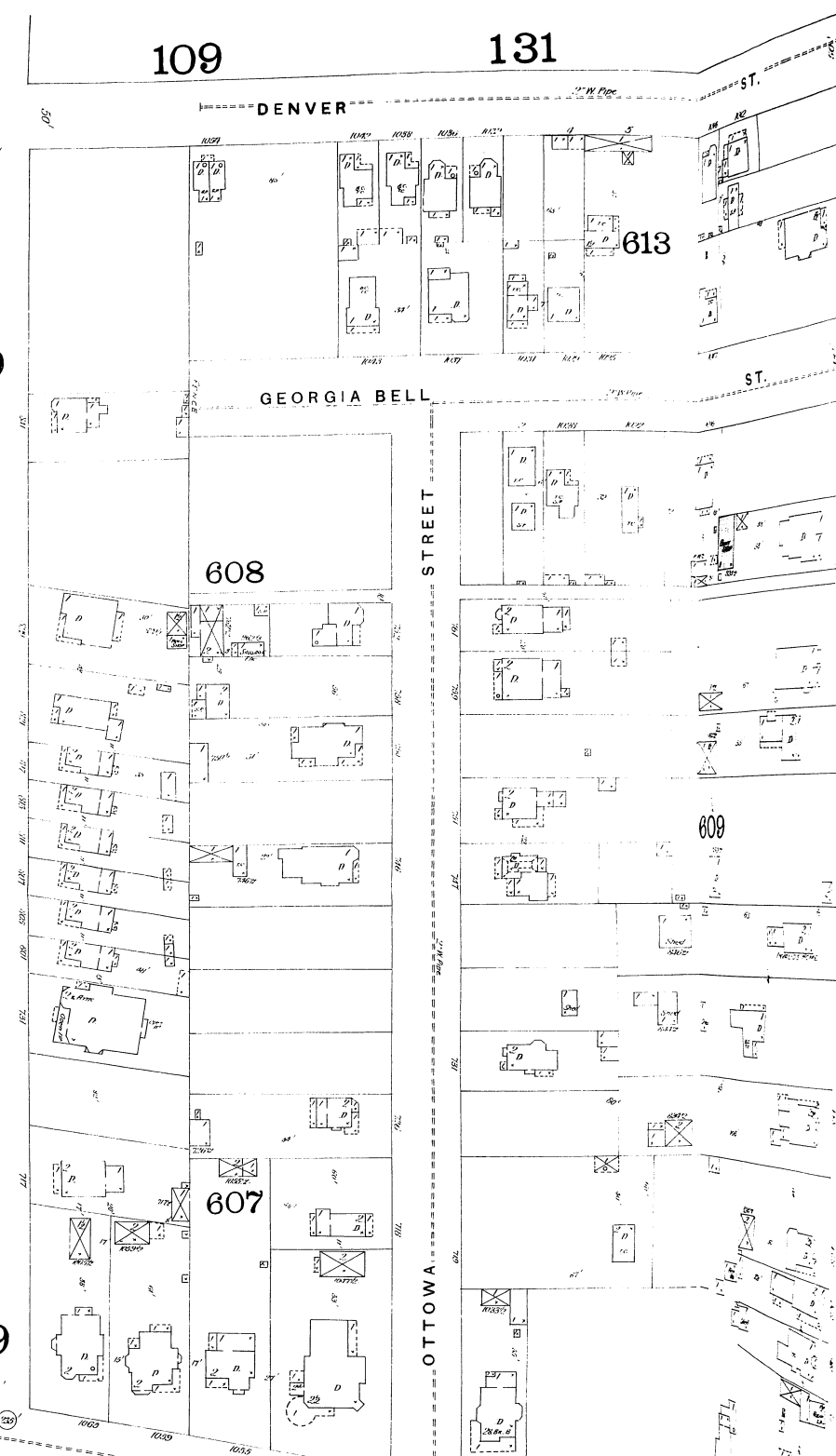
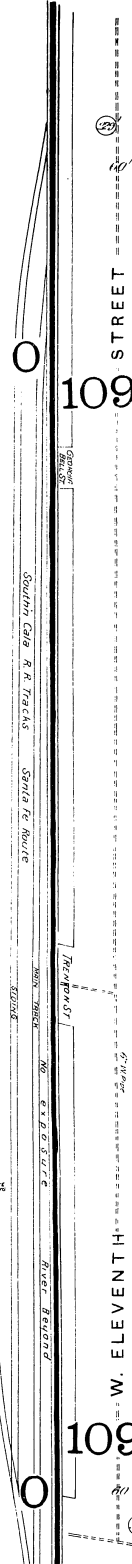
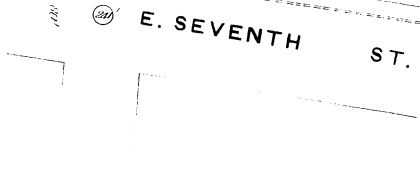
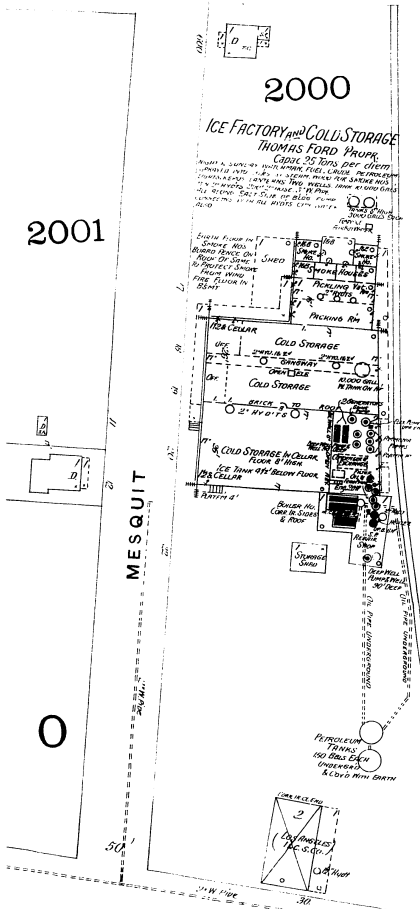
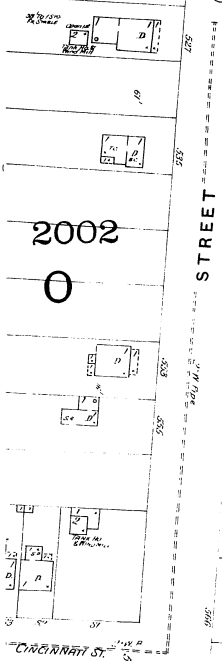
Architectural Historian. ESA prepared an EIR for the Boething Tree Farms project in Los Angeles. The project included redevelopment of the site occupied by a single-family residence and nursery business established in 1956 by self-taught horticulturalist John Boething. ESA conducted a Historic Resources Assessment as part of the EIR, which included a site survey and evaluation of the site, resulting in a recommendation for ineligibility as a historical resource. The project was then evaluated for potential impacts to any historical resources identified in the surrounding area. The report found no direct or indirect impacts to historical resource. The nearby Leonis Adobe, Calenda Ostronic Residential Historic District and Los Encinos Residential Historic District would have no significant views of the project site and each of the historical resources would remain eligible despite project completion. Chris was responsible for preparing the HRA and Historic Technical Report for the EIR.

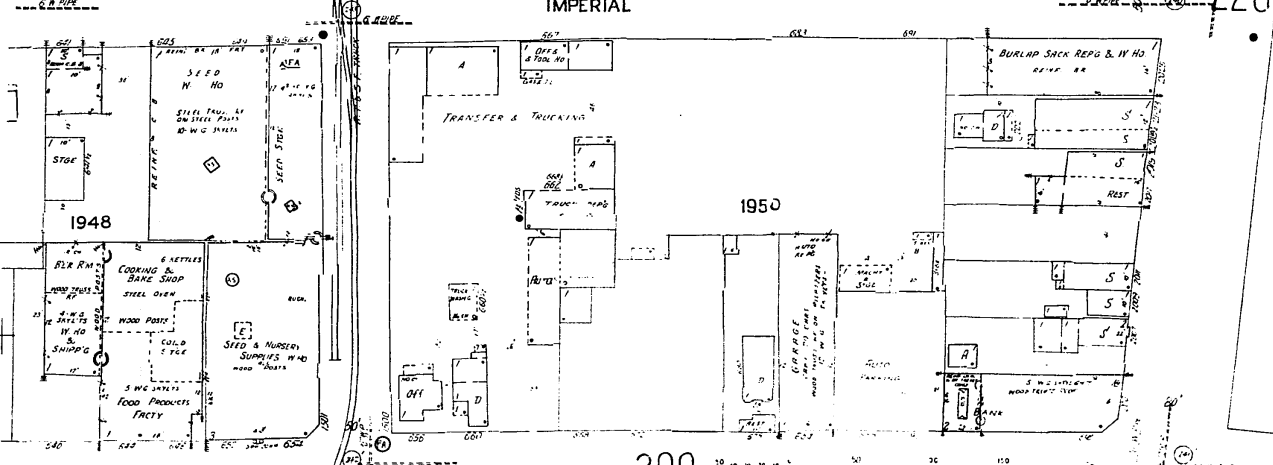
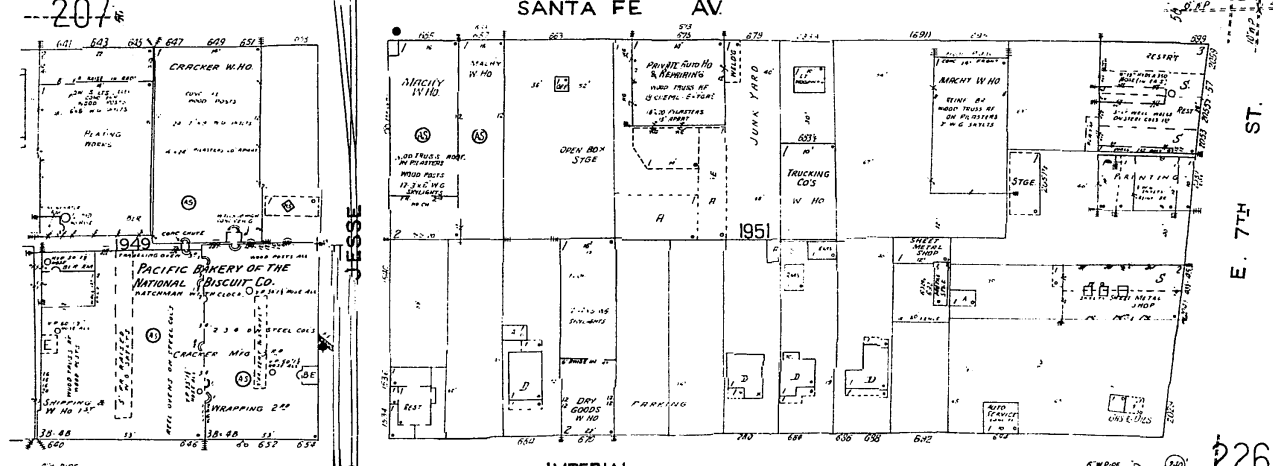
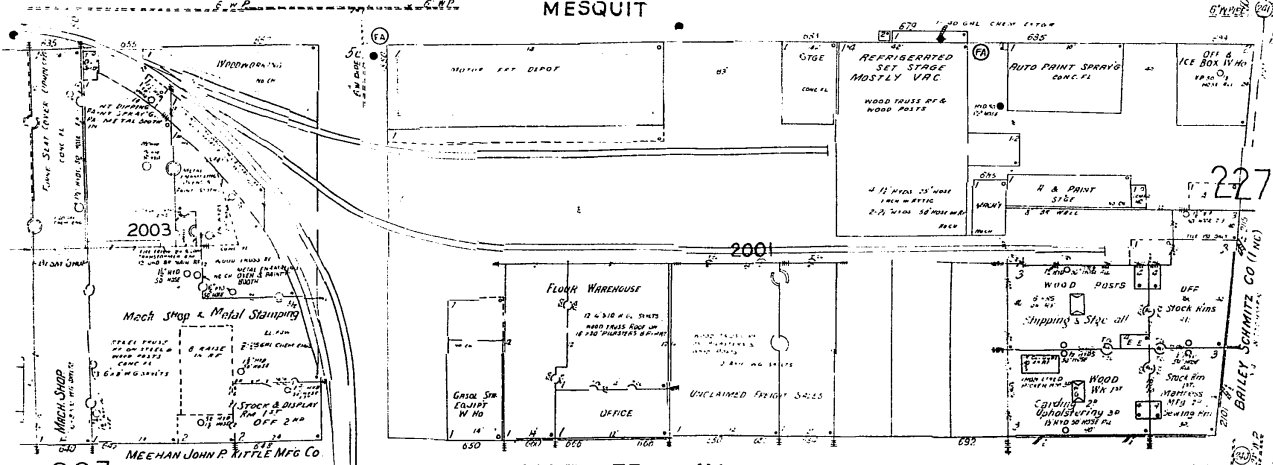
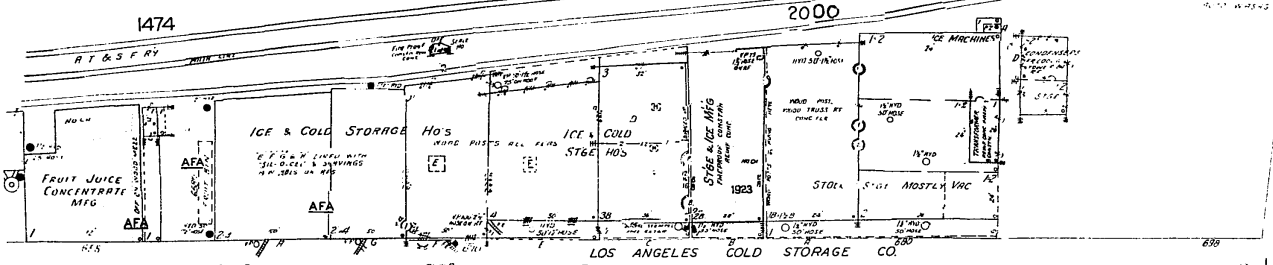
670 Mesquit Street, Los Angeles, CA. *Architectural Historian.* ESA prepared an IS/MND for the 670 Mesquit Street project in Los Angeles. As part of the IS/MND, a Historic Resources Assessment was prepared to determine if the project site was eligible for listing as a historical resource. The project site, originally occupied by the Los Angeles Ice and Cold Storage Company, was determined to lack integrity and therefore, ineligible for listing. Although the core of the building on the project site retained elements of the historic cold storage building, the facility was seismically upgraded resulting in significant alterations to its exterior. In its current condition, the facility does not convey its historical associations. The project was also evaluated to determine if it would result in any potential impacts to nearby historic resources. Located south of the project site is the Seventh Street Bridge, which is listed on the California Register of Historical Resources, and eligible for the National Register of Historic Places. The project would alter the setting of the bridge; however, the impact was determined to be less than significant. Chris was responsible for preparing the Historic Resources Assessment & Historic Technical Report for the IS/MND.

Burbank Bob Hope Airport, 2627 N. Hollywood Way, Burbank, CA. *Architectural Historian.* ESA evaluated the Burbank Airport for eligibility as a historic district, recommending ineligibility due to a lack of integrity. However, it was determined that a number of buildings on the property were individually significant. To make this determination, ESA architectural historians prepared a context covering the airport's historic development and its use by the Lockheed Martin Aircraft Company. ESA staff developed an airplane hangar property type, which was used to evaluate eleven of the airport's individual structures for architectural significance. The report evaluated three different options for the terminal replacement project, identifying the preferred arrangement with the least impact on identified historic resources. Chris assisted in conducting the site survey, archival research and preparation of the historic resource assessment and corresponding EIR section.

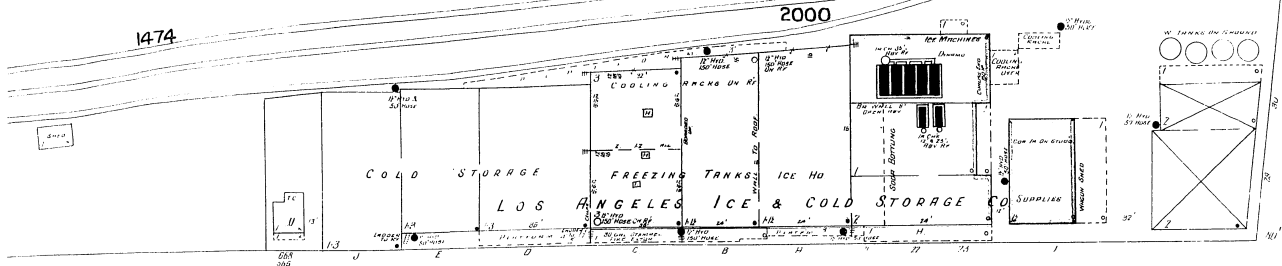
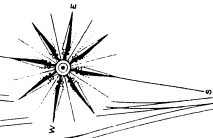
Appendix B

Sanborn Maps

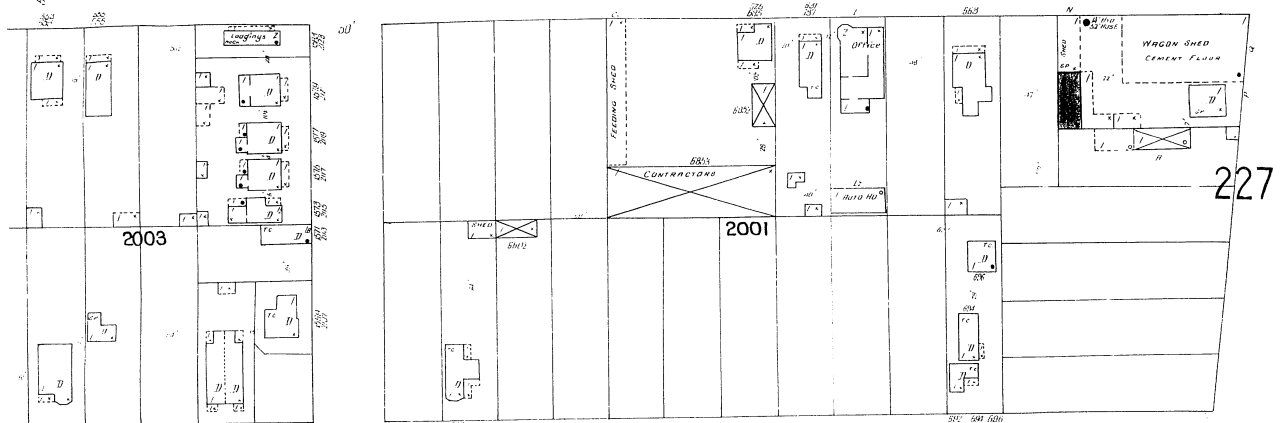




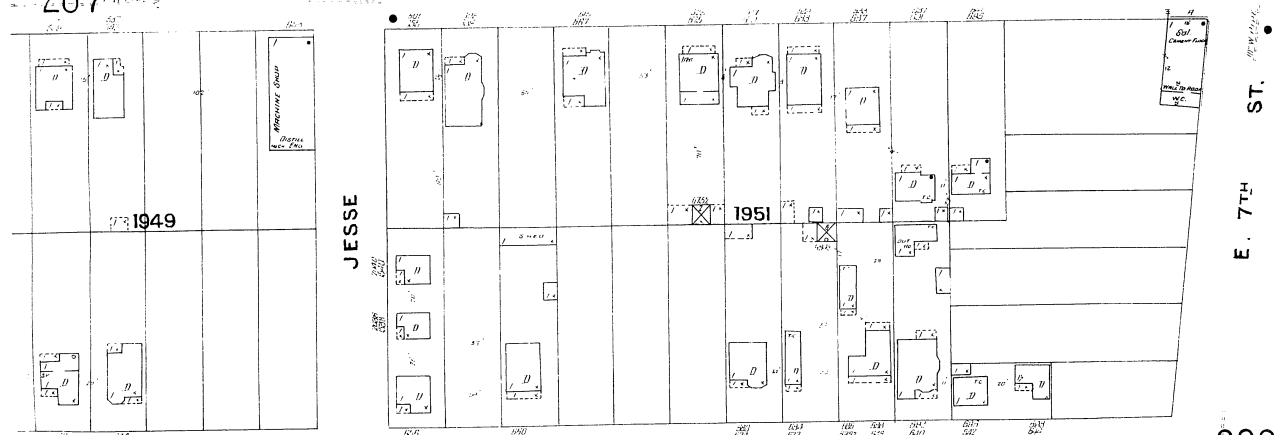
SMALL UNDESIGNATED BUILDING CONSTRUCTION
MAY BE SHOWN. THIS DRAWING IS NOT
TO BE USED FOR CONSTRUCTION PURPOSES
UNLESS SPECIFICALLY NOTED TO THE CONTRARY
BY THE ENGINEER.



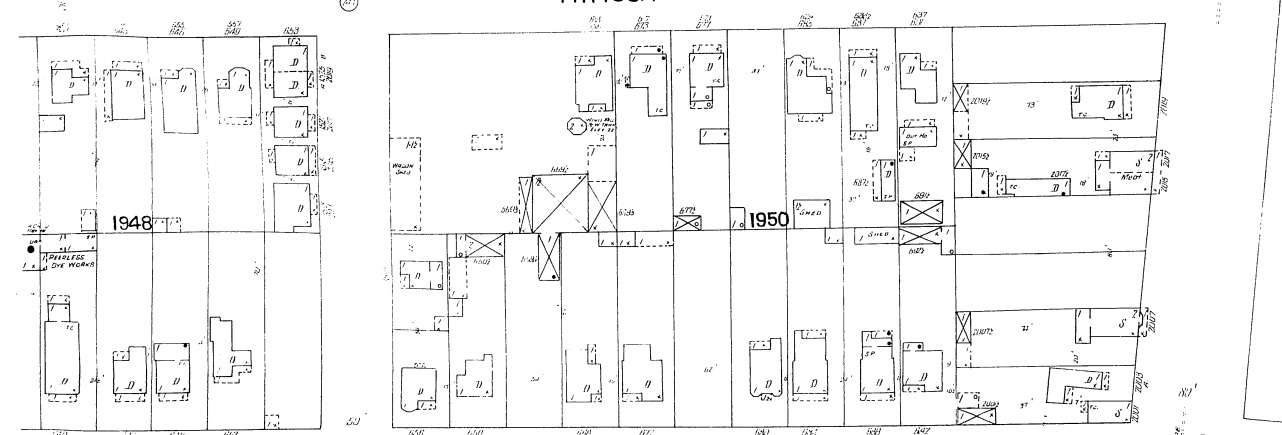
MESQUIT



SANTA FE AV.

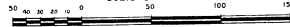


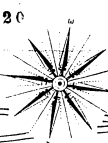
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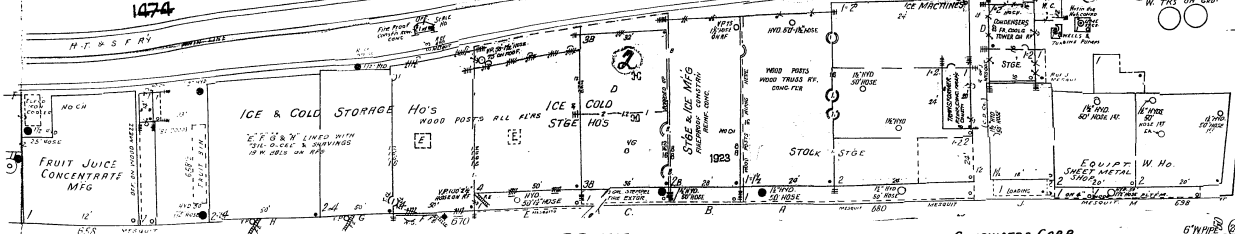
MATEO 209

Scale of Feet.



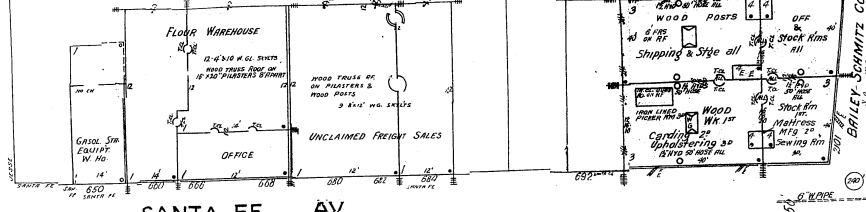
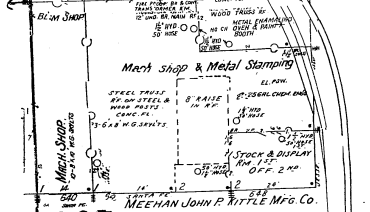
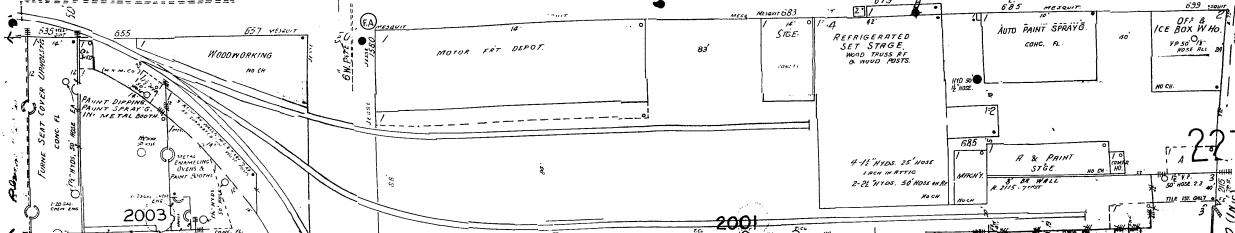


1474
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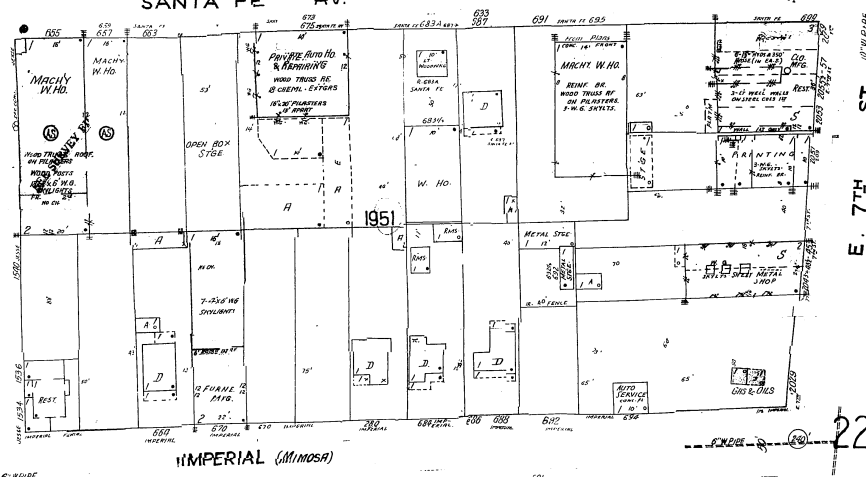
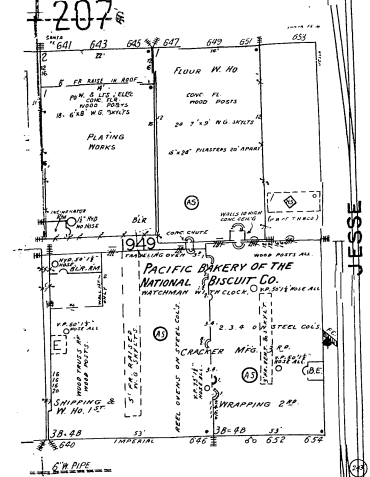


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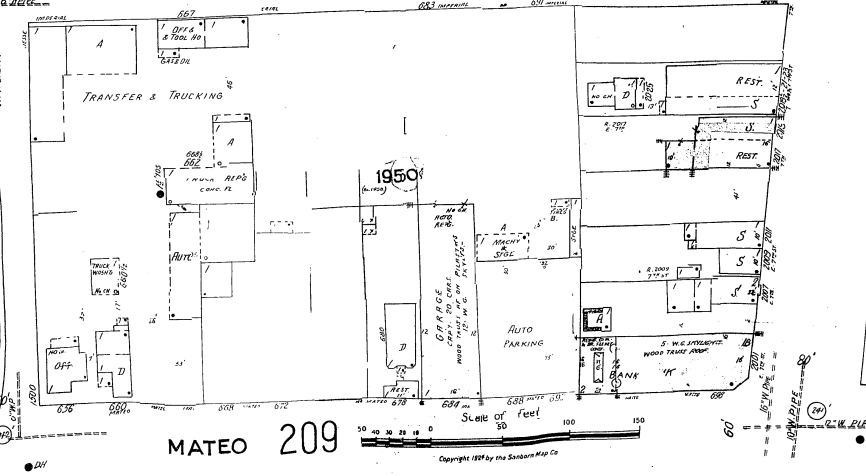
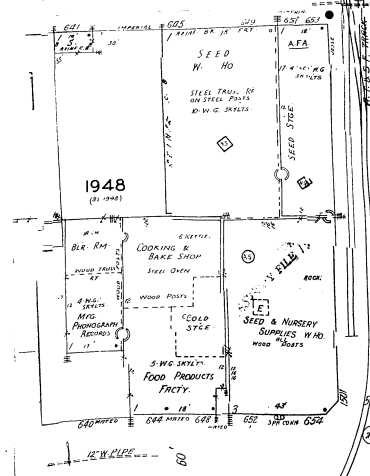
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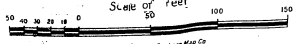
SANTA FE AV.



IMPERIAL (MIMOSA)



MATEO 209



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Appendix C

Building Permits (LADBS)

**TABLE C-1
SURVEY AREA #1 LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
08/22/1907	1907LA04967	Los Angeles Ice and Cold Storage Co.	None	G. H. Wyman	\$1,000	1-story addition, 24'x52' wood frame, concrete foundation (purpose office, room)
10/18/1911	1911LA09891	Union Well Supply Co.	H. E. Coffey	None	\$950	Extend present building 50', post and girder frame, covered, sides and roof with corrugated iron.
11/13/1911	1911LA10929	Union Well Supply Co.	H. E. Coffey	None	\$950	New construction: Warehouse and welding room,
08/08/1913	1913LA10436	Los Angeles Ice and Cold Storage Co.	Owner	George H. Wyman	\$1,000	Build brick vault for storage of books, remodel toilet rooms, and install new toilets and 4 lavatories, also reroof a part of building
09/23/1913	1913LA12426	Los Angeles Ice and Cold Storage Co.	Owner	None	\$250	Add additional girder to support tower
05/13/1915	1915LA07094	Los Angeles Ice and Cold Storage Co.	Owner	None	\$250	Wish to build shelter shed for horses and wagons as an addition to present wagon shed.
05/10/1921	1921LA10418	Los Angeles Ice and Cold Storage Co.	Owner	None	\$150	Demolish part of building to make room for office and cold storage building
07/07/1921	1921LA15421	Los Angeles Ice and Cold Storage Co.	Owner	Hunt and Burns	\$30,000	New construction of ice storage building. 92' x 140'
08/04/1921	1921LA18128	Whiting Wrecking Company	Whiting Wrecking Company	None	\$150	Demolish office building
01/08/1924	1924LA01196	Los Angeles Ice and Cold Storage Co.	Hodge and McMackin	Hunt and Burns	\$5,174	Two-story addition to existing building
07/21/1939	1939LA28691	California Consumers Corp.	None	None	\$300	Add to existing building, fire escape balcony
10/21/1952	1952LA44883	California Consumers Corp.	Owner	None	\$1,000	Repair structural columns damaged by dry rot

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
04/13/1955	1955LA11813	L.A. Wrecking Co.	Owner	None	\$800	Demolition of the existing storage house, 75' x 80'
11/14/1955	1955LA29539	L.A. Wrecking Co.	Owner	None	\$200	Demolition of the existing storage house, 20' x 36'
12/22/1961	1961LA04024*	West Coast Quilting Company	None	None	None	New construction of a 1-story structure, 120'x178' for offices and manufacturing
1963	Certificate of Occupancy	West Coast Quilting Company	None	None	None	1-story, type III-B, 120' x 178' office and manufacturing facility with 41 parking spaces
06/02/2006	06016-10000-10529	Frank H. Gallo	Central Coating Co.	None	None	Remove existing roof materials, install spray polyurethane foam and elastomeric coating roof system

* This information was derived from the Certificate of Occupancy Original building permit not available.

**TABLE C-2
SURVEY AREA #2 LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
03/06/2002	99010-10000-03361*	Rancho Cold Storage	Owner	None	None	New construction of 1-story structure, 193' x 148', reinforced concrete tilt-up cold storage building

* According to Certificate of Occupancy of the Project Site, Survey Area #2 on APN: 5164-017-008 was constructed in 2002. Building #2 does not meet the 50-year threshold for historic resources, therefore was not evaluated in this report.

**TABLE C-3
SURVEY AREA #3 (APN: 5164-017-003) LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
01/02/1908	1908LA00009	Los Angeles Ice and Cold Storage Co.	Owner	Hunt, Eager, and Burns	\$15,000	New construction: Storage building, 80'x96', concrete footing, wood columns and girders, composite roofing, on an empty lot
04/11/1908	1908LA01984	Los Angeles Ice and Cold Storage Co.	Owner	Owner	\$3,000	Raise ceiling and roof 10' and replace burned portions
07/22/1927	1927LA20768	Los Angeles Ice and Cold Storage Co.	Owner	None	\$1,500	Replace wooden platform with concrete platform and steel canopy
07/22/1927	1927LA20769	Los Angeles Ice and Cold Storage Co.	Owner	None	\$800	New concrete floor
03/28/1933	1933LA04113	California Consumers Corp.	Nance Construction Company	None	\$1,000	Tear down then replace fire walls facing Mesquit St and one section of fire wall on east end – wall will extend 2ft above roof line – quake damage
04/04/1933	1933LA04605	California Consumers Corp.	Nance Construction Company	None	\$500	Demolish and replace 30ft of present front wall at south end of building, install new posts under end of 2 trusses – earthquake damage
08/28/1944	1944LA16394	California Consumers Corp.	Owen Roofing Co.	None	\$585	Re-Roof
03/06/1951	1951LA02182	California Consumers Corp.	G. O. Gartz Construction Co.	George J. Fosdyke (engineer)	\$500	Replace portion of cracked wall
07/01/1952	1952LA38819	California Consumers Corp.	El Rancho Lumber Co.	None	\$270	Demolish 45' x 75' warehouse (678 Mesquit)
02/01/1954	1954LA79024	California Consumers Corp.	G. O. Gartz Construction Co.	None	\$6,200	Close 2 doorways in masonry wall, resurface floor, add office portions and ceiling, add toilet rooms, add air-conditioning for above

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
07/15/1965	1965LA99824	Rancho Cold Storage	Owner	None	\$300	Parapet correction as per plans and department file
07/15/1965	1965LA99827	Rancho Cold Storage	Owner	None	\$1,500	Parapet correction as per plans and department file
07/15/1965	1965LA99828	Rancho Cold Storage	Owner	None	\$101	Parapet correction as per plans and department file
08/18/1967	1967LA51570	Jack Levine	Owner	None	\$275	Parapet correction as per plans and department file
02/21/1990	1990HO04582	Don Gallo	None	Merdad Houriani (engineer)	\$280,000	Full compliance with division 88
04/03/1990	1990HO05425	Don Gallo	National Land Clearing	Ining LU (engineer)	\$7,700	Demolition of warehouse 35' x 55'
04/10/1990	1990LA54366	Don Gallo	None	Frank Gallo, Ining LU (engineer)	\$100,000	New cold storage warehouse, 35' x 55'
08/20/1990	1990LA61752	Don Gallo	None	Frank Gallo, Ining LU (engineer)	\$6,000	Change roof system to Kim truss
02/15/1991	1991HO11017	Don Gallo	None	Chester Schultz (engineer), Frank Gallo	\$280,000	Full compliance with section 88
05/02/1991	1991HO12329	Don Gallo	Gangi Builders	Chester Schultz (engineer), Frank Gallo	\$30,000	Change of roof design
10/29/1992	1992HO20055	Rancho Cold Storage	Gangi Builders	Chester Schultz (engineer), Frank Gallo	\$35,000	Replace east URM wall with new concrete block wall and replace south wood wall with steel frame and wood infill

**TABLE C-4
SURVEY AREA #4 LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
04/11/1908	1908LA01983	Los Angeles Ice and Cold Storage Co.	Owner	Owner	\$4,000	Raise ceiling and roof 10' and replace burned portions
09/04/1912	1912LA10666	Los Angeles Ice and Cold Storage Co.	Owner	None	\$1,650	Wish to raise the 2 nd floor of building 4ft 4in and build 1 new floor between 1 st and 2 nd floors
03/11/1932	1932LA04454	California Consumers Corp.	Owner	None	\$6,500	We intend to leave all present work intact in all rooms and expect only to add strength for greater floor load than has previously been possible
07/15/1965	1965LA99826	Rhoda and Jack Levine	Owner	None	\$2,500	Parapet correction as per plans and department file
04/27/1970	1970LA07726	Rancho Cold Storage	Driver and Eddy Construction	None	\$4,500	Repair fire damage
05/14/1970	1970LA08856	Mr. Black	Safe-way Sandblast	None	\$250	Sandblast inside black charred area
08/01/1973	1973LA75413	Jack Levine	None	Frank Lehnen Smith (engineer)	\$8,000	Repair fire damage, roof, walls, roof monitor
03/02/1988	1988LA90832	Don Gallo	None	Merdad Houriani (engineer)	\$15,000	Earthquake repair work, division 88
12/18/1989	1989HO03651	Rancho Cold Storage	Allied Roofing Co.	None	\$9,000	Reroof 3-layer modified bitumen
01/04/1989	1989LA22542	Don Gallo	None	Merdad Houriani (engineer)	\$138,000	Full compliance/ RGA
01/04/1989	1989LA22543	Don Gallo	None	Merdad Houriani (engineer)	\$171,000	Full compliance/ RGA
02/21/1990	1990HO04864	Don Gallo	None	Merdad Houriani (engineer)	\$288,000	Full compliance Division 88
07/14/1994	1994HO30028	Rancho Cold Storage	None	Ruby Quintiner (engineer)	\$171,000	Full compliance Division 88

**TABLE C-5
SURVEY AREA #5 (APN: 5164-017-006) LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
08/05/1909	1909LA04573	A.T.S.F. Railway	None	R. J. Arey	\$450	New construction: dwelling, 20' x 48', one-story, concrete foundation
05/02/1922	1922LA14876	Hyland Stanford Company	Owner	Owner	\$800	New construction: Fruit Juice Concentration factory on an empty lot
05/07/1924	1924LA21305	Hyland Stanford Company	Owner	Owner	\$600	Additions to existing factory, 14' x 66' on north, 16' x 44' on the west
02/11/1926	1926LA04601	Hyland Stanford Company	W. Douglas Lee	W. Douglas Lee	\$800	New construction: Bottling works, 20' x 50' board and baton wood siding
06/21/1928	1928LA17704	Los Angeles Ice and Cold Storage Co.	None	None	\$6,000	Alterations to flooring including concrete and other material (difficult to read), no structural changes
04/10/1931	1931LA07420	Hyland Stanford Company	None	None	\$700	New Construction: Conveyor house, 11'6" x 32', steel exterior walls
05/21/1936	1936LA12241	Hyland Stanford Company	Unreadable	None	\$700	New Construction: Boiler stack, 43" in diameter
07/22/1936	1936LA18577	None	Mars Awning and Shade Co.	None	\$20	New construction: Recovering Awnings
02/16/1938	1938LA04479	Hyland Stanford Company	None	None	\$1,500	Construction of additional fruit bins, 11' x 20'
09/25/1941	1941LA22269	Hyland Stanford Company	The Fluor Corp Ltd.	D. W. Darnell (engineer)	\$250	Dismantling water cooling tower and replacing with new larger cooling tower
09/26/1941	1941LA22427	Hyland Stanford Company	The Fluor Corp Ltd.	D. W. Darnell (engineer)	\$9,800	New construction of large water- cooling tower
04/27/1942	1942LA05785	California Consumers Corp.	G. O. Gartz	None	\$900	Change portion of dock from wood construction to concrete, and enclose with wood sash

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
11/28/1950	1950LA27894	Hyland Stanford Company	G. O. Gartz	George J. Fosdyke (engineer)	Unreadable	New construction: Storage, offices, citrus fruit processing
06/14/1963	1963LA40611	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$65,000	Addition of new produce cooler and storage building
06/18/1969	1963LA40836	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$200	New door between buildings
10/07/1963	1963LA49282	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$200	Relocate column
07/15/1965	1965LA99825	Rancho Cold Storage	Owner	None	\$150	Parapet correction as per plans and department file
07/02/1980	1980LA06114	Alexander Santoorian	Rustern Construction Co.	None	\$500	Seal off upper floors to comply with fire safety and board letter #794798
04/16/1985	1985LA10065	Alexander Santoorian	Kenco	None	\$90,000	Building is demolished, remove debris, clear lot
09/24/1985	1985LA20788	Alexander Santoorian	Foodpro Int'l Inc.	G. O. Dyer (engineer), Foodpro Int'l Inc.	\$500,000	Construction of 14,500 sf concrete block addition to replace one destroyed by fire
09/26/1986	1986LA46851	Alexander Santoorian	EV Erect	Peter Higgins (engineer)	\$18,000	Fabrication and installation of pallet racks
09/29/1986	1986LA47245	Alexander Santoorian	None	Peter Higgins (engineer)	\$22,000	Fabrication and installation of pallet racks

**TABLE C-1
SURVEY AREA #1 LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
08/22/1907	1907LA04967	Los Angeles Ice and Cold Storage Co.	None	G. H. Wyman	\$1,000	1-story addition, 24'x52' wood frame, concrete foundation (purpose office, room)
10/18/1911	1911LA09891	Union Well Supply Co.	H. E. Coffey	None	\$950	Extend present building 50', post and girder frame, covered, sides and roof with corrugated iron.
11/13/1911	1911LA10929	Union Well Supply Co.	H. E. Coffey	None	\$950	New construction: Warehouse and welding room,
08/08/1913	1913LA10436	Los Angeles Ice and Cold Storage Co.	Owner	George H. Wyman	\$1,000	Build brick vault for storage of books, remodel toilet rooms, and install new toilets and 4 lavatories, also reroof a part of building
09/23/1913	1913LA12426	Los Angeles Ice and Cold Storage Co.	Owner	None	\$250	Add additional girder to support tower
05/13/1915	1915LA07094	Los Angeles Ice and Cold Storage Co.	Owner	None	\$250	Wish to build shelter shed for horses and wagons as an addition to present wagon shed.
05/10/1921	1921LA10418	Los Angeles Ice and Cold Storage Co.	Owner	None	\$150	Demolish part of building to make room for office and cold storage building
07/07/1921	1921LA15421	Los Angeles Ice and Cold Storage Co.	Owner	Hunt and Burns	\$30,000	New construction of ice storage building. 92' x 140'
08/04/1921	1921LA18128	Whiting Wrecking Company	Whiting Wrecking Company	None	\$150	Demolish office building
01/08/1924	1924LA01196	Los Angeles Ice and Cold Storage Co.	Hodge and McMackin	Hunt and Burns	\$5,174	Two-story addition to existing building
07/21/1939	1939LA28691	California Consumers Corp.	None	None	\$300	Add to existing building, fire escape balcony
10/21/1952	1952LA44883	California Consumers Corp.	Owner	None	\$1,000	Repair structural columns damaged by dry rot

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
04/13/1955	1955LA11813	L.A. Wrecking Co.	Owner	None	\$800	Demolition of the existing storage house, 75' x 80'
11/14/1955	1955LA29539	L.A. Wrecking Co.	Owner	None	\$200	Demolition of the existing storage house, 20' x 36'
12/22/1961	1961LA04024*	West Coast Quilting Company	None	None	None	New construction of a 1-story structure, 120'x178' for offices and manufacturing
1963	Certificate of Occupancy	West Coast Quilting Company	None	None	None	1-story, type III-B, 120' x 178' office and manufacturing facility with 41 parking spaces
06/02/2006	06016-10000-10529	Frank H. Gallo	Central Coating Co.	None	None	Remove existing roof materials, install spray polyurethane foam and elastomeric coating roof system

* This information was derived from the Certificate of Occupancy Original building permit not available.

**TABLE C-2
SURVEY AREA #2 LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
03/06/2002	99010-10000-03361*	Rancho Cold Storage	Owner	None	None	New construction of 1-story structure, 193' x 148', reinforced concrete tilt-up cold storage building

* According to Certificate of Occupancy of the Project Site, Survey Area #2 on APN: 5164-017-008 was constructed in 2002. Building #2 does not meet the 50-year threshold for historic resources, therefore was not evaluated in this report.

**TABLE C-3
SURVEY AREA #3 (APN: 5164-017-003) LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
01/02/1908	1908LA00009	Los Angeles Ice and Cold Storage Co.	Owner	Hunt, Eager, and Burns	\$15,000	New construction: Storage building, 80'x96', concrete footing, wood columns and girders, composite roofing, on an empty lot
04/11/1908	1908LA01984	Los Angeles Ice and Cold Storage Co.	Owner	Owner	\$3,000	Raise ceiling and roof 10' and replace burned portions
07/22/1927	1927LA20768	Los Angeles Ice and Cold Storage Co.	Owner	None	\$1,500	Replace wooden platform with concrete platform and steel canopy
07/22/1927	1927LA20769	Los Angeles Ice and Cold Storage Co.	Owner	None	\$800	New concrete floor
03/28/1933	1933LA04113	California Consumers Corp.	Nance Construction Company	None	\$1,000	Tear down then replace fire walls facing Mesquit St and one section of fire wall on east end – wall will extend 2ft above roof line – quake damage
04/04/1933	1933LA04605	California Consumers Corp.	Nance Construction Company	None	\$500	Demolish and replace 30ft of present front wall at south end of building, install new posts under end of 2 trusses – earthquake damage
08/28/1944	1944LA16394	California Consumers Corp.	Owen Roofing Co.	None	\$585	Re-Roof
03/06/1951	1951LA02182	California Consumers Corp.	G. O. Gartz Construction Co.	George J. Fosdyke (engineer)	\$500	Replace portion of cracked wall
07/01/1952	1952LA38819	California Consumers Corp.	El Rancho Lumber Co.	None	\$270	Demolish 45' x 75' warehouse (678 Mesquit)
02/01/1954	1954LA79024	California Consumers Corp.	G. O. Gartz Construction Co.	None	\$6,200	Close 2 doorways in masonry wall, resurface floor, add office portions and ceiling, add toilet rooms, add air-conditioning for above

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
07/15/1965	1965LA99824	Rancho Cold Storage	Owner	None	\$300	Parapet correction as per plans and department file
07/15/1965	1965LA99827	Rancho Cold Storage	Owner	None	\$1,500	Parapet correction as per plans and department file
07/15/1965	1965LA99828	Rancho Cold Storage	Owner	None	\$101	Parapet correction as per plans and department file
08/18/1967	1967LA51570	Jack Levine	Owner	None	\$275	Parapet correction as per plans and department file
02/21/1990	1990HO04582	Don Gallo	None	Merdad Houriani (engineer)	\$280,000	Full compliance with division 88
04/03/1990	1990HO05425	Don Gallo	National Land Clearing	Ining LU (engineer)	\$7,700	Demolition of warehouse 35' x 55'
04/10/1990	1990LA54366	Don Gallo	None	Frank Gallo, Ining LU (engineer)	\$100,000	New cold storage warehouse, 35' x 55'
08/20/1990	1990LA61752	Don Gallo	None	Frank Gallo, Ining LU (engineer)	\$6,000	Change roof system to Kim truss
02/15/1991	1991HO11017	Don Gallo	None	Chester Schultz (engineer), Frank Gallo	\$280,000	Full compliance with section 88
05/02/1991	1991HO12329	Don Gallo	Gangi Builders	Chester Schultz (engineer), Frank Gallo	\$30,000	Change of roof design
10/29/1992	1992HO20055	Rancho Cold Storage	Gangi Builders	Chester Schultz (engineer), Frank Gallo	\$35,000	Replace east URM wall with new concrete block wall and replace south wood wall with steel frame and wood infill

**TABLE C-4
SURVEY AREA #4 LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
04/11/1908	1908LA01983	Los Angeles Ice and Cold Storage Co.	Owner	Owner	\$4,000	Raise ceiling and roof 10' and replace burned portions
09/04/1912	1912LA10666	Los Angeles Ice and Cold Storage Co.	Owner	None	\$1,650	Wish to raise the 2 nd floor of building 4ft 4in and build 1 new floor between 1 st and 2 nd floors
03/11/1932	1932LA04454	California Consumers Corp.	Owner	None	\$6,500	We intend to leave all present work intact in all rooms and expect only to add strength for greater floor load than has previously been possible
07/15/1965	1965LA99826	Rhoda and Jack Levine	Owner	None	\$2,500	Parapet correction as per plans and department file
04/27/1970	1970LA07726	Rancho Cold Storage	Driver and Eddy Construction	None	\$4,500	Repair fire damage
05/14/1970	1970LA08856	Mr. Black	Safe-way Sandblast	None	\$250	Sandblast inside black charred area
08/01/1973	1973LA75413	Jack Levine	None	Frank Lehnen Smith (engineer)	\$8,000	Repair fire damage, roof, walls, roof monitor
03/02/1988	1988LA90832	Don Gallo	None	Merdad Houriani (engineer)	\$15,000	Earthquake repair work, division 88
12/18/1989	1989HO03651	Rancho Cold Storage	Allied Roofing Co.	None	\$9,000	Reroof 3-layer modified bitumen
01/04/1989	1989LA22542	Don Gallo	None	Merdad Houriani (engineer)	\$138,000	Full compliance/ RGA
01/04/1989	1989LA22543	Don Gallo	None	Merdad Houriani (engineer)	\$171,000	Full compliance/ RGA
02/21/1990	1990HO04864	Don Gallo	None	Merdad Houriani (engineer)	\$288,000	Full compliance Division 88
07/14/1994	1994HO30028	Rancho Cold Storage	None	Ruby Quintiner (engineer)	\$171,000	Full compliance Division 88

**TABLE C-5
SURVEY AREA #5 (APN: 5164-017-006) LADBS BUILDING PERMITS**

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
08/05/1909	1909LA04573	A.T.S.F. Railway	None	R. J. Arey	\$450	New construction: dwelling, 20' x 48', one-story, concrete foundation
05/02/1922	1922LA14876	Hyland Stanford Company	Owner	Owner	\$800	New construction: Fruit Juice Concentration factory on an empty lot
05/07/1924	1924LA21305	Hyland Stanford Company	Owner	Owner	\$600	Additions to existing factory, 14' x 66' on north, 16' x 44' on the west
02/11/1926	1926LA04601	Hyland Stanford Company	W. Douglas Lee	W. Douglas Lee	\$800	New construction: Bottling works, 20' x 50' board and baton wood siding
06/21/1928	1928LA17704	Los Angeles Ice and Cold Storage Co.	None	None	\$6,000	Alterations to flooring including concrete and other material (difficult to read), no structural changes
04/10/1931	1931LA07420	Hyland Stanford Company	None	None	\$700	New Construction: Conveyor house, 11'6" x 32', steel exterior walls
05/21/1936	1936LA12241	Hyland Stanford Company	Unreadable	None	\$700	New Construction: Boiler stack, 43" in diameter
07/22/1936	1936LA18577	None	Mars Awning and Shade Co.	None	\$20	New construction: Recovering Awnings
02/16/1938	1938LA04479	Hyland Stanford Company	None	None	\$1,500	Construction of additional fruit bins, 11' x 20'
09/25/1941	1941LA22269	Hyland Stanford Company	The Fluor Corp Ltd.	D. W. Darnell (engineer)	\$250	Dismantling water cooling tower and replacing with new larger cooling tower
09/26/1941	1941LA22427	Hyland Stanford Company	The Fluor Corp Ltd.	D. W. Darnell (engineer)	\$9,800	New construction of large water- cooling tower
04/27/1942	1942LA05785	California Consumers Corp.	G. O. Gartz	None	\$900	Change portion of dock from wood construction to concrete, and enclose with wood sash

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
11/28/1950	1950LA27894	Hyland Stanford Company	G. O. Gartz	George J. Fosdyke (engineer)	Unreadable	New construction: Storage, offices, citrus fruit processing
06/14/1963	1963LA40611	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$65,000	Addition of new produce cooler and storage building
06/18/1969	1963LA40836	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$200	New door between buildings
10/07/1963	1963LA49282	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$200	Relocate column
07/15/1965	1965LA99825	Rancho Cold Storage	Owner	None	\$150	Parapet correction as per plans and department file
07/02/1980	1980LA06114	Alexander Santoorian	Rustern Construction Co.	None	\$500	Seal off upper floors to comply with fire safety and board letter #794798
04/16/1985	1985LA10065	Alexander Santoorian	Kenco	None	\$90,000	Building is demolished, remove debris, clear lot
09/24/1985	1985LA20788	Alexander Santoorian	Foodpro Int'l Inc.	G. O. Dyer (engineer), Foodpro Int'l Inc.	\$500,000	Construction of 14,500 sf concrete block addition to replace one destroyed by fire
09/26/1986	1986LA46851	Alexander Santoorian	EV Erect	Peter Higgins (engineer)	\$18,000	Fabrication and installation of pallet racks
09/29/1986	1986LA47245	Alexander Santoorian	None	Peter Higgins (engineer)	\$22,000	Fabrication and installation of pallet racks

Std. Insp. 466-7-6-2000

Ward 7

Applicant must indicate the Building Line or Lines clearly and distinctly on the Drawings.

BOARD OF PUBLIC WORKS

DEPARTMENT OF BUILDINGS

Application for Erection of "Class B & C" Buildings

Application is hereby made to the Chief Inspector of Buildings of the City of Los Angeles for the approval of the detailed statement of the specifications and plans herewith submitted for the erection of the building herein described. All provisions of the Building Ordinances shall be complied with in the erection of said building, whether specified herein or not.

(Sign here) J.R. Burns / J.R. Burns

Los Angeles, Cal. JAN 2-1908 1908

Location: Lot 265-266 by 267 Block 127

Assessor Please Verify

Wingertner Tract

Mallard Jones

Engineer Please Verify

District No. 5 M. B. page 9 F. B. page 127

No. 674 to 678 Mesquite Street W.B.

1. Purpose of the Building and Number of Rooms Storage Building (Ice etc)
- Is any part to be used for store or other business purposes? If so, state what Ice etc
2. Owner's name Los Angeles Ice & Cold Storage Co.
3. Owner's address Mesquite St.
4. Architect's name Hunt Egan & Burns
5. Builder's name L.A.I. & Co.
6. Builder's address Mesquite
7. Estimated Cost of the Proposed Improvements, \$ 15000.00
8. Size of lot? _____ Size of Building 80 ft x 96 ft
- Are there any buildings within 30 feet of proposed structure other than a blank wall? No
9. Number of stories 4
- Height from curb level to highest point: main building? 53 feet
10. What is the character of the ground: rock, clay, sand, filled, etc. Sand or gravel
11. Will there be a basement? No
12. What will be the footing: stone, concrete or brick? Concrete
- Give thickness and width See plans
13. What will be the depth of foundation walls below curb level? 4 ft.
14. Give thickness of foundation walls and material to be used _____
15. Give size of piers and cap plates or stones _____
16. Of what materials will the upper walls be constructed? _____
- What will be thickness of walls? _____
- Basement _____ 5th story: See plans
- 1st story: _____ 6th story: _____
- 2d story: _____ 7th story: _____
- 3d story: _____ 8th story: _____
- 4th story: _____
17. What will be the materials of the front blue brick If stone, what kind? _____
- If ashlar give thickness _____
18. Number and kind of chimneys and flues None

PERMIT NO 9

19. Will any wall be supported on iron or steel girders? no
 Front, material.....size.....weight or thickness.....
 Side, ".....size....."
 Rear, ".....size....."
 Interior, ".....size....."

Will any wall be supported on iron or steel columns? no
 Front, material.....size.....weight or thickness.....
 Side, ".....size....."
 Rear, ".....size....."
 Interior, ".....size....."

20. Give material of girders wood of columns wood
 Under 1st tier—size of girders.....Size of columns.....
 " 2nd " " " " " " " " " " " "
 " 3d " " " " " " " " " " " "
 " 4th " " " " " " " " " " " "
 " 5th " " " " " " " " " " " "
 " 6th " " " " " " " " " " " "
 " 7th " " " " " " " " " " " "
 " Roof " " " " " " " " " " " "

} In plans

21. Give material, size and distance on centres of floor joist.
 1st tier, material.....; size.....; distance on centres.....
 2d " ".....; size.....; " ".....
 3d " ".....; size.....; " ".....
 4th " ".....; size.....; " ".....
 5th " ".....; size.....; " ".....
 6th " ".....; size.....; " ".....
 7th " ".....; size.....; " ".....
 8th " ".....; size.....; " ".....
 Ceiling joist tier, material.....; size.....; " ".....
 Roof " ".....; size.....; " ".....

22. Specify construction of floors.....

23. Of what material will partitions be built?.....

24. What will be the material of roofing? Composition

25. Of what material will elevator shafts and other shafts be constructed?.....

26. What will be the material of the cornices? brick

27. Will fire escapes be provided, and where placed? See elevation

28. Will metal lath be used in entire first floor of proposed building? no

29. Is there any other building erected on lot or permit granted for one? no

30. Are any buildings to be taken down? no; how many.....

31. Will freight elevators be enclosed or be provided with doors and fuseable links?.....

32. How will soffits of stairs be plastered?.....

33. Of what material will stairways be constructed? brick

All applications must be filled out by applicant

Ward 7

Applicant must indicate the Building Line or Lines clearly and distinctly on the Drawings.

BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

Application is hereby made to the Chief Inspector of Buildings of the City of Los Angeles, for the approval of the detailed statement of the specifications herewith submitted for the alteration, repair or demolition of the building herein described. All provisions of the Building Ordinances shall be complied with in the alteration, repair or demolition of said building, whether specified or not.

(Sign Here) L. A. Ice & Cold Storage Co
Los Angeles, Cal., APR 11 1908 1908

CITY ASSESSOR: Please Verify

REMOVED FROM

REMOVED TO

Lot 262 & 263, Block.....

Lot....., Block.....

Tract Thurston

Tract.....

TAKE TO
ROOM NO. 6
FIRST FLOOR

Mabland
for Herzog

Book 5 Page 9 F. B. Page 127

Book..... Page..... F. B. Page.....

TAKE TO
ROOM NO. 34
THIRD FLOOR

CITY ENGINEER: Please Verify Street Number

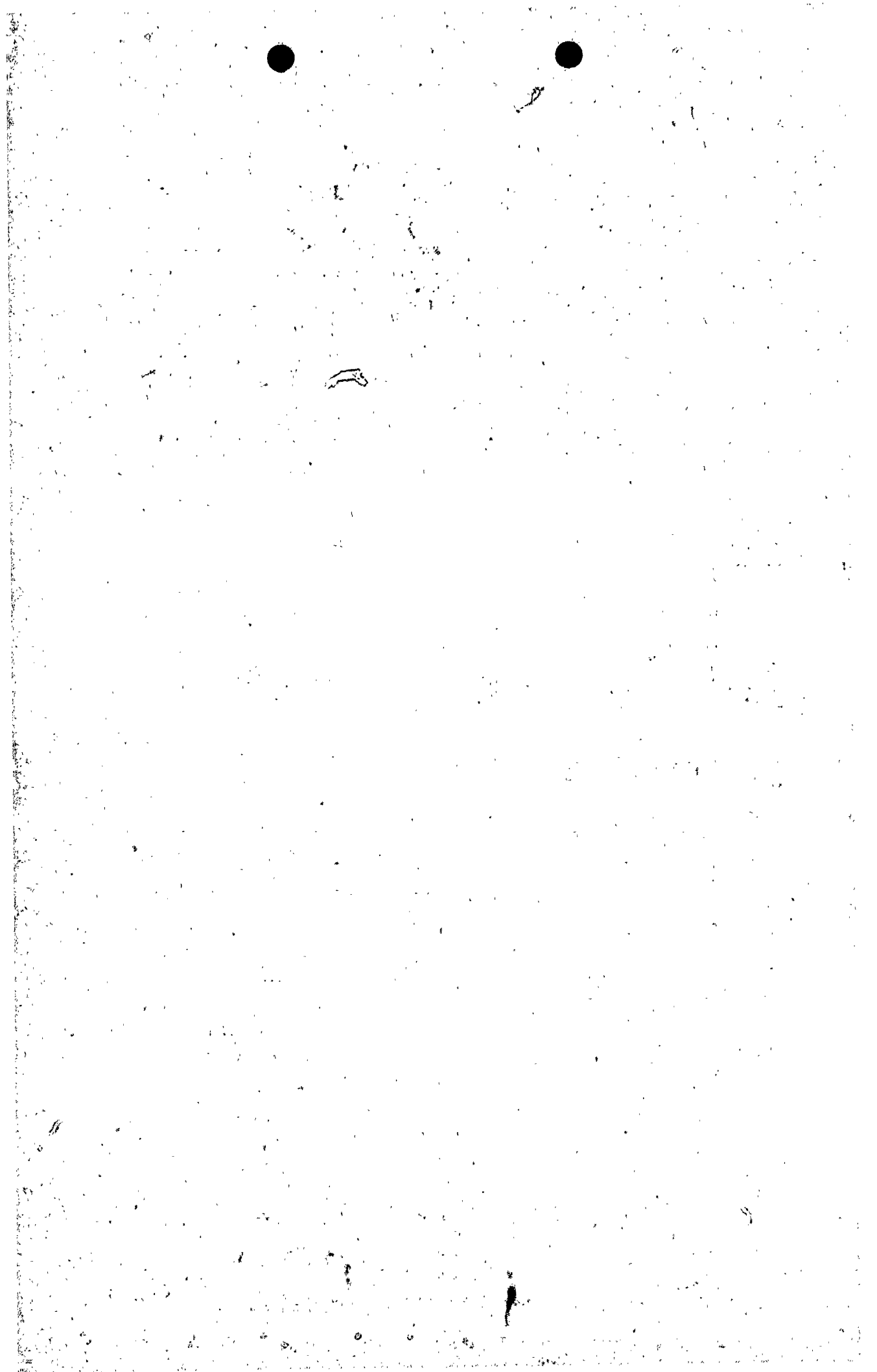
From No. 668-670 Mesquit St To No. 670

1. Owner's name L. A. Ice & Cold Storage Co
2. Owner's address 689 Mesquit
3. Architect's name.....
4. Builder's name Self
5. Builder's address.....
6. Entire cost of the Proposed Improvements, \$ 4000⁰⁰
7. Purpose of building Ice Storage
8. Class of Building Frame No. of rooms at present one
9. No. of stories in height one Size of building 80 X 110
10. Size of addition.....
11. Material of foundation Concrete Size Footing 36" Size of wall 20"
12. Size of exterior studs 2 X 12 Interior studs none
13. Size of mud sills..... Bearing studs X
14. Size of first floor joist 4 X 4 Second floor joist 2x4 in. curved
15. State on following lines just what you want to do: Raise ceiling & roof 10 ft & replace burned portion

1983

Permit No.

(11)



All applications must be filled out by applicant

Ward 7

Applicant must indicate the Building Line or Lines clearly and distinctly on the Drawings.

BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

Application is hereby made to the Chief Inspector of Buildings of the City of Los Angeles, for the approval of the detailed statement of the specifications herewith submitted for the alteration, repair or demolition of the building herein described. All provisions of the Building Ordinances shall be complied with in the alteration, repair or demolition of said building, whether specified or not.

(Sign Here) Los Angeles Ice & Cold Storage Co.
Los Angeles, Cal., APR 11 1908 1908

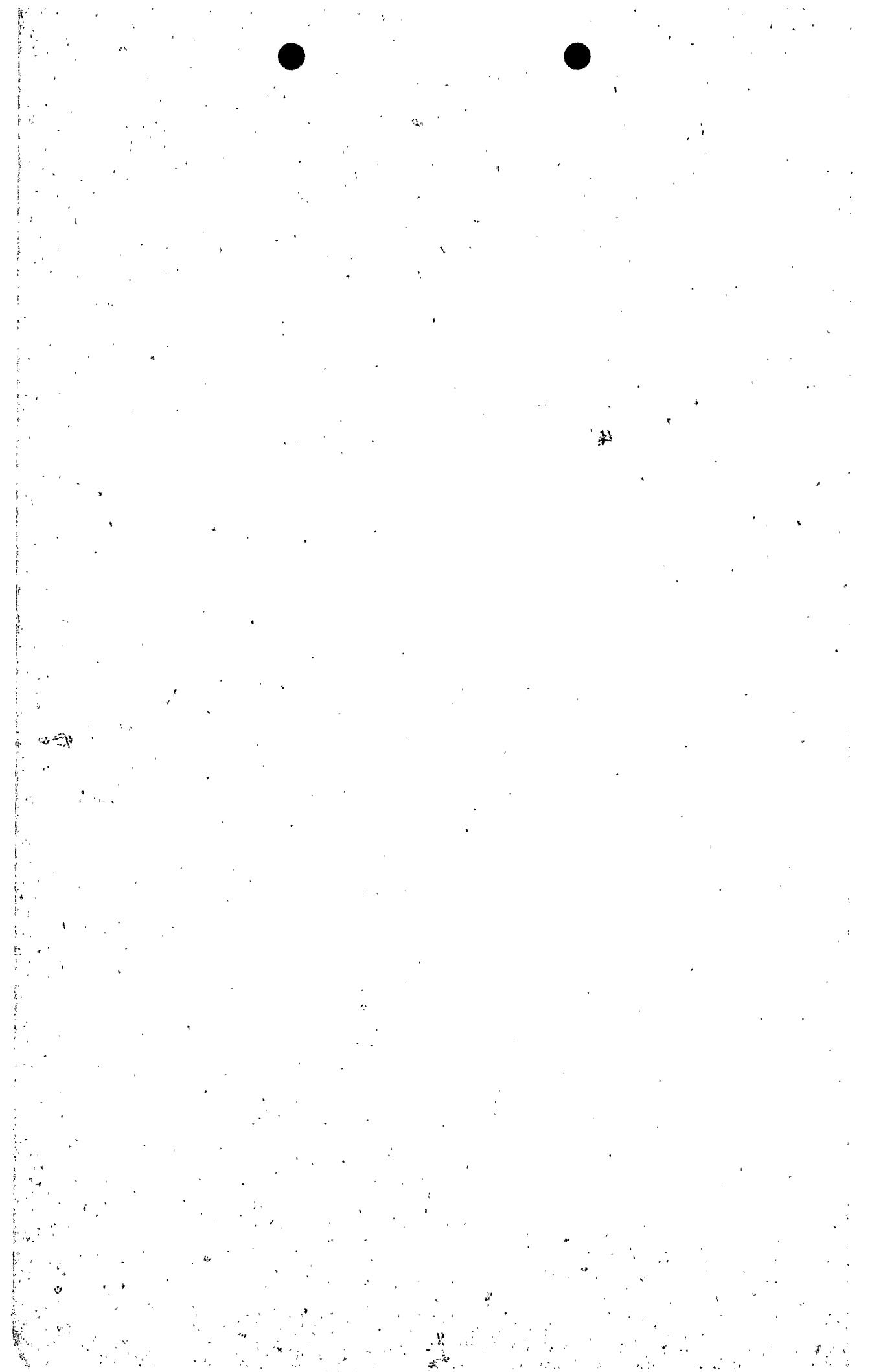
CITY ASSESSOR: Please Verify

REMOVED FROM		REMOVED TO	
Lot. <u>365 & 415</u> , Block	Lot. _____, Block	Tract <u>Hingert</u>	Tract _____
TAKE TO ROOM NO. 8 FIRST FLOOR		M. H. Hertzog	
Book <u>5</u> Page <u>9</u> F. B. Page <u>127</u>	Book _____ Page _____ F. B. Page _____		

TAKE TO ROOM NO. 34 THIRD FLOOR } CITY ENGINEER: Please Verify Street Number
From No. 672-674 Mesquite St To No. 674

- Owner's name L. A. Ice & Cold Storage Co.
- Owner's address 689 Mesquite
- Architect's name _____
- Builder's name Self
- Builder's address _____
- Entire cost of the Proposed Improvements, \$ 3000.00
- Purpose of building Ice Storage
- Class of Building Frame No. of rooms at present one
- No. of stories in height one Size of building 55 X 110
- Size of addition X
- Material of foundation Brick Size Footing _____ Size of wall 20"
- Size of exterior studs 2 X 12 Interior studs none
- Size of mud sills _____ Bearing studs _____
- Size of first floor joist 4 X 4 Second floor joist _____
- State on following lines just what you want to do: Raise ceiling & roof 10 ft. & replace burned portions

Permit No. 1984



All applications must be filled out by applicant.

BOARD OF PUBLIC WORKS
DEPARTMENT OF BUILDINGS

Ward 7

Applicant must indicate the Building Line or Lines clearly and distinctly on the Drawings.

Application for Erection of Frame Buildings
CLASS "D"

Application is hereby made to the Chief Inspector of Buildings of the City of Los Angeles, for the approval of the detailed statement of the specifications and plans herewith submitted for the erection of the building herein described. All provisions of the Building Ordinances shall be complied with in the erection of said building, whether specified herein or not.

(Sign here) Los Angeles Ice Cold Storage Co
Los Angeles, Cal., APR 20 1908

TAKE TO ROOM NO. 6 FIRST FLOOR

Lot No. 275 Block _____

ASSESSOR PLEASE VERIFY

Hungerford Park of Malabar

District No. 5 M. B. page 9 F. B. page 27

TAKE TO ROOM NO. 34 THIRD FLOOR

ENGINEER PLEASE VERIFY

No. 694 Mesquit Street

- Purpose of the building Oil house Shed
- Number of rooms one
- Is any part to be used for store or other business purposes? _____
- Owner's name Los Angeles Ice Cold Storage Co
- Owner's address 1689 Mesquit
- Architect's name none
- Builder's name same
- Builder's address _____
- Entire cost of proposed building, \$ 100.00
- Will building be erected on front or rear of lot? rear
- Size of lot _____ x _____ Size of building 10' x 20'
- Number of stories in height one; height from curb level to highest point 12'
- Height of first floor above curb level _____
- What is the character of the ground: rock, clay, sand, filled, etc.? sandy
- What will be the depth of the foundation below curb level? none
- Of what material will foundation and cellar walls be built? _____
- Give dimensions of footings none
- Give thickness of foundation and cellar walls at top _____
- Number and kind of chimneys _____ Number of flues _____
- Number of inlets to each flue _____ Interior size of flues _____ x _____
- What will be the size of the mud sills 4 x 6 Size of girders or stringers _____ x _____
- What will be the size of exterior studs? 2 x 4 Interior studs _____ x _____
- Bearing partition studs _____ x _____

[Handwritten signature/initials]

PERMIT NO. 2164

OVER

24. Give size of floor joist:

First floor none Third floor x
Second floor x Fourth floor x

25. Give size of ceiling joist 2x4 Size of rafters 2x4

26. Will the roof be peak, flat or mansard? peak Material of roofing Galv Iron

27. Will cellar or basement ceiling be plastered? _____ Metal or wood lath? _____

28. Of what material will floors be constructed? none

How many thicknesses? _____

What kind of fireproofing? _____

29. How will hall and soffits of stairs be plastered? _____

30. How many fire escapes will be provided? _____ Where placed? _____

31. Are any buildings to be demolished? _____ How many? _____

REMARKS

Halls & roof Galv Iron

All applications must be filled out by applicant.

WARD 7

Applicant must indicate the Building Line or Lines clearly and distinctly on the drawings.

BOARD OF PUBLIC WORKS

DEPARTMENT OF BUILDINGS

Application for Erection of Frame Buildings

CLASS "D"

Application is hereby made to the Chief Inspector of Buildings of the City of Los Angeles, for the approval of the detailed statement of the specifications and plans herewith submitted for the erection of the building herein described. All provisions of the Building Ordinances shall be complied with in the erection of said building, whether specified herein or not.

(SIGN HERE) A. T. S. J. Ray

Los Angeles, Cal. AUG 5- 1909, 190...

TAKE TO ROOM NO 6 FIRST FLOOR

Lot No. 88-5990 Block Goodman

ASSESSOR PLEASE VERIFY

District No. 5 M. B. page 9 F. B. page 108

TAKE TO ROOM NO. 34 THIRD FLOOR

ENGINEER PLEASE VERIFY

No. 648-658 Mcquik Street

1. PURPOSE OF BUILDING Dwelling Number of rooms 8
2. OWNER'S NAME A. T. S. J. Ray
3. Owner's address Cor 6th and main Sts 2 A
4. Architect's name Prof. A. Ray
5. CONTRACTOR'S NAME _____
6. Contractor's address _____
7. ENTIRE COST OF PROPOSED BUILDING, \$ 450.00
8. Size of lot 40' x 120' Size of building 20' x 48'
9. Will building be erected on front or rear of lot? Rear
10. NUMBER OF STORIES IN HEIGHT One Height to highest point of roof: 12'
11. Height of first floor joist above curb level? _____
12. Character of ground: rock, clay, sand, filled, etc. clay filled
13. Of what material will FOUNDATION and cellar walls be built? Concrete
14. GIVE depth of FOUNDATION below the surface of ground 1 1/2"
15. GIVE dimensions of FOUNDATION and cellar wall footings 14"
16. GIVE dimensions of FOUNDATION and cellar walls at top 8"
17. NUMBER and KIND of chimneys Two Brick Number of flues Two
18. Number of inlets to each flue One Interior size of flues 8" x 8"
19. Give sizes of following materials: MUDSILLS x Girders and stringers x
 EXTERIOR STUDS 2 x 4 BEARING STUDS 2 x 4 Interior studs 2 x 4
 Ceiling joist 2 x 4 Roof rafters 2 x 4 FIRST FLOOR JOISTS 2 x 6
 SECOND FLOOR JOIST x Third floor joist x Fourth floor joist x
20. Will the roof be peak/flat or mansard? Peak Material of roofing Shingles

4570

Permit No. _____

OVER

All applications must be filled out by applicant.
(USE INK OR INDELIBLE PENCIL)

PLANS AND SPECIFICATIONS and other data must also be filed.

Ward 7

BOARD OF PUBLIC WORKS

DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

Application is hereby made to the Board of Public Works (Chief Inspector of Buildings), of the City of Los Angeles, for the approval of this detailed statement of specifications herewith submitted for the alteration, repair or demolition of the building herein described. All provisions of the building ordinances and state laws shall be complied with in the alteration of said building, whether herein specified or not. It is also understood the granting of a permit on this application does not grant any right or privilege for the alteration, repair or demolition of the building herein described, on any public street or alley or on any land or portion thereof, the title or right of possession to which is in litigation by, or is disputed by the City, County or State; or as giving or granting any right or privilege to use the said structure or building for any purpose which is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

(SIGN HERE) Los Angeles Ice & Cold Storage Co

CITY ASSESSOR: Please Verify

REMOVED FROM		REMOVED TO	
Lot <u>273</u>	Block _____	Lot _____	Block _____
Tract <u>Hungerford Tract</u>	_____	Tract _____	_____
Book <u>5</u>	Page <u>9</u>	F. B. Page <u>122</u>	Book _____ Page _____ F. B. Page _____

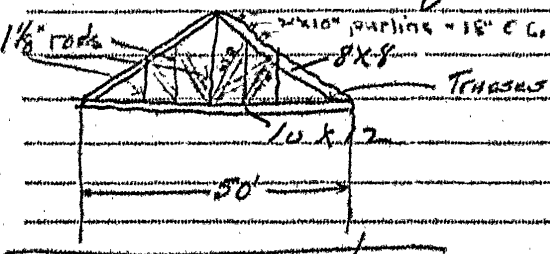
CITY ENGINEER: Please Verify Street Number

From No. _____ To No. 690 Mesquit St

- Owner's name Los Angeles Ice Cold Storage Co
- Owner's address 689 Mesquit St
- Architect's name _____
- Contractor's name Self
- Contractor's address _____
- Entire cost of the Proposed Improvements, \$ 700.00
- Purpose of the building Strengthen roof trusses Engine room
- Class of building Frame No. of rooms at present _____
- No. of stories in height 1 Size of present building 50 x 100
- Size of new addition _____ X _____
- Material of foundation _____ Size Footing _____ Size of wall _____
- Size of exterior studs _____ X _____ Interior studs _____ X _____
- Size of mud sills _____ X _____ Bearing studs _____ X _____
- Size of first floor joist _____ X _____ Second floor joist _____ X _____

STATE ON FOLLOWING LINES JUST WHAT YOU WANT TO DO:—

Put in stronger roof trusses



PERMIT NO. 1789

Date issued FEB 15 1912

Application Received Man dock

(A)

TAKE TO ROOM NO. 6 FIRST FLOOR
 BY C. Tolson

TAKE TO ROOM NO. 34 THIRD FLOOR

REMARKS.

Application Received _____ Time _____ Returned _____

All applications must be filled out by applicant

(USE INK OR INDELIBLE PENCIL)

PLANS AND SPECIFICATIONS and other data must also be filed.

WARD 3

BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be, prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

(SIGN HERE)

L. A. Ice & Cold Storage (Applicant)

CITY ASSESSOR: PLEASE VERIFY.

Table with columns: REMOVED FROM (Lot, Block, Tract, Book, Page, F.B. Page) and REMOVED TO (Lot, Block, Tract, Book, Page, F.B. Page). Includes handwritten entries: Lot 275, Tract Daringer, Book 5, Page 9, F.B. Page 128.

TAKE TO ROOM NO. 6 FIRST FLOOR

CITY ENGINEER: PLEASE VERIFY STREET NUMBER.

TAKE TO ROOM NO. 34 THIRD FLOOR

From No. 694 Mesquit St. To No.

O.K. City Assessor O.K. City Engineer By Deputy

- 1. Owner's name L. A. Ice & Cold Storage
2. Owner's Address 7th & Mesquite Sts
3. Architect's name
4. Contractor's name L. A. Ice & Cold Storage
5. Contractor's address
6. Entire cost of the proposed improvements, \$ 300.00
7. Purpose of the building Store-room
8. Class of building frame No. of rooms at present 2
9. No. of stories in height one Size of present building 45' x 75'
10. Size of new addition x x No. of stories in height
11. Material of foundation wood Size footing x x Size of wall x x Depth below ground
12. Size of Redwood Mudsills 2" x 8" Size of exterior studs 2" x 6"
13. Size of interior bearing studs 4" x 4" Size of interior non-bearing studs x x
14. Size of first floor joist 2" x 6" Second floor joist x x
15. STATE ON FOLLOWING LINES JUST WHAT YOU WANT TO DO.

Want to repair fire damage, put in some new purlins, rafters, skylights, replace all damaged corrugated iron, burn floor and repaint building.

Date issued, AUG 26 1912 191

PERMIT NO. 10291

(5)

Application Received Murdock

OVER

All applications must be filled out by applicant
(USE INK OR INDELIBLE PENCIL)

PLANS AND SPECIFICATIONS
and other data must also be filed.

WARD.....

3

**BOARD OF PUBLIC WORKS
DEPARTMENT OF BUILDINGS**

Application to Alter, Repair or Demolish

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be, prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

(SIGN HERE)

L. L. Lee & Cold Storage Co. Inc. W. E. Vermorel
(Applicant)

CITY ASSESSOR: PLEASE VERIFY.

REMOVED FROM

REMOVED TO

Lot..... Block.....
Tract.....
.....
.....
.....
Book..... Page..... F.B. Page.....

Lots *263 & 264* Block.....
Tract *Wangerter*
.....
.....
.....
Book *5* Page *9* F.B. Page *127*

CITY ENGINEER: PLEASE VERIFY STREET NUMBER.

From No. *670-672 Mesquit St.*
To No.

TAKE TO
ROOM NO. 6
FIRST
FLOOR

TAKE TO
ROOM NO. 34
THIRD
FLOOR

- Owner's name *L. L. Lee & Cold Storage Co.*
- Owner's Address *7th & Mesquit Sts.*
- Architect's name
- Contractor's name *L. L. Lee & Cold Storage Co.*
- Contractor's address *same as above*
- Entire cost of the proposed improvements, \$ *1650.00*
- Purpose of the building *Cold Storage*
- Class of building *Frame (insulated)* No. of rooms at present *3*
- No. of stories in height *3* Size of present building *54 ft x 114 ft*
- Size of new addition x No. of stories in height
- Material of foundation *Brick* Size footing *3 ft* Size of wall *21"* Depth below ground *4 ft*
- Size of Redwood Mudsills x Size of exterior studs *2"* x *6"* *2 sets*
- Size of interior bearing studs *6x8 & 10x10 posts* Size of interior non-bearing studs x
- Size of first floor joist *2"* x *14"* Second floor joist *2"* x *14"* *16" ST*

STATE ON FOLLOWING LINES JUST WHAT YOU WANT TO DO.
Wish to raise the 2nd floor of building 4 ft 4 in and build 1 new floor between 1st & 2nd floors. Building has 5 lines of girders (2- 6x16 & 3- 10x16) supported on 6x8-10x10 & 14x14 posts spaced 16 ft centers and girders are connected with building at one end only.

SEP 4-1912

Date issued, 191.....

PERMIT NO. **10666**

Application Received

Murdoch OVER

O.K. BY *75*

12-9 } 29-3
15-4 }

40-0 in height

O.K. City Engineer
By *[Signature]* Deputy

STW
240

REMARKS

Will add 2x12 on each side of 10x15 Cals -
" " 3x8 " " 6x8 "
Support ends of guides at ends
New 8x10 columns - with 3'-6" x 3'-6" footing -
3ft deep -

Application Received..... Time..... Returned

All applications must be filled out by applicant

(USE INK OR INDELIBLE PENCIL)

PLANS AND SPECIFICATIONS and other data must also be filed.

WARD

3

BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be, prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

(SIGN HERE) Los Angeles Ice & Cold Storage Co (Applicant)

CITY ASSESSOR: PLEASE VERIFY. W. C. Jennings

C.K. BY 73

TAKE TO ROOM NO. 6 FIRST FLOOR

Table with columns: REMOVED FROM, REMOVED TO. Includes Lot, Tract, Book, Page, F.B. Page. Tract: Wangerter Tract.

TAKE TO ROOM NO. 34 THIRD FLOOR

CITY ENGINEER: PLEASE VERIFY STREET NUMBER.

From No. 688-690 Mesquit St

To No.

- 1. Owner's name: L. A. Ice & Cold Storage Co
2. Owner's Address: 689 Mesquit St
3. Architect's name
4. Contractor's name: Same as Owner
5. Contractor's address
6. Entire cost of the proposed improvements, \$: 1000.00
7. Purpose of the building: Bottling works
8. Class of building: Bricks & frame. No. of rooms at present: 2
9. No. of stories in height: one & attic. Size of present building: 74' x 120'
10. Size of new addition
11. Material of foundation: Bricks. Size footing: 24"
12. Size of Redwood Mudsills: 4" x 12"
13. Size of interior bearing studs: 8" x 8" joists
14. Size of first floor joist: 2" x 12"
15. STATE ON FOLLOWING LINES JUST WHAT YOU WANT TO DO.

Wish to demolish present wooden floor in a space 62 x 64 ft. within the building, and replace same with cement floor

PERMIT NO. 1697

Date issued, FEB 7 - 1913

5

Application Received Munday

OVER

REMARKS

5 -

//

11

11

Application Received.....Time.....Returned.....

All applications must be filled out by applicant

PLANS AND SPECIFICATIONS and other data must also be filed

WARD 7

BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

3

Application to Alter, Repair or Demolish

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

- First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.
- Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
- Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

	REMOVED FROM	REMOVED TO	
TAKE TO ROOM No. 6 FIRST FLOOR	Lot <u>2775</u> Block <u>12</u>	Lot _____ Block _____	By _____ Deputy
	Tract _____	Tract _____	
ASSESSOR PLEASE VERIFY	<u>Wilmington</u>		O. K. City Assessor
	Book <u>5</u> Page <u>9</u> F. B. Page <u>192</u>	Book _____ Page _____ F. B. Page _____	By _____
TAKE TO ROOM No. 405 SOUTH ANNEX	From No. <u>690 Mesquit</u>	Street _____	By _____ Deputy
	To No. _____	Street _____	

(USE INK OR INDELIBLE PENCIL)

- What Purpose is the present Building used for? Engine Room
- Owner's name L. A. Ice & Cold Storage Co. Phone 10053
- Owner's address 7th St. & Santa Fe Trachs
- Architect's name Paul C. Pope Phone 7506
- Contractor's name Darby Brien W. Co. Phone Adm. 7254
- Contractor's address 226 Riverside St.
- ENTIRE COST OF PROPOSED WORK (Including Plumbing, Gas Fitting, Sewers, Ceasapools, Elevators, Painting, Finishing, etc.) \$ 1500.00
- Class of Present Building C No. of Rooms at present _____
- No. of stories in height 1 Size of present building _____
- State how many Buildings are on this lot _____
- State purpose Buildings on lot are used for Ice making & Cold Storage
(Tenement House, Hotel, Residence, or any other purpose)

STATE ON FOLLOWING LINES JUST WHAT YOU WANT TO DO.

Build a steel tower to project over roof. Tower to carry a tank which will carry steel pipe stands for condenser purpose.

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER (Sign here) Paul C. Pope
Owner or Authorized Agent

FOR DEPARTMENT USE ONLY		
PERMIT NO. <u>13940</u>	Plans and specifications checked and found to conform to Ordinances, State Laws, etc. (Use Ink) <u>W. F. Gout</u> Plan Examiner	Application checked and found O. K. (Use Rubber Stamp) <u>NOV 3 - 1915 R.R.</u> Clerk
		Stamp: <u>NOV 3 1915</u> J. K. B. E. B.

Steller

All applications must be filled out by applicant

WARD _____

PLANS AND SPECIFICATIONS
and other data must also be filed

BOARD OF PUBLIC WORKS
DEPARTMENT OF BUILDINGS

Application for the Erection of Building

CLASS "A"—"B"—"C"

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

Lot No. 275^A Block _____
(Description of Property) Wingcenter Tract

TAKE TO ROOM No. 6 FIRST FLOOR ASSESSOR PLEASE VERIFY

District No. 5 M. B. Page 9-10 F. B. Page 117

No. 694 Mesquit St Street _____
(Location of Job)

ENGINEER PLEASE VERIFY

(USE INK OR INDELIBLE PENCIL)

O. K. City Assessor By _____ Deputy
O. K. City Engineer By _____ Deputy

- Purpose of Building Boiler room No. of Rooms 1 No. of families _____
- Owner's name L. A. O. & C. Store Co. Phone 10053
- Owner's address 689 Mesquit St
- Architect's name Paul C. Pope Phone 75069
- Contractor's name Owner Phone _____
- Contractor's address _____
- ENTIRE COST OF PROPOSED BUILDING {Including Plumbing, Gas Fitting, Sewers, Cesspools, Elevators, Painting, Finishing, etc.} \$ 2000⁰⁰
- Any other buildings on the lot? yes How used? Store rooms
- Size of proposed building 32' x 35'-3" Size of lot 60' x 135' feet
- Number of stories in height one Height to highest point 34 ft 6 in
- Material of foundation concrete Character of soil sandy clay
- Size of footings 28 in Depth below surface of ground 12 in
- Number of chimneys smoke stack Material of chimneys steel
- Number of inlets to each flue _____ Interior size of such flues _____
- Material of exterior walls Brick
- Material of interior construction ✓
- Material of floors cement
- Material of roof wood covered with 26th galv cov. iron
- Are there any other buildings within 30 feet of the proposed structure? yes ✓

I have carefully examined and read the above application and know the same is true and correct, and hereby certify and agree that if a permit is issued that all of the provisions of the Building Ordinances will be complied with, whether herein specified or not; also certify that the plans and specifications herewith filed conform to all of the provisions of the Building Ordinances and Laws.

2/13/1918 (Sign here) L. A. O. & C. Store Co.
(Owner or Authorized Agent)

FOR DEPARTMENT USE ONLY		
PERMIT NO. <u>769</u> ✓	Plans and specifications checked and found to conform to Ordinances, State Laws, etc. (Use Ink) <u>[Signature]</u> Plan Examiner	Application checked and found O. K. (Use Rubber Stamp) FEB 13 1918 Class <u>B</u>
	[Stamp: FEB 13 1918]	

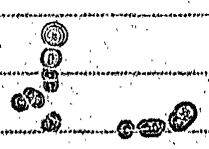
4

REMARKS

[Lined area for handwritten notes, mostly blank]

114 29
29
E 19

35 276
1/72 517
- 3200



All applications must be filled out by applicant

PLANS AND SPECIFICATIONS
and other data must also be filed

WARD.....

BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

Application for the Erection of Building

CLASS "A"-"B"-"C" *Transformer*

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

Lot No. 273 Block.....
(Description of Property) Wingert's Tract

District No..... M. B. Page..... F. B. Page.....

No. 690 Mesquite (Location of Job) Street.....
(USE INK OR INDELIBLE PENCIL)

TAKE TO ROOM No. 6 FIRST FLOOR ASSESSOR PLEASE VERIFY

TAKE TO ROOM No. 405 SOUTH ANNEX ENGINEER PLEASE VERIFY

O. K. City Assessor Deputy
O. K. City Engineer Deputy

- Purpose of Building 6 Covered Transformers No. of Rooms one No. of families 1
- Owner's name Los Angeles E. & E. Land S. Co. Phone ME 8191
- Owner's address 690 Mesquite
- Architect's name Darby Bros. Wks. Phone Bldg 7254
- Contractor's name Darby " " Phone Bldg 7254
- Contractor's address 226 Guerrero St
- ENTIRE COST OF PROPOSED BUILDING {Including Plumbing, Gas Fitting, Sewers, Cesspools, Elevators, Painting, Finishing, etc.} \$700.00
- Any other buildings on the lot? yes How used? Engine Room
- Size of proposed building 9ft x 21ft 10" Size of lot 40 x 155 feet
- Number of stories in height one Height to highest point 20ft
- Material of foundation concrete Character of soil Sandy, Room
- Size of footings 9" x 21" 6" deep Depth below surface of ground 8"
- Number of chimneys x Material of chimneys x
- Number of inlets to each flue x Interior size of such flues x x x
- Material of exterior walls Steel Frame Galv Iron Sheeting
- Material of interior construction.....
- Material of floors concrete
- Material of roof Steel frame Galv Steels
- Are there any other buildings within 30 feet of the proposed structure? yes

I have carefully examined and read the above application and know the same is true and correct, and hereby certify and agree that if a permit is issued that all of the provisions of the Building Ordinances will be complied with, whether herein specified or not; also certify that the plans and specifications here-with filed conform to all of the provisions of the Building Ordinances and Laws.

(Sign here) George W Darby
Owner or Authorized Agent.

PERMIT NO. <u>1097</u>	Plans and specifications checked and found to conform to Ordinances, State Laws, etc. (Use Ink)	Application checked and found O. K. (Use Rubber Stamp)	Stamp here when permit is issued
	Plan Examiner. <u>Thomson</u>	Clerk	<u>MAR 1 1918</u>

4

REMARKS

Lined area for writing remarks, consisting of approximately 25 horizontal lines.

All Applications must be filled out by Applicant

Bldg. Form 2

PLANS AND SPECIFICATIONS
and other data must also be filed

2

**BOARD OF PUBLIC WORKS
DEPARTMENT OF BUILDINGS**

**Application for the Erection of Frame Buildings
CLASS "D"**

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

TAKE TO REAR OF NORTH ANNEX 1st FLOOR
CITY CLERK PLEASE VERIFY

Lot No. 89 & 90 Block _____
(Description of Property)
Goodwin Tract 100 feet north of Jesse Street
between 7th and 6th and on the East side
of Magnit Street
District No. 5 M. B. Page _____ F. B. Page _____

O. H. CIV. ENGR. By _____ Deputy

TAKE TO ROOM No. 405 SOUTH ANNEX
ENGINEER PLEASE VERIFY

No. 658 660 Magnit Street Street _____
(Location of Job)
bet 6th & Jesse
(USE INK OR INDELIBLE PENCIL)

O. H. CIV. ENGR. By _____ Deputy

- Purpose of Building Fruit Juice Concentration No. of Rooms 3 No. of Families _____
- Owner's name Hyland-Stanford Company Phone Main 2067
- Owner's address 316 So Broadway
- Architect's name Same Phone _____
- Contractor's name Same Phone _____
- Contractor's address _____
- VALUATION OF PROPOSED WORK {Including Plumbing, Gas Fitting, Sewers, Cesspools, Elevators, Painting, Finishing, all Labor, etc.} \$ 800
- Is there any existing (old) building on lot? Ne. How used? _____
- Size of proposed building 32' x 50' Height to highest point 16 1/2 feet
- Number of Stories in height one Character of ground Sandy Loam
- Material of foundation Concr. Size of footings 8" x 8" Size of wall 16" Depth below ground 6"
- Material of chimneys _____ Number of inlets to flue _____ Interior size of flues _____
- Give sizes of following materials: REDWOOD MUDSILLS 3" x 4" Girders 6" x 8"
EXTERIOR studs 2" x 4" INTERIOR BEARING studs _____ Interior Non-Bearing studs _____
Ceiling joists 2" x 4" Roof rafters 2" x 4" FIRST FLOOR JOISTS _____
Second floor joists _____ Specify material of roof Composition
- Will all provisions of State Dwelling House Act be complied with? Yes

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER (Sign here) Hyland Stanford Co
(Owner or Authorized Agent.) Per H. Stanford

FOR DEPARTMENT USE ONLY	
PERMIT NO. 14876	Plans and Specifications checked and found to conform to Ordinance, State Laws, etc. <u>[Signature]</u> Plan Examiner
Application checked and found to conform to Ordinance, State Laws, etc. <u>[Signature]</u> Chief Inspector	permit is APPROVED MAY 2 1922 O. H. CIV. ENGR.

PLANS 18 H. V. Lomb 250

REMARKS

2" X 4" Studs

2" X 4" Plate Double

Center Dairing Tie

Corner Braced two ways and Center Braced

Banded up and down and Battened

6" X 6" Columns

6" X 8" Girders in center of building

Roof band from Girders to Ridge Board & center of each Rafter
Ceiling joint 2 feet on centers

REMARKS

Reinforcing Steel 68 Tons
Portland Cement 1250 Barrels

All Applications must be filled out by Applicant

Bldg. Form 3

PLANS AND SPECIFICATIONS
and other data must also be filed

3

BOARD OF PUBLIC WORKS

DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

- First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.
- Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
- Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM	REMOVED TO
Lot <u>272 & 273</u> Block _____	Lot _____ Block _____
Tract <u>Hingertner</u>	Tract _____
Book <u>5</u> Page <u>12</u> F. B. Page <u>61</u>	Book <u>5</u> Page _____ F. B. Page _____
From No. _____	Street _____
To No. <u>690 Mesquite, N of 7th</u>	Street _____

TAKE TO REAR OF NORTH ANNEX 1st FLOOR CITY CLERK PLEASE VERIFY

TAKE TO ROOM No. 405 SOUTH ANNEX ENGINEER PLEASE VERIFY

O. K. Chief Clerk
 O. K. Chief Engineer
 By _____ Deputy

(USE INK OR INDELIBLE PENCIL)

1. What purpose is the present Building now used for? Distilled Water
2. What purpose will Building be used for hereafter? Distilled Water
3. Owner's name L. H. Lee & Coald Stg - Phone 871-861
4. Owner's address 697 Mesquite St -
5. Architect's name _____ Phone _____
6. Contractor's name L. H. Lee & Coald Stg - Phone 871-861
7. Contractor's address _____
8. VALUATION OF PROPOSED WORK Including Plumbing, Gas Fitting, Sewers, Cesspools, Elevators, Painting, Finishing, all Labor, etc. \$ 500.00
9. Class of present Building 6 No. of rooms at present 2
10. Number of stories in height 1 Size of present Building 150 x 100
11. State how many buildings are on this lot 1
12. State purpose buildings on lot are used for Distilled Water
(Tenement House, Hotel, Residence, or any other purpose.)

STATE ON FOLLOWING LINES EXACTLY WHAT ALTERATIONS, ADDITIONS, ETC., WILL BE MADE TO THIS BUILDING:

Rebuild loading Platform

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER (Sign here) A. L. Spicher (Owner or Authorized Agent.)

FOR DEPARTMENT USE ONLY

PERMIT NO. <u>14619</u>	Plans and Specifications checked and found to conform to Ordinances, State Laws, etc. <u>A. H. Miller</u> Plan Examiner	Application checked and found O. K. <u>3/21/24</u> Inspector	Stamp "Name" which permit is issued ISSUED <u>MAR 21 1924</u> 100040
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730 PL 175

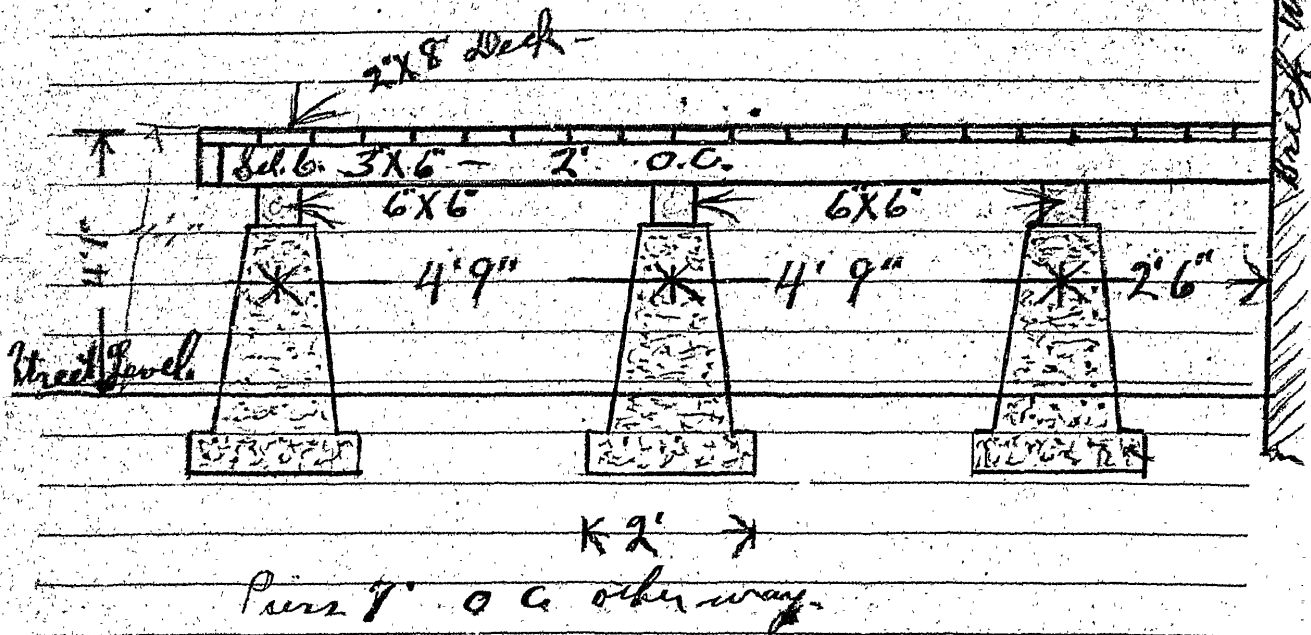
13. Size of new addition 13- x 100 No. of Stories in height Platform only
14. Material of foundation concrete Size footings 2' x 2' base Size wall 1' Depth below ground 1'
15. Size of Redwood Mudsills x Size of interior bearing studs x
16. Size of exterior studs x Size of interior non-bearing studs x
17. Size of first floor joists 3 x 6 Second floor joists x
18. Will all provisions of State Dwelling House Act be complied with? yes

I have carefully examined and read the above blank and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

(Sign here) W. L. Spicher
(Owner or Authorized Agent.)

FOR DEPARTMENT USE ONLY

APPLICATION	O. K. <u>AFM</u>
CONSTRUCTION	O. K. <u>AFM</u>
ZONING	O. K. <u>4</u>
SET-BACK LINE	O. K. <u> </u>
ORD. 33761 (N. S.)	O. K. <u> </u>
FIRE DISTRICT	O. K. <u>4FMT</u>



All Applications must be filled out by Applicant

Bldg. Form 3

PLANS AND SPECIFICATIONS
and other data must also be filed

3

BOARD OF PUBLIC WORKS

DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

5/11/24

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

	REMOVED FROM	REMOVED TO	
Lot.....	579.9.0 Block.....	Lot.....	Block.....
Tract.....	Wriggater	Tract.....	
Book.....	Page.....	Book.....	Page.....
F. B. Page.....	101	F. B. Page.....	
From No.....	658 Mesquit St.	Street	
To No.....	Cor. E. 6th St. & JESSIE ST.	Street	

(USE INK OR INDELIBLE PENCIL)

TAKE TO REAR OF NORTH ANNEX 1st FLOOR CITY CLERK PLEASE VERIFY

TAKE TO ROOM No. 405 SOUTH ANNEX ENGINEER PLEASE VERIFY

O.K. City Clerk Deputy

O.K. City Engineer Deputy

1. What purpose is the present Building now used for? Factory
2. What purpose will Building be used for hereafter? Factory
3. Owner's name. THE HYLAND STANFORD COMPANY Phone Trinity 6568
4. Owner's address. 658 Mesquit St.,
5. Architect's name. THE HYLAND STANFORD COMPANY Phone Trinity 6568
6. Contractor's name. THE HYLAND STANFORD COMPANY Phone Trinity 6568
7. Contractor's address. 658 Mesquit St.,
8. VALUATION OF PROPOSED WORK {Including Plumbing, Gas Fitting, Sewers, Cesspools, Elevators, Painting, Finishing, all Labor, etc.} \$ 600.00
9. Class of present Building D No. of rooms at present 2
10. Number of stories in height 1 Size of present Building 32 x 50
11. State how many buildings are on this lot 1
12. State purpose buildings on lot are used for Factory
(Tenement House, Hotel, Residence, or any other purpose.)

STATE ON FOLLOWING LINES EXACTLY WHAT ALTERATIONS, ADDITIONS, ETC., WILL BE MADE TO THIS BUILDING:

Additions as follows: 14' x 66' on the north - 16' x 44' on the west.

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER (Sign here) THE HYLAND STANFORD COMPANY.
(Owner or Authorized Agent.)

FOR DEPARTMENT USE ONLY		
PERMIT NO. 21305	Plans and Specifications checked and found to conform to Ordinances, State Laws, etc. <u>Howes</u> Plan Examiner	Application checked and found O. K. <u>5/11/24</u> <u>3E</u> <u>210512</u> Clerk
		Stamp here when permit issued MAY 7 1924 L.A. BLDG. DEPT.

Plans

Co. Boney 1 A

200

All Applications Must be Filled Out by Applicant

PLANS AND SPECIFICATIONS
and other data must also be filed

Bldg. Form 2

DEPARTMENT OF BUILDING AND SAFETY

2

Application for the Erection of Frame Buildings CLASS "D"

To the Board of Building and Safety Commissioners of the City of Los Angeles:
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:
First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.
Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

TAKE TO ROOM No. 6 REAR OF NORTH ANNEX 1st FLOOR CITY CLERK PLEASE VERIFY

Lot No. 90 Block _____
(Description of Property) _____
Goodwin Tract

TAKE TO FIRST FLOOR 242 SO. BROADWAY ENGINEER PLEASE VERIFY

District No. 5 M. B. Page 9 F. B. Page 91
No. 658 Mesquit St. Street _____
(Location of Job) _____
Bet. 6th and 5th and 7th Sts.

O.K. City Clerk
O.K. City Engineer
By _____ Deputy

(USE INK OR INDELIBLE PENCIL)

- Purpose of Building Bottling Works No. of Rooms 1 No. of Families ---
- Owner's name The Hyland-Stanford Company, Inc. Phone Tucker 8745
- Owner's address 658 Mesquit St.
- Architect's name W. Douglas Lee Phone VA. 4821
- Contractor's name W. Douglas Lee Phone VA. 4821
- Contractor's address 709 Textile Center Bldg.
- VALUATION OF PROPOSED WORK (Including Plumbing, Gas Fitting, Sewers, Ceaspoils, Elevators, Painting, Finishing, all Labor, etc.) \$ 800.00
- Is there any existing building or permit for a building on lot? yes How used? Bottling Works
- Size of proposed building 20 x 50 Height to highest point 13' - 0" feet
- Number of Stories in height One Character of ground Sandy Loam
- Material of foundation Cement Size of footings 12x12 Size of wall 12 Depth below ground 12
- Material of Chimneys None Number of inlets to flue _____ Interior size of flues _____
- Material of exterior walls Boards and Batts & 2 X 3 Studs
- Give sizes of following materials: REDWOOD MUDSILLS 2 X 6 Girders 4 X 6
EXTERIOR studs 2 x 3 INTERIOR BEARING studs None Interior Non-bearing studs _____
Ceiling joists 2 x 3 Roof rafters 2 x 6 FIRST FLOOR JOISTS 2 X 6
Second floor joists _____ Specify material of roof Composition
- Will all provisions of State Housing Act be complied with? Yes

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER (Sign here) W. Douglas Lee President
(Owner or Authorized Agent.)

FOR DEPARTMENT USE ONLY		
PERMIT NO. <u>4601</u>	Plans and Specifications checked and found to conform to Ordinances, State laws, etc. <u>2/11/26</u> <u>[Signature]</u> Plan Examiner	Application checked and found O. K. <u>2/11/26</u> <u>[Signature]</u> Clerk
		RECEIVED FEB 11 1926 [Stamp]

PLANS
100' Form 7th ?
70 Dept.
250

FOR DEPARTMENT USE ONLY

APPLICATION	O. K. <i>MS</i>
CONSTRUCTION	O. K.
ZONING	O. K. <i>OP</i>
SET-BACK LINE	O. K. <i>OP</i>
ORD. 33761 (N. S.)	O. K. <i>MS</i>
FIRE DISTRICT	O. K. <i>MS</i>

REMARKS

The building referred to in this application will be more than 100 feet from _____ Street
James
 OWNER or Authorized Agent

All Applications must be filled out by Applicant

PLANS AND SPECIFICATIONS and other data must also be filed

BOARD OF PUBLIC WORKS

DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

3

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

- First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.
- Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose not in or may hereafter be prohibited by ordinance of the City of Los Angeles.
- Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession to, the property described in such permit.

TAKE TO ROOM NO 6 FIRST FLOOR CITY CLERK PLEASE VERIFY	REMOVED FROM	REMOVED TO
	Lot <u>271</u> Block _____ Tract <u>Tract 1000</u>	Lot _____ Block _____ Tract _____
TAKE TO ROOM No. 408 SOUTH ANEX ENGINEER PLEASE VERIFY	Book <u>5</u> Page <u>12</u> F. B. Page <u>191</u>	Book _____ Page _____ F. B. Page _____
	From No. <u>670</u> <u>Mesquit</u> Street	To No. <u>21</u> <u>West 57th St</u> Street

O. K. City Clerk
 O. K. Chief Inspector
 Deputy

- (USE INK OR INDELIBLE PENCIL)
- What purpose is the present Building now used for? Cold Storage
 - What purpose will Building be used for hereafter? Cold Storage
 - Owner's name? L. L. Seet Cold Storage Co. Phone T.R. 1861
 - Owner's address _____
 - Architect's name _____ Phone _____
 - Contractor's name Same Phone Same
 - Contractor's address _____
 - VALUATION OF PROPOSED WORK {Including Plumbing, Gas Fitting, Sewers, Compoles, Elevators, Painting, Finishing, all Labor, etc.} \$ 600.00
 - Class of Present Building 6 No. of Rooms at present 3
 - Number of stories in height 3 Size of present building 72 x 96
 - State how many buildings are on this lot 2
 - State purpose buildings on lot are used for. Cold Storage

STATE ON FOLLOWING LINES EXACTLY WHAT ALTERATIONS, ADDITIONS, ETC., WILL BE MADE TO THIS BUILDING:

Take out old wood platform
Put in New concrete platform
concrete retaining wall, fill with good
Soil 5" concrete slab Mix 1-2 1/2 - 3 1/2 Wall 8" Top
12" Bottom Footings 20" Wide 6" Deep under ground.

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER (Sign here) W. L. Spischer
 (Owner or Authorized Agent)

PERMIT NO <u>29157</u>	Plans and specifications checked and found to conform to Ordinances, State Laws etc.	Application checked and found O. K.	Stamp here when permit is issued
	<u>Thomas</u> Plumber	<u>[Signature]</u> Clerk	<u>RECEIVED</u> OCT 7 1926 <u>TOULU</u> L.A. DIST. DEPT.

PLANS
W. L. Spischer

All Applications Must be Filled Out by Applicant

PLANS AND SPECIFICATIONS and other data must also be filed

Bldg. Form 3

BUILDING DIVISION

3

DEPARTMENT OF BUILDING AND SAFETY

Application to Alter, Repair or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles:

Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

- First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place, or portion thereof.
- Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
- Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

TAKE TO ROOM No. 6 REAR OF NORTH ANNEX 1st Floor CITY CLERK PLEASE VERIFY

TAKE TO FIRST FLOOR 242 SO. BROADWAY ENGINEER PLEASE VERIFY

REMOVED FROM		REMOVED TO	
Lot <u>265</u>	Block	Lot	Block
Tract <u>Wingerton</u>		Tract	
Book <u>5</u> Page <u>12</u> F. B. Page <u>101</u>		Book	Page F. B. Page
From No. <u>678 S. Merquitt St.</u>	Street		
To No. <u>But face it and 7th St.</u>	Street		

O. K. City Clerk By [Signature] Deputy

O. K. City Engineer By [Signature] Deputy

(USE INK OR INDELEBIL PENCIL)

- What purpose is the present Building now used for? Wood Storage
- What purpose will Building be used for hereafter? Wood Storage
- Owner's name L. H. Lee & Co. Wood St. Co. Phone T.R. 1861
- Owner's address 697 Merquitt St.
- Architect's name _____ Phone _____
- Contractor's name L. H. Lee & Co. Wood St. Co. Phone T.R. 1861
- Contractor's address 697 Merquitt St.
- VALUATION OF PROPOSED WORK (Including Plumbing, Gas Fitting, Sewers, Ceaspoils, Elevators, Painting, Finishing, all Labor, etc.) \$ 1500.00
- Class of present Building C No. of rooms at present _____
- Number of stories in height 3 Size of present Building 72' x 94' x 102'
- State how many buildings are on this lot one
- State purpose buildings on lot are used for Wood Storage
(Apartment House, Hotel, Residence, or any other purpose.)

STATE ON FOLLOWING LINES EXACTLY WHAT ALTERATIONS, ADDITIONS, ETC., WILL BE MADE TO THIS BUILDING:

Release old wood platforms
With concrete platforms, and steel canopy
covered with Galvalume

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER 7/6 2-95 (Sign here) A. L. Spicher
(Owner or Authorized Agent.)

FOR DEPARTMENT USE ONLY

PERMIT NO. <u>20768</u>	Plans and Specifications checked and found to conform to Ordinances, State Laws, etc. <u>[Signature]</u> Plan Examiner	Application checked and found <u>[Signature]</u> Clerk	Stamp RECEIVED JUL 22 1927 RECEIVED
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PLANS
20768

[Signature]

473

All Applications Must be Filled Out by Applicant

PLANS AND SPECIFICATIONS and other data must also be filed

BUILDING DIVISION

DEPARTMENT OF BUILDING AND SAFETY

3

Application to Alter, Repair or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit: First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof. Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles. Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

TAKE TO ROOM No. 6 REAR OF NORTH ANNEX 1st Floor CITY CLERK PLEASE VERIFY

TAKE TO FIRST FLOOR 242 SO. BROADWAY ENGINEER PLEASE VERIFY

Lot 265-266 Block Tract Huntington Tr

Lot Block Tract

Book 5 Page 12 F. B. Page From No. 680 Mesquite St To No. But 647M St

O. K. City Clerk Deputy O. K. City Engineer Deputy

(USE INK OR INDELEBIL PENCIL)

- 1. What purpose is the present Building now used for? Cold Storage
2. What purpose will Building be used for hereafter? Cold Storage
3. Owner's name: L.L. Ice & Cold Storage Co Phone: T.R. 1761
4. Owner's address: 690 Mesquite St
5. Architect's name: L.L. Ice & Cold Storage Co Phone:
6. Contractor's name: L.L. Ice & Cold Storage Co Phone:
7. Contractor's address:
8. VALUATION OF PROPOSED WORK: \$800.00
9. Class of present Building: C No. of rooms at present: 3
10. Number of stories in height: 3 Size of present Building: 50 x 110
11. State how many buildings are on this lot: 1
12. State purpose buildings on lot are used for: (Apartment House, Hotel, Residence, or any other purpose.)

STATE ON FOLLOWING LINES EXACTLY WHAT ALTERATIONS, ADDITIONS, ETC., WILL BE MADE TO THIS BUILDING:

New concrete floors only

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not

OVER

(Sign here)

(Owner or Authorized Agent.)

FOR DEPARTMENT USE ONLY

PERMIT NO. 20769
Plans and Specifications checked and found to conform to Ordinances, State Laws, etc.
Application checked and found O.K.
Stamp: JUL 22 1927

Handwritten signatures and numbers: 320, 8.00

All Applications Must be Filled Out by Applicant

Bldg. Form 3

PLANS AND SPECIFICATIONS and other data must also be filed

BUILDING DIVISION

3

DEPARTMENT OF BUILDING AND SAFETY

Application to Alter, Repair or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles:

Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

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Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

TAKE TO ROOM No. 6 REAR OF NORTH ANNEX 1st Floor CITY CLERK PLEASE VERIFY

TAKE TO FIRST FLOOR 242 SO. BROADWAY ENGINEER PLEASE VERIFY

Lot.....	Block.....	Lot.....	Block.....
Tract.....		Tract.....	
Book.....	Page.....	F. B. Page.....	Book.....
Page.....			Page.....
F. B. Page.....			F. B. Page.....
Form No. <i>690 Mesquit</i>			Street
T. No. <i>311 6th St</i>			Street

O. K. City Clerk
Deputy
O. K. City Engineer
Deputy

(USE INK OR INDELIBLE PENCIL)

- What purpose is the present Building now used for? *Store Room*
- What purpose will Building be used for hereafter? *Same*
- Owner's name *LA Ice & Cold Storage Co* Phone.....
- Owner's address *699 Mesquit*
- Architect's name *None* Phone.....
- Contractor's name..... Phone.....
- Contractor's address.....
- VALUATION OF PROPOSED WORK [Including Plumbing, Gas Fitting, Sewers, Ceaspoils, Elevators, Painting, Finishing, all Labor, etc.] *\$500.00*
- Class of present Building *D* No. of rooms at present *1*
- Number of stories in height *2* Size of present Building.....
- State how many buildings are on this lot *1*
- State purpose buildings on lot are used for *Store Room*
(Apartment House, Hotel, Residence, or any other purpose.)

STATE ON FOLLOWING LINES EXACTLY WHAT ALTERATIONS, ADDITIONS, ETC., WILL BE MADE TO THIS BUILDING:

What Zone is Property In? *10 Street Zone*
Install Sign NE Cor 7th & Mesquit

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER *1/16/28* *935* (Sign here) *LA Ice & Cold Storage*
(Owner or Authorized Agent.) *[Signature]*

FOR DEPARTMENT USE ONLY

PERMIT NO. 1339	Plans and Specifications checked and found to conform to Ordinances, State Laws, etc. <i>[Signature]</i> Plan Examiner	Application checked and found O. K. <i>[Signature]</i> Clerk	Stamp: RECEIVED JAN 16 1928 TOWN
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PLANS

All Applications Must be Filled Out by Applicant

PLANS AND SPECIFICATIONS and other data must also be filed

Stdg. Form 3

BUILDING DIVISION

3

DEPARTMENT OF BUILDING AND SAFETY

Application to Alter, Repair or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles:

Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

TAKE TO

REMOVED FROM

REMOVED TO

TAKE TO ROOM NO. 248 (REAR OF NORTH ANNEX 1st Floor)

Lot.....Block.....

Lot 365, 366, 367.....Block.....

Tract.....

Tract Wingerter Tr

CITY CLERK PLEASE VERIFY

O. K. City Clerk

TAKE TO FIRST FLOOR

Book.....Page.....F. B. Page.....

Book 5 Page 12 F. B. Page 101

ENGINEER PLEASE VERIFY

Map No. 664 - 670 - 674 Mesquite Street

Tract Mesquite Tr. 1st Floor Street

O. K. City Engineer

(USE INK OR INDELIBLE PENCIL)

- What purpose is the present Building now used for? Cold Storage
- What purpose will Building be used for hereafter? same
- Owner's name L. A. Lee & Cold Storage Co Phone TR 1861
- Owner's address 699 Mesquite st
- Architect's name..... Phone.....
- Contractor's name..... Phone.....
- Contractor's address.....
- VALUATION OF PROPOSED WORK [Including all Material, Labor, Finishing Equipment and Appliances in Completed Building.] \$6000.00
- Class of present Building..... No. of rooms at present 3
- Number of stories in height four Size of present Building 170 x 110
- State how many buildings are on this lot One
- State purpose buildings on lot are used for Cold Storage (Apartment House, Hotel, Residence, or any other purpose.)
- What Zone is Property in?.....

STATE ON FOLLOWING LINES EXACTLY WHAT ALTERATIONS, ADDITIONS, ETC., WILL BE MADE TO THIS BUILDING:

no structural changes
 Room E 4" concrete floor - 1st floor 2nd & 3rd floor to be floored with 3/4" matched floor
 Room F 2nd & 3rd floors floored over with 3/4" matched floor
 Room G 1st floor - 4" concrete floor 2nd & 3rd & 4th floor 3/4"

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER

(Sign here) G. B. Slooper
 (Owner or Authorized Agent) Ed. Dept

FOR DEPARTMENT USE ONLY

PERMIT NO. <u>17704</u>	Plans and Specifications checked and found to conform to Ordinances, State Laws, etc.	Application checked and found O. K.	RECEIVED JUN 21 1928 A. B. Dept.
	<u>Kearney</u> Plan Examiner	<u>Ed. Dept</u> Clerk	

Stop

14. Size of new addition.....x.....No. of Stories in height.....
 15. Material of foundation.....Size footings.....Size wall.....Depth below ground.....
 16. Size of Redwood Mudsills.....x.....Size of interior bearing studs.....x.....
 17. Size of exterior studs.....x.....Size of interior non-bearing studs.....x.....
 18. Size of first floor joists.....x.....Second floor joists.....x.....
 19. Will all Lathing and Plastering Comply with Ordinance?.....
 20. Will all provisions of State Housing Act be complied with?.....

I have carefully examined and read the above blank and know the same is true and correct, and that all provisions of the Ordinance and Laws governing Building Construction will be complied with, whether herein specified or not.

(Sign here) *[Signature]*
 (Owner or Authorized Agent)

FOR DEPARTMENT USE ONLY

APPLICATION	O. K. <i>[Signature]</i>
CONSTRUCTION	O. K. <i>[Signature]</i>
ZONING	O. K. <i>[Signature]</i>
SET-BACK LINE	O. K. <i>[Signature]</i>
ORD. 33761 (N. S.)	O. K. <i>[Signature]</i>
FIRE DISTRICT <i>#</i>	O. K. <i>[Signature]</i>

REMARKS

The building referred to in this application will be more than 100 feet from *[Signature]*

STREET WIDENING

Owner or Authorized Agent

All Applications Must be Filled Out by Applicant

PLANS AND SPECIFICATIONS and other data must also be filed

17
Hdg. Form 2

CITY OF LOS ANGELES

DEPARTMENT OF BUILDING AND SAFETY
BUILDING DIVISION

2

Application for the Erection of Frame Buildings
CLASS "D"

To the Board of Building and Safety Commissioners of the City of Los Angeles:
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.
Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

TAKE TO ROOM No. 248 (2ND FLOOR)

CITY CLERK PLEASE VERIFY

TAKE TO ROOM No. 5 (MAIN ST.) FLOOR

ENGINEER PLEASE VERIFY

Lot No. Block
(Description of Property)
.....
.....
.....
District No. M. B. Page F. B. Page
No. 658 Mesquit St. Street
bet. 6 & 7
(Location of Job)

O. K. City Clerk
By Deputy
O. K. City Engineer
By Deputy

(USE INK OR INDELIBLE PENCIL)

- 1. Purpose of Building Conveyor house No. of Rooms One No. of Families None
- 2. Owner's name Hyland Stanford Co Phone No. 7876
- 3. Owner's address 658 Mesquit St.
- 4. Architect's name Not to be filled in unless with name of Certified Architect or Licensed Engineer under State Act
- 5. Contractor's name Nigg Eng Corp Phone Covina 18071
- 6. Contractor's address Covina Cal STATE LICENSE NO 12046
- 7. VALUATION OF PROPOSED WORK {Including all Material, Labor, Finishing, Equipment and Appliances in Completed Building.} \$ 7.00⁰⁰
- 8. Is there any existing building or permit for a building on lot? Yes How used? Industrial
- 9. Size of proposed building 166 x 520 Height to highest point 12 ft. Size of Lot 80 x 160
- 10. Number of stories in height One Character of ground Level
- 11. Material of foundation Conc Size of footings 2x2 Size of wall Depth below ground 18"
- 12. Material of chimneys Number of Inlets to flue Interior size of flues X
- 13. Material of exterior walls Steel
- 14. Give sizes of following materials: REDWOOD MUDSILLS Girders
EXTERIOR studs INTERIOR BEARING studs Interior Non-Bearing Studs
Ceiling joists Roof Rafters FIRST FLOOR JOISTS
Second floor joists Specify material of roof
- 15. Will all provisions of State Housing Act be Complied with? Yes
- 16. Will all lathing and plastering Comply with Ordinance? None
- 17. What Zone is property in? 7A-3

I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not.

OVER

(Sign Here) Nigg Eng Corp
Gas A. ...
(Owner or Authorized Agent)

FOR DEPARTMENT USE ONLY

PERMIT No. 7420	Plans and Specifications checked and found to conform to Ordinances, State Laws, etc. <u>[Signature]</u> Plan Examiner	Application checked and found O. K. <u>4/10/31 2 No-3</u> <u>[Signature]</u> Clerk	Stamp here when permit is received RECEIVED APR 10 1931 J. H. ...
---------------------------	--	---	--

[Signature]

[Signature]

7030

3

CITY OF LOS ANGELES
DEPARTMENT OF BUILDING AND SAFETY
BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles:

Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

- First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.
- Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
- Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

Lot..... Lot.....
Tract..... Tract.....

Present location of building } 670 Mesquite Street } Approved by City Engineer.
(House Number and Street)
New location of building } Same } Deputy.
(House Number and Street)
Between what cross streets } 7th & Jessie }

- Purpose of PRESENT building COLD STORAGE Families..... Rooms 3
Store, Residence, Apartment House, or any other purpose.
- Use of building AFTER alteration or moving SAME Families..... Rooms 3
- Owner (Print Name) CALIF. CONSUMERS COMPANY Phone RI. 2211
- Owner's address 230 W. JEFFERSON ST. L.A.
- Certificated Architect..... State License No..... Phone.....
- Licensed Engineer..... State License No..... Phone.....
- Contractor CALIF. CONSUMERS CO. State License No..... Phone.....
- Contractor's address 230 W. JEFF. L.A. (WILL SUB-LET PORTIONS)
- VALUATION OF PROPOSED WORK {Including all Material, Labor, Finishing, Equipment } \$ 6500.00
{and Appliances in Completed Building}
- State how many buildings NOW } ENG. ROOM, TANK ROOM, COLD STORAGE & ICE
on lot and give use of each. } Residence, Hotel, Apartment House, or any other purpose.
- Size of existing building 30' x 95' Number of stories high 3 Height to highest point 43'
- Class of building C Material of existing walls BRICK Exterior framework -
Wood or Steel

Describe briefly and fully all proposed construction and work:

WE INTEND TO LEAVE ALL PRESENT WORK INTACT IN ALL ROOMS AND EXPECT ONLY TO ADD STRENGTH FOR GREATER FLOOR LOAD THAN HAS PREVIOUSLY BEEN POSSIBLE.

Fill in Application on other Side and Sign Statement

(OVER)

PERMIT NO. 4154	FOR DEPARTMENT USE ONLY <i>3/10/32</i>			Fee <u>1475</u> Stamp here when Permit is issued RECEIVED MAR 11 1932 RECEIVED
	Plans and Specifications checked <i>J. W. [Signature]</i>	Zone <u>Z 111 3</u>	Fire District No. <u>2</u>	
	Corrections verified <i>J. W. [Signature]</i>	Set Back Fl. <u>10</u> Ft.	Street Widening Fl. <u>10</u> Ft.	
	Plans, Specifications and Applications rechecked and approved <i>J. W. [Signature]</i>	Application checked and approved <i>[Signature]</i>		
PLANS <i>[Signature]</i>	SPRINKLER Acquired <input type="checkbox"/> Valuation included <input type="checkbox"/>	Inspector <i>[Signature]</i>		

PLANS, SPECIFICATIONS, and other data must be filed if required.

NEW CONSTRUCTION

Size of Addition None Size of Lot x Number of Stories when complete 3
Material of Foundation CONC. Width of Footings 4x4 Depth of footing below ground 18"
Width Foundation Wall x Size of Redwood Sill x Material Exterior Walls x
Size of Exterior Studs x Size of Interior Bearing Studs x
Joists: First Floor x Second Floor x Rafters x Material of Roof x

I have carefully examined and read both sides of this completed Application and know the same is true and correct and hereby certify and agree, if a Permit is issued, that all the provisions of the Building Ordinances and State Laws will be complied with whether herein specified or not; also certify that plans and specifications, if required to be filed, will conform to all of the provisions of the Building Ordinances and State laws.

Sign Here Calif. Consumers Co.
(Owner or Authorized Agent)

By H. Kelly

FOR DEPARTMENT USE ONLY

Application <u>Sept</u>	Fire District <u>1007</u>	Set back <u>7</u>	Termite Inspection <u>x</u>
Construction <u>SWF</u>	Zoning <u>1007</u>	Street Widening <u>7</u>	Forced Draft Ventil. <u>x</u>

(1) REINFORCED CONCRETE
Barrels of Cement x
Tons of Reinforcing Steel x

(2) The building (and, or, addition) referred to in this Application is, or will be when moved, more than 100 feet from Street
Sign Here H. Kelly
(Owner or Authorized Agent)

(3) No required windows will be obstructed.
Sign Here H. Kelly
Owner or Authorized Agent.

(4) There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
Sign Here H. Kelly
(Owner or Authorized Agent)

REMARKS: This building as it now stands is in good condition - Solid & Substantial - we need additional floor load capacity for commodities we are at present unable to take care of -
H. Kelly
Floor load will not exceed 300 lbs per sq. ft. -
H. Kelly, for Calif. Consumers Co.

3

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit: First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof. Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles. Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

Lot..... Tract.....

Present location of building } 680 Mesquit St (House Number and Street) Approved by City Engineer. New location of building } (House Number and Street) Deputy. Between what cross streets } 6th + 7th St

1. Purpose of PRESENT building Factory Families Rooms

2. Use of building AFTER alteration or moving Same Families Rooms

3. Owner (Print Name) CALIFORNIA INSURERS CO Phone RI 2211

4. Owner's address 230 West Jefferson St

5. Certificated Architect License No. State Phone

6. Licensed Engineer License No. State Phone

7. Contractor Hayes Construction Co License No 12358 Phone TU 1607

8. Contractor's address 1305 E 9th St

9. VALUATION OF PROPOSED WORK {Including all Material, Labor, Finishing, Equipment and Appliances in Completed Building} \$ 1000.00

10. State how many buildings NOW on lot and give use of each. Residence, Hotel, Apartment House, or any other purpose.

11. Size of existing building x Number of stories high 1 Height to highest point 30'

12. Class of building Co Material of existing walls Buck Exterior framework Wood or Steel

Describe briefly and fully all proposed construction and work:

Tear Down then Replace Fire Walls facing Mesquit St and Opp Section of fire wall on East End - All walls will extend 2 ft above roof line - Quake damage

Fill in Application on other Side and Sign Statement

(OVER)

FOR DEPARTMENT USE ONLY PERMIT NO. 4113 PLANS Inspected by [Signature] MAR 28 1933

3

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit: First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof. Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles. Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

Lot... Tract...

Present location of building } 680 Mesquite St (House Number and Street) Approved by City Engineer. New location of building } (House Number and Street) Deputy. Between what cross streets } 6th & 7th

- 1. Purpose of PRESENT building. Storage Families Rooms. 2. Use of building AFTER alteration or moving. Same Families Rooms. 3. Owner (Print Name) East Consumers Co Phone RT 2211 4. Owner's address 230 W Jefferson 5. Certificated Architect State License No. Phone 6. Licensed Engineer State License No. Phone 7. Contractor Vance Construction Co State License No 12358 Phone TU 1607 8. Contractor's address 1305 E. 9th St 9. VALUATION OF PROPOSED WORK [Including all Material, Labor, Finishing, Equipment] and Appliances in Completed Building \$500.00 10. State how many buildings NOW } 1 on lot and give use of each. Residence, Hotel, Apartment House, or any other purpose. 11. Size of existing building x Number of stories high Height to highest point. 12. Class of building. C Material of existing walls Brick Exterior framework Wood or Steel Describe briefly and fully all proposed construction and work:

Demolish and Replace 30 ft of front front wall at south end of Build Install new posts under ends of 2 trusses

Earthquake Damage - Fill in Application on other Side and Sign Statement

FOR DEPARTMENT USE ONLY PERMIT NO. 2605 Zone M-3 Fire District No. 2 Plans checked and approved 4/4/33 Inspector [Signature]

2

CITY OF LOS ANGELES
DEPARTMENT OF BUILDING AND SAFETY
BUILDING DIVISION

Application for the Erection of a Building
OF
CLASS "D"

To the Board of Building and Safety Commissioners of the City of Los Angeles;
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:
First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.
Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

Lot No.

Tract

Location of Building: 658 Mesquit St (House Number, and Street)
Between what cross streets: 6th & 7th St.
Approved by City Engineer
Deputy.

USE INK OR INDELIBLE PENCIL

- 1. Purpose of building: Boiler Stack Families: Rooms:
2. OWNER (Print Name): H. L. AND J. STANFORD Phone:
3. Owner's address: 658 Mesquit St
4. Certificated Architect: State License No. Phone
5. Licensed Engineer: State License No. Phone
6. Contractor: Chas Terry & Son State License No. 7471 Phone P.G. 4642
7. Contractor's address: 2014 W 29th St
8. VALUATION OF PROPOSED WORK \$ 700.00
9. State how many buildings NOW on lot and give use of each.
10. Size of new building: 43' diam No. Stories: Height to highest point: 90' Size lot: x
11. Type of soil: Sand Foundation (Material): Concrete Depth in ground: 2 1/2'
12. Width of footing: 8.6' Width of foundation Wall: 2' Size of Redwood Sill: x
13. Material Exterior Wall: Steel Size of studs: (Exterior) x (Interior Bearing) x
14. Joists: First Floor: x Second floor: x Rafters: x Material of Roof:
15. Chimney (Material): Size Flue: x No. Inlets each flue: Depth footing in ground: x

I have carefully examined and read the above completed Application and know the same is true and correct, and hereby certify and agree that if a permit is issued all the provisions of the Building Ordinances and State Laws will be complied with, whether herein specified or not; I also certify that plans and specifications filed will conform to all the Building Ordinances and State Laws.

Sign here: H. L. and J. Stanford Co (Owner or Authorized Agent)
By: G. Baker Aggr.

FOR DEPARTMENT USE ONLY
PERMIT NO. 12241
Plans and Specifications checked: M. S. ...
Zone: E Fire District No. 2
Bldg. Line: 140 Ft. Street Widening: Ft.
Application checked and approved: 5/20/36 L. ... Clerk
SPRINKLER
Specified Valuation included: Yes/No

3

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth.

REMOVED FROM

REMOVED TO

Lot..... Lot.....

Tract..... Tract.....

Present location of building } 658 Mesquit St. Los Angeles, Calif.

New location of building } (House Number and Street)

Between what cross streets }

Approved by City Engineer. Deputy.

1. Purpose of PRESENT building Recovering Awnings Families..... Rooms.....

2. Use of building AFTER alteration or moving..... Families..... Rooms.....

3. Owner (Print Name) John Doe Phone.....

4. Owner's Address 658 Mesquite

5. Certificated Architect Name State License No. Phone.....

6. Licensed Engineer Name State License No. 38122 Phone Ken-2121

7. Contractor Mars Awning & Shade Co. State License No. Phone.....

8. Contractor's Address 701 So.Brand, Glendale, Calif.

9. VALUATION OF PROPOSED WORK \$ 20.00 DOUBLE FEES

10. State how many buildings NOW on lot and give use of each. (Residence, Hotel, Apartment House, or any other purpose)

11. Size of existing building.....x.....Number of stories high.....Height to highest point.....

12. Class of building D Material of existing walls.....Exterior framework..... (Wood or Steel)

Describe briefly and fully all proposed construction and work:

Fill in Application on other Side and Sign Statement

DOUBLE FEE (OVER)

FOR DEPARTMENT USE ONLY PERMIT NO. 18577 PLANS Rec'd. Plans and Specifications checked Zone Fire District No. Corrections verified Bldg. Line Street Widening Ft. Ft. Plans, Specifications and Applications rechecked and approved Application checked and approved Pierce 7/22/36 Cash Required Valuation Included Specified Yes-No Inspector

Fee 2.00

Stamp here when Permit is issued

JUL 22 1936

mail

PLANS, SPECIFICATIONS, and other data must be filed, if required.

NEW CONSTRUCTION

Size of Additionx.....Size of Lot.....x.....Number of Stories when complete.....
Material of Foundation.....Width of Footing.....Depth of footing below ground.....
Width Foundation Wall.....Size of Redwood Sill.....x.....Material Exterior Walls.....
Size of Exterior Studsx.....Size of Interior Bearing Studs.....x.....
Joists: First Floor.....x.....Second Floor.....x.....Rafters.....x.....Roofing Material.....

I have carefully examined and read both sides of this completed Application and know the same is true and correct and hereby certify and agree, if a Permit is issued, that all the provisions of the Building Ordinances and State Laws will be complied with whether herein specified or not; also certify that plans and specifications, if required to be filed, will conform to all of the provisions of the Building Ordinances and State laws.

Sign Here.....
(Owner or Authorized Agent)

By.....

Table with 4 columns: Application, Fire District, Bldg. Line, Termite Inspection; Construction, Zoning, Street Widening, Forced Draft Ventil.

(1) REINFORCED CONCRETE
Barrels of Cement.....
Tons of Reinforcing Steel.....

(2) The building (and, or, addition) referred to in this Application is, or will be when moved, more than 100 feet from
.....Street
Sign Here.....
(Owner or Authorized Agent)

(3) No required windows will be obstructed.
Sign Here.....
(Owner or Authorized Agent)

(4) There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
Sign Here.....
(Owner or Authorized Agent)

REMARKS:

3

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

- First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley or other public place or portion thereof.
Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

Lot..... Tract.....

Present location of building } 658 MESQUIT ST. LOS ANGELES, CAL.
New location of building } 658 MESQUIT ST. LOS ANGELES, CAL.
Between what cross streets } 6th and 7th and Santa Fe and R.R.

Approved by City Engineer. Deputy.

- 1. Purpose of PRESENT building... FRUIT BINS Families... Rooms...
2. Use of building AFTER alteration or moving... FRUIT BINS Families... Rooms...
3. Owner (Print Name)... H. L. D. STANFORD CORP. Phone...
4. Owner's Address... 658 MESQUIT ST. LOS ANGELES, CAL.
5. Certificated Architect... NONE State License No... Phone...
6. Licensed Engineer... NONE State License No... Phone...
7. Contractor... NONE State License No... Phone...
8. Contractor's Address... NONE
9. VALUATION OF PROPOSED WORK {including all labor and material and all permanent lighting, heating, ventilating, water supply, plumbing, fire sprinkler, electrical wiring and/or elevator equipment therein or thereon} \$ 1,500.00
10. State how many buildings NOW } ONE SMALL FRUIT BIN.
11. Size of existing building... 11' x 20' Number of stories high... ONE Height to highest point... 20'
12. Class of building... D Material of existing walls... WOOD Exterior framework... WOOD

Describe briefly and fully all proposed construction and work:

6" x 6" UPRIGHTS 2" x 4" BRACES 2" x 3" SLATS 1" APART NO ROOF 2" x 4" FLOOR JOISTS WITH 1" x 1/2" FLOOR BOARDS COVERED WITH ZINC PLATE METAL This class "D" Building is to be used as storage bins for Oranges, Grapefruit, Lemons & Limes, etc.

3/8" TIE RODS GOING THROUGH BOTH WAYS, 1/2" BOLTS USED IN ALL JOINTS Fill in Application on other Side and Sign Statement (OVER)

FOR DEPARTMENT USE ONLY PERMIT NO. 4479 PLANS Fee 7.50 Plans and Specifications checked... Zoned E Fire District No. 2... Application checked and approved 2/16/38... FEB 16 1938

PLANS, SPECIFICATIONS, and other data must be filed if required.

NEW CONSTRUCTION

Size of Addition 11' x 30' Size of Lot 30' x 110' Number of Stories when complete ONE
Material of Foundation STEEL AND CONCRETE Width of Footing 24" Depth of footing below ground 3'
Width Foundation Wall 6" Size of Redwood Sill. Material Exterior Walls WOOD
Size of Exterior Studs 6" x 6" Size of Interior Bearing Studs 2" x 6"
Joists: First Floor 2" x 8" Second Floor Rafters Roofing Material

I have carefully examined and read both sides of this completed Application and know the same is true and correct and hereby certify and agree, if a Permit is issued, that all the provisions of the Building Ordinances and State Laws will be complied with whether herein specified or not; also certify that plans and specifications, if required to be filed, will conform to all of the provisions of the Building Ordinances and State laws.

Sign Here George P. Baker (Owner or Authorized Agent)

By

FOR DEPARTMENT USE ONLY

Application #1155 Fire Dist. 5 Bldg. Line 15 Termite Inspection
Construction Zoning Street Widening Forced Draft Ventil.

(1) REINFORCED CONCRETE
Barrels of Cement
Tons of Reinforcing Steel

(2) The building (and, or, addition) referred to in this Application is, or will be when moved, more than 100 feet from Street
Sign Here (Owner or Authorized Agent)

(3) No required windows will be obstructed.
Sign Here (Owner or Authorized Agent)

(4) There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
Sign Here (Owner or Authorized Agent)

REMARKS: OK 15 - Jan 31, 1938 JMS.

PLAN CHECKING
RECEIPT NO. 18997
VALUATION \$ 1500.00
FEE PAID \$ 300

3

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit: First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley or other public place or portion thereof. Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles. Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

Lot 87-88, 89, 90

Tract FOODWIN

Present location of building } 658 MESQUIT ST. (House Number and Street)
New location of building }
Between what cross streets } LESSE & 6 ST.

Approved by City Engineer. Deputy.

1. Purpose of PRESENT building WATER COOLING TOWER Families Rooms

2. Use of building AFTER alteration or moving Families Rooms

3. Owner (Print Name) THE HOWARD STANFORD CORP Phone TP 2694

4. Owner's Address 658 MESQUIT ST

5. Certificated Architect State License No. Phone

6. Licensed Engineer D.W. JARVELL State License No. 4371 Phone AN-2-1111

7. Contractor THE FLUOR CORP LTD State License No. 511 Phone AN-2-1111

8. Contractor's Address 2500 SO. ATLANTIC BLVD

9. VALUATION OF PROPOSED WORK \$250 (including all labor and material and all permanent lighting, heating, ventilating, water supply, plumbing, fire sprinkler, electrical wiring and/or elevator equipment therein or thereon)

10. State how many buildings NOW on lot and give use of each SEVERAL FOOD PROCESS PLANT (Residence, Hotel, Apartment House, or any other purpose)

11. Size of existing building 18' x 30' Number of stories high 42' Height to highest point

12. Class of building D Material of existing walls R.W. Exterior framework WOOD (Wood or Steel)

Describe briefly and fully all proposed construction and work:

CONSISTS OF DISMANTLING EXISTING WATER COOLING TOWER & CONCRETE BASTION. NEW TOWER OF SAME SIZE WILL REPLACE THIS. FILED PLAN CHECK NO. 0323 X

9-24-41 Fill in Application on other Side and Sign Statement (OVER)

PERMIT NO. 22269 FOR DEPARTMENT USE ONLY
Plans and Specifications checked Zone M3 Fire District No. 2
Corrections verified Bldg. Line NO Ft. NO Ft.
Plans, Specifications and Applications rechecked and approved Application checked and approved
For Plans See Filed with Required SPRINKLER Specified Valuation Included Yes-No
Inspector HOCAN

PLANS, SPECIFICATIONS, and other data must be filed if required.

NEW CONSTRUCTION

Size of Addition 12'-6" x 50'-0" Size of Lot..... x..... Number of Stories when complete 26'-0" H.
Material of Foundation CONC. Width of Footing..... Depth of Footing below ground.....
Width Foundation Wall..... Size of Redwood Sill..... Material Exterior Walls.....
Size of Exterior Studs 2x4 x..... Size of Interior Bearing Studs 4" x 4"
Joists: First Floor..... Second Floor..... Rafters..... Roofing Material.....

I have carefully examined and read both sides of this completed Application and know the same is true and correct and hereby certify and agree, if a Permit is issued, that all the provisions of the Building Ordinances and State Laws will be complied with whether herein specified or not; also certify that plans and specifications, if required to be filed, will conform to all of the provisions of the Building Ordinances and State Laws.

Sign Here..... E. The Fluor Corp. Ltd (Owner or Authorized Agent)
By..... E. D. Bentley

FOR DEPARTMENT USE ONLY
Application..... Fire District..... Bldg. Line..... Termite Inspection.....
Construction..... Zoning..... Street Widening..... Forced Draft Ventil.....

(1) REINFORCED CONCRETE
Barrels of Cement.....
Tons of Reinforcing Steel.....

(2) The building (and, or, addition) referred to in this Application is, or will be when moved, more than 100 feet from
..... Street
Sign Here..... (Owner or Authorized Agent)

(3) No required windows will be obstructed.
Sign Here..... (Owner or Authorized Agent)

(4) There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
Sign Here..... (Owner or Authorized Agent)

REMARKS:

2

CITY OF LOS ANGELES
DEPARTMENT OF BUILDING AND SAFETY
BUILDING DIVISION

Application for the Erection of a Building
OF
CLASS "D"

To the Board of Building and Safety Commissioners of the City of Los Angeles:
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:
First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley or other public place or portion thereof.
Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.
Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

Lot No. 87-88-89-90

Tract. GOODWIN

Location of Building. 658 MESQUIT ST. (House Number and Street)

Approved by City Engineer

Between what cross streets. JESSE & 6TH ST

Deputy.

USE INK OR INDELIBLE PENCIL

- 1. Purpose of building. WATER COOLING TOWER Families - Rooms -
2. Owner (Print Name). THE HYLAND-STANFORD CORP. Phone TR 2694
3. Owner's address. 658 MESQUIT ST.
4. Certificated Architect. State License No. Phone.
5. Licensed Engineer D.W. DARNELL State License No. 4371 Phone AN 2-1111
6. Contractor THE FLUOR CORP. LTD. State License No. 511 Phone AN 2-1111
7. Contractor's address 2500 So. ATLANTIC BLVD. HENRE
8. VALUATION OF PROPOSED WORK {including all labor and material and all permanent lighting, heating, ventilating, water supply, plumbing, fire sprinkler, electrical wiring and/or elevator equipment therein or thereon} \$ 9800.00
9. State how many buildings NOW SEVERAL COOLING TOWER, OFFICE BLDG, HOUSE, FOOD PROCESS. on lot and give use of each. (Store, Residence, Apartment House, Hotel, or any other purpose)
10. Size of new building 26' x 50' No. Stories. Height to highest point 26' Size lot. x
11. Type of soil. L.A.M. Foundation (Material) CONC. Depth in ground 2' 0"
12. Width of footing SEE PLANS Width of foundation wall. Size of redwood sill. x
13. Material exterior wall. C.W. Size of studs: (Exterior) 4" x 4" (Interior bearing) 2" x 4"
14. Joist: First floor. Second floor. Rafters. Material of roof.
15. Chimney (Material). Size Flue. No. inlets each flue. Depth footing in ground.

I have carefully examined and read the above completed Application and know the same is true and correct, and hereby certify and agree that if a permit is issued all the provisions of the Building Ordinance and State Laws will be complied with whether herein specified or not; I also certify that plans and specifications filed will conform to all the Building Ordinances and State Laws.

Sign here. The Fluor Corp Ltd. (Owner or Authorized Agent)

Plans, Specifications and other data must be filed if required.

By E.L. Bradley 30 240

PERMIT NO. 22437 FOR DEPARTMENT USE ONLY
Plans and Specifications checked
Zones M3 Fire District No. 2
Corrections verified
Bldg. Line NO Ft. Street Widening NO Ft.
Plans, Specifications and Application rechecked and approved
Application checked and approved
SPRINKLER
Required Valuation Included Specified Yes-No
Inspector HOCAN

FOR DEPARTMENT USE ONLY

Application	Fire District	Bldg. Line	Forced Draft Ventl.....
Construction.....	Zoning	Street Widening	

(1) REINFORCED CONCRETE

(2) The building referred to in this Application will be more than 100 feet from

Barrels of Cement.....

Tons of Reinforcing Steel.....

..... Street

Sign Here.....
(Owner or Authorized Agent)

(3) This building will be not less than 10 feet from any other building used for residential purposes on this lot.

(4) There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.

Sign here.....
(Owner or Authorized Agent)

Sign Here.....
(Owner or Authorized Agent)

REMARKS:

John D. Pierson

PLAN CHECKING

REC'D NO

VALUATION \$

FE PAID

4468
2800
20.00

3

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth.

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley or other public place or portion thereof. Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles. Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

Lot..... Lot.....

Tract..... Tract.....

Present location of building } 660 Mesquit St. (House Number and Street) New location of building } Same (House Number and Street) Between what cross streets } 6th + 7th Sts.

Approved by City Engineer

Deputy

- 1. Purpose of PRESENT building: Ice Storage. Families: — Rooms: — (Store Residence, Apartment House, Hotel, or any other purpose) 2. Use of building AFTER alteration or moving: Same. Families: — Rooms: — 3. Owner (Print Name): CALIFORNIA CONSUMERS CORP. Phone: RI-7211 4. Owner's Address: 230 W. Jefferson 5. Certificated Architect: — State License No. Phone. 6. Licensed Engineer: — State License No. Phone. 7. Contractor: G. O. GARTZ State License No. 27568 Phone: CA-3801 8. Contractor's Address: 3908 Mission Road R 972 9. VALUATION OF PROPOSED WORK (including all labor and material and all permanent lighting, heating, ventilating, water supply, plumbing, fire sprinkler, electrical wiring and/or elevator equipment therein or thereon): \$ 900.00 10. State how many buildings NOW on lot and give use of each: One - Ice plant (Residence, Hotel, Apartment House, or any other purpose) 11. Size of existing building: 200 x 200 Number of stories high: 1 Height to highest point: 50' 12. Class of building: R. Material of existing walls: Brick Exterior framework: Wood (Wood or Steel)

Describe briefly and fully all proposed construction and work:

Change portion of Dock from wood construction to concrete, and enclose with wood pash.

See Board letter of 4-23-42

Fill in Application on other Side and Sign Statement

(OVER)

FOR DEPARTMENT USE ONLY PERMIT NO. 5785 PLANS Rec'd Zone E Fire District No. 2 Bldg. Line No. Ft. Street Widening Ft. Application checked and approved [Signature] Clerk SPRINKLER Required Valuation Included Specified Yes-No Fee: 550 Stamp here when Permit is issued Date: 11/15/42 Inspector: [Signature]

PLANS, SPECIFICATIONS, and other data must be filed if required.

NEW CONSTRUCTION

Size of Addition.....x.....Size of Lot.....x.....Number of Stories when complete.....
Material of Foundation.....Width of Footing.....Depth of footing below ground.....
Width Foundation Wall.....Size of Redwood Sill.....x.....Material Exterior Walls.....
Size of Exterior Studs.....x.....Size of Interior Bearing Studs.....x.....
Joists: First Floor.....x.....Second Floor.....x.....Rafters.....x.....Roofing Material.....

I have carefully examined and read both sides of this completed Application and know the same is true and correct and hereby certify and agree, if a Permit is issued, that all the provisions of the Building Ordinances and State Laws will be complied with whether herein specified or not; also certify that plans and specifications, if required to be filed, will conform to all of the provisions of the Building Ordinances and State laws.

Sign Here California Consumers Corp.
(Owner or Authorized Agent)

By J. C. Hart

FOR DEPARTMENT USE ONLY

Application <u>RM</u>	Fire District.....	Bldg. Line <u>RM</u>	Termite Inspection.....
Construction.....	Zoning.....	Street Widening.....	Forced Draft Ventil.....

(1)
REINFORCED CONCRETE
Barrels of Cement.....
Tons of Reinforcing Steel.....

(2)
The building (and, or, addition) referred to in this Application is, or will be when moved, more than 100 feet from
.....Street
Sign Here.....
(Owner or Authorized Agent)

(3)
No required windows will be obstructed.
Sign Here.....
(Owner or Authorized Agent)

(4)
There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
Sign Here.....
(Owner or Authorized Agent)

REMARKS:

2

CITY OF LOS ANGELES
DEPARTMENT OF BUILDING AND SAFETY
BUILDING DIVISION

Application for the Erection of a Building
OF
CLASS "D"

To the Board of Building and Safety Commissioners of the City of Los Angeles:
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:
First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley or other public place or portion thereof.
Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinances of the City of Los Angeles.
Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in each permit.

Lot No. _____

Tract _____

Location of Building 2155 E 7th St. L.O. Approved by City Engineer _____
(House Number and Street)

Between what cross streets S. Mission Rd. & Meyer St. Deputy _____

USE INK OR INDELIBLE PENCIL

1. Purpose of building Wood Ramp Families _____ Rooms _____
(Store, Residence, Apartment House, Hotel, or any other purpose)

2. Owner (Print Name) Lockheed Aircraft Corp. Phone ML 7551
EX. 113

3. Owner's address 1705 Victory Pl. Burbank

4. Certificated Architect _____ State License No. _____ Phone _____

5. Licensed Engineer Overly State License No. _____ Phone _____

6. Contractor H. M. Kellu Co. State License No. 67210 Phone CL 2118

7. Contractor's address 4604 Hollywood Blvd. OT 221

8. VALUATION OF PROPOSED WORK (Including all labor and material and all permanent lighting, heating, ventilating, water supply, plumbing, fire sprinkler, electrical wiring and/or elevator equipment therein or thereon) \$200.00

9. State how many buildings NOW on lot and give use of each 2 - Aircraft Mfg. (Store, Residence, Apartment House, Hotel, or any other purpose)

10. Size of new building 104 x 36 ft. No. Stories 4 Height to highest point 52' Size lot X

11. Type of soil Fill Foundation (Material) _____ Depth in ground _____

12. Width of footing 36" Width of foundation wall _____ Size of redwood sill X

13. Material exterior wall Steel studs (Exterior) X (Interior bearing) X

14. Joist: First floor X Second floor X Rafters X Material of roof _____

15. Chimney (Material) _____ Size Flue X No. inlets each flue _____ Depth footing in ground _____

I have carefully examined and read the above completed Application and know the same is true and correct, and hereby certify and agree that if a permit is issued all the provisions of the Building Ordinance and State Laws will be complied with whether herein specified or not; I also certify that plans and specifications filed will conform to all the Building Ordinances and State Laws.

Sign here Lockheed Aircraft Corp. (Owner or Architect or Agent)
By H. Kellu

Plans, Specifications and other data must be filed if required.

FOR DEPARTMENT USE ONLY					
PERMIT NO. <u>7575</u>	Plans and Specifications checked <u>[Signature]</u>	Zone <u>M-3</u>	Five District <u>No. 11</u>	For <u>2</u>	Stamp here when Permit is issued
	Corrections verified <u>[Signature]</u>	Blkg. Line <u>None</u>	Street Widening Ft.		
PLANS	Plans, Specifications and Application rechecked and approved <u>[Signature]</u>	Application checked and approved <u>[Signature]</u>			
	For Plans See	Filed with	SPRINKLER Required Valuation Included	Specified Yes-No	Inspector <u>[Signature]</u>

FOR DEPARTMENT USE ONLY

Application	Fire District	Bldg. Type	Forced Draft Ventil.
Construction	Zoning	Street widening	

(1) **REINFORCED CONCRETE**

Barrels of Cement.....

Tons of Reinforcing Steel.....

(2) The building referred to in this Application will be more than 100 feet from _____ Street

Sign here.....
(Owner or Authorized Agent)

(3) This building will be not less than 10 feet from any other building used for residential purposes on this lot.

Sign here.....
(Owner or Authorized Agent)

(4) There will be an unobstructed passageway at least 10 feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.

Sign here.....
(Owner or Authorized Agent)

REMARKS:

WORKMEN'S COMPENSATION INSURANCE

Date 9/3/43

I hereby certify that I am the applicant for this permit and that in doing the work authorized thereby, I will not employ any person in violation of the Labor Code of the State of California relating to Workmen's Compensation Insurance.

H. H. Hestrickland
Signature of Applicant

PLAN CHECKING

RECEIPT NO. _____

VALUATION \$ _____

FEES PAID \$ _____

3

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the essence of the permit:

- First: That the permit does not grant any right or privilege to erect any building or other structures therein described, or any portion thereof, upon any street, alley or other public place or portion thereof.
Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereinafter be prohibited by ordinance of the City of Los Angeles.
Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

REMOVED FROM

REMOVED TO

Lot, Tract, Present location of building, New location of building, Between what cross streets

680 Mesquit St. (House Number and Street)

Approved by City Engineer Deputy

- 1. Purpose of PRESENT building: one (Share, Residence, Apartment House, Hotel, or any other purpose) Families: Rooms:
2. Use of building AFTER alteration or moving: Families: Rooms:
3. Owner (Print Name): California Consumers Corp Phone:
4. Owner's Address: 230 West Jefferson Blvd
5. Certificated Architect: State License No: Phone:
6. Licensed Engineer: State License No: Phone:
7. Contractor: Owen Roofing Co State License No: 3242 Phone Tr: 7167
8. Contractor's Address: 915 Santa Fe Ave
9. VALUATION OF PROPOSED WORK (including all labor and material and all permanent fixtures, building, ventilating, water supply, plumbing, fire apparatus, electrical wiring and or elevator equipment therein or thereon): \$ 585.00
10. State how many buildings NOW on lot and give use of each.
11. Size of existing building: x Number of stories high Height to highest point.
12. Class of building: Material of existing walls Exterior framework (Wood or Steel)
Describe briefly and fully all proposed construction and work: Re-roof: 1/2 layer 15# felt and 65# Cap Sheet

Fill in Application on other Side and Sign Statement (OVER)

FOR DEPARTMENT USE ONLY PERMIT NO. 394 PLANS Rec'd

PLANS, SPECIFICATIONS, and other data must be filed if required.

NEW CONSTRUCTION

Size of Addition: Size of Lot: Number of Stories when complete: _____
 Material of Foundation: _____ Width of Footing: _____ Depth of footing below ground: _____
 Width Foundation Wall: _____ Size of Redwood Sill: Material Exterior Walls: _____
 Size of Exterior Studs: Size of Interior Bearing Studs: _____
 Joists: First Floor: Second Floor: Rafters: Roofing Material: _____

I have carefully examined and read both sides of this completed Application and know the same is true and correct and hereby certify and agree, if a Permit is issued, that all the provisions of the Building Ordinances and State Laws will be complied with whether herein specified or not; also certify that plans and specifications, if required to be filed, will conform to all of the provisions of the Building Ordinances and State Laws.

Sign Here... Owen Roofing Co.
(Owner or Authorized Agent)

By: *[Signature]* Secretary

FOR DEPARTMENT USE ONLY

Application	Fire District	Bldg. Use	Termite Inspection
Construction	Zoning	Street Widening	Forced Draft Ventil.
(1) REINFORCED CONCRETE Barrels of Cement: _____ Tons of Reinforcing Steel: _____	(2) The building (and/or, addition) referred to in this Application is, or will be when moved, more than 100 feet from _____ Street Sign Here: _____ <small>(Owner or Authorized Agent)</small>		
(3) No required windows will be obstructed. Sign Here: _____ <small>(Owner or Authorized Agent)</small>	(4) There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width. Sign Here: _____ <small>(Owner or Authorized Agent)</small>		

REMARKS:

WORKMAN'S COMPENSATION INSURANCE

Date 8/25/44

I hereby certify that I am the applicant for this permit, and that in doing the work authorized thereby, I will not employ any person in violation of the Labor Code of the State of California relating to Workman's Compensation Insurance.

[Signature]
Signature of Applicant

APPLICATION TO ERECT A NEW BUILDING AND FOR A Certificate of Occupancy

CITY OF LOS ANGELES
DEPARTMENT
OF
BUILDING AND SAFETY
BUILDING DIVISION

1

Lot No. 87-88-89-90

Tract Dormer Tract

Location of Building 658 Mesquit St.
(From Mesquit and 5th)

Approved by
City Engineer

Between what cross streets? 6th St. & 7th St.

USE INK OR INDELIBLE PENCIL

1. Purpose of building Clarifier Families Rooms
(State Dwelling, Apartment House, Hotel or other purpose)
2. Owner Hyland-Stanford Corp. Phone
(Print Name)
3. Owner's Address 658 Mesquit St. P.O. L.A.
4. Certificated Architect None State License No. Phone
5. Licensed Engineer Geo. V. Fosdyke State License No. 4822 Phone ML 6538
6. Contractor G.O. Gorte Const. Co. State License No. 27562 Phone CA 1123
7. Contractor's Address 3508 Mission Rd. ET 12

8. EVALUATION OF PROPOSED WORK

Including all labor and material and all preliminary layout, setting, erecting, water supply, plumbing, fire protection, electrical wiring and structure, structural details of design.

9. State how many buildings NOW on lot and give use of each 2 - storage offices, citrus fruit processing
(State Dwelling, Apartment House, Hotel or other purpose)
10. Size of new building 5'-2" x 12'-2" No. Stories Height to highest point Size lot
11. Material Exterior Walls Concrete Type of Roofing
12. Buildings and similar structures:
 - (a) Footing: Width Depth in Ground Width of Wall 10" x 12"
 - (b) Size of Studs Material of Floor Concrete
 - (c) Size of Floor Joists Size of Rafters 7' x 7'

I hereby certify that to the best of my knowledge and belief the above application is correct and that this building or construction work will comply with all laws, and that in the doing of the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to Workmen's Compensation Insurance.

Sign Here Geo. V. Fosdyke
(Print Name of Authorized Agent)
By Edwin W. Kuhn

DISTRICT OFFICE

FOR DEPARTMENT USE ONLY

Valuation <u>500.00</u>		Investigation Fee <u>3.00</u>	
Fee <u>1.00</u>		Bldg. Permit Fee <u>3.00</u>	
Total <u> </u>		Total <u> </u>	
TYPE <u>Dit</u>	Maximum No. Dwelling <u>Inside Lot</u>	Net Lot <u> </u>	Lot Size <u>160' x 40'</u>
GROUP <u>Dormer</u>	Plans and Specifications checked <u> </u>	Corner Lot Keyed <u>M-3</u>	Fire District <u>2</u>
App. Plans Rec. <u> </u>	Corrections Verified <u> </u>	Bldg. Line <u> </u>	District Map No. <u>557-5</u>
Used with <u> </u>	Plans, Specifications and Application reviewed and approved <u> </u>	Special Inspected <u> </u>	Application checked and approved <u>ET BROWN NOV 28 1950</u>
	Inspector <u> </u>	Special Inspected Valuation Included <u> </u>	Inspector <u> </u>

DO NOT WRITE BELOW THIS LINE

TYPE OF RECEIPT	DATE ISSUED	TRACER NO. (M)	RECEIPT NO.	CODE	FEE PAID
Plan Checking	NOV 28 1950		A14101		
Supplemental Plan Checking					
Building Permit	NOV 29 50		LA27894		

160'

Pop Line

40'

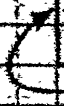
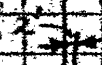
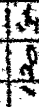
Exist Office,
Storage, and
Other Foot
Processing

New
Clarifier

120'

Pop Line

MESQUIT ST



3

APPLICATION TO ALTER, REPAIR, or DEMOLISH AND FOR A Certificate of Occupancy

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Lot No. 215-22-227 4 Blk 20 Lot 2 4
Tract Winter track
Location of Building 682 Mesquit St.
Between what cross streets 6th St. & 7th St.

Approved by City-Engineer Deputy

USE INK OR INDELIBLE PENCIL

- 1. Present use of building Ice Manufacturing
2. State how long building has been used for present occupancy 40 yrs
3. Use of building AFTER alteration or moving Same
4. Owner California Consumers Corp
5. Owner's Address 230 W. Jefferson Blvd P. O. L.A. 7
6. Certificated Architect None
7. Licensed Engineer Geo. V. Fosdyke
8. Contractor G.O. Gartz Const. Co
9. Contractor's Address 3908 N. Mission Rd
10. VALUATION OF PROPOSED WORK
11. State how many buildings NOW 1 - Ice Manuf., Storage, Offices
12. Size of existing building 123' x 160' Number of stories high 1 Height to highest point 33'
13. Material Exterior Walls Masonry
14. Describe briefly all proposed construction and work.

Replace portion of cracked wall.

NEW CONSTRUCTION

- 15. Size of Addition x Size of Lot x Number of Stories when complete
16. Footing: Width Depth 8' Ground Width of Wall Size of Floor Joists x
17. Size of Studs x Material of Floor Size of Rafters x Type of Roofing

I hereby certify that to the best of my knowledge and belief the above application is correct and that this building or construction work will comply with all laws, and that in the doing of the work authorized thereby I will not employ any person in violation of the Labor Code of the State of California relating to Workmen's Compensation Insurance.

Sign here Geo. V. Fosdyke (Owner or Authorized Agent)
By Elmer W. Kahn

DISTRICT OFFICE

FOR DEPARTMENT USE ONLY

Table with columns: PLAN CHECKING, OCCUPANCY SURVEY, Investigation Fee, Cert. of Occupancy Fee, Bldg. Permit Fee, Total. Includes handwritten values like \$500, \$100, \$350.

DO NOT WRITE BELOW THIS LINE

Table with columns: TYPE OF RECEIPT, DATE ISSUED, TRACTR NO. (M), RECEIPT NO., CODE, FEE PAID. Includes entries for Plan Checking, Supplemental Plan Checking, Building Permit.

Vertical text on the left margin: Certificate of Occupancy, DATE ISSUED, TRACTR NO. (M), RECEIPT NO., CODE, FEE PAID

Messout St. I

140'

C.P. 1.

Main Engine Room

Norm

ENGINE ROOM

21

Brick walls to
be repaired

153'

C.P. 2



3

APPLICATION TO ALTER, REPAIR, or DEMOLISH AND FOR A Certificate of Occupancy

Form B-3 CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Lot No.

Tract

Location of Building 678 S. MESQUIT ST. (House Number and Street) Approved by City Engineer

Between what cross streets? 7TH ST. + JESSE Deputy

USE INK OR INDELIBLE PENCIL

1. Present use of building WARE HOUSE Families - Rooms 2 (Store, Dwelling, Apartment House, Hotel or other purpose)

2. State how long building has been used for present occupancy

3. Use of building AFTER alteration or moving DEMOLISH Families Rooms

4. Owner CALIFORNIA CONSUMERS GARR

5. Owner's Address 678 S. MESQUIT, P. O.

6. Certificated Architect State License No. Phone

7. Licensed Engineer State License No. Phone

8. Contractor EL RANCHO LUMBER CO License No. 307 Phone 2035-J

9. Contractor's Address P.O. BOX 1577 EL CENTRO, CALIF 200 20

10. VALUATION OF PROPOSED WORK Including all labor and material and all permanent fighting, heating, ventilating, water supply, plumbing, fire sprinkler, electrical wiring and elevator equipment therein or thereon \$ 200 20

11. State how many buildings NOW on lot and give use of each (Store, Dwelling, Apartment House, Hotel or other purpose)

12. Size of existing building 45 x 75 Number of stories high 2 Height to highest point 25

13. Material Exterior Walls CORRUGATED IRON Interior framework WOOD (Wood, Steel or Masonry) (Wood or Steel)

14. Describe briefly all proposed construction and work:

DEMOLISH BUILDING

NEW CONSTRUCTION

15. Size of Addition x Size of Lot x Number of Stories when complete

16. Footing: Width Depth in Ground Width of Wall Size of Floor Joists x

17. Size of Studs x Material of Floor Size of Rafters x Type of Roofing

I hereby certify that to the best of my knowledge and belief the above application is correct and that this building or construction work will comply with all laws, and that in the doing of the work authorized thereby I will not employ any person in violation of the Labor Code of the State of California relating to Workmen's Compensation Insurance

Sign here (Owner or Authorized Agent)

DISTRICT OFFICE

By

FOR DEPARTMENT USE ONLY

Table with columns for PLAN CHECKING, OCCUPANCY SURVEY, and various fees. Includes rows for Valuation, Area of Bldg., Fee, TYPE, Maximum No. Occupants, Inside Lot, Key Lot, Lot Size, Fire District, and SPRINKLER.

DO NOT WRITE BELOW THIS LINE

Table with columns: TYPE OF RECEIPT, DATE ISSUED, TRACER NO. (M), RECEIPT NO., CODE, FEE PAID. Includes rows for Plan Checking, Supplemental Plan Checking, and Building Permit.

TH STREET BRIDGE 4. SEVENTH ST.

MESQUIT ST.

678 2. MESQUIT ST.

PERMITS
CORROATED
IRON WAREHOUSE

75'

25'

ST.

VESSIE

SCALE 1" = 10'

3

APPLICATION TO ALTER, REPAIR, or DEMOLISH AND FOR A Certificate of Occupancy

Marked
Form B-3
CITY OF LOS ANGELES
DEPARTMENT
OF
BUILDING AND SAFETY
BUILDING DIVISION

Lot No.

Tract

Location of Building 674 MESQUIT STREET } Approved by
(House Number and Street) City Engineer

Between what cross streets? 6TH & 7TH } Deputy.

USE INK OR INDELIBLE PENCIL

1. Present use of building WAREHOUSE Families — Rooms —
(Store, Dwelling, Apartment House, Hotel or other purpose)

2. State how long building has been used for present occupancy 25 YEARS

3. Use of building AFTER alteration or moving OFFICES & Warehouse Families — Rooms —

4. Owner CALIFORNIA CONSUMERS CORP. Phone RI. 2211

5. Owner's Address 230 W. JEFFERSON BLVD. P. O.
(Print Name)

6. Certificated Architect — State License No. — Phone —

7. Licensed Engineer — State License No. — Phone —

8. Contractor G.O. GARTZ CONST. CO. State License No. 117301 Phone CA. 1-1123

9. Contractor's Address 3908 No. MISSION ROAD - L.A. 31

10. VALUATION OF PROPOSED WORK 6,200⁰⁰
(Including all labor and material and all permanent lighting, heating, ventilating, water supply, plumbing, fire sprinkler, electrical wiring and elevator equipment therein or thereon.)

11. State how many buildings NOW } 1 - WAREHOUSE
on lot and give use of each. }
(Store, Dwelling, Apartment House, Hotel or other purpose)

12. Size of existing building 63' x 145' Number of stories high — Height to highest point 34'

13. Material Exterior Walls BRICK Exterior framework BRICK
(Wood, Steel or Masonry) (Wood or Steel)

14. Describe briefly all proposed construction and work:

1. CLOSE 2 DOORWAYS IN MASONRY WALL
2. RESURFACE FLOOR
3. ADD OFFICE PARTITIONS & CEILING
4. ADD TOILET ROOMS
5. ADD AIR CONDITIONING FOR ABOVE

NEW CONSTRUCTION

15. Size of Addition x Size of Lot x Number of Stories when complete —

16. Footing: Width x Depth in Ground x Width of Wall x Size of Floor Joists x

17. Size of Studs x Material of Floor x Size of Rafters x Type of Roofing x

I hereby certify that to the best of my knowledge and belief the above application is correct and that this building or construction work will comply with all laws, and that in the doing of the work authorized thereby I will not employ any person in violation of the Labor Code of the State of California relating to Workmen's Compensation Insurance.

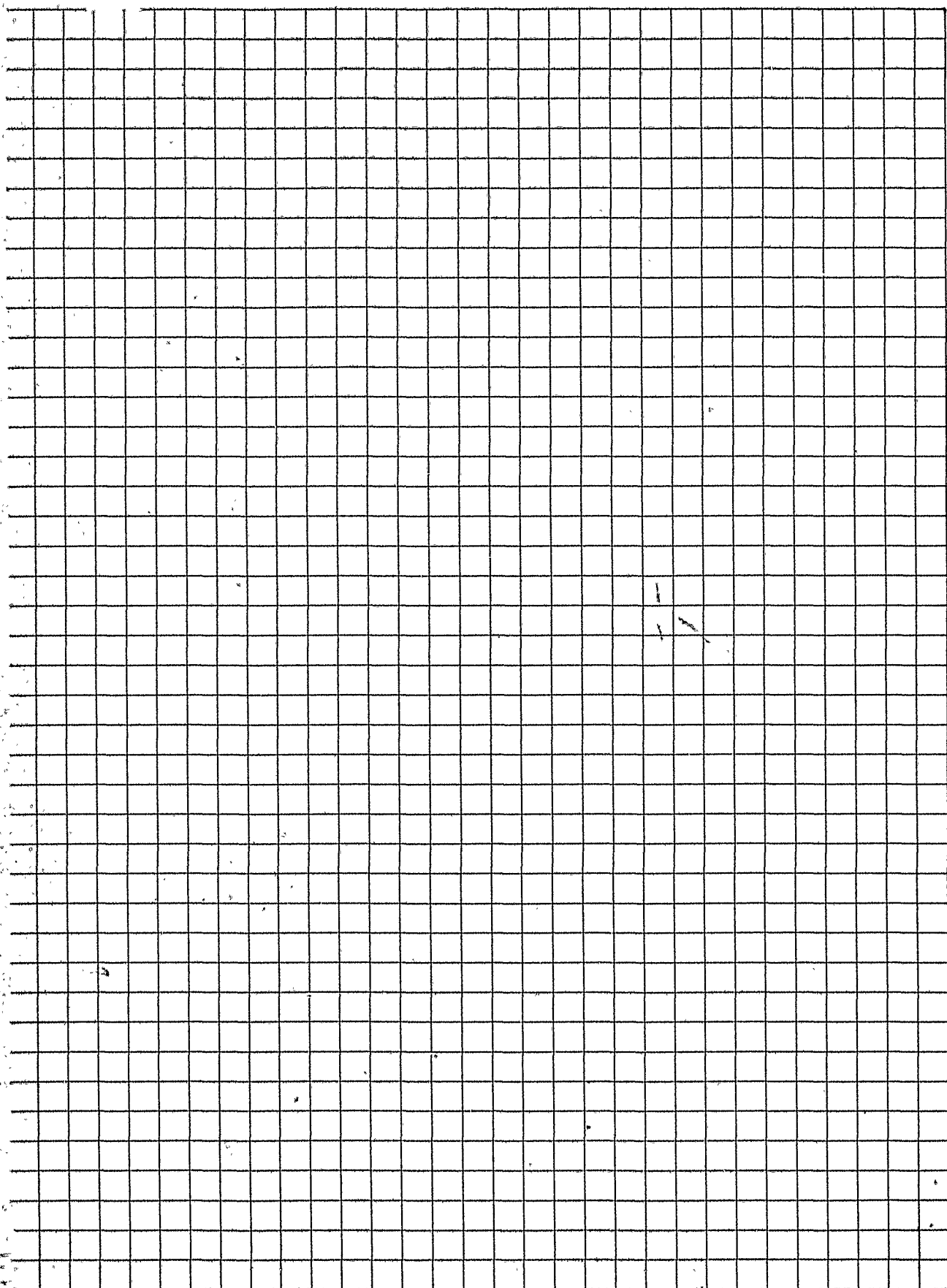
GRADING INVOLVED Sign here G.O. GARTZ CONST. CO.
(Owner or Authorized Agent)
By Ronald K. Seibert

FOR DEPARTMENT USE ONLY

PLAN CHECKING		OCCUPANCY SURVEY		Investigation Fee \$ <u>—</u>	
Valuation \$ <u>6200</u>	Area of Bldg. <u>—</u> Sq. Ft.	Cert. of Occupancy Fee \$ <u>—</u>		Bldg. Permit Fee \$ <u>2150</u>	
Fee \$ <u>150</u>	Fee \$ <u>—</u>	Total \$ <u>—</u>		Total \$ <u>—</u>	
TYPE <u>III-A</u>	Maximum No. Occupants <u>—</u>	Key Lot <u>NO</u>	Lot Size <u>Legal</u>	Clerk <u>Chen</u>	
GROUP <u>G-1</u>	Plans and Specifications checked <u>Schroeder</u>	Corner Lot Keyed <u>—</u>	Fire District <u>II 50</u>	District Map No. <u>5525</u>	
For Plans Sec <u>—</u>	Correction Verified <u>Schroeder</u>	Bldg. Line <u>—</u> Ft.	Street Widening <u>—</u> Ft.	Application checked and approved <u>Feb 1954</u>	
Filed with <u>—</u>	Plans, Specifications and Application rechecked and approved <u>—</u>	Continuous Inspection <u>X</u>	SPRINKLER Specified—Required Valuation Included Yes—No <u>—</u>	Inspector <u>—</u> Clerk <u>—</u>	

DO NOT WRITE BELOW THIS LINE

TYPE OF RECEIPT	DATE ISSUED	TRACER NO. (M)	RECEIPT NO.	CODE	FEE PAID
Plan Checking	<u>JAN 26 1954</u>		<u>45632</u>		
Supplemental Plan Checking		<u>FFR 2 1954</u>			
Building Permit				<u>LA79024</u>	



3

APPLICATION TO ALTER - REPAIR - DEMOLISH
AND FOR CERTIFICATE OF OCCUPANCY

250

CITY OF LOS ANGELES

DEPT. OF BUILDING AND SAFETY

DIST. MAP 7288	1. LEGAL LOT <i>No LEGAL</i>	BLK	TRACT
ZONE M-3	2. BLDG. ADDRESS 2135 E 7th St.		APPROVED
FIRE DIST. 2	3. BETWEEN CROSS STS. Santa Fe AND <i>Boyle Mission</i>		
INSIDE KEY	4. PRESENT USE OF BLDG. Store	NEW USE OF BLDG.	
COR. LOT	5. OWNER Ernest Paper Co.		
REV. COR. LOT SIZE X	6. OWNER'S ADDRESS 2135 E. 7th St.		
REAR ALLEY	7. CERT. ARCH.		STATE LICENSE NUMBER
SIDE ALLEY	8. LIC. ENG.		STATE LICENSE NUMBER
BLDG. LINE	9. CONTRACTOR Canvas Specialty Mfg. Co. 640 So. San Pedro St. Los Angeles		
AFFIDAVITS	10. SIZE OF EX. BLDG.		
SPRINKLERS REQ'D. SPECIFIED	11. MATERIAL EXT. WALLS: <input type="checkbox"/> WOOD <input type="checkbox"/> METAL <input type="checkbox"/> CONC. BLOCK <input checked="" type="checkbox"/> STUCCO <input type="checkbox"/> BRICK <input type="checkbox"/> CONCRETE		ROOF CONST: <input type="checkbox"/> WOOD <input type="checkbox"/> STEEL <input type="checkbox"/> CONC. <input type="checkbox"/> OTHER

3

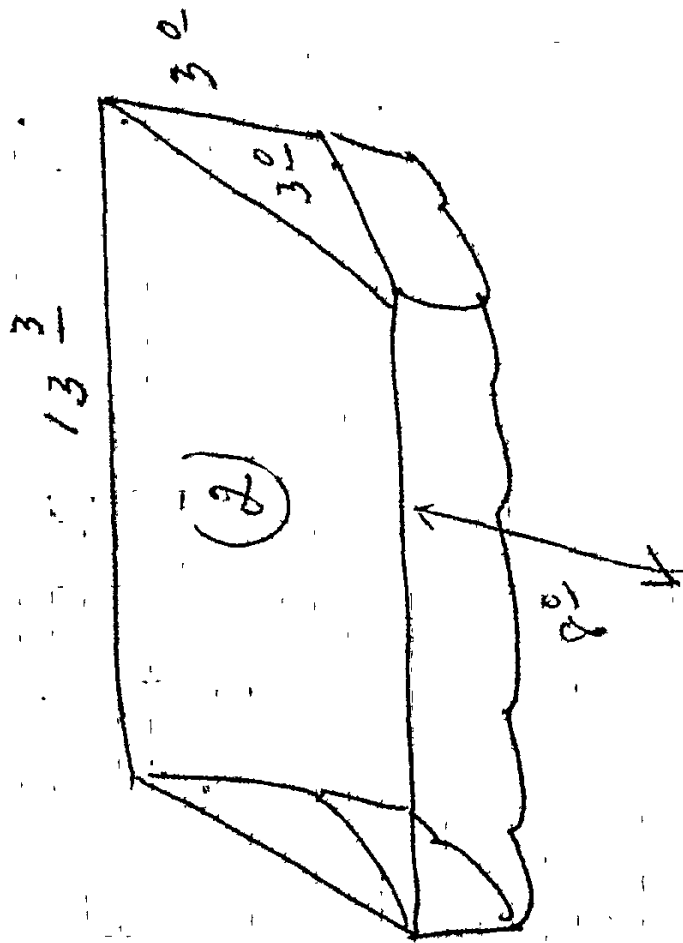
2135 E. 7th St.

VALIDATION LA12847	
TYPE	GROUP
	MAX. OCC.
MAY 2 1955 LA12847	
DIST. OFFICE	
C. OF O. ISSUED	
<i>No P.C. & P.I.P. 250</i>	

DWELL. UNITS	12. VALUATION. TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BLDG. \$ 68.50	VALUATION APPROVED <i>[Signature]</i>
PARKING SPACES	13. SIZE OF ADDITION Recover 2 Pullup Awnings X STORIES HEIGHT	APPLICATION CHECKED <i>[Signature]</i>
GUEST ROOMS	14. NEW WORK: MATERIAL EXT. WALLS MATERIAL ROOF	PLANS CHECKED <i>[Signature]</i>
FILE WITH	I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance. <i>Lee Summers</i> SIGNED	CORRECTIONS VERIFIED <i>[Signature]</i>
CONT. INSP.		PLANS APPROVED <i>[Signature]</i>
		APPLICATION APPROVED <i>[Signature]</i>

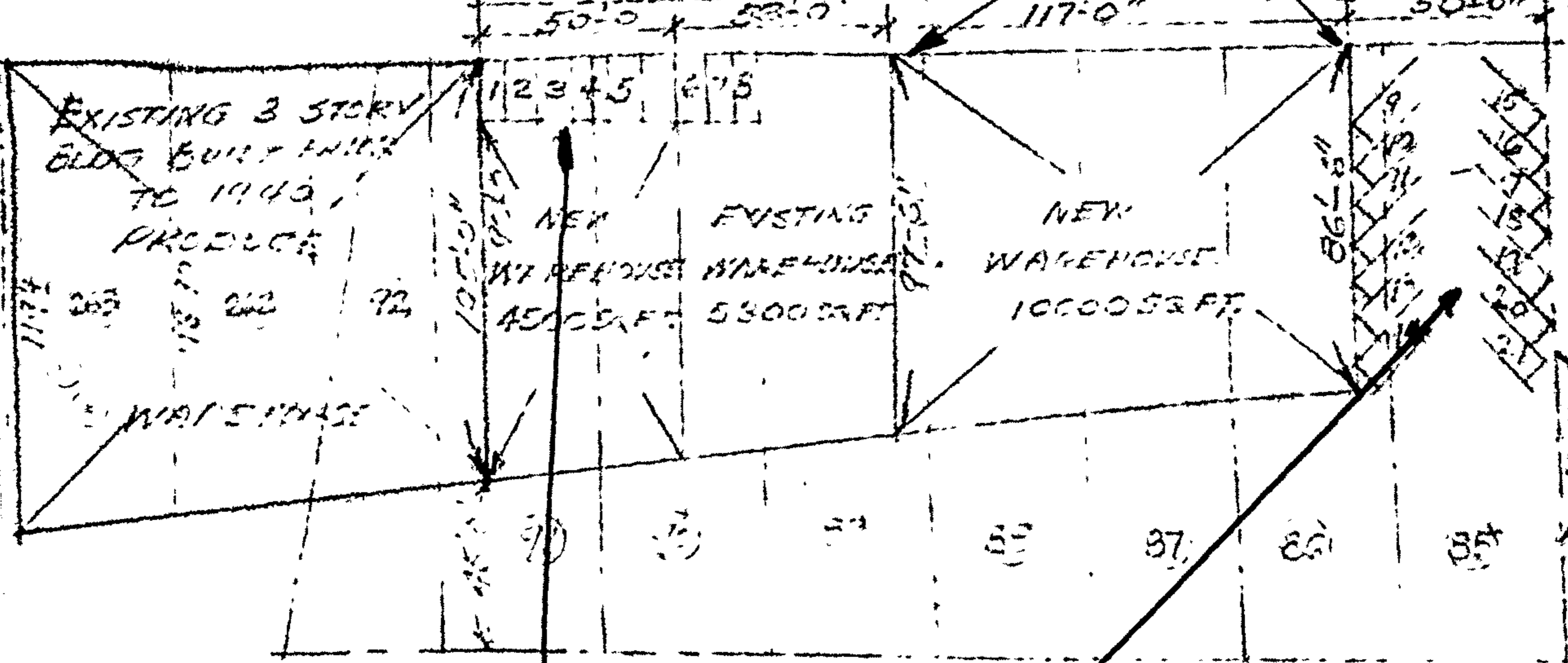
This form when properly validated is a permit to do the work described.

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.
2. Plot Plan Required on Back of Original.



APR 25 PM 8:43

MESQUIT STREET. 15'
 P.W. Permit #12379 for
 70'-0" IN FLAT. fty. encroachment on public property



21 TOTAL PAVED
 8' x 18' PARKING SPACES

DRIVEWAY APPROVED UNDER PROVISIONS OF ORD. NO. 115310.
 BY Fuller 5/24/63 - 70' IN FLAT PER RECARR
 NOTE. PERMIT REQUIRED FOR ALL WORK WITHIN STREET DEDICATION

Based on Warehouse Requirements



Handwritten notes:
 5/16/63
 5/24/63
 Fuller

Vertical handwritten note:
 This permit is for applying for the work... as authorized or pending the of Los Angeles, for any board, responsible for the performance of the work which such work

OFFICE MEMO

TO Susan Osborne, Room 225.....

DATE June 25, 1963

FROM K. J. Werminghaus.....

SUBJECT: Adjustments to Building Permit No. 41408, dated June 25, 1963.

The plan check validation line indicated below was added to this permit (omitted in error when permit was issued.)

June 25, 1963

32474

2-2 CS

\$2.75



EJW:YK:mb

3 No H.I.D.

APPLICATION TO ALTER - REPAIR - DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

B&S Form B-3

CITY OF LOS ANGELES

DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

1. LEGAL DESCR.	LOT 262-263 BLK. 85-92	TRACT WINGERTER TR Goodwin Tr	ADDRESS APPROVED LD
2. BUILDING ADDRESS	658 Mesquit St		DIST. MAP 123-217
3. BETWEEN CROSS STREETS	7th St AND 6th St		ZONE M-3-3
4. PRESENT USE OF BUILDING	Produce Storage	NEW USE OF BUILDING Same	FIRE DIST. 1/50
5. OWNER'S NAME	So. Coast Packing Co		PHONE INSIDE
6. OWNER'S ADDRESS	Same		KEY COR. LOT
7. CERT. ARCH.	None		STATE LICENSE PHONE
8. LIC. ENGR.	John E. Mackel	STATE LICENSE SE 435	PHONE HU 30530
9. CONTRACTOR	Morgan Luper & Sons	STATE LICENSE HU 39961	PHONE (6493)
10. CONTRACTOR'S ADDRESS	1901 W. 8th St L.A.		REAR ALLEY SIDE ALLEY BLDG. LINE
11. SIZE OF EXISTING BLDG.	104x93	STORIES 1	HEIGHT 27
NO. OF EXISTING BUILDINGS ON LOT AND USE			2 produce storage
3 658 Mesquit St			BLDG. AREA 1,800 sq ft
			1,500 sq ft
			DISTRICT OFFICE L.A.
12. MATERIAL	<input type="checkbox"/> WOOD <input type="checkbox"/> METAL <input type="checkbox"/> CONG. BLOCK	ROOF <input checked="" type="checkbox"/> WOOD <input type="checkbox"/> STEEL	ROOFING SPRINKLERS REQ'D. SPECIFIED
EXT. WALLS:	<input type="checkbox"/> STUCCO <input type="checkbox"/> BRICK <input checked="" type="checkbox"/> CONCRETE	CONST. <input type="checkbox"/> CONG. <input type="checkbox"/> OTHER	AFFIDAVITS
13. VALUATION: TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING.	\$ 65,000	VALUATION APPROVED	
	\$ 55,000	Ward	
14. SIZE OF ADDITION	STORIES 1	HEIGHT 27	APPLICATION CHECKED Boehm
90x117 # 50x90			
15. NEW WORK: (Describe)	EXT. WALLS <u>Put up concrete</u>	ROOFING <u>Campo</u>	PLANS CHECKED <u>Ward</u>
New produce cooler, storage building			DWELL. UNITS
			CORRECTIONS VERIFIED
			SPACES PARKING 21 total
			PLANS APPROVED
			GUEST ROOMS
			APPLICATION APPROVED
			FILE WITH
Signed <u>John E. Mackel</u>			INSPECTOR
I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance, and I have read reverse side of Application.			CONT. INSP. L.A. 11, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100
This Form When Properly Validated is a Permit to Do the Work Described.			Lic. Feb. 1963

Special Chap. 6-6-63
 DUES/FEES (Available)

CRITICAL SOIL

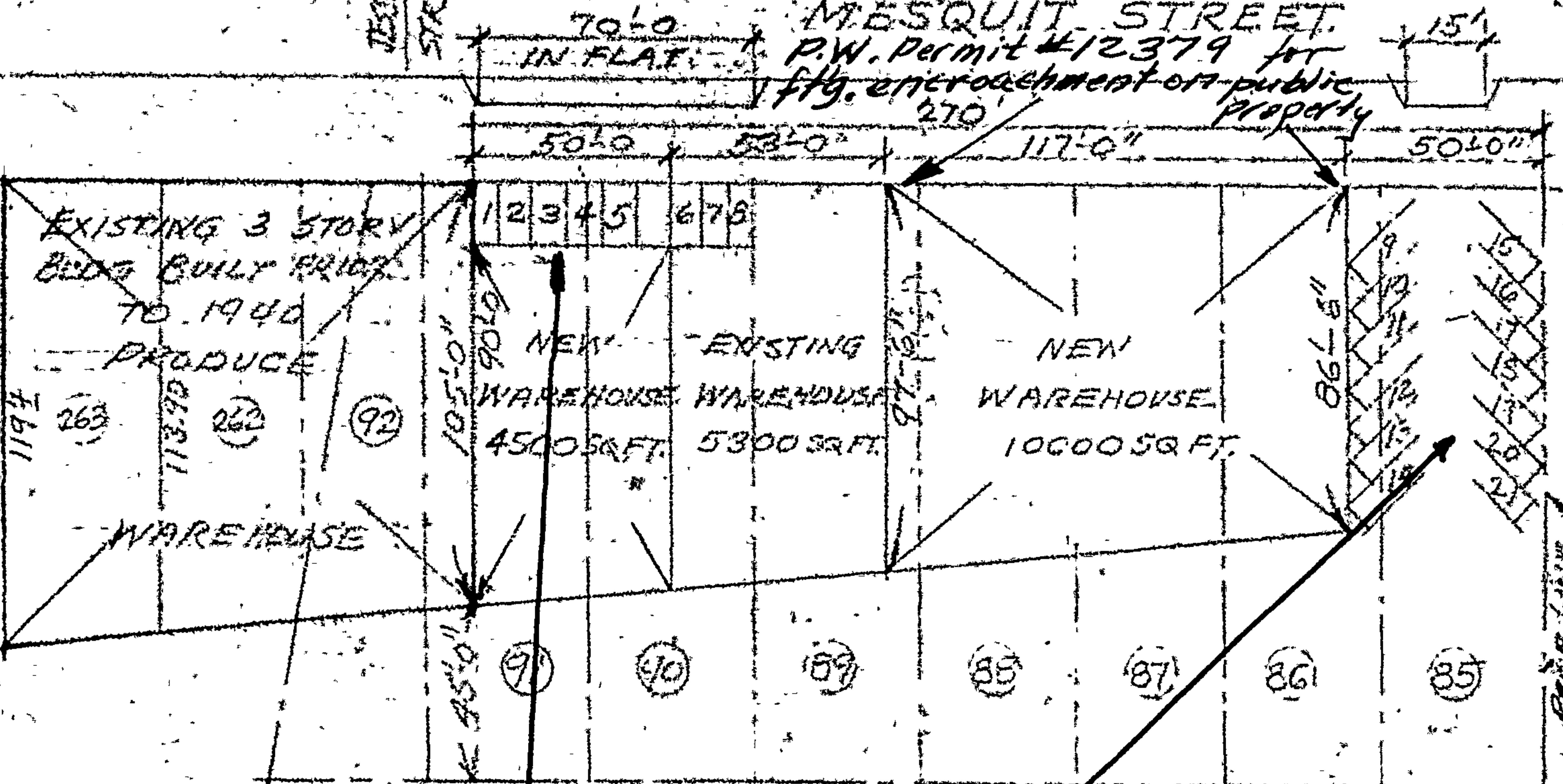
TYPE <u>W/B</u>	GROUP <u>G-1</u>	MAX. OCC. <u>5000</u>	P.C. <u>83.00</u>	S.F.C. <u>(7.80)</u>	G.P.I.	B.P. <u>184.00</u>	I.F.	O.S.	C/O
-----------------	------------------	-----------------------	-------------------	----------------------	--------	--------------------	------	------	-----

CASHIERS USE ONLY	APR-23-63	20498	F	•	•	Z	=	2 CK	83.00
	JUN-14-63	30535	F	•	•	Z	=	2 CK	9.00
	JUN-14-63	30536	F	•	•	Z	=	1 CK	184.00

P.C. No. S-1992 GRADING CRIT. SOIL CONS.

70'-0" IN FLAT

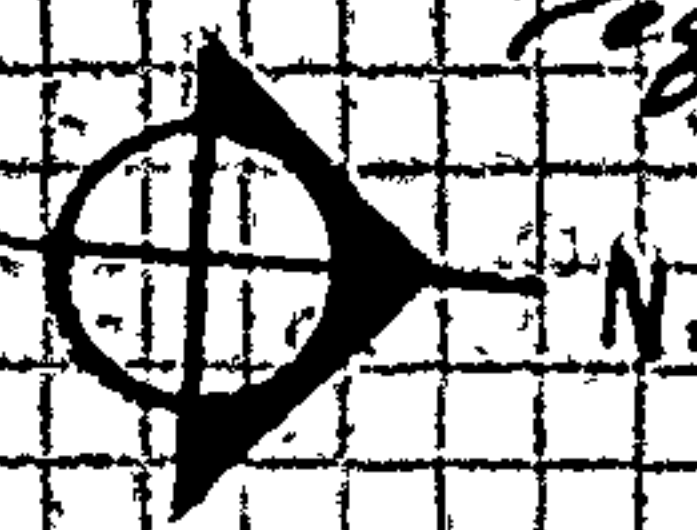
MESQUIT STREET
P.W. Permit #12379 for
ftg. encroachment on public property



LEGAL DESCRIPTION
1194
263
11370
262
92
WAREHOUSE
1194
263
11370
262
92
WAREHOUSE

20' TOTAL PAVED
8' x 18' PARKING SPACES
DRIVEWAY APPROVED UNDER PROVISIONS OF ORD. NO. 115316
BY Fuller 8/24/63 70' IN FLAT PER HECCARR
NOTE: PERMIT REQUIRED FOR ALL WORK WITHIN STREET DEDICATION

Based on Warehouse requirements



"This permit is not applicable to the installation of the work specified in this permit without the approval of the Board of Public Works of Los Angeles, nor any board or commission which such work is subject to."

APPLICATION TO ALTER - REPAIR - DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

CITY OF LOS ANGELES

DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

1. LEGAL DESCR. LOT 91&92 BLK. TRACT Goodwin ADDRESS APPROVED
2. BUILDING ADDRESS DIST. MAP
3. BETWEEN CROSS STREETS 658 Mesquit St 123-217 ZONE
4. PRESENT USE OF BUILDING AND NEW USE OF BUILDING 6th AND 7th M-3-3 FIRE DIST.
5. PRESENT NAME Warehouse S. Coast Pack SAME INSIDE XXXX KEY
6. OWNER'S ADDRESS P.O. ZONE COR. LOT REV. COR.
7. CERT. ARCH. STATE LICENSE PHONE LOT SIZE Irreg
8. LIC. ENGR. STATE LICENSE PHONE John Mackel HU30530
9. CONTRACTOR STATE LICENSE PHONE 306 South Union Pl. REAR ALLEY SIDE ALLEY
10. CONTRACTOR'S ADDRESS P.O. ZONE BLDG. LINE Morgan Luper & Co
11. SIZE OF EXISTING BLDG. STORIES HEIGHT NO. OF EXISTING BUILDINGS ON LOT AND USE BLDG. AREA
119x116& 1 26' 2-Cold Storage 5780
90x220
3 658 Mesquit St DISTRICT OFFICE L.A.

SEWER (Available) (Not Available)

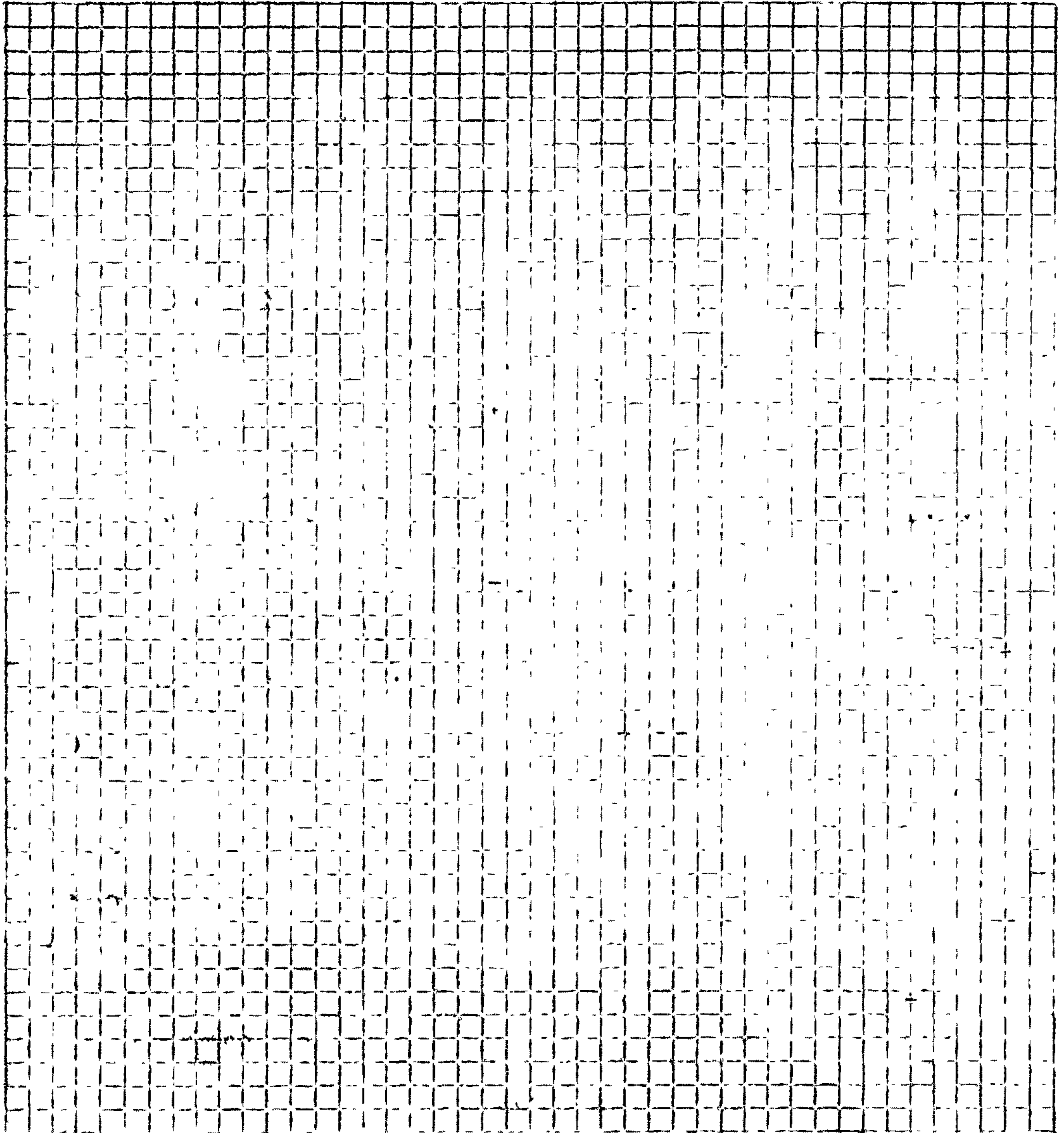
12. MATERIAL WOOD METAL CONC. BLOCK ROOF WOOD STEEL ROOFING SPRINKLERS REQ'D SPECIFIED
EXT. WALLS: STUCCO BRICK CONCRETE CONST. CONC. OTHER
13. VALUATION: TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING. \$ 200.00 VALUATION APPROVED AFFIDAVITS
14. SIZE OF ADDITION STORIES HEIGHT APPLICATION CHECKED WONG (en)
15. NEW WORK: EXT. WALLS ROOFING PLANS CHECKED DWELL. UNITS
(Describe) New door between bldgs. CORRECTIONS VERIFIED SPARKS PARKING
I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance, and I have read reverse side of Application.
Signed John Mackel INSPECTOR
This Form When Properly Validated is a Permit to Do the Work Described.
PLANS APPROVED GUEST ROOMS
APPLICATION APPROVED FILE WITH LA30536/63
CONT. INSP.

CRITICAL SOIL

TYPE I GROUP MAX. OCC. P.C. S.P.C. G.P.I. B.P.O. I.F. O.S. C/O
IV G-1 - 1 - - 200 X - -

CASHIER'S USE ONLY
JUN-18-63 31093 E •40836 Z - 2 CK 1.00
JUN-18-63 31094 E •40836 Z - 1 CK 2.00

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



SCOPE OF PERMIT

"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty, or shall be responsible for the performance or results of any work described herein, or the condition of the property on which such work is performed." (Section 91.0202 L.A.M.C.)

3

APPLICATION TO ALTER - REPAIR - DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

DAS Form B-3

CITY OF LOS ANGELES

DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.
2. Plot Plan Required on Back of Original.

1. LEGAL DESCR.	LOT	BLK.	TRACT	ADDRESS APPROVED
2. BUILDING ADDRESS 658 Mesquit St.				DIST. MAP 123-217
3. BETWEEN CROSS STREETS 7th St. AND 6th St.				ZONE M-3-3
4. PRESENT USE OF BUILDING Produce Storage		NEW USE OF BUILDING same		FIRE DIST. IT/50
5. OWNER'S NAME So. Coast Packing Co.		PHONE		INSTR. XXXX
6. OWNER'S ADDRESS Above		P.O.	ZONE	KEY COR. LOT
7. CERT. ARCH. None		STATE LICENSE	PHONE	REV. COR. LOT SIZE 280x150
8. LIC. ENGR. John E. Mackel		STATE LICENSE SE 435	PHONE HU 30530	REAR ALLEY
9. CONTRACTOR Morgan Lapher & Co.		STATE LICENSE 64931	PHONE HU 30530	SIDE ALLEY /
10. CONTRACTOR'S ADDRESS 1901 W. 8th St. LA		P.O.	ZONE	BLOG. LINE
11. SIZE OF EXISTING BLDG. STORIES HEIGHT NO. OF EXISTING BUILDINGS ON LOT AND USE				BLDG. AREA

SEWER (Available) (Use Available)

3		658 Mesquit St.		DISTRICT OFFICE L. A.	
12. MATERIAL		ROOF		SPRINKLERS	
<input type="checkbox"/> WOOD <input type="checkbox"/> METAL <input type="checkbox"/> CONC. BLOCK <input type="checkbox"/> STUCCO <input type="checkbox"/> BRICK <input type="checkbox"/> CONCRETE		<input type="checkbox"/> WOOD <input type="checkbox"/> STEEL <input type="checkbox"/> CONC. <input type="checkbox"/> OTHER		REG'D. SPECIFIED	
13. VALUATION: TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING.		\$20000		AFFIDAVITS	
14. SIZE OF ADDITION		STORIES HEIGHT		APPLICATION CHECKED	
None				Boehm*	
15. NEW WORK: (Describe)		EXT. WALLS ROOFING		PLANS CHECKED	
Relocate column				Boehm	
I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance, and I have read reverse side of Application.				CONNECTIONS VERIFIED	
Signed <i>John E. Mackel</i>				PLANS APPROVED	
This Form When Properly Validated is a Permit to Do the Work Described.				APPLICATION APPROVED	
				INSPECTOR	
				FILE WITH LA40611/ 63	
				CONT. INSP.	

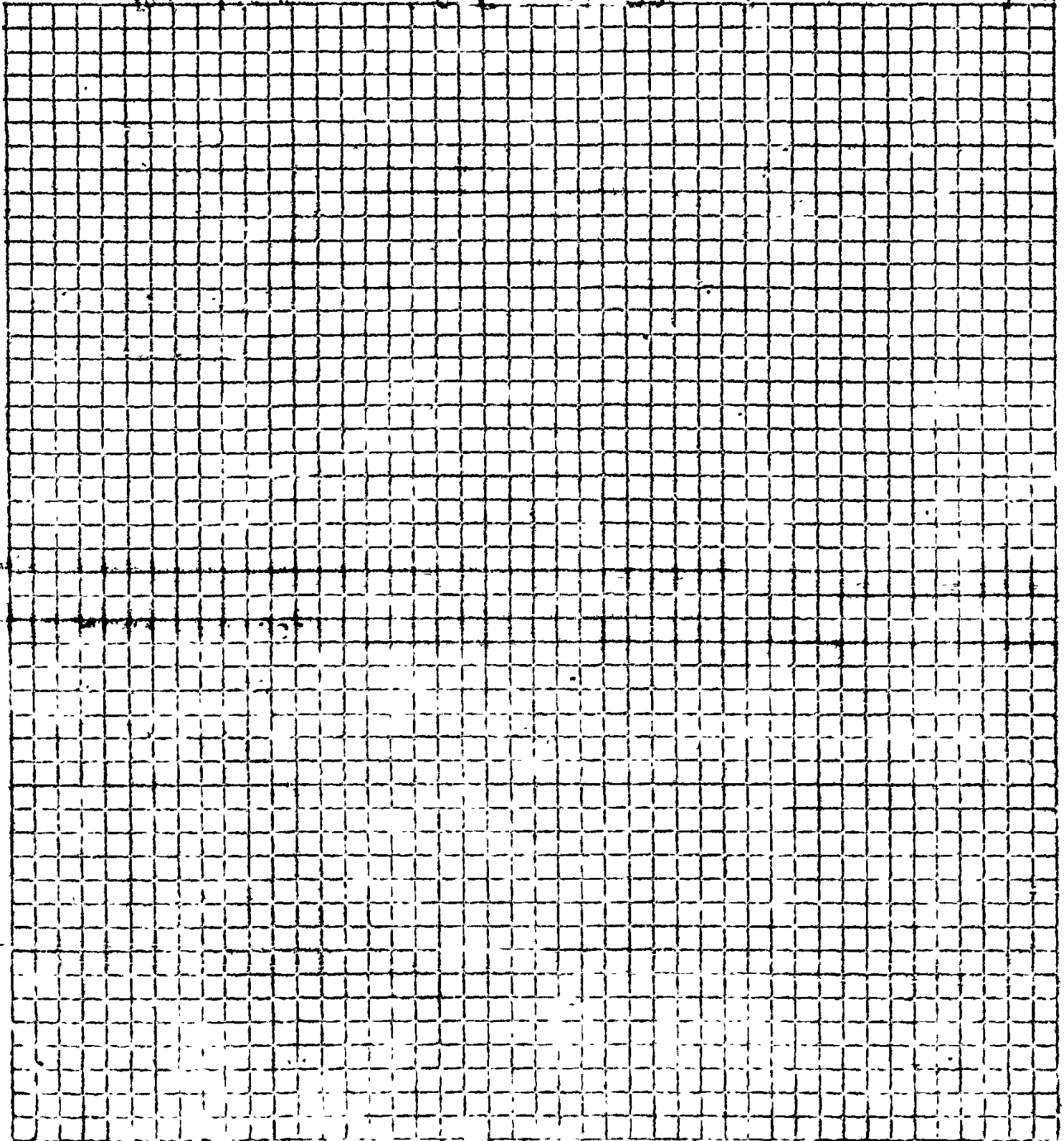
CRITICAL SOIL

TYPE	GROUP	MAX. OCC.	P.C. 100	S.P.C.	G.P.I.	B.P. 200	I.F.	O.S.	C/O
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CASHIER'S USE ONLY	OCT--7-63	50330 E	•49282	Z - 2 CK	1.00
	OCT--7-63	50331 E	•49282	Z - 1 CK	2.00

01-40 017012 01135 5- 17 500

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



SCOPE OF PERMIT

"This permit is an approval of the proposed construction of which is not an approval or authorization of the work provided herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the contractor to fail to comply with any applicable Law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof, shall make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or of the work which is performed." (Ord. 181,0902 L.A.M.C.)

3

APPLICATION TO ALTER - REPAIR - DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

B&S Form B-3

CITY OF LOS ANGELES

DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only 2. Plot Plan Required on Back of Original.

Form with fields for LEGAL DESCR., PRESENT USE OF BUILDING, JOB ADDRESS, OWNER'S NAME, ARCHITECT OR DESIGNER, ENGINEER, CONTRACTOR, SIZE OF EXISTING BLDG., MATERIAL OF CONSTRUCTION, NEW WORK, NEW USE OF BUILDING, TYPE, BLDG. AREA, DWELL. UNITS, P.C. No., P.C., S.P.C., G.P.I., B.P., I.F., O.S., C/O, TYPIST.

Table with columns for date, address, parcel number, and area. Includes entries for JUL-15-65 at 36932 E and 36933 E.

STATEMENT OF RESPONSIBILITY

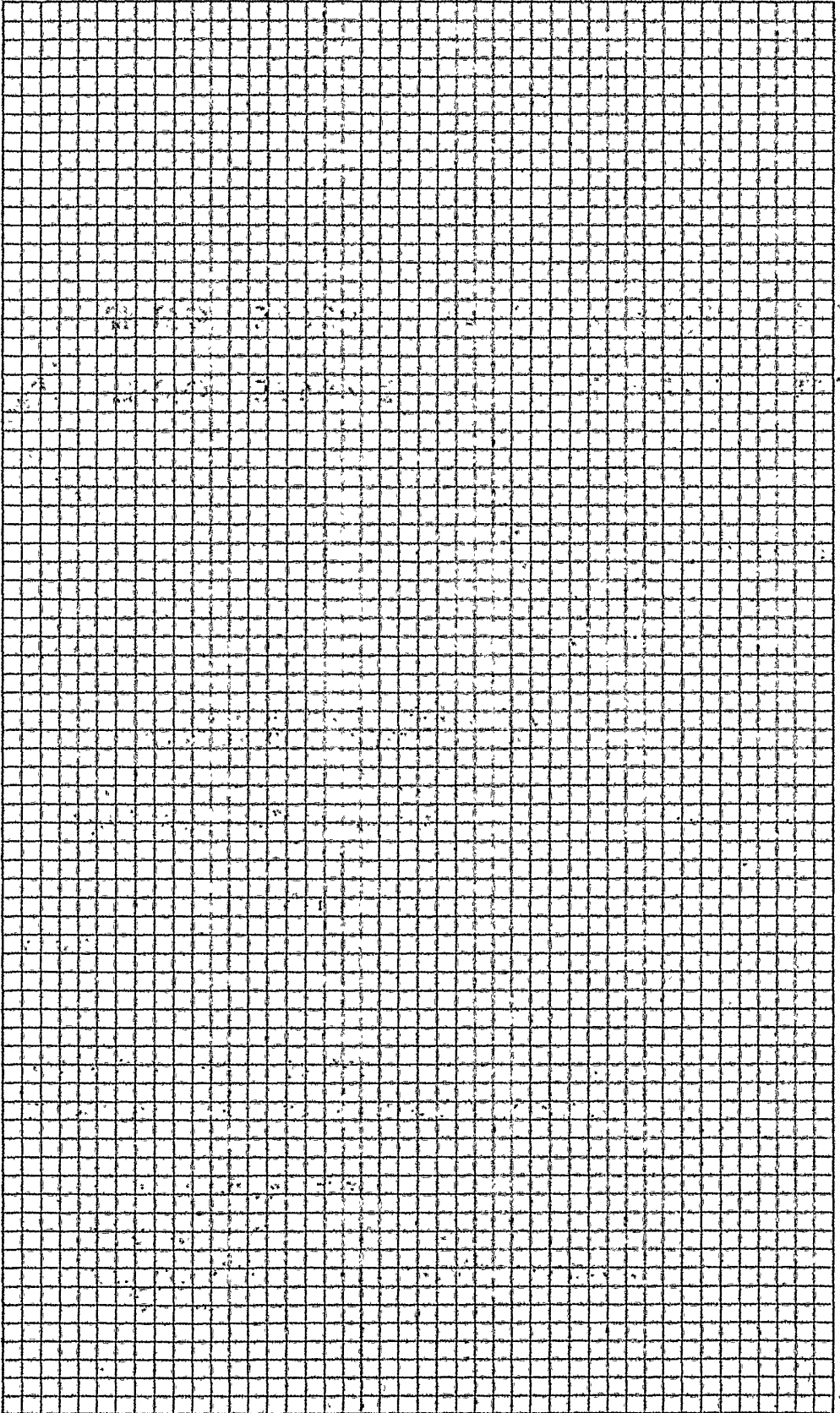
I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance.

"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed." (See Sec. 91.0202 L.A.M.C.)

Signed section with signature and table for approvals from Bureau of Engineering, Conservation, Plumbing, Planning, Fire, and Traffic.

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



**APPLICATION TO ALTER - REPAIR - DEMOLISH
AND FOR CERTIFICATE OF OCCUPANCY**

CITY OF LOS ANGELES

DEPT. OF BUILDING AND SAFETY

**INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only
2. Plot Plan Required on Back of Original.**

1. LEGAL DESCR. 265 thru 269				BLK.	TRACT	CENSUS TRACT	
2. PRESENT USE OF BUILDING (22 Cold Storage)				NEW USE OF BUILDING (22 Same)		DIST. MAP	
3. JOB ADDRESS 666-74 South Mesquit Street				TRACT Wingerter Tract		ZONE	
4. BETWEEN CROSS STREETS				AND		FIRE DIST.	
5. OWNER'S NAME Rhoda and Jack Levine				PHONE		INSIDE COR. LOT KEY REV. COR. LOT SIZE	
6. OWNER'S ADDRESS 670 South Mesquit Street				P.O. BOX ZIP		REAR ALLEY	
7. ARCHITECT OR DESIGNER				STATE LICENSE NO. PHONE		SIDE ALLEY BLDG. LINE	
8. ENGINEER				STATE LICENSE NO. PHONE		AFFIDAVITS	
9. CONTRACTOR Owner				STATE LICENSE NO. PHONE		DISTRICT OFFICE	
10. SIZE OF EXISTING BLDG.		STORIES 2&4	HEIGHT 24' & 45'	NO. OF EXISTING BUILDINGS ON LOT AND USE			
11. MATERIAL OF CONSTRUCTION		EXT. WALLS Brick	ROOF	FLOOR		Los Angeles	
12. JOB ADDRESS 666-74 South Mesquit Street				VALUATION: TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING. \$ 2500.00		GRADING	
13. VALUATION: TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING.				NEW WORK: (Describe) Parapet correction as per plans and Department File.		CRIT. SOIL	
NEW USE OF BUILDING				SIZE OF ADDITION	STORIES	HEIGHT	FLOOD
TYPE III-A	GROUP G-2	SPRINKLERS REQ'D SPECIFIED		VALUATION APPROVED		CONS.	
BLDG. AREA	MAX. OCC.	TOTAL		PLANS CHECKED		ZONED BY	
DWELL. UNITS	GUEST ROOMS	SPACES PARKING	REQ'D PROVIDED	PLANS APPROVED		Parapet Div. Room 225	
P.C. No.	CONT. INSP.		APPLICATION APPROVED		INSPECTOR		
P.C. 780	S.P.C.	G.P.I.	B.P. 12	I.F.	O.S.	C/O	TYPIST

.....	JUL-15-65	36936 E	•99826	Z - 2 CK	7.80
.....	JUL-15-65	36937 E	•99826	Z - 1 CK	12.00

STATEMENT OF RESPONSIBILITY

I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance.

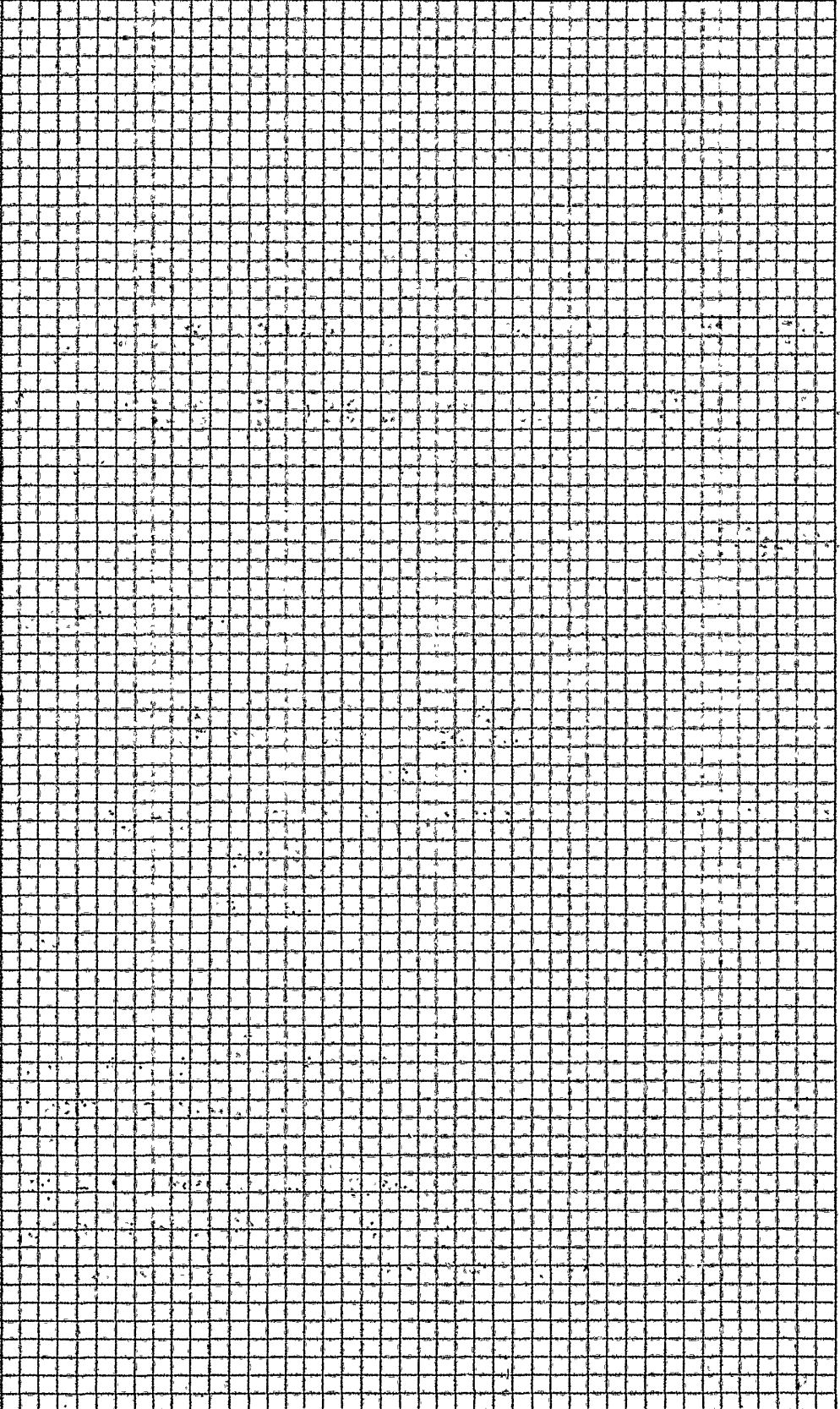
"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed. (See Sec. 91.0202 L.A.M.C.)"

Signed [Signature]
(Owner or Agent)

	Name	Date
Bureau of Engineering	ADDRESS APPROVED	
	SEWERS AVAILABLE	
	NOT AVAILABLE	
	DRIVEWAY APPROVED	
	HIGHWAY DEDICATION REQUIRED	
	COMPLETED	
	FLOOD CLEARANCE APPROVED	
Conservation	APPROVED FOR ISSUE	
	FILE #	
Plumbing	PRIVATE SEWAGE DISPOSAL SYSTEM APPROVED	
Planning	APPROVED UNDER CASE #	
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)	
Traffic	APPROVED FOR	

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



APPLICATION TO ALTER - REPAIR - DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

CITY OF LOS ANGELES

DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only 2. Plot Plan Required on Back of Original.

Form with numbered sections 1-14 containing details: LEGAL DESCR., PRESENT USE OF BUILDING (Cold Storage), JOB ADDRESS (678-82 South Mesquit Street), OWNER'S NAME (Rancho Cold Storage c/o Harley R. Tulpis), ARCHITECT OR DESIGNER, ENGINEER, CONTRACTOR, SIZE OF EXISTING BLDG., MATERIAL OF CONSTRUCTION, JOB ADDRESS (678-82 South Mesquit Street), VALUATION (\$1500.00), NEW WORK (Parapet correction), NEW USE OF BUILDING (Same), TYPE (III-B), GROUP (G-2), VALUATION APPROVED, PLANS CHECKED, PLANS APPROVED, APPLICATION APPROVED, P.C. No., P.C., S.P.C., G.P.I., B.P., I.F., O.S., C/O, TYPIST.

SHIERS USE ONLY JUL-15-65 36938 5 •99827 Z-2 CK 5.20 JUL-15-65 36939 5 •99827 Z-1 CK 8.00

STATEMENT OF RESPONSIBILITY

I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance.

"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed." (See Sec. 91.0202 L.A.M.C.)

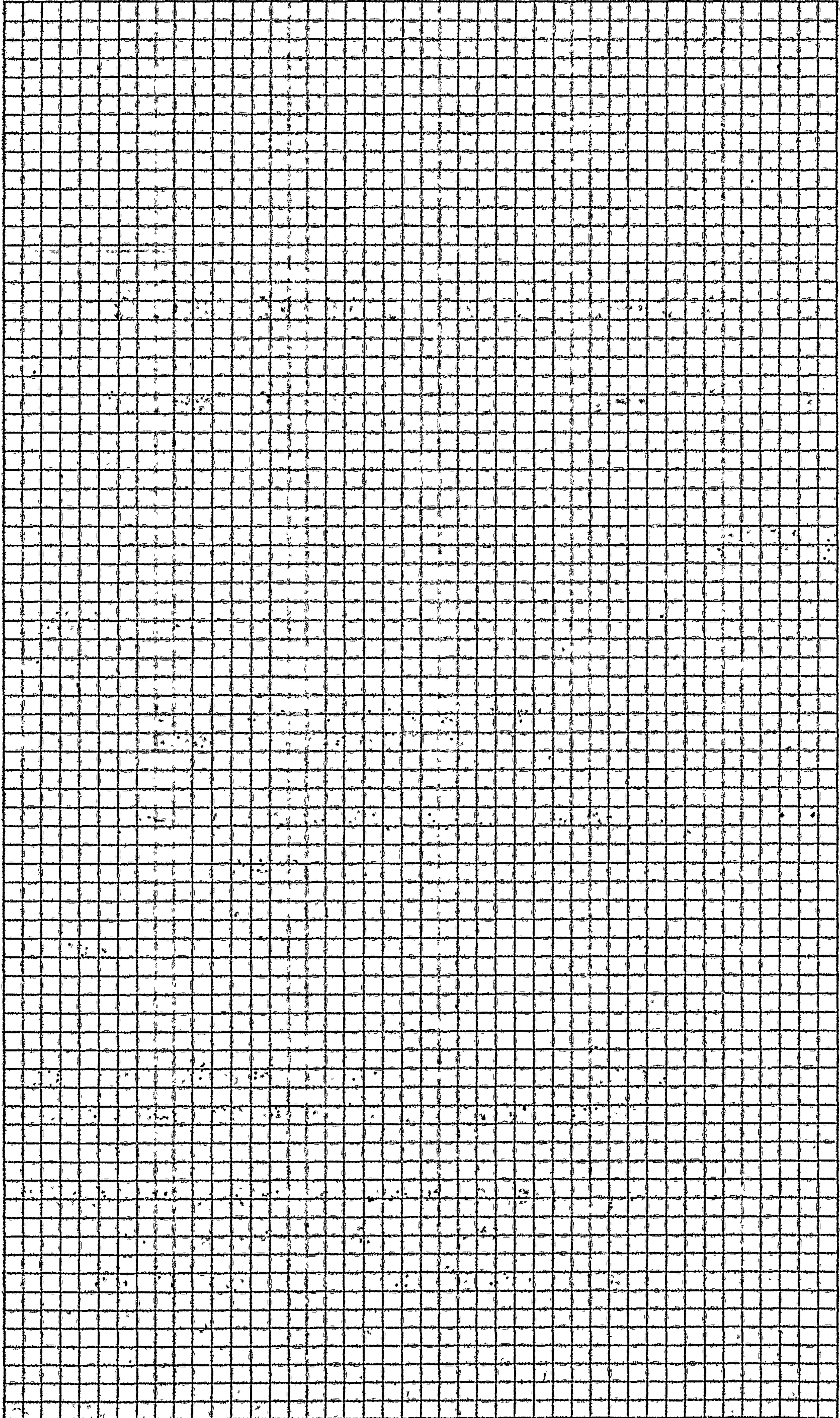
Signed [Signature] (Owner or Agent)

Table with columns: Department (Bureau of Engineering, Conservation, Plumbing, Planning, Fire, Traffic), Approval Status (ADDRESS APPROVED, SEWERS AVAILABLE, DRIVEWAY APPROVED, HIGHWAY DEDICATION REQUIRED, FLOOD CLEARANCE APPROVED, APPROVED FOR ISSUE, PRIVATE SEWAGE DISPOSAL SYSTEM APPROVED, APPROVED UNDER CASE #, APPROVED (TITLE 19), APPROVED FOR)

LEGAL DESCRIPTION

Faint, illegible text lines, likely a legal description or survey notes.

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



3

APPLICATION TO ALTER - REPAIR - DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

B&S Form B-3

CITY OF LOS ANGELES

DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only 2. Plot Plan Required on Back of Original.

Form with fields for LEGAL DESCR., LOT, BLK., TRACT, PRESENT USE OF BUILDING, NEW USE OF BUILDING, JOB ADDRESS, BETWEEN CROSS STREETS, OWNER'S NAME, PHONE, OWNER'S ADDRESS, P.O. BOX, ZIP, ARCHITECT OR DESIGNER, STATE LICENSE NO., PHONE, ENGINEER, STATE LICENSE NO., PHONE, CONTRACTOR, STATE LICENSE NO., PHONE, SIZE OF EXISTING BLDG., STORIES, HEIGHT, NO. OF EXISTING BUILDINGS ON LOT AND USE, MATERIAL OF CONSTRUCTION, EXT. WALLS, ROOF, FLOOR, JOB ADDRESS, VALUATION, NEW WORK, TYPE, GROUP, SPRINKLERS, VALUATION APPROVED, BLDG. AREA, MAX. OCC., TOTAL, PLANS CHECKED, ZONED BY, DWELL. UNITS, GUEST ROOMS, SPACES REQ'D PROVIDED, PLANS APPROVED, P.C. No., CONT. INSP., APPLICATION APPROVED, P.C., S.P.C., G.P.I., B.P., I.F., O.S., C/O, TYPIST

Table with 6 columns: Date, Lot No., Block, Parcel No., Zone, Area. Includes entries for JUL-15-65, 36940 E, 99828, Z-2 CK, 1.30 and JUL-15-65, 36941 E, 99828, Z-1 CK, 2.00

STATEMENT OF RESPONSIBILITY

I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance.

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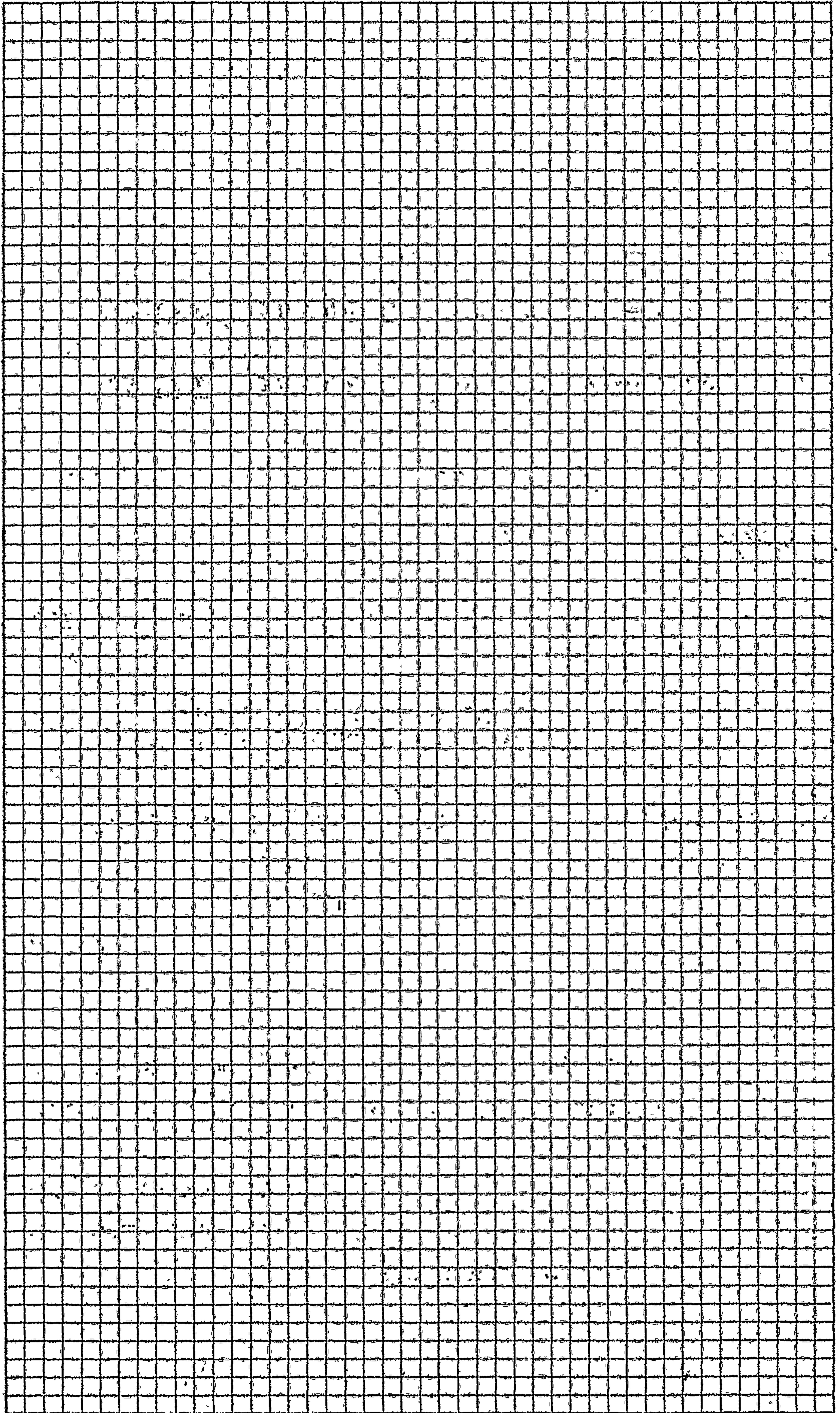
Signed [Signature] (Owner or Agent)

Table with 2 columns: Name, Date. Rows for Bureau of Engineering, Conservation, Plumbing, Planning, Fire, Traffic.

Form with fields for ADDRESS APPROVED, SEWERS AVAILABLE, DRIVEWAY APPROVED, HIGHWAY DEDICATION REQUIRED, FLOOD CLEARANCE APPROVED, APPROVED FOR ISSUE FILE #, PRIVATE SEWAGE DISPOSAL SYSTEM APPROVED, APPROVED UNDER CASE #, APPROVED (TITLE 19) (L.A.M.C.-S700), APPROVED FOR

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



3 APPLICATION TO ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

CITY OF LOS ANGELES
DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Application to Complete Numbered Items Only
2. Plot Plan Required on Back of Original.

1. LEGAL DESCR.	LOT	BLK.	TRACT	CENSUS TRACT
	275		Wingenter Tr	DIST-MAP 123 217
2. PRESENT USE OF BUILDING	NEW USE OF BUILDING			ZONE
(1) Water Cooling Tower	(2) same			M-3-3
3. JOB ADDRESS	684 S. Mesquit St			FIRE DIST. II 50
4. BETWEEN CROSS STREETS	7th St AND Jessee St			INSIDE COR. LOT
5. OWNER'S NAME	Jack Levine MA 48861			KEY REV. COR. LOT SIZE
6. OWNER'S ADDRESS	670 Mesquit St 21			40x162.6 40.17x158.84
7. ARCHITECT OR DESIGNER				REAR ALLEY
8. ENGINEER				SIDE ALLEY
9. CONTRACTOR	owner			BLDG. LINE
10. SIZE OF EXISTING BLDG.	STORIES	HEIGHT	NO. OF EXISTING BUILDINGS ON LOT AND USE	
30x55	1	28		
11. MATERIAL OF CONSTRUCTION	EXT. WALLS	ROOF	FLOOR	
	brick	metal	conc	

3 12. JOB ADDRESS: 684 S Mesquit St

13. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING: \$275 00

14. NEW WORK: (Describe) parapet CORRECTION AS PER PLANS & DEPT. FILE.

NEW USE OF BUILDING	N/C	SIZE OF ADDITION	STORIES	HEIGHT	FLOOR
TYPE	GROUP	SPRINKLERS REQ'D SPECIFIED	VALUATION APPROVED		CONS.
III-A	G-1		YES		YES
BLDG. AREA	MAX. OCC.	TOTAL	PLANS CHECKED	ZONED BY	
			OK	Parapet	
DWELL. UNITS	GUEST ROOMS	SPACES REQ'D PROVIDED	APPROVED	FILE WITH	
				HOWE	
P.C. No.	CONT. INSP.	APPLICATION APPROVED		INSPECTOR	
				VOUTURE	
P.C.	S.P.C.	G.P.I.	B.P.	I.F.	O.S.
1.63			2.50		
					C/O
					TYPIST
					lj

Plan check expires six months after fee is paid. Permit expires one year after fee is paid or six month after fee is paid if construction is not commenced.

CASHIER'S ONLY	AUG 18 8-67	40083	5	51570	X - 2 CK	1.63
	AUG 18 8-67	40084	5	51570	X - 1 CK	2.50

STATEMENT OF RESPONSIBILITY

I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance.

"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed." (See Sec. 91.0202 L.A.M.C.)

Signature: Jack Levine (Owner or Agent)

	Name	Date
Bureau of Engineering	BLogan	8/9/67
Conservation		
Plumbing		
Planning		
Fire		
Traffic		

3

APPLICATION TO ADD-ALTER-REPAIR-DEMOLISH

P.S.B-3-102-60
A.C.I.

CITY OF LOS ANGELES

AND FOR CERTIFICATE OF OCCUPANCY

DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

1. LEGAL DESCR.	LOT	BLK.	TRACT	CENSUS TRACT
	267		Wingerter Tr.	2065
2. PRESENT USE OF BUILDING	NEW USE OF BUILDING		DIST. MAP	
02 Commercial	(2) Same		123-217	
3. JOB ADDRESS	ZONE			
670 Mesquit St.	M3-3			
4. BETWEEN CROSS STREETS	FIRE DIST.			
7th St. AND Jesse	II 50			
5. OWNER'S NAME	PHONE		LOT (TYPE)	
Rancho Cold Storage, Inc.			Int	
6. OWNER'S ADDRESS	CITY	ZIP	LOT SIZE	
Same			40x128.88	
7. ARCHITECT OR DESIGNER	STATE LICENSE No.		PHONE	
			40.17x132.	
8. ENGINEER	STATE LICENSE No.		PHONE	
			ALLEY 62	
9. CONTRACTOR	STATE LICENSE No.		PHONE	
Driver & Eddy Const.	122002	870-7821	BLDG. LINE	
10. LENDER	BRANCH	ADDRESS		AFFIDAVITS
				c.c.p.d.
11. SIZE OF EXISTING BLDG.	STORIES	NO. OF EXISTING BUILDINGS ON LOT AND USE		
LENGTH 60 WIDTH 300	4	1 Cold Storage		
12. MATERIAL OF CONSTRUCTION OF EXISTING BLDG.	EXT. WALLS	ROOF	FLOOR	
Brick	Brick	Compo	Concrete	
13. JOB ADDRESS	DISTRICT OFFICE			
670 Mesquit St.	L.A.			
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 4,500.00			GRADING
15. NEW WORK: (Describe)	Repair fire damage			CRIT. SOIL
2% Damage - Verify!				HIGHWAY DED.
NEW USE OF BUILDING	SIZE OF ADDITION	STORIES	HEIGHT	FLOOD
(22) Same	0	N/C	N/C	/
TYPE	GROUP	SPRINKLERS REQ'D SPECIFIED	INSPECTION ACTIVITY	
III-B	G-2	N/C	COMB (GEN)	MAJ. S. CONS
BLDG. AREA	MAX. OCC.	TOTAL	PLANS CHECKED	
			ZONED BY	
			Iyle-Ph	
DWELL. UNITS	GUEST ROOMS	PARKING REQ'D SPACES	PROVIDED	PLANS APPROVED
				FILE WITH
P.C. No.	CONT. INSP	APPLICATION APPROVED		INSPECTOR
		[Signature]		(E)
P.C.	S.P.C.	G.P.I.	B.P. 2200	I.E. /
				G.S.
			C/O	TYPIST sel

PLAN CHECK EXPIRES SIX MONTHS AFTER FEE IS PAID. PERMIT EXPIRES ONE YEAR AFTER FEE IS PAID OR SIX MONTHS AFTER FEE IS PAID IF CONSTRUCTION IS NOT COMMENCED.

CASHIER'S USE ONLY

APR-27-70 217415 • 7726 Z-1CK 22.00

STATEMENT OF RESPONSIBILITY

I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance.

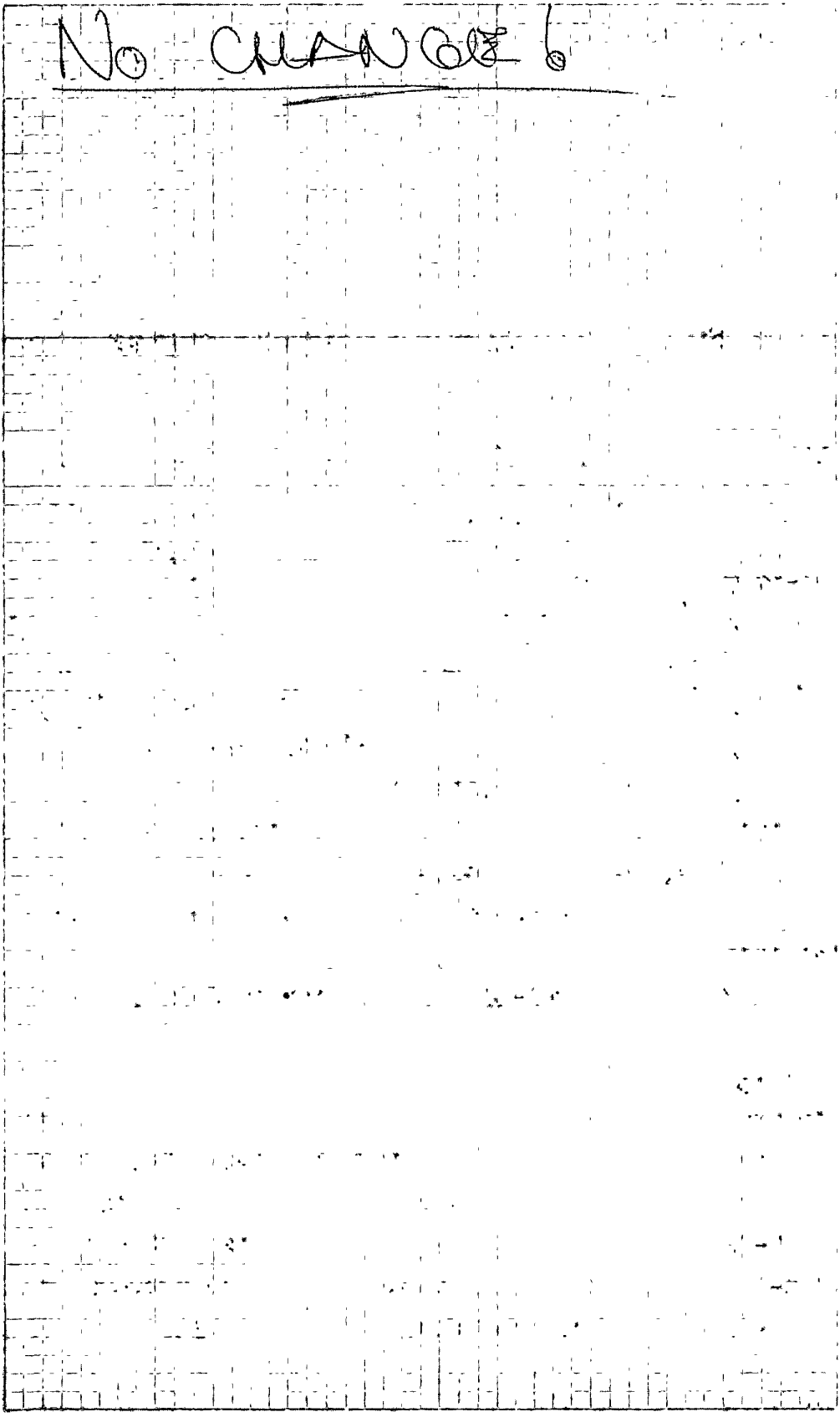
"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed." (See Sec. 91.0202 L.A.M.C.)

Signed: *Anthony Honor*
(Owner or Agent)

Name	Date
Dalton	4/27/70
Bureau of Engineering	ADDRESS APPROVED
	SEWERS AVAILABLE
	NOT AVAILABLE
	DRIVEWAY APPROVED
	HIGHWAY DEDICATION REQUIRED COMPLETED
	FLOOD CLEARANCE APPROVED
Conservation	APPROVED FOR ISSUE
	FILE #
Plumbing	PRIVATE SEWAGE DISPOSAL SYSTEM APPROVED
Planning	APPROVED UNDER CASE #
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)
Traffic	APPROVED FOR

ON LOT 1111 SHOW ALL BUILDINGS ON LOT AND USE OF EACH

No CHANGE



3

CITY OF LOS ANGELES

APPLICATION TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

AB 55B-3-R14.68

DEPT. OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

Form with 15 numbered sections: 1. LEGAL DESCR. LOT 267, TRACT Wingerton Tr; 2. PRESENT USE OF BUILDING (16) commercial bldg.; 3. JOB ADDRESS 670 Mesquit St; 4. BETWEEN CROSS STREETS Whittier AND Jesse; 5. OWNER'S NAME mr. Black; 6. OWNER'S ADDRESS Same; 7. ARCHITECT OR DESIGNER; 8. ENGINEER; 9. CONTRACTOR Safe-Way Sandblast; 10. LENDER; 11. SIZE OF EXISTING BLDG. 30X150; 12. MATERIAL OF CONSTRUCTION OF EXISTING BLDG. stucco; 13. JOB ADDRESS 670 Mesquit; 14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING 250.00; 15. NEW WORK: Sandblast inside black charred area to remove burnt area.

PLAN CHECK EXPIRES SIX MONTHS AFTER FEE IS PAID. PERMIT EXPIRES ONE YEAR AFTER FEE IS PAID OR SIX MONTHS AFTER FEE IS PAID IF CONSTRUCTION IS NOT COMMENCED.

CASHIER'S USE ONLY

MAY-14-70 254625 • 8856 Z-1CK 2.75

STATEMENT OF RESPONSIBILITY

I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance.

"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed." (See Sec. 91.0202 L.A.M.C.)

Signed Charles Jones (Owner or Agent)

Table with columns for Department (Bureau of Engineering, Conservation, Plumbing, Planning, Fire, Traffic) and Approval Status (Address Approved, Sewers Available, Driveway Approved, Highway Dedication, Flood Clearance, Approved for Issue, Private Sewage Disposal, Approved Under Case, Approved Title 19, Approved for).

1970 MAY -4 PM 1:29

CITY OF LOS ANGELES

DEPT OF BUILDING AND SAFETY

INSTRUCTIONS: Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	267-275	BLK.	Wingenter Tract		CENSUS TRACT	201500
2. PRESENT USE OF BUILDING	23 Cold Storage	NEW USE OF BUILDING	Same		DIST. MAP	123-217
3. JOB ADDRESS	670-684 South Mesquit Street				ZONE	M3-3
4. BETWEEN CROSS STREETS	Sixth Street		AND Seventh Street		FIRE DIST.	TWO
5. OWNER'S NAME	Jack Levine		PHONE		LOT (TYPE)	INT.
6. OWNER'S ADDRESS	670 South Mesquit Street		CITY	Los Angeles	ZIP	90021
7. ARCHITECT OR DESIGNER	None		STATE LICENSE No.		PHONE	TRREG.
8. ENGINEER	Frank Lehnen Smith		5505 481-3908		ALLEY	
9. CONTRACTOR	Not Selected		STATE LICENSE No.		PHONE	
10. LENDER	None		BRANCH ADDRESS		AFFIDAVIT	C.C.P.D.
11. SIZE OF EXISTING BLDG.	LENGTH 98.5	WIDTH 49.7	STORIES 1	HEIGHT 38'	NO. OF EXISTING BUILDINGS ON LOT AND USE 1 Cold Storage	
12. MATERIAL OF CONSTRUCTION OF EXISTING BLDG.	EXT. WALLS Corr. Iron		ROOF Compo		FLCOR Conc.	
13. JOB ADDRESS	670-684 South Mesquit St.				DISTRICT OFFICE	LA.
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 8,000				GRADING	
15. NEW WORK (Describe)	Repair fire damage 5%--Roof, Walls Roof Monitor in 49.7x98.5 portion of 196' x 380' building.				CRIT. SOIL	
NEW USE OF BUILDING	Sams		SIZE OF ADDITION	None		FLOOD
TYPE	GROUP G-1	SPRINKLERS REQ'D SPECIFIED	STORIES 1	HEIGHT 38'	INSPECTION ACTIVITY	
BLDG. AREA	MAX. OCC.	TOTAL	COMB	GEN	MAJ. S.	CONS
CHILL. UNITS	GUEST ROOMS	PARKING REQ'D SPACES	PROVIDED	PLANS APPROVED		FILE WITH
P.C. No.	CONT. INSP.	APPLICATION APPROVED		INSPECTOR		
P.C. 39.52	S.P.C.	G.P.I.	B.P. 465.0	I.E.	O.S.	C/O
TYPYST						

PLAN CHECK EXPIRES SIX MONTHS AFTER FEE IS PAID. PERMIT EXPIRES ONE YEAR AFTER FEE IS PAID OR SIX MONTHS AFTER FEE IS PAID IF CONSTRUCTION IS NOT COMMENCED.

CASHIER'S USE ONLY	AUG-1 -73	15243 E	75413	\$ = 6 CK	39.52
	AUG-1 -73	15243 E	75413	\$ = 1 CK	46.50

STATEMENT OF RESPONSIBILITY

I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of the State of California relating to workmen's compensation insurance.

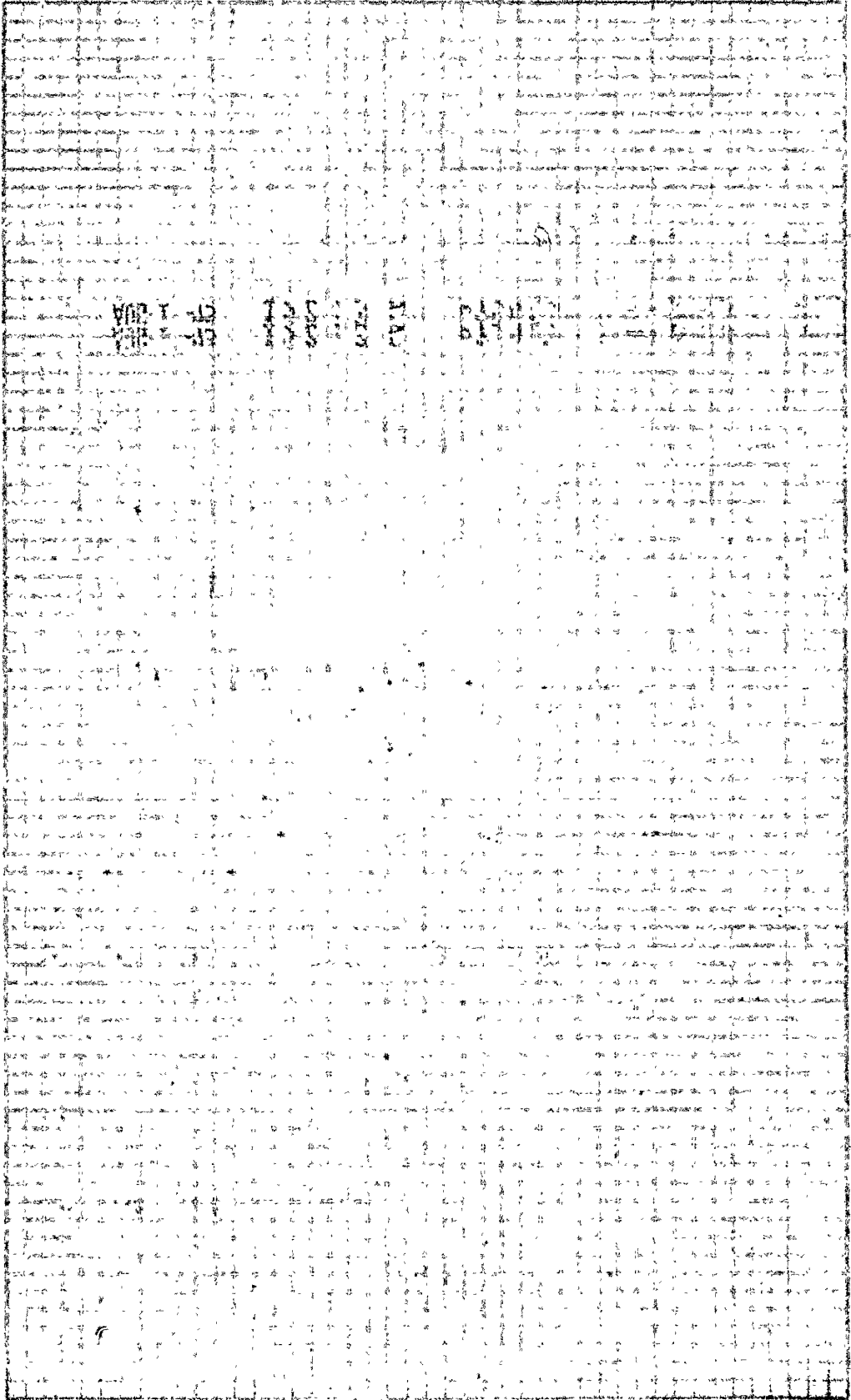
"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed." (See Sec. 91.0202 L.A.M.C.)

Rancho Cold Storage

Signed Frank Lehnen Smith (Owner or Agent)

Bureau of Engineering	Address Approved	Name	Date
	Sewers Available	Sholl	7-30-73
	Not Available		
	Driveway Approved		
	Highway Dedication Required Completed		
	Flood Clearance Approved		
Conservation	Approved for Issue File #		
Plumbing	Private Sewage Disposal System Approved		
Planning	Approved Under Case #		
Fire	Approved (Title 19) (L.A.M.C.-5700)		
Traffic	Approved For		

ON FLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

1. LOT LEGAL DESCR. 276	BLOCK ---	TRACT Wingertor Tract	COUNCIL DISTRICT NO 9	DIST. MAP 123 217
2. TYPE OF SIGN OR NEW WORK 19) poster-panel--L/A STD #85 <input type="checkbox"/> RESIDENTIAL <input checked="" type="checkbox"/> COMMERCIAL				CENSUS TRACT 2065.00
3. JOB ADDRESS 694 MESQUIT STREET				ZONE M3-3
4. BETWEEN CROSS STREETS 7th STREET AND JESSE ST				EIRE DIST. 11
5. OWNER'S NAME ARNOLD RUBIN & FRED KORT				LOT (TYPE) REV COR 41
6. OWNER'S ADDRESS 2060 E. 7th STREET				LOT SIZE 40.21x143.48
7. ARCHITECT OR ENGINEER KEVIN KELLY BUS. LIC NO 12494 ACTIVE STATE LIC NO 828-34331				ALLEY ---
8. QUALIFIED INSTALLER BAUM ELECTRIC BUS. LIC NO 127751 ACTIVE STATE LIC NO 731-5111				BLDG. LINE ---
9. INSTALLER'S ADDRESS 1550 W. WASHINGTON BOULEVARD 90007				AFFIDAVITS CCPD
10. SIZE OF EXISTING BUILDING WIDTH LENGTH		TYPE	STORIES	EXT. WALL CONST.
11. SIZE OF SIGN 12' x 25'		TOTAL COPY AREA 300 S/ft	OVERALL HEIGHT 42'	FROM GRADE FROM ROOF
12. MATERIAL OF SIGN CONSTRUCTION stl		SUPPORTING FRAME stl	FRAME OF COPY stl	SURFACE OF SIGN incomb
13. JOB ADDRESS 694 MESQUIT STREET				HIGHWAY DED. YES
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED SIGN \$ 5600.00				DIST. OFFICE L.A.
15. TYPE OF SIGN OR NEW WORK poster-panel #85 <input checked="" type="checkbox"/> SINGLE FACE <input type="checkbox"/> DOUBLE FACE				GRADING ---
16. ILLUMINATION <input type="checkbox"/> NONE <input type="checkbox"/> INTERNAL <input checked="" type="checkbox"/> EXTERNAL <input type="checkbox"/> FLASHING <input type="checkbox"/> REVOLVING <input checked="" type="checkbox"/> OTHER NONE				CONV. ---
17. NO. OF SIGNS OR GAS TUBE SYSTEMS 1		NO OF ADDITIONAL BRANCH CIRCUITS 0	NO. OF CONTROL DEVICES 0	ZONED BY BRINKMAN
CLEARANCES AND/OR APPROVALS REQUIRED		PERMIT FEES		PLANS CHECKED
Yes No		SIGNS/G T. SYSTEMS 6 00		PLANS APPROVED
FREEWAY SURVEY <input type="checkbox"/>		ADDITIONAL CIRCUITS ---		APPLICATION APPROVED
TRAFFIC DEPT <input type="checkbox"/>		ELECTRICAL SERVICE ---		DATE 9/7/79
BOARD <input type="checkbox"/>		CONTROL DEVICES ---		FILED WITH
P.C. 34.51		BLDG. PERMIT 40 60		INSPECTOR
S.P.C.		ISSUING FEE 8 00		CONT. lic INSP. fab
P.C. NO		TOTAL 54 60		INSP. ACTIVITY BMI
WORKER'S COMPENSATION INSURANCE CERTIFICATE <input checked="" type="checkbox"/> ON FILE <input type="checkbox"/> EXEMPT				INSPECTOR

PLAN CHECK EXPIRES ONE YEAR AFTER FEE IS PAID. PERMIT EXPIRES TWO YEARS AFTER FEE IS PAID OR 180 DAYS AFTER FEE IS PAID IF CONSTRUCTION IS NOT COMMENCED.

SEP-7-79 18447 F.A. #89341 \$ 6.00 34.51
 SEP-7-79 18446 F.A. #89341 \$ 2.00 54.60

CASHIER'S USE ONLY

LIMIT OF PERMIT

"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed." See Sec. 91.0202 L.A.M.C.

Signed *Bernard* Signature/Date
 (Owner or Agent having Property Owner's Consent)
 ALSO, sign statement on reverse side, if applicable.

Bureau of Engineering	ADDRESS APPROVED	BERNARD 9-7-79
Conservation	HIGHWAY DEDICATION	
Traffic	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>	
Municipal Arts	APPROVED FOR ISSUE	
Planning	APPROVED UNDER CASE #	
Board of Building and Safety Commissioners File #		

WORKER'S COMPENSATION CERTIFICATION

I certify that in the performance of my duties as an employee of the above named employer, I have not been injured or become subject to any occupational disease as a result of my work.

Applicant's Signature _____ Date _____

Employer's Signature _____

If, after signing this certificate, the employee is injured or becomes subject to any occupational disease as a result of his work, he shall file a claim with the Industrial Commission of the State of Michigan.

ON RIGHT HAND SHOW ALL BUILDINGS ON LOT AND USE OF EACH

The form contains a large grid for drawing buildings on a lot. The grid is approximately 20 units wide and 40 units high. The top right corner of the grid is shaded with a cross-hatch pattern. The grid is intended for the worker to draw and label buildings on the lot and specify their uses.

PUBLIC RECORD

APPLICATION FOR INSPECTION — TO ADD-ALTER-REPAIR-DEMOLISH

CITY OF LOS ANGELES

AND FOR CERTIFICATE OF OCCUPANCY

PART OF BUILDING AND SAFETY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1.	LOT 90	BLOCK	TRACT Goodwin	COUNCIL DISTRICT NO. 9	DIST. MAP 123-217 CENSUS TRACT 2065.00
2.	PRESENT USE OF BUILDING () Warehouse		NEW USE OF BUILDING () Same		ZONE M3-3
3.	JOB ADDRESS 658 Mesquit Street				FIRE DIST. II
4.	BETWEEN CROSS STREETS 7th Street		AND	LOT TYPE Interior	
5.	OWNER'S NAME Alexander Santoorian			PHONE	LOT SIZE Irreg.
6.	OWNER'S ADDRESS 658 Mesquit Street		CITY	ZIP	
7.	ENGINEER	BUS. LIC NO.	ACTIVE STATE LIC. NO.	PHONE	ALLEY
8.	ARCHITECT OR DESIGNER	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	BLDG. LINE
9.	ARCHITECT OR ENGINEER'S ADDRESS			CITY	ZIP 984-1551
10.	CONTRACTOR CONTRACTOR RUSTUERN CONSTRUCTION CO. #251609		BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE 984-1551
11.	SIZE OF EXISTING BLDG. WIDTH 130 LENGTH 100	STORIES 4	HEIGHT	NO. OF EXISTING BUILDINGS ON LOT AND USE One CCPD	
12.	CONST. MATERIAL OF EXISTING BLDG. →	EXT. WALLS Wood	ROOF Wood	FLOOR Wood	
13.	JOB ADDRESS 658 Mesquite Street				STREET GUIDE
14.	VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING \$ 500.00				DISTRICT OFFICE LA SEISMIC STUDY ZONE
15.	NEW WORK (Describe) Seal off upper floors to comply with Fire Safety Notice & Board Letter #794798				GRADING FLOOD yes
NEW USE OF BUILDING			SIZE OF ADDITION	STORIES	HEIGHT
TYPE	GROUP OCC.	BLDG. AREA	PLANS CHECKED		FILE WITH
DWELL UNITS N/C	MAX OCC.	TOTAL	APPLICATION APPROVED		TYPIST MET
GUEST ROOMS	PARKING REQ'D	PARKING PROVIDED STD COMP.	INSPECTION ACTIVITY		INSPECTOR Gillespie
SPRINKLERS REQ'D SPEC.		CONT. INSP.	COMB		GEN. MAJ. S. (CONS)
P.C.	P.M.	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.	CASHIERS USE ONLY 9.00 BR-C 6.14 0001 A-869 3 07/08/80 9.00 CATO		
S.P.C.	I.F.				
B.P.	O.S.				
G.P.I.	C/O				
DIST. OFFICE	ENERGY:				
P.C. NC.					

DECLARATIONS AND CERTIFICATIONS

LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
 Date 7-2-80 Lic. Class B1 Lic No. 251609 Contractor Rustuern Coast
 Contractor's Mailing Address 12500 Riverside Dr N.H. 91607

OWNER-BUILDER DECLARATION

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code): Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500.):

I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code). The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such project with a contractor(s) licensed pursuant to the Contractor's License Law.)

I am exempt under Sec. N/A A, B, & P. C. for this reason N/A

Date N/A Owner N/A

WORKERS' COMPENSATION DECLARATION

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
 Policy No. TO BE DETERMINED Company State of Calif.

Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.

Date 7-2-80 Applicant Rustuern Coast
 Applicant's Mailing Address 12500 Riverside Dr N.H. 91607

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.
 Date _____ Applicant _____

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).
 Lender's Name N/A
 Lender's Address _____

I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

21. I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 94023 LAMC.)

Signed Russell H. Hemmuth Owner 7-2-80
 (Owner or agent having property owner's consent) Position Date

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APPLICATION FOR INSPECTION

CITY OF LOS ANGELES DEPT. OF BUILDING AND SAFETY

04901704.8

6/77 5770

TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 90	BLOCK	TRACT GOODWIN TRACT	COUNCIL DISTRICT NO. 9	DIST. MAP 123 B 217 CENSUS TRACT 2065
2. PRESENT USE OF BUILDING	WAREHOUSE		NEW USE OF BUILDING		ZONE M3-3
3. JOB ADDRESS	658 S. MESQUIT St.				FIRE DIST. II
4. BETWEEN CROSS STREETS	XX JESSE ST.		AND SIXTH ST		LOT TYPE INT
5. OWNER'S NAME	AL SANTOYRAIL				LOT SIZE IRREG
6. OWNER'S ADDRESS	SAME				
7. ENGINEER	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	ALLEY	
8. ARCHITECT OR DESIGNER	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	BLDG. LINE	
9. ARCHITECT OR ENGINEER'S ADDRESS	CITY				ZIP
10. CONTRACTOR	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	AFFIDAVITS	
KENCO		916	507-6900	AFF 48394 AFF 30549 CC PD	
11. SIZE OF EXISTING BLDG.	STORIES 4	HEIGHT	NO. OF EXISTING BUILDINGS ON LOT AND USE		WAREHOUSE
WIDTH	LENGTH				
12. CONST. MATERIAL OF EXISTING BLDG.	EXT. WALLS WOOD	ROOF WOOD	FLOOR WOOD	P.C REQ YES	
13. JOB ADDRESS	658 S. MESQUIT St.				DISTRICT OFFICE L.A.
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 90,000				SEISMIC STUDY ZONE
15. NEW WORK (Describe)	Demolish Bldg in Demolished - Remove Debris SC#				GRADING FLOOD HWY. DED. CONS. yes
NEW USE OF BUILDING	clear lot		SIZE OF ADDITION	STORIES	HEIGHT
TYPE	GROUP OCC.	FLOOR AREA	PLANS CHECKED		ZONED BY C. POWELL
DWELL UNITS	MAX OCC.	TOTAL	APPLICATION APPROVED Ken Co		FILE WITH
GUEST ROOMS	PARKING REQ'D	PARKING PROVIDED	INSPECTION ACTIVITY		INSPECTOR DICKSON
		STD. COMP.	COMP	GEN.	M.A.S.
P.C.	G.P.I.	CON. INSP.	CASHIER'S USE ONLY CB 417.00 SP-C C1 6.30 E.I. C2 8.47 OSS 10065 0021 64933 3-04/16/85 421.77 CHTD		
S.P.C.	P.M.				
BP. 417	E.I. 630	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.			
I.F.	O.S.S. 847				
O/S	S.O.S.S.				
DIST. OFFICE LA	C/O	SPRINKLERS REQ'D SPEC.			
P.C. NO.		ENERGY NO	PLAN CHECK EXPIRES ONE YEAR AFTER FEE IS PAID. PERMIT EXPIRES TWO YEARS AFTER FEE IS PAID OR 180 DAYS AFTER FEE IS PAID IF CONSTRUCTION IS NOT COMMENCED.		

DECLARATIONS AND CERTIFICATIONS

LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect. Date 4-16-85 Lic. Class 131 Lic. Number 27146 Contractor [Signature]

OWNER-BUILDER DECLARATION

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).) I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.) I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.) I am exempt under Sec. _____, B. & P. C. for this reason: _____ Date _____ Owner's Signature _____

WORKERS' COMPENSATION DECLARATION

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.). Policy No. OPEN Insurance Company ST FUND [Signature] Date 4-16-85 Applicant's Signature [Signature] Applicant's Mailing Address 425 Los Feliz

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California. Date _____ Applicant's Signature _____

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.). Lender's Name _____ Lender's Address _____

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes. I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91.0202 LAMC)

Signed [Signature] Position [Signature] Date 4-16-85

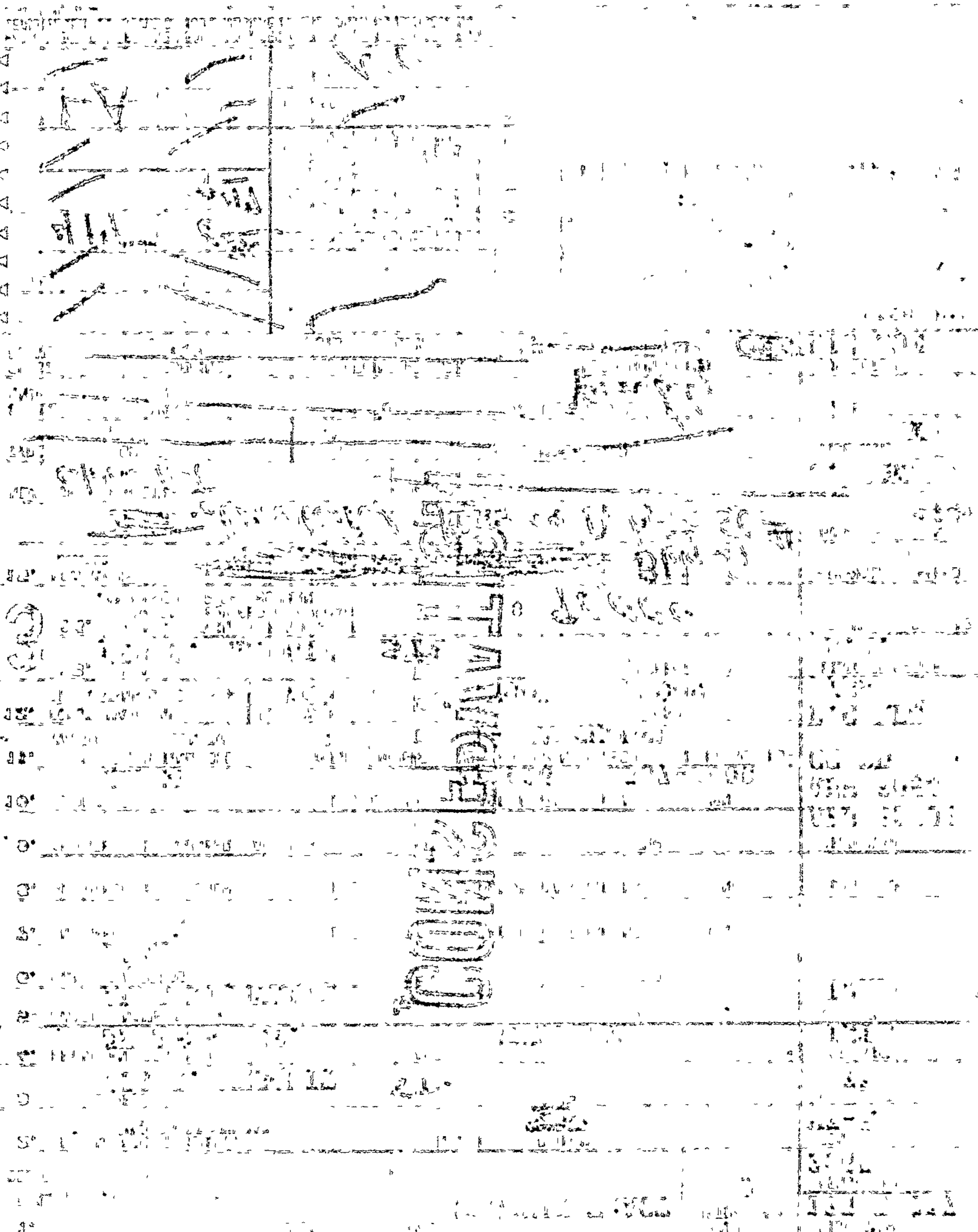
50° COMMISSION OF FINANCIAL SERVICES

COMMISSION OF FINANCIAL SERVICES

COMMISSION OF FINANCIAL SERVICES

COMMISSION OF FINANCIAL SERVICES

COMMISSION OF FINANCIAL SERVICES



PERMIT TO DEMOLISH
AND FOR CERTIFICATE
OF COMPLETION

OWNER'S NAME Alexander Andoerjian		CITY LA		LOT TRACT 23-2065	
OWNER'S ADDRESS 8045 Mulholland Dr.		CITY LA		ZONE M3-3	
ENGINEER G. O. Dyer		STATE LIC. NO. 90045		PHONE 822-3919	
ARCHITECT OR DESIGNER Woodpro Int'l Inc		BUS. LIC. NO. 458934		ACTIVE STATE LIC. NO. 408 227-2332	
ARCHITECT OR ENGINEER'S ADDRESS P.O. Box 53110, San Jose		CITY San Jose		PHONE 95153	
CONTRACTOR Woodpro Int'l Inc		BUS. LIC. NO. 458934		ACTIVE STATE LIC. NO. 408 227-2332	
SIZE OF EXISTING BLDG. WIDTH 110 LENGTH 300 STORIES 2 HEIGHT 25		NO. OF EXISTING BUILDINGS ON LOT AND USE three used as one		AFFIDAVITS APP 40394 APP 30549 CCPD PC NO	
CONSTR. MATERIAL OF EXISTING BLDG. WALLS concrete blk FLOOR built-up FLOOR concrete		STREET GUIDE		DISTRICT OFFICE LA	
JOB ADDRESS 658 Mesquit Street		VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING \$ 500,000 \$ 100,000.00		SEISMIC STUDY ZONE	
NEW WORK (Describe) Design & Construct 14,500 SF concrete blk addition to replace one destroyed by fire		GRADING		FLOOD	
NEW USE OF BUILDING Produce Distribution		SIZE OF ADDITION 106X136		STORIES 1	
TYPE (III)		GROUP OCC. B2		FLOOR AREA 14,500	
DWELL UNITS 48		TOTAL		PLANS CHECKED Paul Wong	
GUEST ROOMS		PARKING PROVIDED STD, 22 COMR 3		APPLICATION APPROVED P.W.	
P.C. NO. 71074		SPRINKLERS REQ'D SPEC. Yes		INSPECTION ACTIVITY COMB GEN MAJ.S CONS.	
DIST. OFFICE LA		ENERGY		FILE WITH GC	
PLAN CHECK EXPIRES ONE YEAR AFTER FEE IS PAID. PERMIT EXPIRES TWO YEARS AFTER FEE IS PAID OR 180 DAYS AFTER FEE IS PAID IF CONSTRUCTION IS NOT COMMENCED.		CASHIERS USE ONLY		INSPECTOR GC	

DECLARATIONS AND CERTIFICATIONS
LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
Date 9/24/85 Lic. Class B Lic. Number 453934 Contractor [Signature]
(Signature)

OWNER-BUILDER DECLARATION

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code. Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).
 I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code. The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale).
 I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code. The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law).
 I am exempt under Sec. _____ B. & P. C. for this reason _____
Date _____ Owner's Signature _____

WORKERS' COMPENSATION DECLARATION

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
Policy No 822820-85 Insurance Company STATE COMPENSATION INSURANCE FUND
 Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.
Date 9/24/85 Applicant's Signature [Signature]
Applicant's Mailing Address PO Box 53110 San Jose Ca 95153

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California
Date _____ Applicant's Signature _____

NOTICE TO APPLICANT. If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).
Lender's Name _____ Lender's Address _____

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed (See Sec. 91 0202, LAMC)

and [Signature] Process Analyst 9/24/85
(Owner or agent having property owners control) Position Date

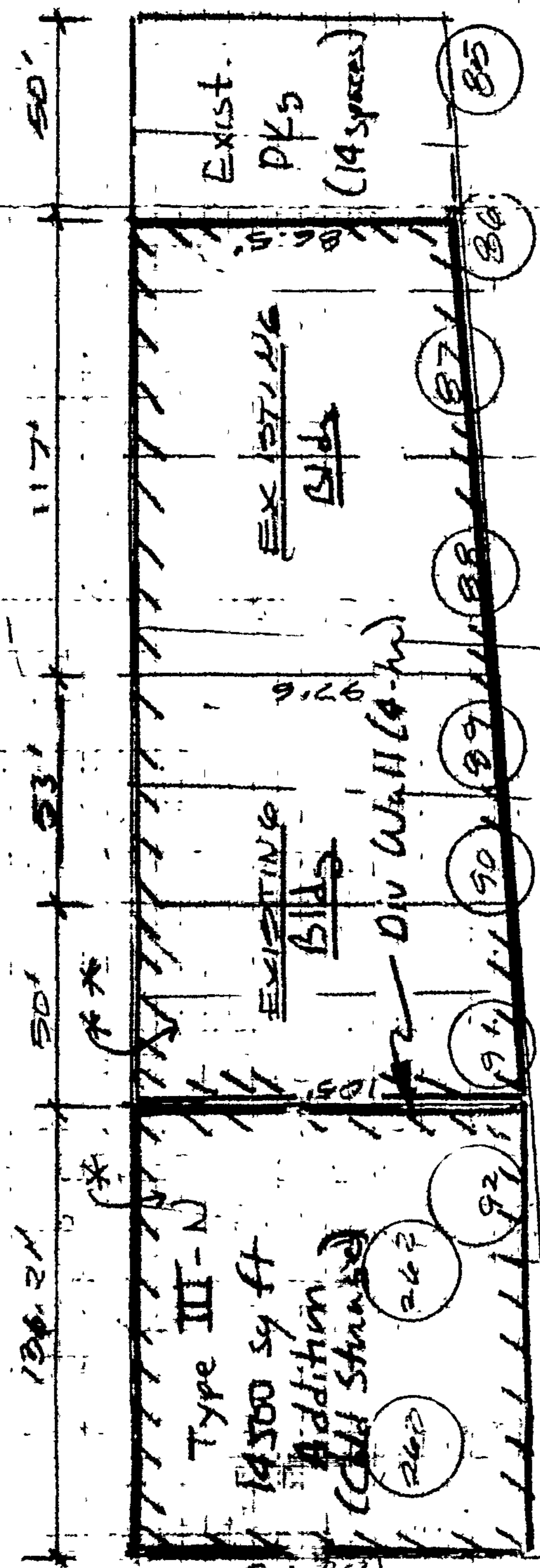
No new driveways to be built. 0 0 1/2 A Affected taller 9/19/85

123-217A X X J. Chen 9/19/85
X J. Chen

MS - 9/19/85

N

Street



Parking 14 in existing parking lot
 8 in existing bldg *
 3 in new addition * (Compact)
 25 top

Addition to be
 for Cold Storage.

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

1. LEGAL DESCR.	LOT 90	BLOCK	TRACT GOODWIN	COUNCIL DISTRICT NO. 9	DIST. MAP 123-217 CENSUS TRACT 2065
2. PURPOSE OF BUILDING	PRODUCE WAREHOUSE SAME				ZONE M3-3
3. JOB ADDRESS	658 MESQUITE ST.				FIRE DIST. ---
4. BETWEEN CROSS STREETS	JESSE ST. AND 6TH ST.				LOT TYPE INT.
5. OWNER'S NAME	AL SANTOPIAN (213) 620-0650				LOT SIZE 40x150
6. OWNER'S ADDRESS	212 ALDERGATE ST. MONTEREY PARK 91754				ALLEY ---
7. ENGINEER	PETER HIGGINS 524725628 663-9218				BLDG. LINE ---
8. ARCHITECT OR DESIGNER	LADDRACK I-06778-L LA FAB #1211 (714) 987-7477				AFFIDAVITS CCPD
9. ARCHITECT OR ENGINEER'S ADDRESS	1626 SILVERLAKE BLVD LA 90026				AFF 48394 30549
10. CONTRACTOR	EV ERECT 476691 C61 (818) 246-8381				NO (F)
11. SIZE OF NEW BLDG.	WIDTH 110-6	LENGTH 106-6	STORIES 1	HEIGHT 20	NO. OF EXISTING BUILDINGS ON LOT AND USE 1 WAREHOUSE
12. MATERIAL OF CONSTRUCTION	EXT. WALLS CONC. BLK		ROOF WD.	FLOOR CONC.	STREET GUIDE
13. JOB ADDRESS	658 MESQUITE ST.				DISTRICT OFFICE LA
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$40,000 / \$8,000				SEISMIC STUDY ZONE ---

15. NEW WORK: FABRICATION & INSTALLATION OF PALLET RACKS.

PURPOSE OF BUILDING	SAME		STORIES	HEIGHT	ZONED BY RODRIGUEZ
TYPE	GROUP OCC.	FLOOR AREA	PLANS CHECKED	FILE WITH	
DWELL UNITS	MAX. OCC.	TOTAL	APPLICATION APPROVED	TYPIST cn	
GUEST ROOMS	PARKING REQ'D.	PARKING PROVIDED	INSPECTION ACTIVITY		INSPECTOR
P.C. 190.40	G.P.I.	CONT INSP	COMB. GEN. MAJ.S. CONS.		B & S B-1 (R 6 85)
S.P.C.	P.M. 6-		CASHIER'S USE ONLY		
BP 122	EA 126	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC	103.70 B-PC 122.00 BP-R 6.00 PL/M 1.26 E.S. 4.66 OSS 46651.0001		61578 3 09/26/86 237.62 CHTD.
O/S	SOSS.	SPRINKLERS			
DIST OFFICE	C/O	ENERGY			
P.C. NO. 24396	F.H.				

PLAN CHECK EXPIRES ONE YEAR AFTER FEE IS PAID. PERMIT EXPIRES TWO YEARS AFTER FEE IS PAID OR 180 DAYS AFTER FEE IS PAID IF CONSTRUCTION IS NOT COMMENCED

DECLARATIONS AND CERTIFICATIONS

LICENSED CONTRACTORS DECLARATION

15. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.

Date _____ Lic. Class _____ Lic. No. _____ Contractor's Signature _____

Contractor's Mailing Address _____

OWNER-BUILDER DECLARATION

16. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code): Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500.):

I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)

I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)

I am exempt under Sec. _____, B. & P. C. for this reason _____

Date Sept 26, 1986 Owner's Signature Al Santopian

WORKERS' COMPENSATION DECLARATION

17. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).

Policy No. _____ Insurance Company _____

Certified copy is hereby furnished.

Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.

Date _____ Applicant's Signature _____

Applicant's Mailing Address _____

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

18. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.

Date 9-26-86 Applicant's Signature Al Santopian

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

19. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).

Lender's Name _____

Lender's Address _____

20. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91.200 LAMC)

Signed Al Santopian Owner Owner Date 9-26-86

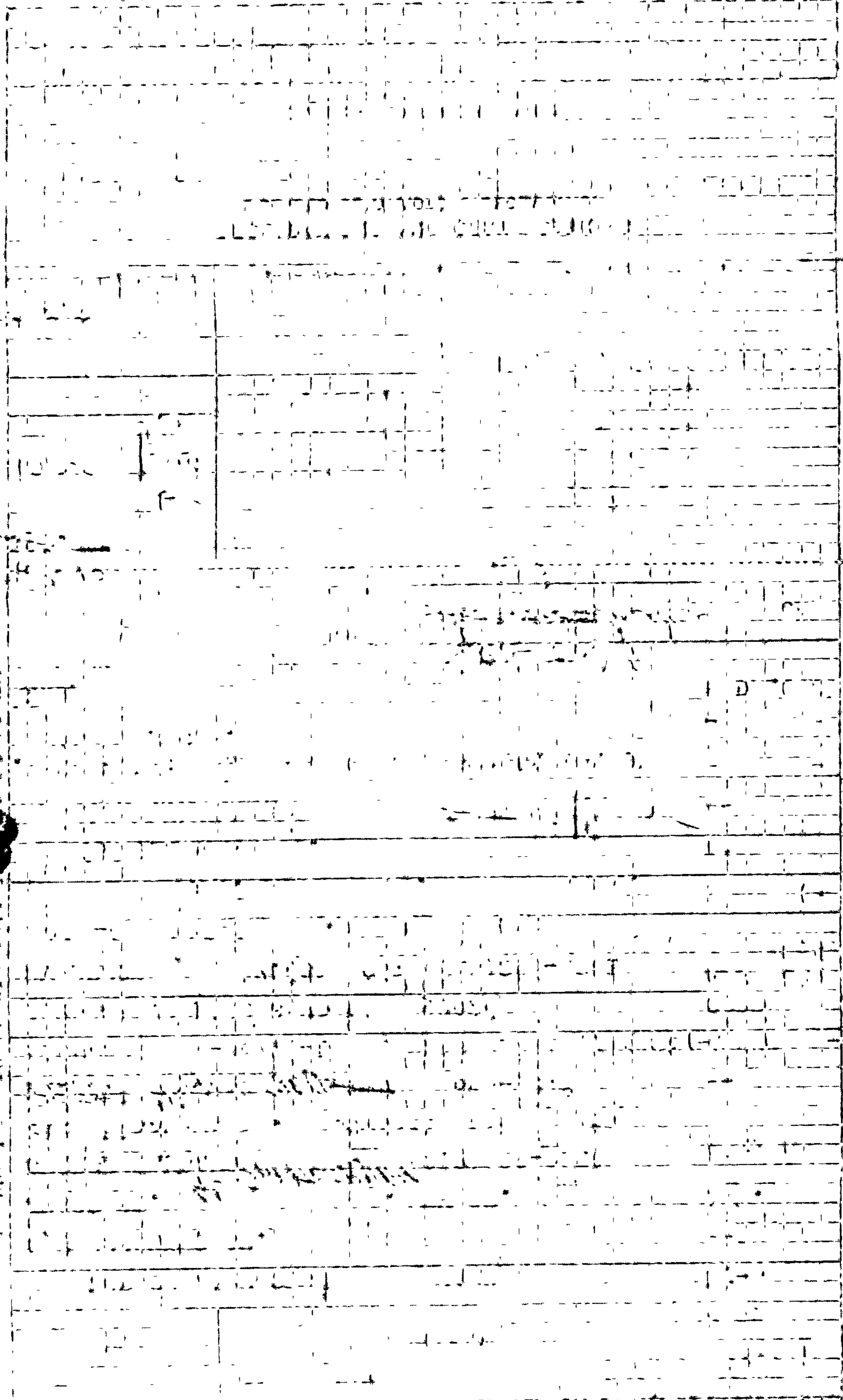
(Owner or agent having property owner's consent) Position Date

1 2 8 0 0 2 0 0 3 6 1 0 2 2

100 100 100 100 100 100 100 100 100 100 100 100 100 100

100 100 100 100 100 100 100 100 100 100 100 100 100 100

0. LOT 1. 2. 3. 4. 5. 6. 7. 8. 9. 10. 11. 12. 13. 14. 15. 16. 17. 18. 19. 20. 21. 22. 23. 24. 25. 26. 27. 28. 29. 30. 31. 32. 33. 34. 35. 36. 37. 38. 39. 40. 41. 42. 43. 44. 45. 46. 47. 48. 49. 50. 51. 52. 53. 54. 55. 56. 57. 58. 59. 60. 61. 62. 63. 64. 65. 66. 67. 68. 69. 70. 71. 72. 73. 74. 75. 76. 77. 78. 79. 80. 81. 82. 83. 84. 85. 86. 87. 88. 89. 90. 91. 92. 93. 94. 95. 96. 97. 98. 99. 100.



AMERICAN

AMERICAN

3

APPLICATION FOR INSPECTION

CITY OF LOS ANGELES DEPT. OF BUILDING AND SAFETY

TO ADD-ALTER, REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only

Form with 15 numbered sections: 1. LOT 90, BLOCK, TRACT Goodwin, COUNCIL DISTRICT NO. 9, DIST. MAP 123-217, CENSUS TRACT 2065; 2. PRESENT USE OF BUILDING () Produce Warehouse, NEW USE OF BUILDING SAME () Produce Ripening Room; 3. JOB ADDRESS 658 Mesquite St.; 4. BETWEEN CROSS STREETS Jesse St. AND 6th St.; 5. OWNER'S NAME Al Santoojian, 619 328-5070; 6. OWNER'S ADDRESS 3116 MARBER DR CITY RANCH MIRAGE ZIP 92270; 7. ENGINEER PETER HIGGINS BUS. LIC. NO. 6642 ACTIVE STATE LIC. NO. PHONE Eugene Birnbaum 6628 663-9218; 8. ARCHITECT OR DESIGNER Laddrack I-06778-L LA FAB #1211 714-987-7477; 9. ARCHITECT OR ENGINEER'S ADDRESS 1626 silver Lake Bl. L.A. 90026; 10. CONTRACTOR NOT SELECTED; 11. SIZE OF EXISTING BLDG. WIDTH 110'-6" LENGTH 106'-6" STORIES 1 HEIGHT 20' NO. OF EXISTING BUILDINGS ON LOT AND USE 1-WAREHOUSE; 12. CONST. MATERIAL OF EXISTING BLDG. CONC. BLK. ROOF WD. FLOOR CONC.; 13. JOB ADDRESS 658 MESQUITE ST.; 14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING \$18,000.00; 15. NEW WORK (Describe) Fabrication & Installation of Pallet racks

Form with 16 numbered sections: 16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect. Date 9/29/86 Lic. Class Lic. Number Contractor. OWNER-BUILDER DECLARATION: I, as owner of the property, am exclusively contracting with licensed contractors to construct the project. Date 9/29/86 Owner's Signature. WORKERS' COMPENSATION DECLARATION: I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof. Policy No. Insurance Company. CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE: I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California. Date 9/29/86 Applicant's Signature. CONSTRUCTION LENDING AGENCY: I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued. Lender's Name Not Selected Lender's Address. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes. I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. Signed (Owner or agent having property owner's consent) Position Date 9-29-86

DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect. Date 9/29/86 Lic. Class Lic. Number Contractor.

OWNER-BUILDER DECLARATION

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).) I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.) I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.) I am exempt under Sec. B. & P. C. for the reason. Date 9/29/86 Owner's Signature.

WORKERS' COMPENSATION DECLARATION

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.). Policy No. Insurance Company. Certified copy is hereby furnished. Certified copy is filed with the Los Angeles City Dept. of Building and Safety. Date Applicant's Signature. Applicant's Mailing Address.

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California. Date 9/29/86 Applicant's Signature.

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.). Lender's Name Not Selected Lender's Address.

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91.0202 LAMC)

Signed (Owner or agent having property owner's consent) Position Date 9-29-86

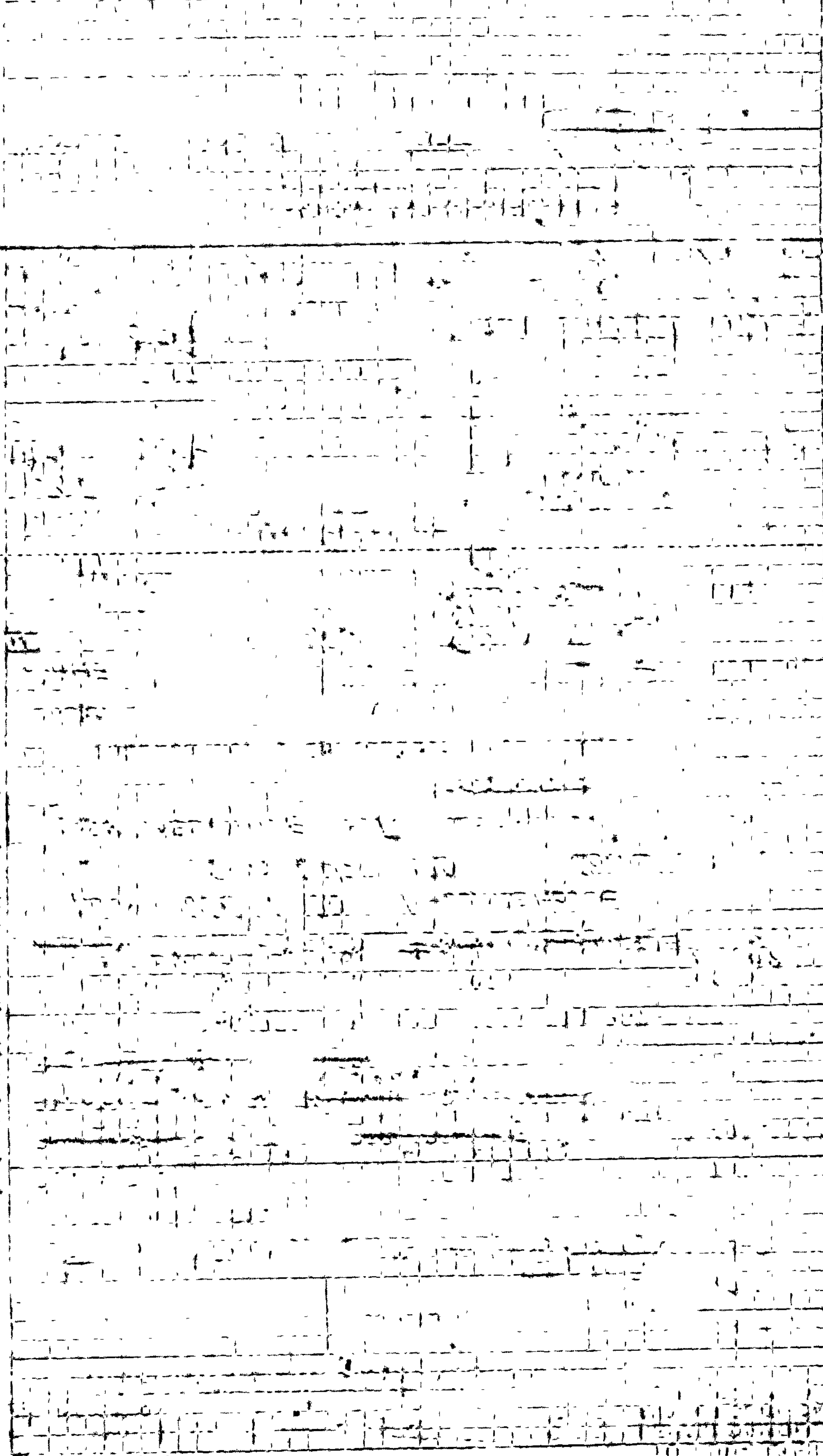
Table with columns for fees: PC, S.P.C, B.P, LF, O/S, DIST. OFFICE, P.C. NO. and corresponding amounts and dates. Includes a vertical stamp 'CASHIER'S USE ONLY' and a date stamp '2-10/02/86'.

~~1 2 3 0 0 3 0 0 4 2 8~~ Sworek 7/22/86

1 2 8 0 0 6 0 0 0 2 4

V. Hart 9-24-86

0.107 1.110 2.110 3.110 4.110 5.110 6.110 7.110 8.110 9.110



3

APPLICATION FOR INSPECTION

EARTHQUAKE SAFETY DIVISION 460Y

CITY OF LOS ANGELES DEPT. OF BUILDING AND SAFETY

TO ADD-ALTER REPAIR-DEMOLISH AND FOR CERTIFICATION OF OCCUPANCY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

Form with numbered items 1-15: LEGAL, PRESENT USE OF BUILDING, JOB ADDRESS, BETWEEN CROSS STREETS, OWNER'S NAME, OWNER'S ADDRESS, ENGINEER, ARCHITECT OR DESIGNER, ARCHITECT OR ENGINEER'S ADDRESS, CONTRACTOR, SIZE OF EXISTING BLDG., CONST. MATERIAL, JOB ADDRESS, VALUATION TO INCLUDE ALL FIXED EQUIPMENT, NEW WORK (Describe).

Form with numbered items 16-20: NEW USE OF BUILDING, TYPE, DWELL UNITS, GUEST ROOMS, PARKING REQD, CONT INSP, PLAN CHECKED, APPLICANT APPROVED, INSPECTION ACTIVITY, ZONED BY, FILE WITH, TYPIST, INSPECTOR.

CASHIER'S USE ONLY

Handwritten list of fees: 88.40, 104.00, 2.25, 3.89, 90832

DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION

I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect. Date 3-2-88 Lic. Class B Lic. Number 404469 Contractor James J. Green

OWNER-BUILDER DECLARATION

I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code)...

WORKERS' COMPENSATION DECLARATION

I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or certified copy thereof (Sec. 2800, Lab. C.). Policy No. 1-06328-1000 Insurance Company FAIRMOUNT INS. CO.

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

I certify that in the performance of the work for which this permit is issued, it shall not employ any person in any manner as to become subject to the Workers' Compensation Laws of California.

CONSTRUCTION LENDING AGENCY

I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued.

I certify that I have read this application and state that the above information is correct. I agree to comply with all city ordinance and state laws relating to building construction, and hereby authorize representatives of this city to inspect the above-mentioned property for inspection purposes.

Signature of James J. Green, Owner, dated 3-2-88.

Bureau of Engineering

13100

ADDRESS APPROVED 339

CITY/STATE

HIGHWAY REQUIRED

DEDICATION COMPLETED

FLOOD CLEARANCE

SEWERS

SEWERS AVAILABLE

NOT AVAILABLE

SFC PAID

SFC DUE

SFC NOT APPLICABLE

Grading

PRIVATE SEWAGE SYSTEM APPROVED

Conservation

APPROVED FOR ISSUE NO FILE FILE CLOSED

Fire

APPROVED (TITLE 19) (L.A.M.C.-5700)

Housing

HOUSING AUTHORITY APPROVAL

Planning

APPROVED UNDER CASE #

Traffic

APPROVED FOR

Construction Tax

RECEIPT NO.

DWELLING UNITS

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH

TS WFL

Mesquit St

↓
N

North wall
Parquet Repair

1/2" out of Plumb
Replace Parquet
damaged.

North wall Elev.

ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE

NOT TO SCALE
REVISIONS
10-11-1958

3

APPLICATION

CITY OF LOS ANGELES DEPT OF BUILDING AND SAFETY

TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

FOR INSPECTION 2 6 7 0 0 4 0 0 2 7 6

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 264	BLOCK -	TRACT WINGERTER TRACT MR 15-52	COUNCIL DISTRICT NO. 09	DIST MAP 123B217 CENSUS TRACT 2065
2. PRESENT USE OF BUILDING (08) Commercial	NEW USE OF BUILDING () SAME			ZONE M3-3	
3. JOB ADDRESS 670 Mesquit St. Los Angeles, CA 90021					FIRE DIST. -
4. BETWEEN CROSS STREETS AND					LOT TYPE -
5. OWNER'S NAME Rancho Cold Storage, Attn: Vince Gallo				PHONE 624-8862	LOT SIZE -
6. OWNER'S ADDRESS 670 Mesquit St. Los Angeles, CA 90021					ALLEY -
7. ENGINEER					BUS. LIC. NO. ACTIVE STATE LIC. NO. PHONE
8. ARCHITECT OR DESIGNER					BUS. LIC. NO. ACTIVE STATE LIC. NO. PHONE BLDG. LINE
9. ARCHITECT OR ENGINEER'S ADDRESS					CITY ZIP AFFIDAVITS 21 #1572
10. CONTRACTOR ALLIED ROOFING CO. 394067 (805) 252-3800					BUS LIC. NO. ACTIVE STATE LIC. NO. PHONE
11. SIZE OF EXISTING BLDG. WIDTH LENGTH		STORIES	HEIGHT	NO. OF EXISTING BUILDINGS ON LOT AND USE	
12. CONST. MATERIAL OF EXISTING BLDG.		EXT. WALLS	ROOF	FLOOR	
13. JOB ADDRESS 670 Mesquit St. Los Angeles, CA					STREET GUIDE DISTRICT OFFICE LA
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING \$ 9,000.00					SEISMIC STUDY ZONE
15. NEW WORK (Describe) Reroof- 3 Layer modified bitumen, 82sq. Smoke det', req'd. CLASS A					GRADING FLOOD HWY. DED. CONS.
NEW USE OF BUILDING Commercial			SIZE OF ADDITION none	STORIES	HEIGHT
TYPE nc	GROUP OCC. nc	FLOOR AREA	PLANS CHECKED		FILE WITH
DWELL UNITS nc	MAX OCC. nc	TOTAL	REGISTRATION APPROVED		TYPIST
GUEST ROOMS	PARKING REQ'D nc	PARKING PROVIDED STD. COMP.	INSPECTION ACTIVITY		INSPECTOR
P.C. -	G.P.I. -	CONT INSP	<p style="text-align: right;">B.S.B-3 (R.2/87)</p> <p style="text-align: center;"><i>Robert E. ...</i></p> <p style="text-align: center;">1-35 E1-8 1-35 H55 3661 0001</p> <p style="text-align: center;">W-781-3 12/18/89</p>		
S.P.C. -	P.M. -				
S.P. \$ 68.00	EI. \$ 1.35	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC			
IF -	FH. -				
S.D. -	O.S.S. \$ 1.38				
DIST OFFICE HO	S.O.S.S. -	SPRINKLERS REQ'D SPEC. NO			
P.C. NO. -	C/O -	ENERGY NO			

DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION

I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect. Date 9-18-89 Lic. Class C-39 Lic. Number 394067 Contractor [Signature]

OWNER-BUILDER DECLARATION

I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).):

- I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)
I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)
I am exempt under Sec. B. & P. C. for this reason

WORKERS' COMPENSATION DECLARATION

I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Dmg. 3800, Lab C) Policy No W-0007-736 Insurance Company Beaver Insurance CO.

Certified copy is hereby furnished
Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety
Date 9-18-89 Applicant's Signature [Signature]
Applicant's Mailing Address 18316 Sierra Hwy. #D Canyon Country, CA 91351

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California. Date Applicant's Signature

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.). Lender's Name Lender's Address

I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91.0202 LAMC)

Signed [Signature] President 09-18-89
(Owner or agent having property owner's consent) Position Date

Bureau of Engineering

2 6 7 0 0

ADDRESS APPROVED
DRIVEWAY 0 2 7 7

HIGHWAY REQUIRED
DEDICATION COMPLETED

FLOOD CLEARANCE

SEWERS

SEWERS AVAILABLE

NOT AVAILABLE

SFC PAID

SFC NOT APPLICABLE

SFC DUE

Grading

PRIVATE SEWAGE SYSTEM APPROVED

Conservation

APPROVED FOR ISSUE NO FILE FILE CLOSED

Fire

APPROVED (TITLE 19) (L.A.M.C.-S700)

Housing

HOUSING AUTHORITY APPROVAL

Planning

APPROVED UNDER CASE #

Traffic

APPROVED FOR

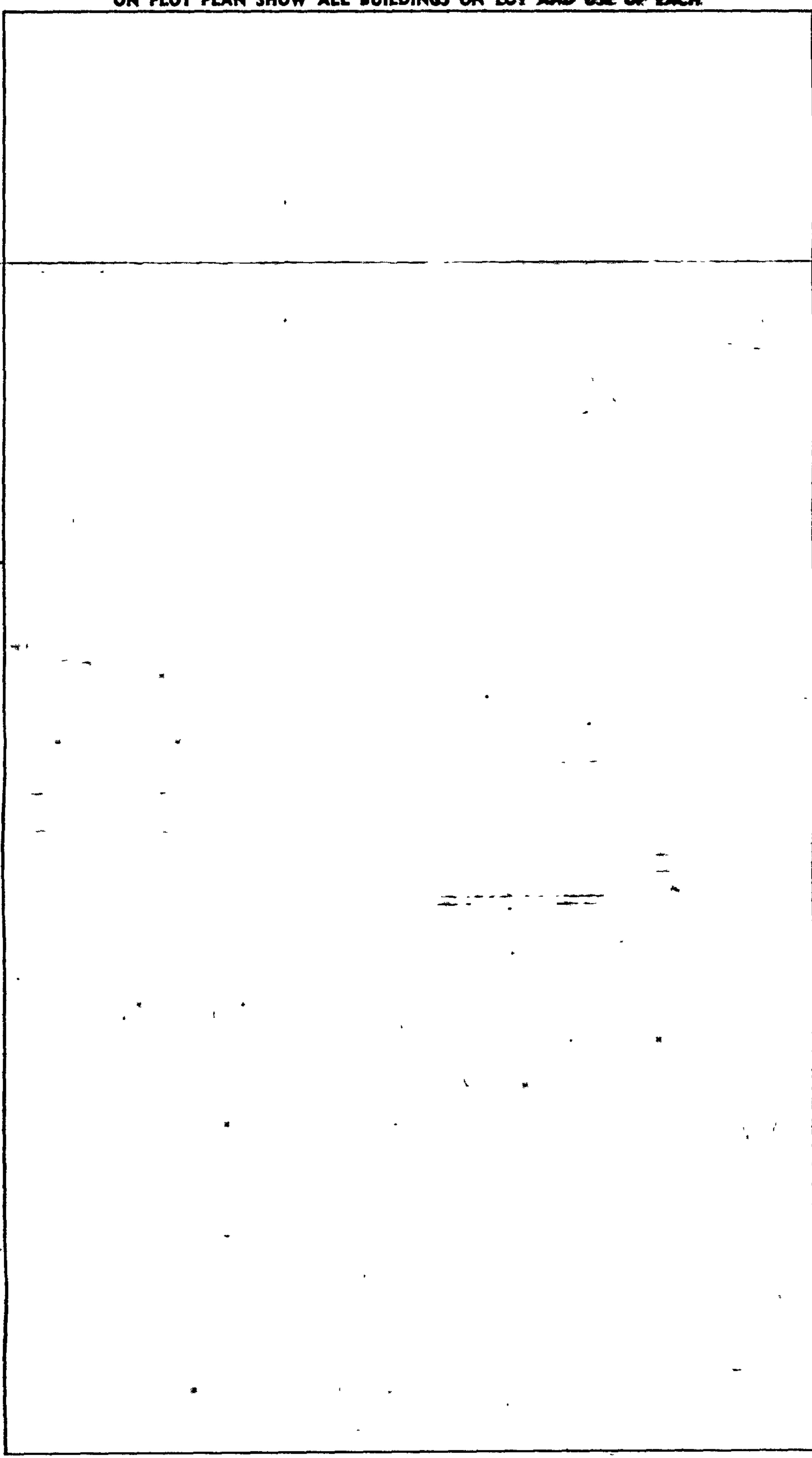
Construction Tax

RECEIPT NO.

DWELLING UNITS

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



↑ ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE

3

APPLICATION FOR INSPECTION

CITY OF LOS ANGELES DEPT. OF BUILDING AND SAFETY EARTHQUAKE SAFETY DIVISION 460Y

TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

Form with 15 numbered sections: 1. LOT, BLOCK, TRACT, COUNCIL DISTRICT NO., DIST. MAP; 2. PRESENT USE OF BUILDING, NEW USE OF BUILDING, ZONE; 3. JOB ADDRESS, FIRE DIST.; 4. BETWEEN CROSS STREETS AND, LOT TYPE, LOT SIZE; 5. OWNER'S NAME, PHONE, LOT SIZE; 6. OWNER'S ADDRESS, CITY, ZIP; 7. ENGINEER, BUS. LIC. NO., ACTIVE STATE LIC. NO., PHONE; 8. ARCHITECT OR DESIGNER, BUS. LIC. NO., ACTIVE STATE LIC. NO., PHONE; 9. ARCHITECT OR ENGINEER'S ADDRESS, CITY, ZIP; 10. CONTRACTOR, BUS. LIC. NO., ACTIVE STATE LIC. NO., PHONE; 11. SIZE OF EXISTING BLDG., STORIES, HEIGHT, NO. OF EXISTING BUILDINGS ON LOT AND USE; 12. CONST. MATERIAL, EXT. WALLS, ROOF, FLOOR; 13. JOB ADDRESS, STREET GUIDE, DISTRICT OFFICE; 14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING; 15. NEW WORK (Describe), GRADING, FLOOD, HWY. DED. COLL, CONS.

DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.

OWNER-BUILDER DECLARATION

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).)

I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)

I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)

I am exempt under Sec. B. & P. C. for this reason

WORKERS' COMPENSATION DECLARATION

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).

Policy No. Insurance Company Certified copy is hereby furnished. Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety. Date Applicant's Signature

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.). Lender's Name Lender's Address

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that the City of Los Angeles nor any board, department, officer or employee thereof make any warranty, express or implied, as to the performance of results of any work described herein or the condition of the property or structure to be constructed or improved (See Sec. 91.0202 LAMC)

Signed [Signature] Position Date

901.97

CASHIER'S USE ONLY

Handwritten notes and stamps: 466.65, 549.00, 303.60, 17.69, 225.42, 01/27/89, 901.97, 901.97

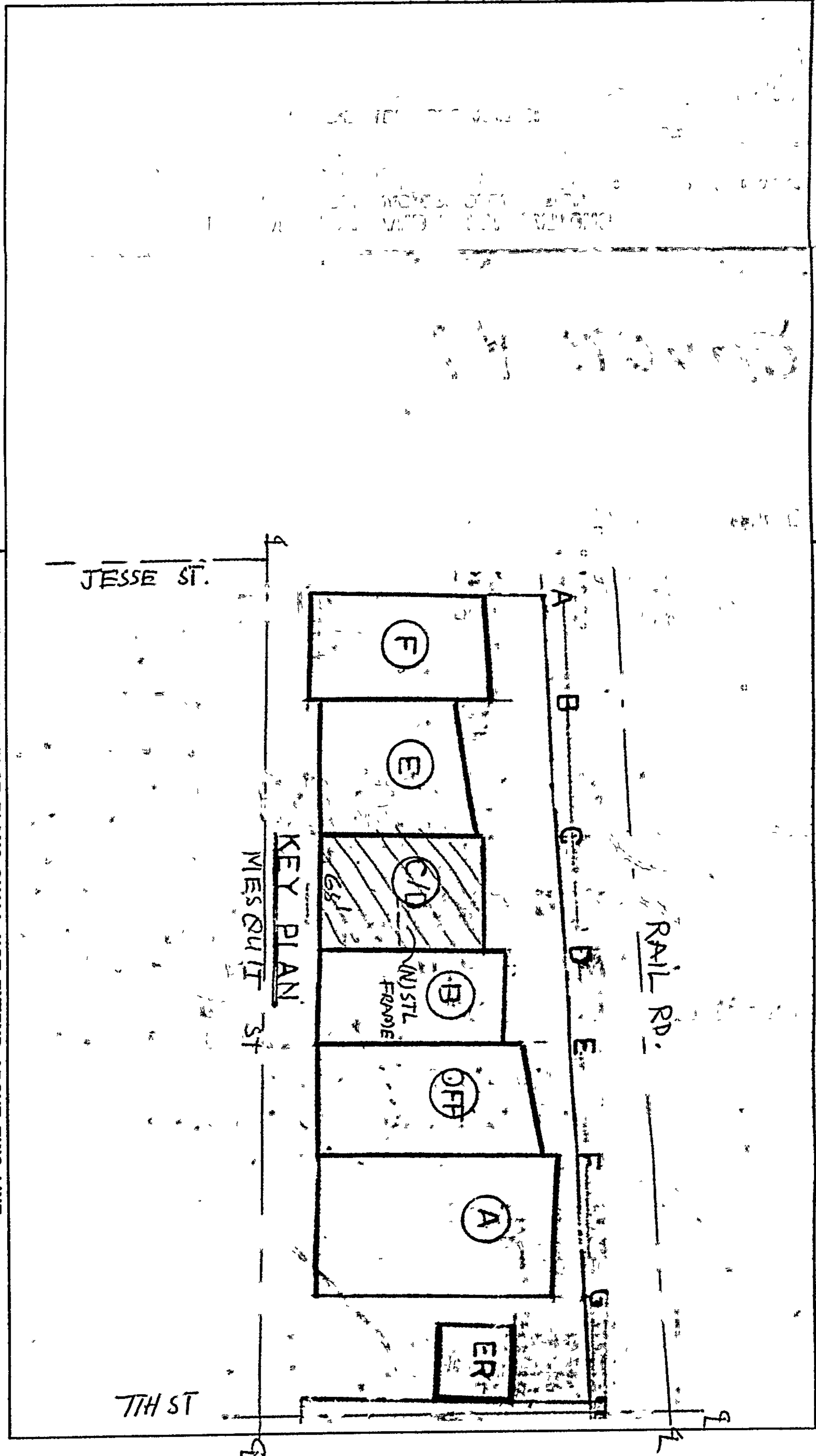
LA 22542



Bureau of Engineering	2 2 3 0 0 2	ADDRESS APPROVED		
		DRIVEWAY		
		HIGHWAY DEDICATION	REQUIRED	
			COMPLETED	
SEWERS		FLOOD CLEARANCE		
		SEWERS AVAILABLE		
		NOT AVAILABLE		
		SFC PAID		
		SFC DUE		
		SFC NOT APPLICABLE		
Grading	PRIVATE SEWAGE SYSTEM APPROVED			
Conservation	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>			
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)			
Housing	HOUSING AUTHORITY APPROVAL			
Planning	APPROVED UNDER CASE #			
Traffic	APPROVED FOR			
Construction Tax	RECEIPT NO.	DWELLING UNITS		

LEGAL DESCRIPTION
Time ext. filed and pending board action.

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



NO CONSTRUCTION SERVICE-DEPARTMENT

3

APPLICATION FOR INSPECTION

CITY OF LOS ANGELES DEPT. OF BUILDING AND SAFETY EARTHQUAKE SAFETY DIVISION 460Y

TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

Form with 15 numbered sections: 1. LOT, BLOCK, TRACT, COUNCIL DISTRICT NO., DIST. MAP; 2. PRESENT USE OF BUILDING, NEW USE OF BUILDING, ZONE; 3. JOB ADDRESS; 4. BETWEEN CROSS STREETS AND; 5. OWNER'S NAME, PHONE, LOT SIZE; 6. OWNER'S ADDRESS, CITY, ZIP, Irreg; 7. ENGINEER, BUS. LIC. NO., ACTIVE STATE LIC. NO., PHONE, ALLEY; 8. ARCHITECT OR DESIGNER, BUS. LIC. NO., ACTIVE STATE LIC. NO., PHONE, BLDG. LINE; 9. ARCHITECT OR ENGINEER'S ADDRESS, CITY, ZIP, AFFIDAVITS; 10. CONTRACTOR, BUS. LIC. NO., ACTIVE STATE LIC. NO., PHONE, Aff 51166; 11. SIZE OF EXISTING BLDG., STORIES, HEIGHT, NO. OF EXISTING BUILDINGS ON LOT AND USE, P.C. REQ'D; 12. CONST. MATERIAL OF EXISTING BLDG., EXT. WALLS, ROOF, FLOOR, No(e); 13. JOB ADDRESS, STREET GUIDE, DISTRICT OFFICE; 14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING, \$ 171,000.00, SEISMIC STUDY ZONE; 15. NEW WORK (Describe), FULL COMPLIANCE / RGA, GRADING, FLOOD, HWY. IMP., CONS.

DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).)

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

Handwritten signatures and stamps: '1066-90', 'LA 22543', 'CASHIER'S USE ONLY', 'NOTARY PUBLIC - CALIFORNIA LOS ANGELES COUNTY My Comm. Expires Oct 9, 1989', 'Signed (Owner or agent having property owner's consent)', 'Position', 'Date'.

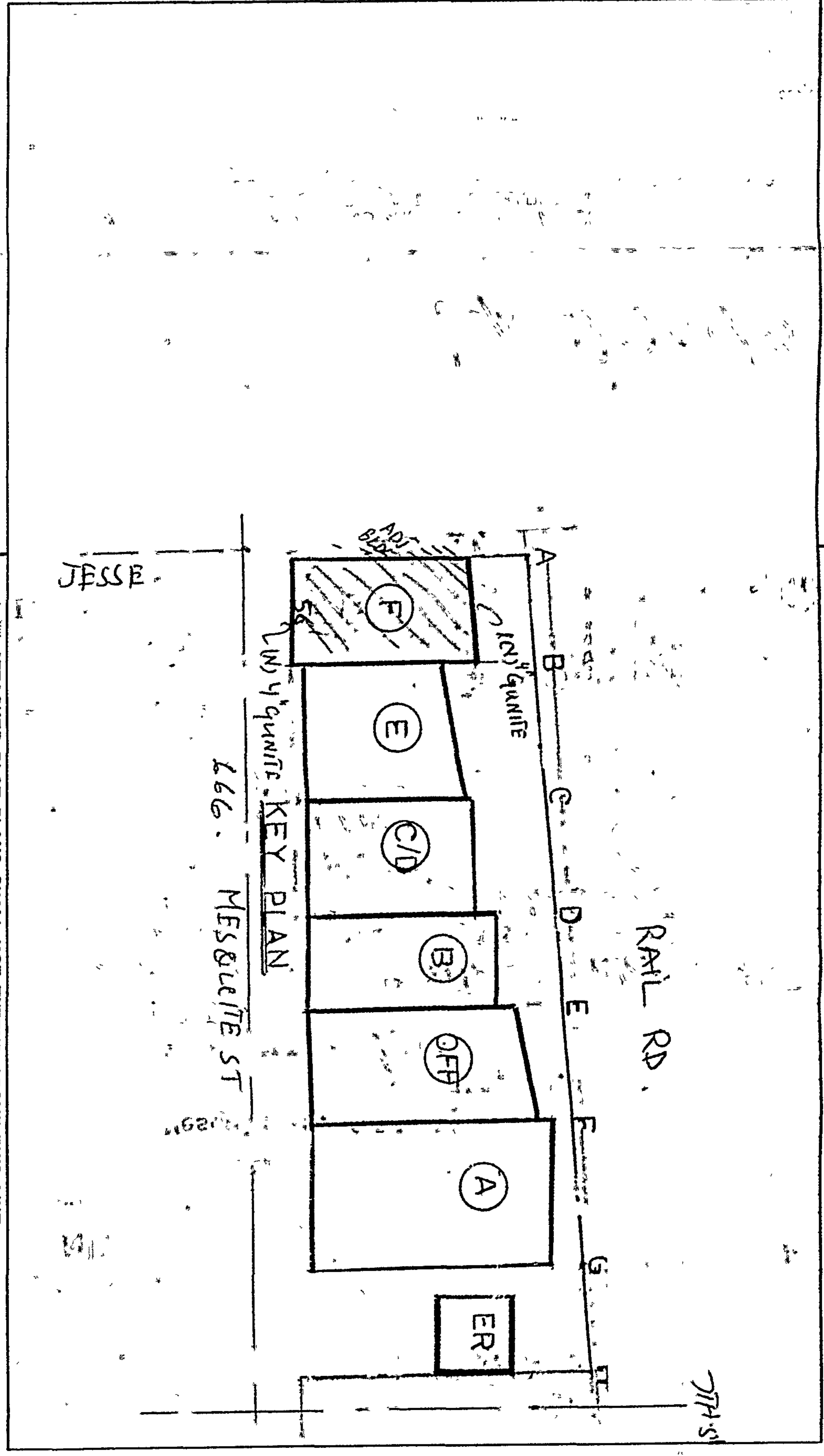
Bureau of Engineering # 23003

ADDRESS APPROVED	
DRIVEWAY	
HIGHWAY	REQUIRED
DEDICATION	COMPLETED
FLOOD CLEARANCE	
SEWERS AVAILABLE	
NOT AVAILABLE	
SFC PAID	
SFC DUE	
SFC NOT APPLICABLE	

Grading	PRIVATE SEWAGE SYSTEM APPROVED
Conservation	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)
Housing	HOUSING AUTHORITY APPROVAL
Planning	APPROVED UNDER CASE #
Traffic	APPROVED FOR
Construction Tax	RECEIPT NO. DWELLING UNITS

LEGAL DESCRIPTION
Time extension filed and under pending board action

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE

OFFICE OF THE COUNTY CLERK
JENNIFER BROWN
1000 VAN NESS

3

APPLICATION FOR INSPECTION

CITY OF LOS ANGELES DEPT OF BUILDING AND SAFETY

TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

Earthquake Safety Division

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 271, 272 273, 274	BLOCK	TRACT WINGERTER MR 15-52	COUNTY REF. NO.	DIST. MAP 123-217 CENSUS TRACT 2065
2. PRESENT USE OF BUILDING	() COLD STORAGE/OFFICE	NEW USE OF BUILDING	() SAME	ZONE	M3-3
3. JOB ADDRESS	678-682 MESQUITE ST (OFFICE BLDG) & BLDG A			FIRE DIST.	TWO
4. BETWEEN CROSS STREETS	7TH STREET	AND	JESSE	COUN. DIST.	9
5. OWNER'S NAME	DON GALLO			LOT TYPE	INT
6. OWNER'S ADDRESS	670 MESQUITE STREET LOS ANGELES			LOT SIZE	IRREG
7. ENGINEER	MERDAD HOURIANI	BUS. LIC. NO.	C040786	PHONE	213-624-8861
8. ARCHITECT OR DESIGNER		BUS. LIC. NO.		PHONE	213-388-0998
9. ARCHITECT OR ENGINEER'S ADDRESS	714 SO. PLYMOUTH BL. LOS ANGELES			ALLEY	/
10. CONTRACTOR	N/S			BLDG. LINE	/
11. SIZE OF EXISTING BLDG.	WIDTH 164' LENGTH 150	STORIES 14+8	HEIGHT 25	NO. OF EXISTING BUILDINGS ON LOT AND USE	ONE/COLD STORAGE
12. FRAMING MATERIAL OF EXISTING BLDG.	URM/MASONRY	EXT. WALLS	WOOD	FLOOR	CONC
13. JOB ADDRESS	682 MESQUITE STREET			STREET GUIDE	
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 280,000			DIST. OFF. L.A.	P.C. REQ'D NO
15. NEW WORK (Describe)	FULL COMPLIANCE DIVISION 88			GRADING	SEISMIC
	RCIII - B			HWY. DED. COLL.	FLOOR
NEW USE OF BUILDING	STORAGE/OFFICE N/C	SIZE OF ADDITION	N/C	STORIES	HEIGHT
TYPE	III N	GROUP OCC.	B-2	FLOOR AREA	N/C
DWELL UNITS	N/A	MAX OCC.	N/C	TOTAL	
GUEST ROOMS	N/A	PARKING REQ'D	N/C	PARKING PROVIDED	STD. N/C COMP.
P.C. NO.	768.40	G.P.I. + NP		CONT. INSP.	GROUT AMC TORQUE TEST
S.P.C.		P.M.	18.08	CLAIMS FOR REFUND OF FEES PAID PERMITS MUST BE FILED: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.	
B.P.	904.00	E.I.	42.00	SPRINKLERS REQ'D SPEC.	
I.F.		F.H.	616.00	ENERGY	DAS
S.D.		O.S.S.	46.97		
ISS. OFF.	H0	S.O.S.S.			
P.C. NO.	E7410	C/O			

CASHIER'S USE ONLY	
E.O. PLAN CHEC	768.40
E.I. PERMITS	904.00
PLAN MAINTENAN	18.08
EI COMMERCIAL	42.00
FIRE HYDRANT I	616.00
ONE STOP	46.97
TOTAL	2,395.45
CHECK	2,395.45

2395.45

90HD 04582

DECLARATIONS AND CERTIFICATIONS

LICENSED CONTRACTORS DECLARATION

I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.

OWNER-BUILDER DECLARATION

I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).):

I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)

I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)

I am exempt under Sec. _____, B. & P. C. for this reason _____

WORKERS' COMPENSATION DECLARATION

I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3900, Lab. C.).

Policy No. _____ Insurance Company _____

Certified copy is hereby furnished. / Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.

Date _____ Applicant's Signature _____

Applicant's Mailing Address _____

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.

Date _____ Applicant's Signature _____

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).

Lender's Name _____ Lender's Address _____

I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I understand that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91.0205 LAMC)

Signed _____ Position _____ Date 2/21/90

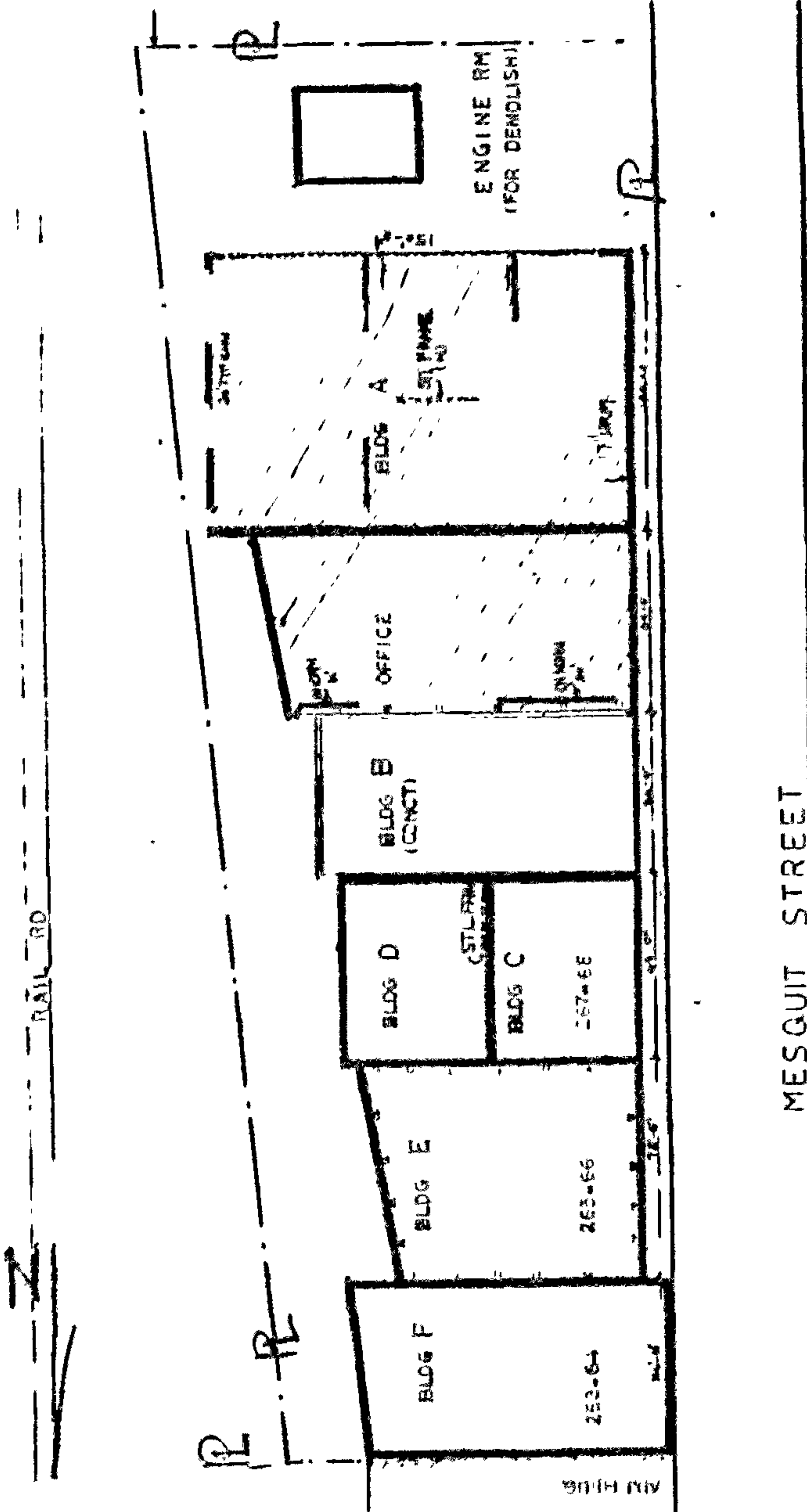
(Owner or agent having property owner's consent) _____

Bureau of Engineering	ADDRESS APPROVED		
	DRIVEWAY		
	HIGHWAY DEDICATION	REQUIRED	
		COMPLETED	
SEWERS RES. NO. CERT. NO.	FLOOD CLEARANCE		
		SEWERS AVAILABLE	
		NOT AVAILABLE	
		SFC PAID	
	SFC NOT APPLICABLE	SFC DUE	
Grading	PRIVATE SEWAGE SYSTEM APPROVED		
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
Fire	APPROVED (TITLE 19) (L.A.M.C.-5700)		
Housing	HOUSING AUTHORITY APPROVAL		
Planning	APPROVED UNDER CASE #		
Transportation	APPROVED FOR		
Construction Tax	RECEIPT NO.	DWELLING UNITS	

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH

ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE



7 3 0 0 6 3 0 4 2 3

1. JOB ADDRESS 678-82 MESQUITE ST (OFF. 5A)

2. Have you the Hazardous Material Information Guide and instructions by REGULATIONS under Section 2003, 2003 and 2004 of the Health and Safety Code and Section 2703 of the CMC regarding hazardous materials.

3. Have also you the Information Guide regarding Air Pollution Hazards and instructions by REGULATIONS under Section 4003 of the Health and Safety Code.

4. Will the applicant or owner building complete handle a hazardous or an acutely hazardous material or a mixture containing a hazardous or acutely hazardous material equal to or greater than the threshold quantities specified in the Hazardous Materials Information Guide?

.. YES NO NO
[Signature]
CHIEF OF POLICE, JEFFERSON

5. Is the proposed work within 1000 feet from the outer boundary of a school? (Fire Department clearance is required prior to issuance of the permit if answer yes).

N/A YES NO NO
CHIEF OF POLICE, JEFFERSON

6. Will the intended use of the building by the applicant or owner building complete require a permit for construction or maintenance from the Air Quality Management District?

YES NO NO
[Signature]
CHIEF OF POLICE, JEFFERSON

4587

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 267-266	BLOCK -	TRACT WINGESTER MR 15-52	COUNTY REF. NO. MR 15-52	DIST. MAP 123B217	CENSUS TRACT 206500
2. PRESENT USE OF BUILDING	() COLD STORAGE			NEW USE OF BUILDING () SAME	ZONE M3-1	
3. JOB ADDRESS	670 MESQUIT STREET (BLDG E)				FIRE DIST. TWO	COUN. DIST. 9
4. BETWEEN CROSS STREETS	8TH		AND JESSE		LOT TYPE IRREG INT	
5. OWNER'S NAME	DON GALLO			PHONE	LOT SIZE	
6. OWNER'S ADDRESS	670 MESQUIT STREET			CITY LOS ANGELES	ZIP 90010	IRREG
7. ENGINEER	MERDAD HOURIANI		BUS. LIC. NO. C040786	ACTIVE STATE LIC. NO. 213-388-0998	PHONE	ALLEY
8. ARCHITECT OR DESIGNER			BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	BLDG. LINE
9. ARCHITECT OR ENGINEER'S ADDRESS	CITY			ZIP		
10. CONTRACTOR	N/S			BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE
11. SIZE OF EXISTING BLDG.	WIDTH 56	LENGTH 117	STORIES 4	HEIGHT 51	NO. OF EXISTING BUILDINGS ON LOT AND USE 1	
12. FRAMING MATERIAL OF EXISTING BLDG.	URM		EXT. WALLS	ROOF WOOD	FLOOR	
13. JOB ADDRESS	670 MESQUIT STREET				STREET GUIDE	
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$288,000				DIST. OFF. LA	P.C. REQ'D None
15. NEW WORK (Describe)	FULL COMPLIANCE DIVISION 88				GRADING	SEISMIC
	RC III-B				HWY. DED. YES	FLOOD
NEW USE OF BUILDING	STORAGE N/C		SIZE OF ADDITION N/C	STORIES N/C	HEIGHT	
TYPE III N	GROUP OCC. B-2	FLOOR AREA N/C	TOTAL	PLANS CHECKED SYED ALI	ZONED BY SYED ALI	
DWELL UNITS N/A	MAX OCC. N/C			APPLICATION APPROVED Syed Ali	TYPYST	
GUEST ROOMS N/A	PARKING REQ'D N/C	PARKING PROVIDED STD. N/C COMP.		INSPECTION ACTIVITY		INSPECTOR
				CS	GEN.	MAJ.S. EQ
PC 785.40	G.P.I. + N/A	CONT. INSP. GROUT. INC. TORQUE TEST		B & S B-3 (R.7/80)		
B.P.C.	P.M. 18.48			02/28/90 04:01:30PM H001 T-1101 C 10		
B.P. 924.00	E.I. 43.20	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.		E.Q. PLAN CHECK 785.40		
I.F.	F.V. 633.60			ONE STOP 15.71		
S.D.	O.S.S. 45.71			TOTAL 801.11		
ISS. OFF. H0	S.O.S.S. 32.39	SPRINKLERS REQ'D SPEC.		CHECK 801.11		
P.C. NO. E7453	C/O	ENERGY		DAS		
<p>Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced.</p> <p>801.11 1651.67</p>						

DECLARATIONS AND CERTIFICATIONS
 LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
 Date _____ Lic. Class _____ Lic. Number _____ Contractor _____ (Signature)

OWNER-BUILDER DECLARATION

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).):
 I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)
 I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)
 I am exempt under Sec. _____, B. & P. C. for this reason _____
 Date 2/21/90 Owner's Signature [Signature]

WORKERS' COMPENSATION DECLARATION

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
 Policy No. _____ Insurance Company _____
 Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.
 Date _____ Applicant's Signature _____

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.
 Date 2/21/90 Applicant's Signature [Signature]

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).
 Lender's Name _____ Lender's Address _____

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 10202 LAMC)

Signed [Signature] (Owner or agent having property owner's consent) Position C.D.L. 10815310 Date 2/21/90

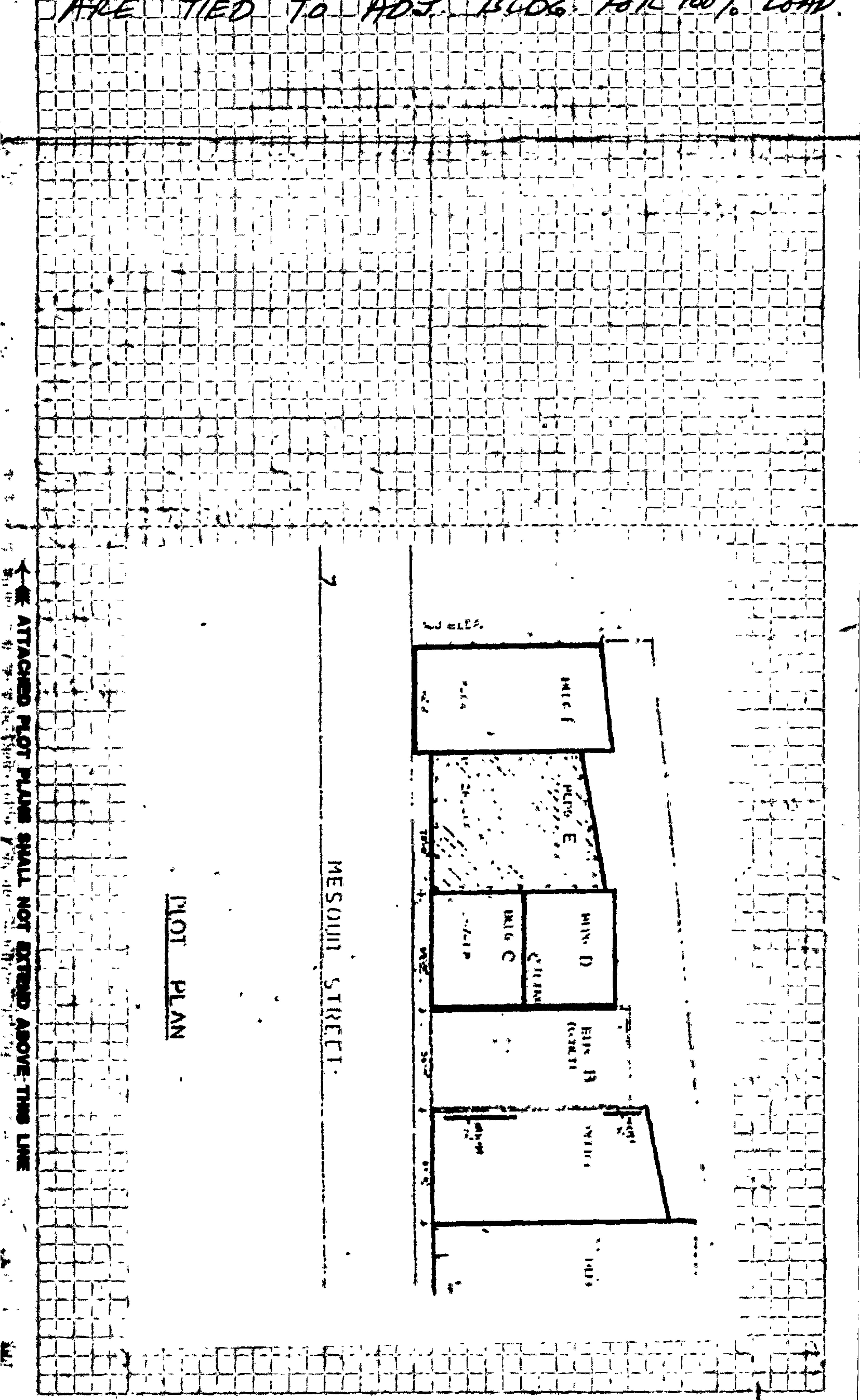
27600100118

Bureau of Engineering	ADDRESS APPROVED		
	DRIVEWAY		
	HIGHWAY DEDICATION	REQUIRED	
		COMPLETED	
SEWER RES. NO. CERT. NO.	FLOOD CLEARANCE		
	SEWERS AVAILABLE		
	NOT AVAILABLE		
	SFC PAID		
	SFC NOT APPLICABLE	SFC DUE	
Grading	PRIVATE SEWAGE SYSTEM APPROVED		
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
Fire	APPROVED (TITLE 19) (L.A.M.C.-5700)		
Housing	HOUSING AUTHORITY APPROVAL		
Planning	APPROVED UNDER CASE #		
Transportation	APPROVED FOR		
Construction Tax	RECEIPT NO.	DWELLING UNITS	

LEGAL DESCRIPTION
 AFF 90-370623-26 FILED FOR AGREEMENT
 TO PROVIDE TENSION ANCHORS AT N & S
 WALLS OF BLDG. IN CASE OF ADJ. BLDG
 DEMOLITION. AT THIS TIME COMMON WALLS

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH

ARE TIED TO ADJ. BLDG. FOR 100% LOAD.



ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE

27600 | 10113

THE ADDRESS: 670 MESQUITE ST Bldg F

I have read the National Medical Information Guide and understand its requirements under Section 2001, 2002 and 2003 of the Health and Safety Code and Section 2701 of the CFC regarding information materials.

I have also read the Information Guide regarding Air Pollution Records and understand its requirements under Section 4001 of the Health and Safety Code.

Will the applicant or other building owners make a maximum of an annual inspection report of a system containing a maximum of annual inspection report equal to or greater than the annual inspection required in the National Medical Information Guide?

.. YES

[Signature]
DATE OF APPL. 12/12/78

Is the proposed work within 1000 feet from the outer boundary of a hospital (This department's definition is required prior to issuance of the permit to proceed)?

MA

YES

[Signature]
DATE OF APPL. 12/12/78

Will the proposed use of the building by the applicant or other building owners require a permit for construction or maintenance from the Air Quality Department?

YES

[Signature]
DATE OF APPL. 12/12/78

4864

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 275	BLOCK	TRACT Wingerter Tract	COUNTY REF. NO. MR 15-52	DIST. MAP 123 B 217
2. PRESENT USE OF BUILDING (22) Storage	NEW USE OF BUILDING () Demo			ZONE M3-1	
3. JOB ADDRESS 684 Mesquit Street	FIRE DIST. two			COUN. DIST. 9	
4. BETWEEN CROSS STREETS 7th Street AND Jesse Street	LOT TYPE Int			LOT SIZE	
5. OWNER'S NAME Don Gallo	PHONE			Irreg	
6. OWNER'S ADDRESS 670 Mesquit St	CITY LA	ZIP 9021	ALLEY		
7. ENGINEER Ining LU	BUS. LIC. NO.	ACTIVE STATE LIC. NO. 3042	PHONE (818) 795-1250	BLDG. LINE	
8. ARCHITECT OR DESIGNER	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	AFFIDAVITS CCPD	
9. ARCHITECT OR ENGINEER'S ADDRESS 2500 E. Foothill	CITY Pasadena	ZIP 91107	AFF 48394		
10. CONTRACTOR NATIONAL LAND CLEARING	BUS. LIC. NO. 246508	ACTIVE STATE LIC. NO. 8052526131	PHONE	Ord. 164855	
11. SIZE OF EXISTING BLDG. WIDTH 35' LENGTH 55'	STORIES 1	HEIGHT 22'7"	NO. OF EXISTING BUILDINGS ON LOT AND USE 1 WAREHOUSE	AFF 51166	
12. FRAMING MATERIAL OF EXISTING BLDG. →	EXT. WALLS URM	ROOF Wood	FLOOR Wood	ZI 1572	
13. JOB ADDRESS 684 Mesquit St.	STREET GUIDE			DIST. OFF. LA	
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING \$ 7,700.00	P.C. REQ'D NO (F)			GRADING ---	
15. NEW WORK (Describe) Demolition BY HAND WRECKING	SEISMIC ---			HWY. DED. Yes	
NEW USE OF BUILDING DEMO	SIZE OF ADDITION	STORIES	HEIGHT	FILE WITH	
DWELL UNITS NE	GROUP OCC. BT	FLOOR AREA -1925	TOTAL	ZONED BY S. Ayers	
GUEST ROOMS NC	PARKING REQ'D	PARKING PROVIDED STD. COMP.	INSPECTION ACTIVITY	TYPIST SM	
NC	G.P.I. + NP	CONT. INSP.	OB	INSPECTOR	
S.P.C.	P.M.		GEN.	B & B-3 (R.7/89)	
B.P.	E.I.	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.	MAJ. B.	CASHIER'S USE ONLY	
S.D.	O.S.S.		EQ.	04/03/90 02:52:00PM H001 T-4168 C 10	
ISS. OFF. HO	S.O.S.S.	SPRINKLERS REQ'D SPEC. NC	DEMO PERMITS 60.20		
P.C. NO. 5128	C/O NC	ENERGY	PLAN MAINTENAN 6.00		
		DAS	EI COMMERCIAL 1.16		
			ONE STOP 1.35		
			TOTAL 68.71		
			CHECK 68.71		

DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION

I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
 Date 3/3/90 Lic. Class C-21 Lic. Number 246508 Contractor Kenneth Halson (Signature)

OWNER-BUILDER DECLARATION

I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code): Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).
 I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)
 I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)
 I am exempt under Sec. _____, B. & P. C. for this reason.
 Date _____ Owner's Signature _____

WORKERS' COMPENSATION DECLARATION

I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
 Policy No. 1152539-89 Insurance Company State Fund
 Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.
 Date _____ Applicant's Signature Kenneth Halson
 Applicant's Mailing Address _____

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.
 Date _____ Applicant's Signature _____

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).
 Lender's Name _____ Lender's Address _____

I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.
 I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the City of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91.0202 LAMC)

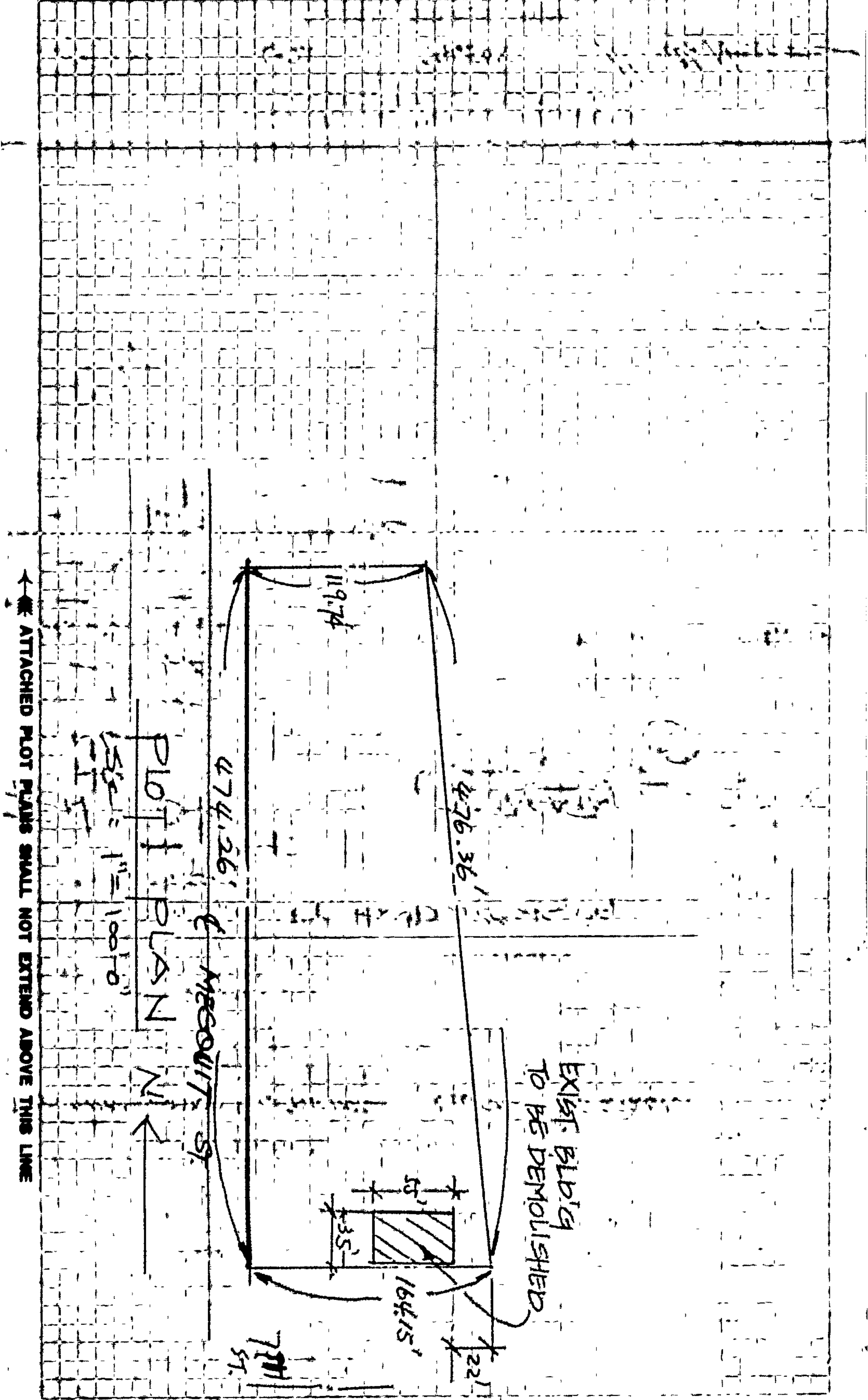
Signed Kenneth Halson (Owner or agent having property owner's consent) Position Date 3/30/90

27900500214

Bureau of Engineering	ADDRESS APPROVED		
	DRIVEWAY		
	HIGHWAY DEDICATION	REQUIRED	
		COMPLETED	
	FLOOD CLEARANCE		
SEWERS RES. NO. 123 217 A	SEWERS AVAILABLE		
CERT NO. No permit for sewer connection	NOT AVAILABLE		
Grading	SFC NOT APPLICABLE	SFC PAID	
		SFC DUE	
Comm. Safety	PRIVATE SEWAGE SYSTEM APPROVED		
Fire	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
Housing	APPROVED (TITLE 19) (L.A.M.C.-5700)		
Planning	HOUSING AUTHORITY APPROVAL		
Transportation	APPROVED UNDER CASE #		
Construction Tax	RECEIPT NO.	DWELLING UNITS	

LEGAL DESCRIPTION
 Lot 14 # 5871 verified by phone 4/3/90 by
 ok to issue 4/3/90 SMH

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



FOR INSPECTION 100950016.6.

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

1. LEGAL DESCR.	LOT 266-279	BLOCK -	TRACT WINGERTER TRACT	COUNTY REF. NO. MR 14-52	DIST. MAP 123B217
2. PURPOSE OF BUILDING	COLD STORAGE				ZONE M3-1
3. JOB ADDRESS	684 MESQUIT STREET				FIRE DIST. 09
4. BETWEEN CROSS STREETS	JESSE & 7TH STREET				LOT TYPE INT.
5. OWNER'S NAME	DON GALLO				LOT SIZE IRREG.
6. OWNER'S ADDRESS	670 MESQUIT STREET LOS ANGELES CA 90021				ALLEY -
7. ENGINEER	NING LU SE 3043 818-795-1250				BLDG. LINE -
8. ARCHITECT OR DESIGNER	FRANK GALLO LOS ANGELES, CA 90021				AFFIDAVIT ZI 223,157
9. ARCHITECT OR ENGINEER'S ADDRESS	N/S				ORD 164,855
10. CONTRACTOR	N/S				AFF 51166
11. SIZE OF NEW BLDG.	WIDTH 35	LENGTH 55	STORIES 1	HEIGHT 30'	NO. OF EXISTING BUILDINGS ON LOT AND USE COLD STORAGE W/OFF.
12. MATERIAL OF CONSTRUCTION	EXT. WALLS CONC BLK		ROOF WOOD	FLOOR CONC	CCPD
13. JOB ADDRESS	684 MESQUIT STREET				DIST. OFF. LA
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 100,000.00				P.C. REQ'D. YES

New cold storage

PURPOSE OF BUILDING	COLD STORAGE		STORIES 1	HEIGHT 30'	FILE WITH
TYPE	GROUP OCC. B-4	FLOOR AREA 1925	PLANS CHECKED		ZONED
DWELL UNITS	MAX. OCC.	TOTAL	APPLICATION APPROVED		MILLER
GUEST ROOMS	PARKING REQ'D.	PARKING PROVIDED	INSPECTION ACTIVITY		INSPECTOR

P.C. 463.08	G.P.I. -	CONT. INSP.	CASHIER'S USE ONLY
SPC 28.94	P.M.		
BP. 510.75	EI/S. 00	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.	
I.F.	O.S.S. 9.26		
SD.	S.O.S.S. 15.49		
ISSUING OFFICE LA	C/D	SPRINKLERS -	
P.C. NO. AA 8327	F.H. 220.00	ENERGY -	
		DAS YES	
Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced.			

03-2131
 04/10/90 03:59:05PM LA03 T-2131 C 07
 BUILD PLAN CHE 28.94
 BLD PER CONNER 510.75
 EI COMMERCIAL 15.00
 FIRE HYDRANT I 220.00
 ONE STOP 15.49
 TOTAL 790.18
 CHECK 790.18
 90LA 54366
 90LA 54366

DECLARATIONS AND CERTIFICATIONS
 LICENSED CONTRACTORS DECLARATION

15. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
 Date _____ Lic. Class _____ Lic. No. _____ Contractor's Signature _____
 Contractor's Mailing Address _____

OWNER-BUILDER DECLARATION

16. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).):
 I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.).
 I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law).
 I am exempt under Sec. _____, B. & P. C. for this reason _____
 Date 4/10/90 Owner's Signature _____

WORKERS' COMPENSATION DECLARATION

17. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
 Policy No. _____ Insurance Company _____
 Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.
 Date _____ Applicant's Signature _____
 Applicant's Mailing Address _____

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

18. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.
 Date 4/10/90 Applicant's Signature _____
 NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

19. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).
 Lender's Name _____
 Lender's Address _____

20. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.
 I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91.0202 LAMC)

Signed _____ Position _____ Date 4/10/90
 (Owner or agent having property owner's consent)

2 8 0 0 0 5 0 0 1 6 7

Bureau of Engineering RES. NO. CERT. NO.	Dedication of a 7 foot slong Mesquit. Improvements to be determined C of O contingent on year completion of dedication 3-8-90	ADDRESS APPROVED	
		DRIVEWAY	
		HIGHWAY	<input checked="" type="checkbox"/> REQUIRED
		DEDICATION	<input checked="" type="checkbox"/> COMPLETED
SEWERS NO INCREASE DEMO OLD		FLOOD CLEARANCE	
<input checked="" type="checkbox"/> SFC NOT APPLICABLE		<input checked="" type="checkbox"/> SEWERS AVAILABLE	
<input checked="" type="checkbox"/> SFC PAID		<input checked="" type="checkbox"/> NOT AVAILABLE	
<input checked="" type="checkbox"/> SFC DUE		Eleva 11-17-89	
Grading PRIVATE SEWAGE SYSTEM APPROVED		SFC PAID	
Comm. Safety APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		SFC DUE	
Fire APPROVED (TITLE 19) (L.A.M.C.-S700)		RECEIPT NO.	
Housing HOUSING AUTHORITY APPROVAL		DOWELLING UNITS	
Planning <input checked="" type="checkbox"/> APPROVED UNDER CASE # W590-1217		2I 1572 w/s (Proposed) - PC-3a (Ord. 164855)	
Transportation APPROVED FOR		OK TO ISSUE PERMIT 4-10-90	
Construction Tax		BY X. H. W.	

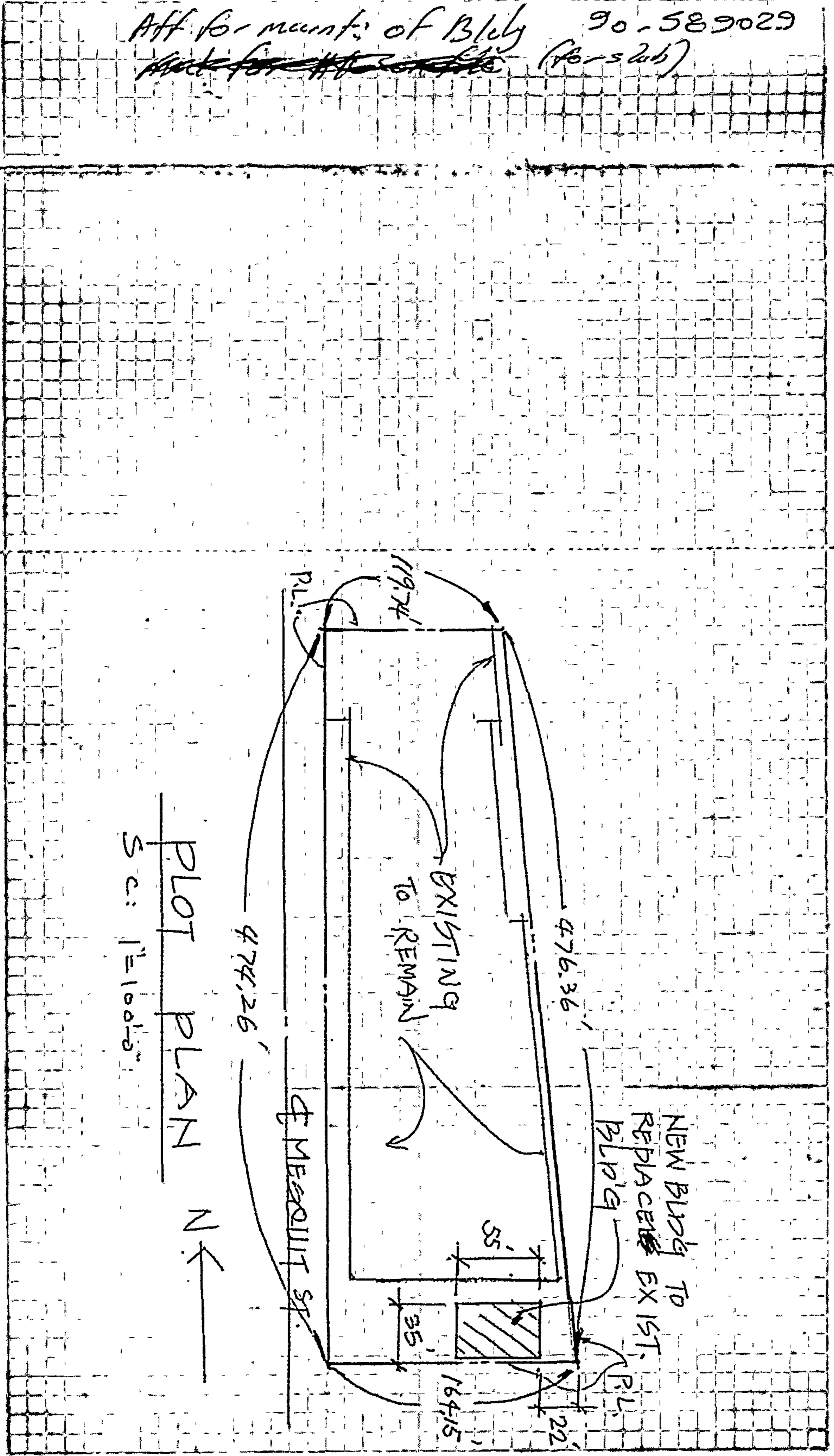
BLDG. INSP. (C OF O): FD AQMD BY X. H. W.

FIRE (HAZ. MAT'L S, AB 3203): Disabled Access exemption DD 900247

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH

Att for maint: of Bldg 90-589029
~~Att for maint: of Bldg~~ (for slab)

↑ ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE



INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

1. LEGAL DESCR.	LOT 276	BLOCK	TRACT Wingerter	COUNTY REF. NO. MR15-52	DIST. MAP 123B217 CENSUS TRACT 2065
2. PURPOSE OF BUILDING	(.) storage of trucks for truck driving school				ZONE M3-1
3. JOB ADDRESS	694 Mesquit St				FIRE DIST. 2 COUN. DIST. 9
4. BETWEEN CROSS STREETS	7th St		AND Jesse St		LOT TYPE corner
5. OWNER'S NAME	Freire Edison		213-887-7159		LOT SIZE irreg
6. OWNER'S ADDRESS	433 Markland Drive		CITY Monterey Park	ZIP 91754	
7. ENGINEER	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	ALLEY	
8. ARCHITECT OR DESIGNER	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	BLDG. LINE	
9. ARCHITECT OR ENGINEER'S ADDRESS	CITY		ZIP		
10. CONTRACTOR	OWNER		BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE
11. SIZE OF NEW BLDG.	WIDTH	LENGTH	STORIES	HEIGHT	NO. OF EXISTING BUILDINGS ON LOT AND USE
12. MATERIAL OF CONSTRUCTION	EXT. WALLS		ROOF	FLOOR	
13. JOB ADDRESS	694 Mesquit St				STREET GUIDE
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$				DIST. OFF. LA P.C. REQ'D NO(A)
STORAGE OF TRUCKS FOR TRUCK DRIVING SCHOOL / NO BACKING UP ON STREET					GRADING HWY. DED. YES FLOOD
PURPOSE OF BUILDING USE OF LAND - STORAGE OF TRUCKS					FILE WITH
TYPE	GROUP OCC.	FLOOR AREA	PLANS CHECKED		ZONED BY
DWELL UNITS	MAX. OCC.	TOTAL	APPROVED		TYPIST
GUEST ROOMS	PARKING REQ'D.	PARKING PROVIDED	INSPECTION ACTIVITY		INSPECTOR
P.C.	G.P.I.	CONT. INSP.	B & SB-1 (R.9/86) 07/02/90 02:40:25PM LA04 T-9643 C 16 BLD PER COMMER 10.00 TOTAL 10.00 CASH 20.00 CHANGE 10.00 90LA 59043		
S.P.C.	P.M.				
B.P. 70.00	E.I.	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.			
I.F.	O.S.S.				
S.D.	S.O.S.S.				
ISSUING OFFICE LA	C/O	SPRINKLERS			
P.C. NO.	F.H.	ENERGY NO DAS NO			
Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced.					

DECLARATIONS AND CERTIFICATIONS

LICENSED CONTRACTORS DECLARATION

15. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
 Date _____ Lic. Class _____ Lic. No. _____ Contractor's Signature _____
 Contractor's Mailing Address _____

OWNER-BUILDER DECLARATION

16. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).):
 I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.).
 I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.).
 I am exempt under Sec. _____, B. & P. C. for this reason _____
 Date 7-2-90 Owner's Signature _____

WORKERS' COMPENSATION DECLARATION

17. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
 Policy No. _____ Insurance Company _____
 Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.
 Date _____ Applicant's Signature _____
 Applicant's Mailing Address _____

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

18. I certify that in the performance of the work for which this permit is issued, I will not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.
 Date 7-2-90 Applicant's Signature _____
 NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

19. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).
 Lender's Name _____
 Lender's Address _____

20. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.
 I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit a violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91.0202 LAMC.)

Signed _____ Position _____ Date 7-2-90
 (Owner or agent having property owner's consent)

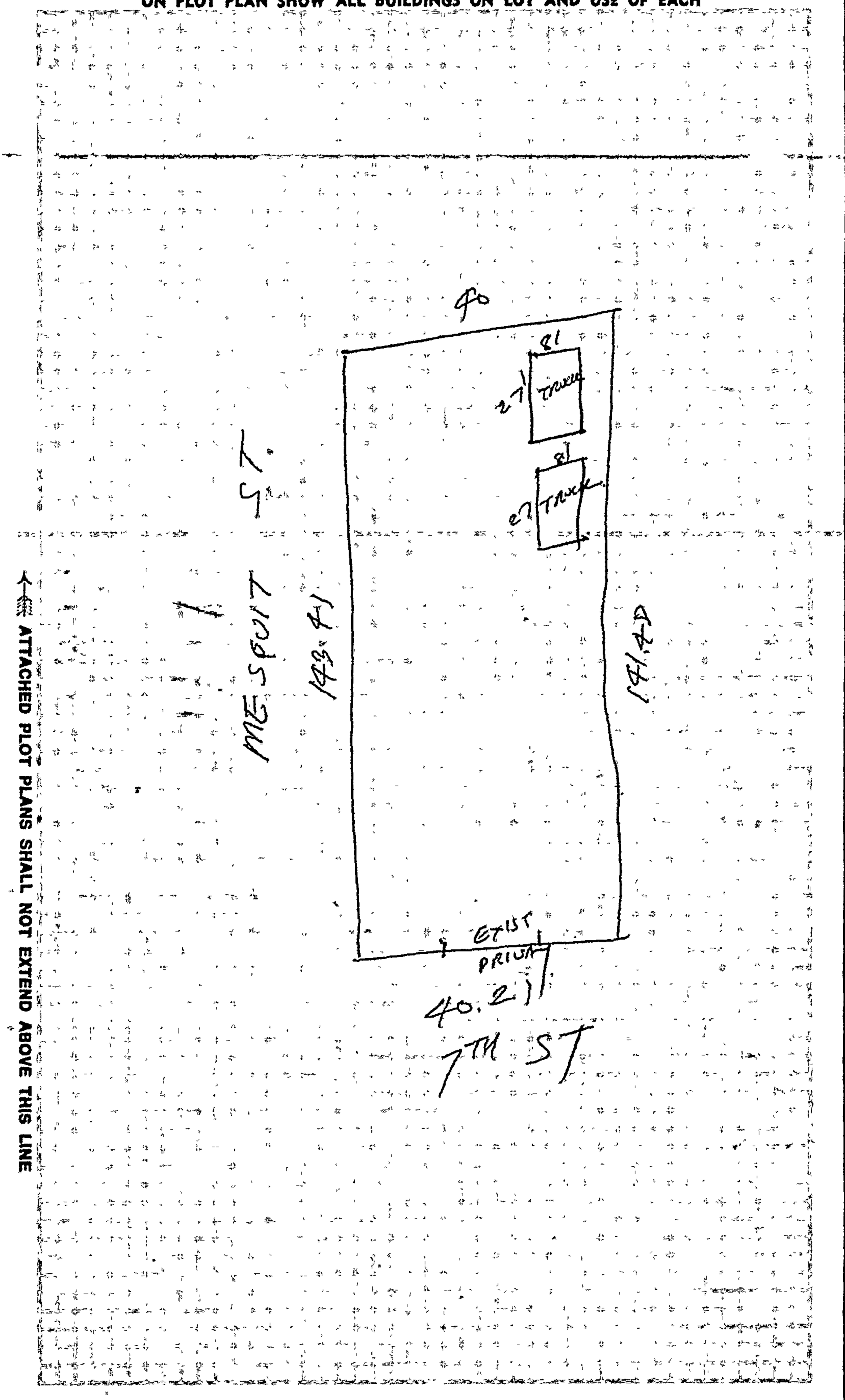
CAME 12.37

Not construction - Not Applicable 2 9 9

Bureau of Engineering	ADDRESS APPROVED		
	DRIVEWAY		
	HIGHWAY DEDICATION	<input checked="" type="checkbox"/> REQUIRED	Unity
		<input type="checkbox"/> COMPLETED	
FLOOD CLEARANCE			
SEWERS	SEWERS AVAILABLE		
RES. NO.	NOT AVAILABLE		
CERT. NO.	SFG PAID		
	SFG NOT APPLICABLE	SFG DUE	
Grading	PRIVATE SEWAGE SYSTEM APPROVED		
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)		
Housing	HOUSING AUTHORITY APPROVAL		
Planning	APPROVED UNDER CASE #		
Transportation	APPROVED FOR		
Construction Tax	RECEIPT NO.	DWELLING UNITS	

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



3

FOR INSPECTION 29700100183

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 266-279	BLOCK --	TRACT Wingerter Tract	COUNTY REF. NO. MR 14-52	DIST. MAP 123B217
2. PRESENT USE OF BUILDING	Cold Storage			NEW USE OF BUILDING	() cold storage
3. JOB ADDRESS	684 Mesquit Street				FIRE DIST. COUN. DIST. -- 09
4. BETWEEN CROSS STREETS	Jesse AND 7th Street			LOT TYPE Int.	
5. OWNER'S NAME	Don Gallo			PHONE 624-0061	LOT SIZE Irreg.
6. OWNER'S ADDRESS	670 Mesquit Street			CITY LA	ZIP 90023
7. ENGINEER	BUS. LIC. NO. Ting Lu SE3043		ACTIVE STATE LIC. NO. 818/785-1250	PHONE	ALLEY --
8. ARCHITECT OR DESIGNER	BUS. LIC. NO.		ACTIVE STATE LIC. NO.	PHONE	BLDG. LINE --
9. ARCHITECT OR ENGINEER'S ADDRESS	Frank Gallo			CITY LA	ZIP 90021
10. CONTRACTOR	owner-builder			BUS. LIC. NO.	ACTIVE STATE LIC. NO.
11. SIZE OF EXISTING BLDG.	WIDTH 35	LENGTH 55	STORIES 1	HEIGHT 30'	NO. OF EXISTING BUILDINGS ON LOT AND USE Cold Storage W/Office
12. FRAMING MATERIAL OF EXISTING BLDG.	EXT. WALLS conc. blk		ROOF wd	FLOOR conc.	AFFIDAVITS ZI-223 ZI 1572 PH ORD 164,855 AFF 1166 CCPD
13. JOB ADDRESS	684 Mesquit Street				STREET GUIDE
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 6000				DIST. OFF. P.C. REQ'D LA --
15. NEW WORK (Describe)	Change roof sys. to kimtruss				GRADING SEISMIC Yes --
NEW USE OF BUILDING		SIZE OF ADDITION		STORIES	HEIGHT
Cold Storage					
TYPE	GROUP OCC.	FLOOR AREA	PLANS CHECKED		ZONED BY
			Forwarded Computer		Miller
DWELL UNITS	MAX OCC.	TOTAL	APPLICATION APPROVED		INSPECTOR
			[Signature]		pg
GUEST ROOMS	PARKING REQ'D	PARKING PROVIDED		INSPECTION ACTIVITY	
		STD.	COMP.	CS	GEN. MAJ.S. EQ.
P.C.	G.P.I. + NP	CONT. INSP.		B & S B-3 (R.7/89)	
17.00					
S.P.C.	P.M.				
B.P.	E.I.	Claims for refund of fees paid on permits must be filed 1 Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.		CASHIER'S USE ONLY	
20.00	0.50				
I.F.	F.H.				
S.D.	O.S.S.	SPRINKLERS REQ'D SPEC.			
1.00					
ISS. OFF.	S.O.S.S.	ENERGY		DAS	
LA					
P.C. NO.	C/O				
AA8327					

08/20/90 02:59:15PM LA04 T-55
 BUILD PLAN CHE 17.00
 BLD PER CONMER 20.00
 EI RESIDENTIAL .50
 ONE STOP SURCH 1.00
 TOTAL
 CHECK

90LA 61752

DECLARATIONS AND CERTIFICATIONS

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
 Date _____ Lic. Class _____ Lic. Number _____ Contractor _____ (Signature)

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).)
 I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)
 I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)
 I am exempt under Sec. _____ B. & P. C. for this reason
 Date 8/17/90 Owner's Signature [Signature]

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
 Policy No. _____ Insurance Company _____
 Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.
 Date _____ Applicant's Signature _____

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.
 Date 8/17/90 Applicant's Signature [Signature]
 NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).
 Lender's Name _____ Lender's Address _____

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.
 I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 910202 LAMC)
 Signed [Signature] Position [Signature] Date 8/17/90
 (Owner or agent having property owner's consent)

2 9 7 0 0 1 0 0 1 8 4

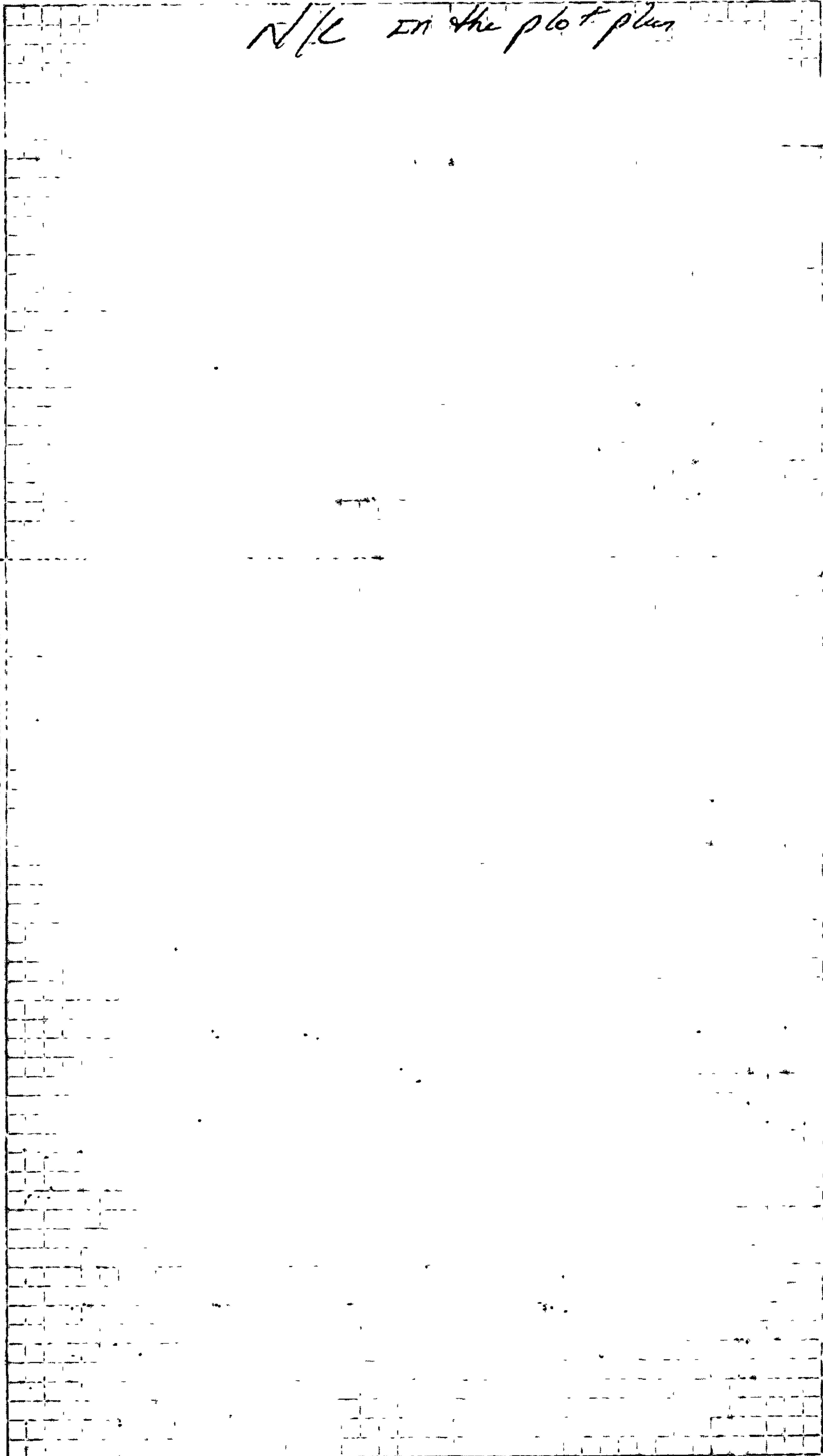
Bureau of Engineering	ADDRESS APPROVED		
	DRIVEWAY		
	HIGHWAY DEDICATION	REQUIRED	
		COMPLETED	
SEWERS RES. NO. CERT. NO.	FLOOD CLEARANCE		
		SEWERS AVAILABLE	
		NOT AVAILABLE	
		SFC PAID	
	SFC NOT APPLICABLE		SFC DUE
Grading	PRIVATE SEWAGE SYSTEM APPROVED		
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)		
Housing	HOUSING AUTHORITY APPROVAL		
Planning	APPROVED UNDER CASE #		
Transportation	APPROVED FOR		
Construction Tax	RECEIPT NO.	DWELLING UNITS	

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH

N/C in the plot plan

↑
ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE



INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. 2. Plot Plan Required on Back of Original.

1. LEGAL DESCR.	LOT 276 279	BLOCK --	TRACT Wingerter	COUNTY REF. NO. MR15-52	DIST. MAP 124.5A217
2. PURPOSE OF BUILDING	61) AUTO IMPOUND YARD - USE OF LAND				ZONE M3-1
3. JOB ADDRESS	690 Mesquit Street				FIRE DIST. 2 COUN. DIST. 9
4. BETWEEN CROSS STREETS	7th	AND Jessee		LOT TYPE CORNER	
5. OWNER'S NAME	Imperial Toy	213-489-2100	PHONE	LOT SIZE 40x143.1 irreg	
6. OWNER'S ADDRESS	2060 E. 7th		CITY	ZIP 90008	
7. ENGINEER	---	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	ALLEY 0---
8. ARCHITECT OR DESIGNER	---	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	BLDG. LINE ---
9. ARCHITECT OR ENGINEER'S ADDRESS	---		CITY	ZIP	AFFIDAVITS CCPD AFF 51166 ZI 1572
10. CONTRACTOR	---	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	
11. SIZE OF NEW BLDG.	WIDTH	LENGTH	STORIES	HEIGHT	NO. OF EXISTING BUILDINGS ON LOT AND USE
12. MATERIAL OF CONSTRUCTION	EXT. WALLS		ROOF		FLOOR
13. JOB ADDRESS	690 Mesquit Street				STREET GUIDE
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$				DIST. OFF. LA P.C. REQ'D NO

use of land - auto impound 1 of 4
W 8' HIGH SOLID FENCE.

PURPOSE OF BUILDING	auto impound	STORIES	HEIGHT	FILE WITH
TYPE	GROUP OCC.	FLOOR AREA	PLANS CHECKED	ZONED BY
DWELL UNITS	MAX. OCC.	TOTAL	BRIAN KANEGAWA	Brian
GUEST ROOMS	PARKING REQ'D.	PARKING PROVIDED	APPLICATION APPROVED	TYPIST
	6	6	INSPECTION ACTIVITY	INSPECTOR
		STD. 6 COMP.	CS GEN. MAJ.S. EQ.	

P.C.	G.P.I. + NP	CONT. INSP.	CASHIER'S USE ONLY 11/26/90 12:47:55PM LAD4 T-4749 C 06 ELO PER CORNER 10.00 ONE STOP SURCH 1.00 TOTAL 11.00 CASH 21.00 CHANGE 10.00
S.P.C.	P.M.		
B.P.	E.I.	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.	
I.F.	O.S.S.	1.00	
S.D.	S.O.S.S.		
ISSUING OFFICE	C/O	SPRINKLERS	
P.C. NO.	F.H.	ENERGY - DAS -	

Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced.

90LA 66998

DECLARATIONS AND CERTIFICATIONS

LICENSED CONTRACTORS DECLARATION

15. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
Date _____ Lic. Class _____ Lic. No. _____ Contractor's Signature _____
Contractor's Mailing Address _____

OWNER-BUILDER DECLARATION

16. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).):
 I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)
 I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)
 I am exempt under Sec. _____, B. & P. C. for this reason.
Date 11-13-90 Owner's Signature _____

WORKERS' COMPENSATION DECLARATION

17. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
Policy No. _____ Insurance Company _____
 Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.
Date _____ Applicant's Signature _____
Applicant's Mailing Address _____

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

18. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.
Date 11-13-90 Applicant's Signature _____

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

19. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).
Lender's Name _____
Lender's Address _____

20. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.
I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed (See Sec. 91.0202 LAMC)

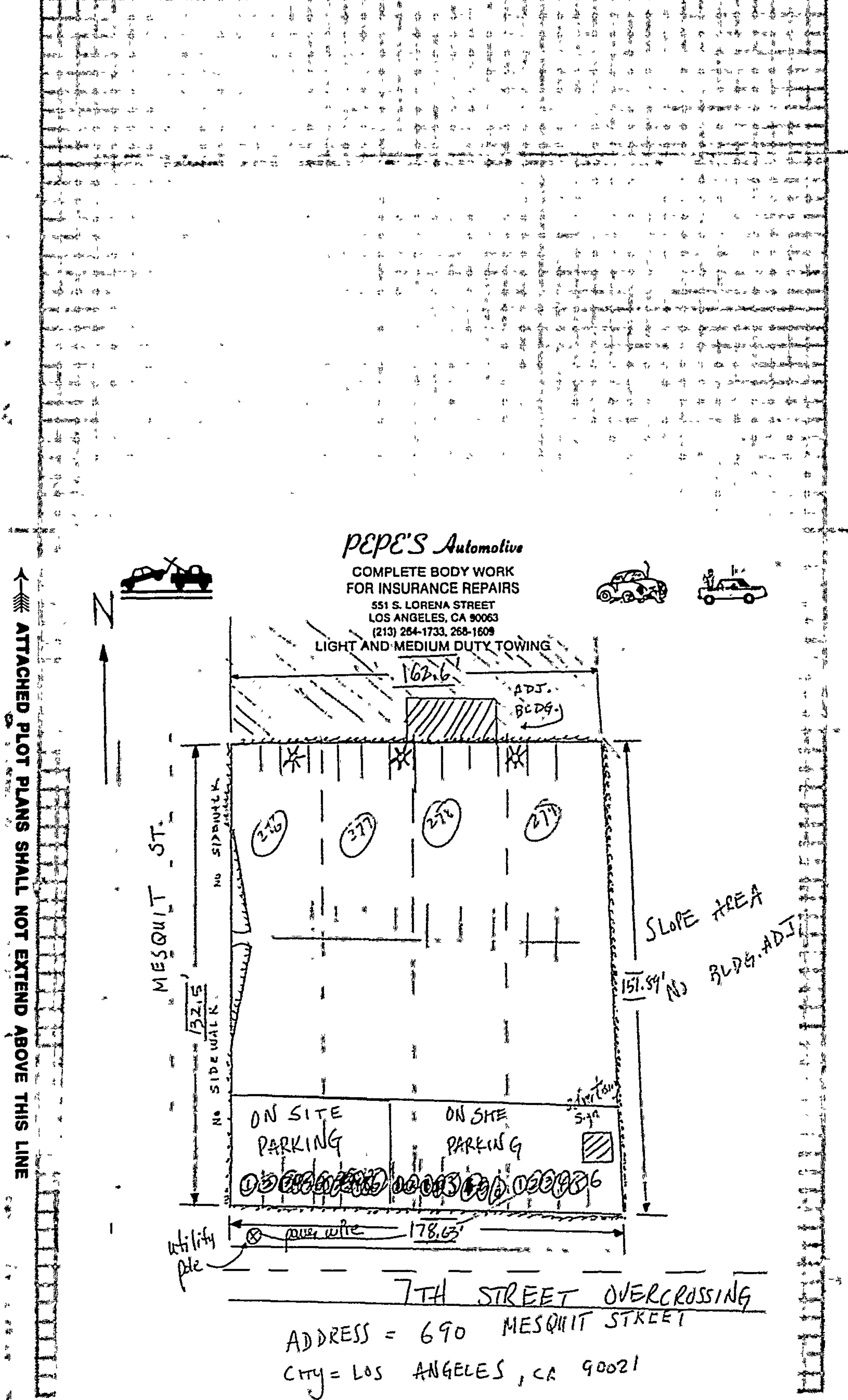
X Signed _____ Position _____ Date 11-13-90
(Owner or agent having property owner's consent)

3 0 8 0 0 3 0 0 2 2 0

Bureau of Engineering	ADDRESS APPROVED		Gomez 11-9-90
	DRIVEWAY <i>No driveways</i>		<i>Red Dams 11/9/90</i>
	HIGHWAY DEDICATION	REQUIRED	
		COMPLETED	
FLOOD CLEARANCE			
SEWERS	SEWERS AVAILABLE		
RES. NO.	NOT AVAILABLE		
CERT. NO.	SFC PAID		
	SFC NOT APPLICABLE	SFC DUE	
Grading	PRIVATE SEWAGE SYSTEM APPROVED		
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)		
Housing	HOUSING AUTHORITY APPROVAL		
Planning	APPROVED UNDER CASE # <i>W.S. 90-4805; ZI-1572 NA</i>		<i>Shyldrick 11-9-90</i>
Transportation	APPROVED FOR		
Construction Tax	RECEIPT NO.	DWELLING UNITS	

LEGAL DESCRIPTION
LOT-TIE AFFIDAVIT 90-1956202

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



7TH STREET OVERCROSSING
 ADDRESS = 690 MESQUIT STREET
 CITY = LOS ANGELES, CA 90021
 ZONE = M3.1 AVAIL. SF: 24,296.
 ☼ = LIGHTS ON POLE. ACRES: .56

↑ ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE

3

APPLICATION

CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY

TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

FOR INSPECTION Earthquake Safety Division

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 271, 272 273, 274	BLOCK	TRACT Wingerter	COUNTY REF. NO. MR 15-52	DIST. MAP 123-217 CENSUS TRACT 2065
2. PRESENT USE OF BUILDING	22/13 Warehouse/Storage			NEW USE OF BUILDING 22/13 Warehouse/Office	ZONE M3-3
3. JOB ADDRESS	682 Mesquit St & Bldg A				FIRE DIST. two COUN. DIST. 9
4. BETWEEN CROSS STREETS	Jesse	AND	7th St.	LOT TYPE interior	
5. OWNER'S NAME	Don Gallo	PHONE	624-8862	LOT SIZE irregular	
6. OWNER'S ADDRESS	670 Mesquit St.	CITY L.A.	90021	ALLEY	
7. ENGINEER	Chester Schultz	BUS. LIC. NO. 50930	ACTIVE STATE LIC. NO. 714/773-9433	BLDG. LINE	
8. ARCHITECT OR DESIGNER	Frank Gallo	BUS. LIC. NO.	ACTIVE STATE LIC. NO. 818/247-2414	AFFIDAVITS CCPD	
9. ARCHITECT OR ENGINEER'S ADDRESS	2000 E. Chapman Ave.	CITY Fullerton	ZIP 92631	AFF51166	
10. CONTRACTOR	ns	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	
11. SIZE OF EXISTING BLDG.	WIDTH 164' LENGTH 150'	STORIES 1+M	HEIGHT 30'	NO. OF EXISTING BUILDINGS ON LOT AND USE one warehouse/office	
12. FRAMING MATERIAL OF EXISTING BLDG.	um	EXT. WALLS um	ROOF wood	FLOOR conc	
13. JOB ADDRESS	682 Mesquit & Bldg A				
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 280,000.00				DIST. OFF. LA P.C. REQ'D no(e)
15. NEW WORK (Describe)	full compliance with Div 88 (change of engineer)				GRADING HWY. DED. coll FLOOD

NEW USE OF BUILDING	SIZE OF ADDITION	STORIES	HEIGHT	FILE WITH
TYPE III-N	GROUP OCC. B-2	FLOOR AREA	PLANS CHECKED HURDE COLEMAN	ZONED BY Syed Ali
DWELL UNITS	MAX OCC.	TOTAL	APPLICATION APPROVED	INSPECTOR cd 01/15/91
GUEST ROOMS	PARKING REQ'D	PARKING PROVIDED	INSPECTION ACTIVITY	
P.C. 768.40	G.P.I. + NP	CONT. INSP.	01/15/91 12:01:15PM H001 T-4642 C 10	
S.P.C. 1040	P.M. 20.80		E.O. PLAN CHECK ONE STOP TOTAL CHECK 768.40 15.37 783.77	
B.P. 1040	E.I. 42.00	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee, or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.	MESQUIT (BLDG A) F01 F1819 / CK 13571	
I.F.	F.H.		02/15/91 02:49:30AM H001 T-7313 C 10	
S.D. N/A	O.S.S. 15.37	SPRINKLERS REQ'D SPEC.	E.O. PERMITS PLAN MAINTENAN EI COMMERCIAL ONE STOP TOTAL CHECK 1,040.00 20.80 42.00 22.06 1,124.86	
ISS. OFF. HO	S.O.S.S.	ENERGY	DAS	
P.C. NO. F 1819	C/O			

Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced.

*DECLINE TO PAY
F.H.
112486

91HO 11017

DECLARATIONS AND CERTIFICATIONS

LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.

OWNER-BUILDER DECLARATION

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code): Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500.):

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.

CONSTRUCTION LENDING AGENCY

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

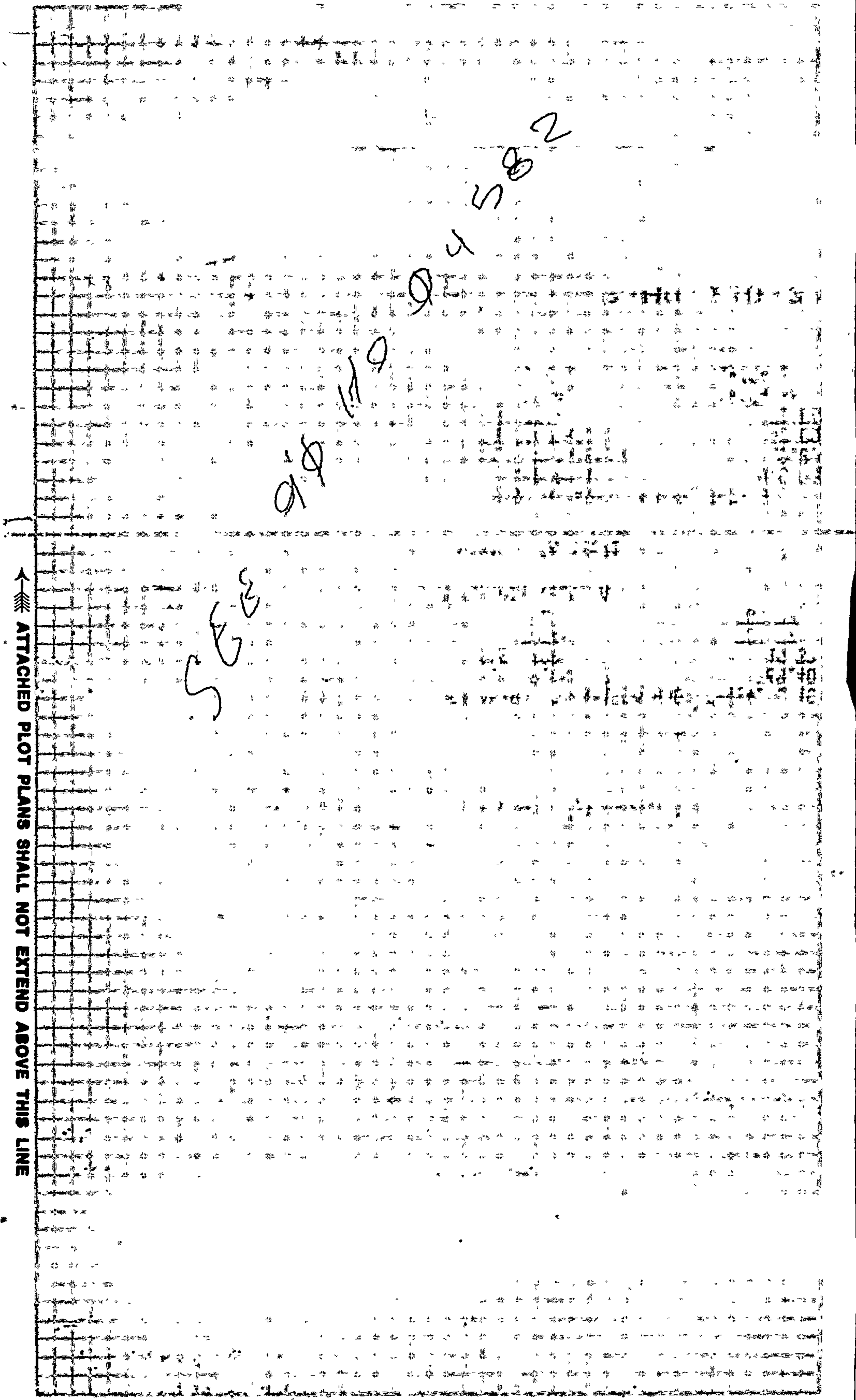
Signed: [Signature] OWNER, 2/15/91. Position: OWNER. Date: 2/15/91.

3 1 6 0 0 3 8 0 0 4 9

Bureau of Engineering	ADDRESS APPROVED		
	DRIVEWAY		
	HIGHWAY DEDICATION	REQUIRED	
		COMPLETED	
SEWERS		FLOOD CLEARANCE	
RES. NO.		SEWERS AVAILABLE	
CERT. NO.		NOT AVAILABLE	
		SFC PAID	
	SFC NOT APPLICABLE	SFC DUE	
Grading	PRIVATE SEWAGE SYSTEM APPROVED		
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)		
Housing	HOUSING AUTHORITY APPROVAL		
Planning	APPROVED UNDER CASE #		
Transportation	APPROVED FOR		
Construction Tax	RECEIPT NO.	DWELLING UNITS	

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



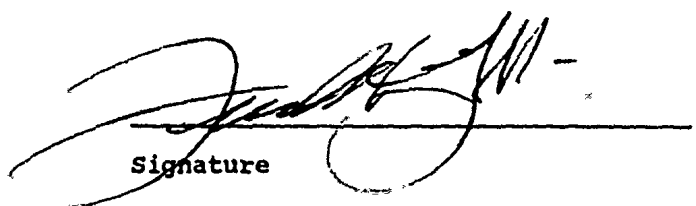
↑ ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE

11017

31600300050

FIRE HYDRANT FEE NOTICE: The City of Los Angeles may amend the Fire Hydrant Fee Ordinance. (LAMC Section 91.0304 (b) 8). The owner of the project designated in this permit shall be obligated to pay to the Department a Fire Hydrant Fee in the amount to be calculated pursuant to any amendment to the Fire Hydrant Fee Ordinance. This fee will be used to provide adequate fire safety facilities and services for new development.

EXCEPTION: This paragraph Number 8 shall not apply to any permit for demolition of a building or structure.


Signature

2/15/91
Date

91 HO 11017



February 5, 1991

900705

RANCHO COLD STORAGE, INC.
670 Mesquit Street
Los Angeles, CA 90021

11017

Attention: Mr. Frank Gallo

Reference: 666-674 Mesquit Street
Building Rehabilitation

Dear Mr. Gallo:

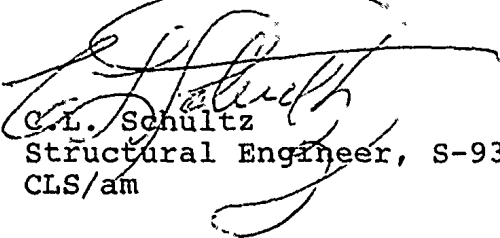
As discussed with you, McLean & Schultz will take over the plans and calculations that were signed by Mendad Mouriani to provide professional services to Rancho Cold Storage to complete the work of rehabilitating the buildings at the above-referenced address.

The drawings were prepared by ASD Engineering Company for Buildings A through F, as shown on the plans that are now in the possession of the City and have been approved by the City of Los Angeles.

You may take this letter to the City of Los Angeles as proof that we will be the responsible engineer for the above project.

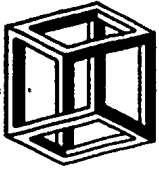
Very truly yours,

McLEAN & SCHULTZ


C.L. Schultz

Structural Engineer, S-930, Expires 12-31-92
CLS/am

316030051



Lehigh Construction Company

PROFESSIONAL ENGINEERS AND CONSTRUCTION SPECIALISTS

January 30, 1991

CITY OF LOS ANGELES
Building and Safety
443 Shatto Place
Los Angeles, California

TO WHOM IT MAY CONCERN:

Our company, Lehigh Construction Company did the seismic engineering for the project located at 678-682 Mesquit Street (Building A, B, C, D, E & F), owned by Rancho Cold Storage, Inc.

Please accept this letter as authorization to release the plans and calculations for above mentioned buildings to the engineering firm of McLean & Schultz.

This letter releases Lehigh Construction Company from any liability on this particular building 678-682 Mesquit Street (Building A, B, C, D, E & F).

Sincerely,

MERDAD HOURIANI

MH:lt

316003B00.52

3

APPLICATION

CITY OF LOS ANGELES DEPT. OF BUILDING AND SAFETY

TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

FOR INSPECTION Earthquake Safety Division 9 5

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 271, 272 273, 274	BLOCK	TRACT Wingenter	COUNTY REF. NO. MR 15-52	DIST. MAP 123-217 CENSUS TRACT 2065
2. PRESENT USE OF BUILDING () Cold Office/Storage	NEW USE OF BUILDING <i>Storage</i> () 22/13 <i>House/office</i>			ZONE M3-3	
3. JOB ADDRESS 682 Mesquit Street Building A	FIRE DIST. Two				COUN. DIST. 9
4. BETWEEN CROSS STREETS Jesse	AND 7th Street				LOT TYPE Interior
5. OWNER'S NAME Don Gallo	PHONE (213) 624-8862				LOT SIZE Irregular
6. OWNER'S ADDRESS 670 Mesquite Street	CITY Los Angeles				ZIP 90021
7. ENGINEER Chester Schultz	BUS. LIC. NO. 50930	ACTIVE STATE LIC. NO. 8939	PHONE 714- 871-7370		ALLEY -----
8. ARCHITECT OR DESIGNER Frank Gallo	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE 213- 376-6723		BLDG. LINE -----
9. ARCHITECT OR ENGINEER'S ADDRESS 2000 E. Chapman Avenue	CITY Fullerton				ZIP 92631
10. CONTRACTOR Gangi Builders	BUS. LIC. NO. 465868	ACTIVE STATE LIC. NO.	PHONE 818- 247-2414		AFFIDAVITS CCPD AFF51166
11. SIZE OF EXISTING. BLDG. WIDTH 164 LENGTH 150	STORIES 1	HEIGHT 30	NO. OF EXISTING BUILDINGS ON LOT AND USE Office/Storage		
12. FRAMING MATERIAL OF EXISTING BLDG. \Rightarrow	EXT. WALLS URM	ROOF Wood	FLOOR Concrete		
13. JOB ADDRESS 692 Mesquite Street, Los Angeles	STREET GUIDE				
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 30,000.				DIST. OFF. LA
15. NEW WORK (Describe) Change of roof design (25'x48')					P.C. REQ'D No (E)
NEW USE OF BUILDING N/C					FILE WITH 91HO 11017
TYPE III-N	GROUP OCC. N/C	FLOOR AREA TOTAL	PLANS CHECKED SAMPOSTO ARI	ZONED BY S.OSTO-6r	
DWELL UNITS	MAX OCC.		APPLICATION APPROVED S.W.O.	TYPYST	
GUEST ROOMS	PARKING REQ'D	PARKING PROVIDED STD. COMP.	INSPECTION ACTIVITY CS GEN. MAJ.S. (EQ)		
INSPECTOR	B & S B-3 (R.7/89)				
P.C. 189.12	G.P.I. + NP	CONT. INSP.	CASHIER'S USE ONLY 05/02/91 10:00:45AM H001 T-2995 C 06 E.Q. PLAN CHEC 189.12 E.Q. PERMITS 222.50 PLAN MAINTENAN 7.00 EI COMMERCIAL 6.30 ONE STOP 8.50 TOTAL 433.42 CHECK 433.42		
S.P.C.	P.M.				
B.P. 222.50	E.I.	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.			
I.F.	F.H.				
S.D. N/A	O.S.S.				
ISS. OFF. HP	S.O.S.S.	SPRINKLERS REQ'D SPEC.			
P.C. NO. CC	C/O	ENERGY	DAS		

Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced.

91HO 12329

DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION

I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect. Date 5/3/91 Lic. Class BR Lic. Number 45360 Contractor [Signature]

OWNER-BUILDER DECLARATION

I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).) I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.) I am exempt under Sec. B. & P. C. for this reason. Date Owner's Signature

WORKERS' COMPENSATION DECLARATION

I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.). Policy No. Insurance Company Certified copy is hereby furnished. Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety. Date Applicant's Signature Applicant's Mailing Address

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California. Date Applicant's Signature

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.). Lender's Name Lender's Address

I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 81.0202 LAMC)

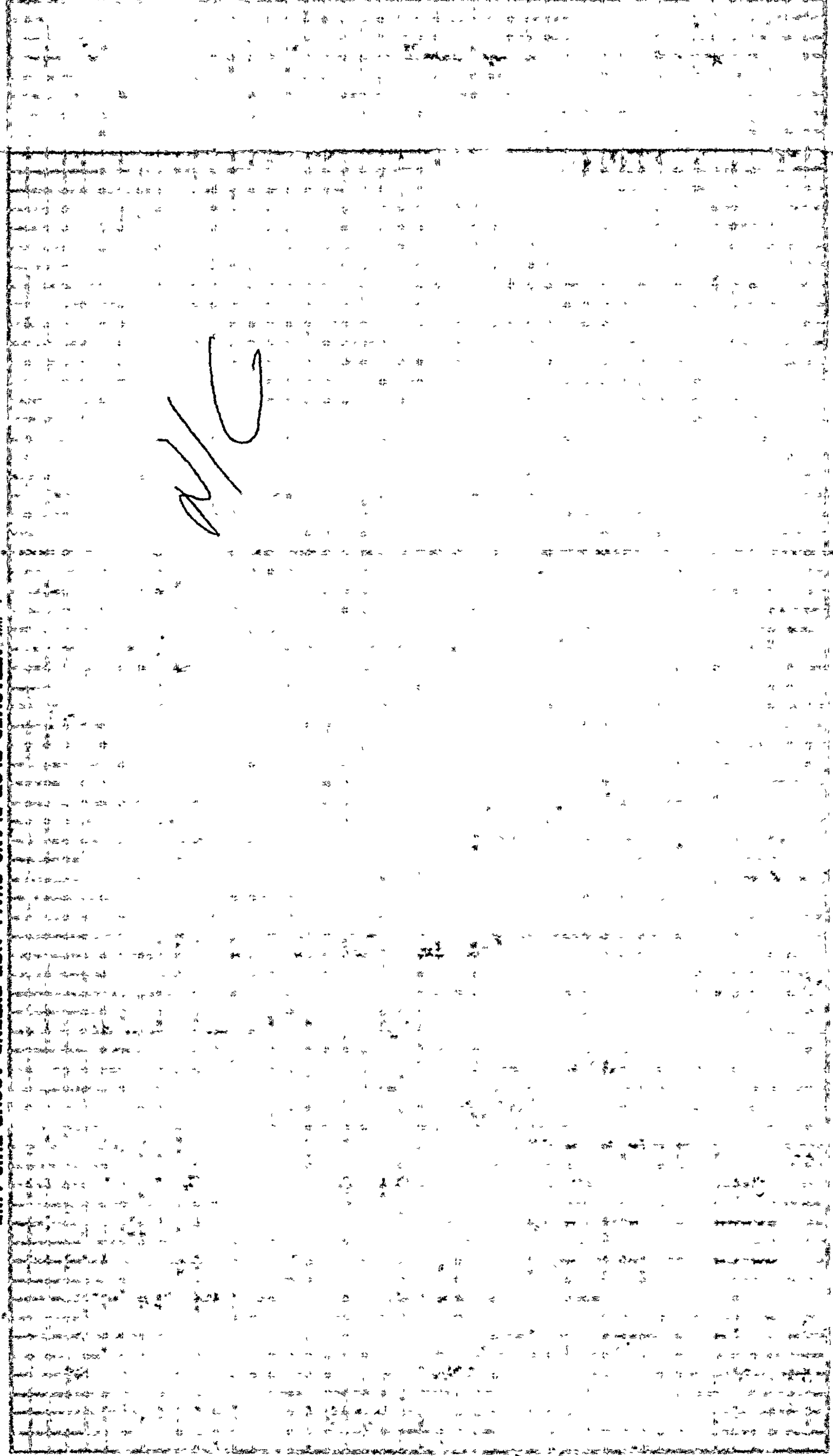
Signed [Signature] (Owner or agent having property owner's consent) Position [Signature] Date 5/2/91

3 2 4 0 0 4 0 0 4 9 6

Bureau of Engineering	ADDRESS APPROVED		
	DRIVEWAY		
	HIGHWAY DEDICATION	REQUIRED	
		COMPLETED	
SEWERS RES. NO. CERT. NO.	FLOOD CLEARANCE		
		SEWERS AVAILABLE	
		NOT AVAILABLE	
		SFC PAID	
	SFC NOT APPLICABLE	SFC DUE	
Grading	PRIVATE SEWAGE SYSTEM APPROVED		
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)		
Housing	HOUSING AUTHORITY APPROVAL		
Planning	APPROVED UNDER CASE #		
Transportation	APPROVED FOR		
Construction Tax	RECEIPT NO.	DWELLING UNITS	

LEGAL DESCRIPTION

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



↑
ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE

FOR INSPECTION 100 Earthquake Safety Division

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 271-270	BLOCK --	TRACT Wingarter	CITY CLERK REF. NO. MR14-52	DIST. MAP 123B217 CENSUS TRACT 2065
2. PRESENT USE OF BUILDING	() warehouse/cooler		NEW USE OF BUILDING () same		ZONE M13-1
3. JOB ADDRESS	682 Mesquit			SUITE/UNIT NO.	FIRE DIST. 9 COUN. DIST. 9
4. BETWEEN CROSS STREETS	Jessee		AND 7th	LOT TYPE Int	
5. OWNER'S NAME	() TENANT () BUILDING		PHONE		LOT SIZE irreg
6. OWNER'S ADDRESS	670 Mesquit LA 90007		CITY ZIP		ALLEY
7. ENGINEER	Chet Shulte		BUS. LIC. NO. S930	ACTIVE STATE LIC. NO. (714) 871-7370	PHONE
8. ARCHITECT OR DESIGNER	Frank Gallo		BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE
9. ARCHITECT OR ENGINEER'S ADDRESS	670 Mesquit		CITY ZIP		BLDG. LINE
10. CONTRACTOR	Gangi Bldrs		BUS. LIC. NO. 465868	ACTIVE STATE LIC. NO.	PHONE
11. SIZE OF EXISTING BLDG.	WIDTH	LENGTH	STORIES 1	HEIGHT 30	NO. OF EXISTING BUILDINGS ON LOT AND USE cold/storage/office
12. FRAMING MATERIAL OF EXISTING BLDG.	EXT. WALLS steel frame/		ROOF conc blk / wood	FLOOR conc	CCPD
13. JOB ADDRESS	682 Mesquit			SUITE/UNIT NO.	
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 35,000.				DIST. OFF. LA P.C. REQ'D
15. NEW WORK (Describe)	Replace east URM wall with new conc block wall & replace sox wall w/steel frame, wood infill.				GRADING SEISMIC HWY. DED. FLOOD
NEW USE OF BUILDING	SIZE OF ADDITION	STORIES	HEIGHT		FILE WITH 91HO12329
TYPE Y-N	GROUP OCC. B-2	MAX. OCC.	PLANS CHECKED		ZONED BY 10-29
DWELL UNITS	BUILDING AREA	ZONING AREA	APPLICATION APPROVED 70397		INSPECTOR
GUEST ROOMS	PARKING REQ'D	PARKING PROVIDED S C HC.		INSPECTION ACTIVITY CS GEN. MAJ.S. EQ.	
P.C. 239.91	G.P.I. + NP	CONT. INSP.		B & S 08-B-3 (R.7/90)	
S.P.C. -	PM.			CASHIER'S USE ONLY	
B.P. 282.25	E.I. 7.35	Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of fee; or 2. Within one year from date of expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC.		10/29/92 08:35:25AM H001 T-4506 C 26	
I.F.	F.H.			E Q PLAN CHECK 239.91	
S.D.	O.S.S. YES	SPRINKLERS REQ'D SPEC. -		EQ PERMIT 282.25	
ISS. OFF. HO	S.O.S.S.			INVOICE 1/2 0070397 BB 7.35	
P.C. NO. CC	C/O	ENERGY	DAS	SYS DEV 31.77	
Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced.					
NEW AFFIDAVITS					
PLAN CHECK EXTENDED TO _____ PER					
ADMINISTRATIVE APPROVAL DATED _____					
BY _____					
D.A.D. PLANS CHECKED					
HOUSING MITIGATION FEE ORDINANCE					
<input type="checkbox"/> REQUIRED <input type="checkbox"/> EXEMPT					
ASBESTOS NOTIFICATION					
Check Box: <input type="checkbox"/> Notification letter sent to AQMD or EPA					
<input checked="" type="checkbox"/> I declare that notification of asbestos removal is not applicable to this project.					
Signature _____ Date _____					

DECLARATIONS AND CERTIFICATIONS

LICENSED CONTRACTORS DECLARATION

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.

Date 10-26-92 Lic. Class 452866 Lic. Number Contractor

OWNER-BUILDER DECLARATION

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).):

I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.).

I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.).

I am exempt under Sec. _____, B. & P. C. for this reason.

Date _____ Owner's Signature _____

WORKERS' COMPENSATION DECLARATION

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).

Policy No. 21199183-91 Insurance Company STATE FORM

Certified copy is hereby furnished.

Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.

Date _____ Applicant's Signature _____

Applicant's Mailing Address 670 MESQUIT

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.

Date _____ Applicant's Signature _____

NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

CONSTRUCTION LENDING AGENCY

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.).

Lender's Name _____ Lender's Address _____

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed. (See Sec. 91.0202 LAMC)

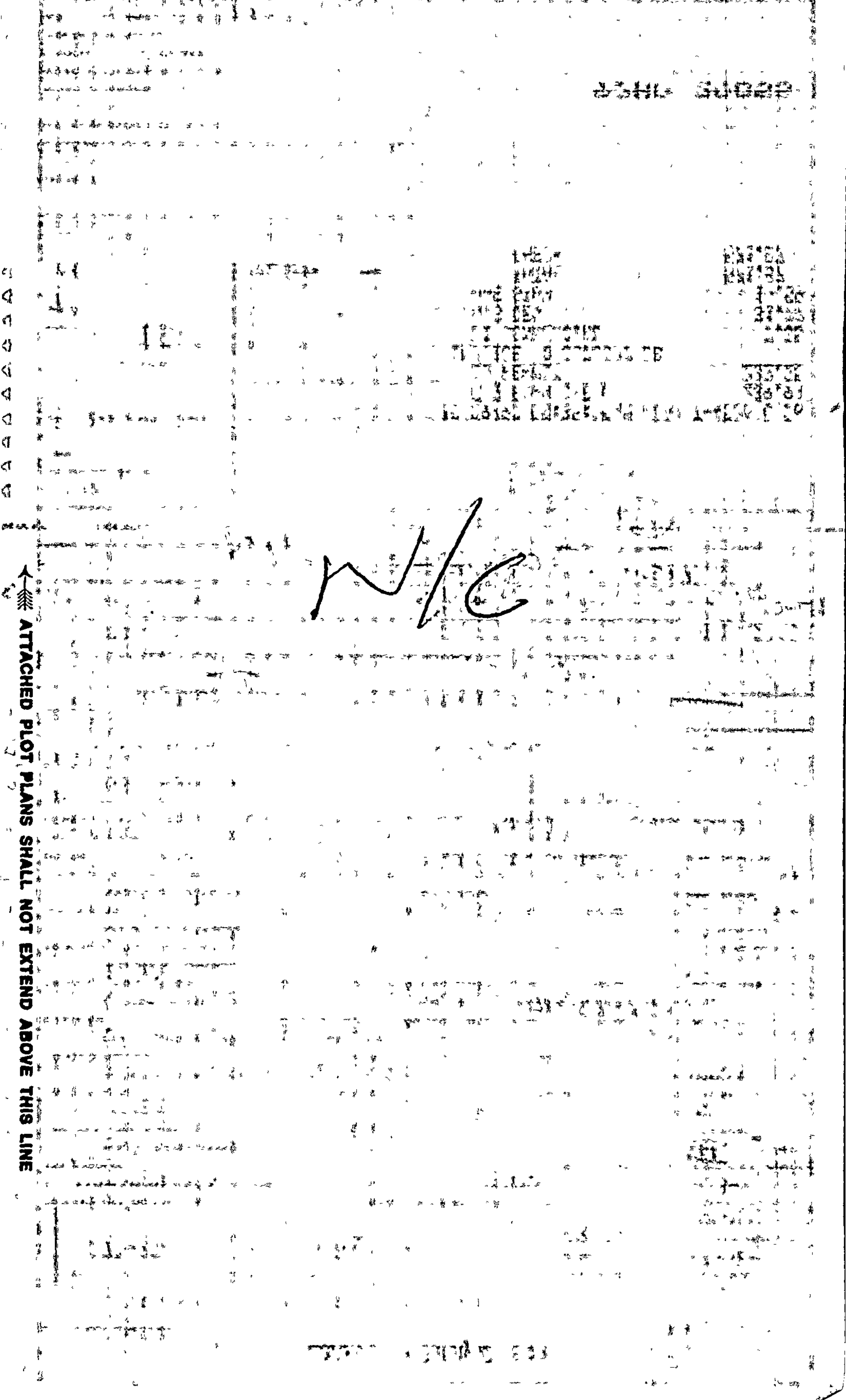
Signed _____ Position _____ Date 10-26-92

33430579042

Bureau of Engineering		ADDRESS APPROVED		
		DRIVEWAY		
		HIGHWAY	REQUIRED	
		DEDICATION	COMPLETED	
		FLOOD CLEARANCE		
Public Works Improvement	Required YES <input type="checkbox"/> NO <input type="checkbox"/>	PERMIT #		
SEWERS		SEWERS AVAILABLE		
RES. NO.		NOT AVAILABLE		
CERT. NO.		SFC PAID		
	SFC NOT APPLICABLE	SFC DUE		
Grading	PRIVATE SEWAGE SYSTEM APPROVED			
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>			
CEQA				
Fire	APPROVED (TITLE 19) (L.A.M.C.-S700)			
	APPROVED - HYDRANT UNIT, ROOM 920 CHE			
CRA	APPROVED PER REDEV. PROJECT			
Transportation	APPROVED FOR DRIVEWAY LOCATION			
	APPROVED FOR ORD. #			
Planning	WORK SHEET #			
	APPROVED UNDER CASE #			
	LANDSCAPE / XERISCAPE			
	SIGHT PLAN REVIEW			
Housing	HOUSING AUTHORITY AFFIDAVIT NO.			
Construction Tax	RECEIPT NO.	DWELLING UNITS		
Cultural Affairs				
Rent Stabilization Division				

LEGAL DESCRIPTION
 STEEL FRAME IS USED TO LIMIT HEIGHT REQUIREMENT OF wood STUD, LATERAL FORCE TAKEN BY PLYWOOD SYSTEM.

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



ATTACHED PLOT PLANS SHALL NOT EXTEND ABOVE THIS LINE

INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.

1. LEGAL DESCR.	LOT 263 & 264	BLOCK --	TRACT WINGESTER	CITY CLERK REF. NO. MR15-52	125B127
2. PRESENT USE OF BUILDING	(23) COLD STORAGE		NEW USE OF BUILDING (23) SAME	ZONE M3-3	
3. JOB ADDRESS	666 MESQUIT ST. (BLDG. F)			SUITE/UNIT NO.	FIRE DIST. TWO COUN. DIST. 11
4. BETWEEN CROSS STREETS	7TH ST.		AND JESSEE	LOT TYPE INT.	
5. OWNER'S NAME	RANCHO COLD STORAGE INC.			PHONE	LOT SIZE IRREG.
6. OWNER'S ADDRESS	670 MESQUIT ST.	CITY L.A.	CA 90021	ZIP 218	
7. ENGINEER	RUBY QUINTINER	BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	ALLEY --
8. ARCHITECT OR DESIGNER		BUS. LIC. NO.	ACTIVE STATE LIC. NO.	PHONE	BLDG. LINE --
9. ARCHITECT OR ENGINEER'S ADDRESS	14402 HAYNES ST.		CITY VAN NUYS, CA.	ZIP 91401	DOCUMENTS/EASEMENTS
10. CONTRACTOR	WALTER BROTHERS INC.			BUS. LIC. NO. --	ACTIVE STATE LIC. NO. --
11. SIZE OF EXISTING BLDG	WIDTH 56	LENGTH 117	STORIES 4	HEIGHT 51	NO. OF EXISTING BUILDINGS ON LOT AND USE
12. FRAMING MATERIAL OF EXISTING BLDG.	EXT. WALLS		ROOF WOOD	FLOOR	
13. JOB ADDRESS	666 MESQUIT ST. (BLDG. F)			SUITE/UNIT NO.	
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING	\$ 171,000.00				DIST OFF -- P.C. REQ'D --
15. NEW WORK (Describe)	Full Compliance w/ Div. 88 Risk Class II (building is vacant)				GRADING -- SEISMIC -- HWY DEED -- FLOOD --
NEW USE STORAGE	SIZE OF ADDITION	STORIES	HEIGHT	BUILDING ZONING	FILE WITH
TYPE III-N	GROUP OCC B-2	MAX OCC	PLANS CHECKED OSCAR ONDOY		ZONED BY 7/14/94
DWELL UNITS NA	BUILDING AREA	ZONING AREA	APPLICATION APPROVED Oscar Ondoy 27537		INSPECTOR E.S. CAUDILLO
GUEST ROOMS NA	PARKING REQ'D	PARKING PROVIDED	INSPECTION ACTIVITY		
PC. 677.74	G.P.I. + NP	CONT. INSP. REINF. MASONRY	06/03/94 04:08:20 PM H001 T-1665 C 09		
S.P.C.	P.M. 15.94		E & B PLAN CHECK 677.74		
B.P. 797.35	E.I. 35.91	Claims for refund of fees paid on permits must be filed 1 Within one year from date of payment of fee, or 2 Within one year from date of expiration of extension for building or grading permits granted by the Dept of B & S SECTIONS 22 12 & 22 13 LAMC	SYS DEV 43.66		
IF	F.H. *		ONE STOP 13.55		
SD.	O.S.S. YES		TOTAL 731.95		
ISS. OFF. HO	S.O.S.S. YES	SPRINKLERS REQ'D SPEC --	CHECK 731.95		
PC. NO. 62727	SSYS- YES	ENERGY	07/14/94 11:19:00 AM H001 T-6265 C 09		
		DAS	EQ PERMIT 797.35		
Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced			INVOICE # 0027537 88		
* Applicant refused to pay at this time.			PLAN MAINTENAN 15.94		
NEW AFFIDAVITS (See back)			E.I. COMMERCIAL 35.91		
PLAN CHECK EXTENDED TO PER			ONE STOP 16.98		
ADMINISTRATIVE APPROVAL DATED			SYS DEV 50.95		
BY DAD PLANS CHECKED			TOTAL 917.13		
HOUSING MITIGATION FEE ORDINANCE			CHECK 917.13		
<input type="checkbox"/> REQUIRED <input type="checkbox"/> EXEMPT			94HO 30028		
ASBESTOS NOTIFICATION					
Check Box. <input type="checkbox"/> Notification letter sent to AQMD or EPA.					
I declare that notification of asbestos removal is not applicable to this project.					
Signature: [Signature]			Date: 7-9-94		

DECLARATIONS AND CERTIFICATIONS

16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.
 Date: 7-14-94 Lic. Class: B Lic. Number: 40580 Contractor: [Signature]

17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5 of the Business and Professions Code): Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).
 I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.)
 I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.)
 I am exempt under Sec. 7044, B. & P. C. for this reason: [Signature]
 Date: 7/14/94 Owner's Signature: [Signature]

18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.).
 Policy No. _____ Insurance Company _____
 Certified copy is hereby furnished.
 Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety.
 Date: _____ Applicant's Signature: _____

19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California.
 Date: 7-14-94 Applicant's Signature: [Signature]

20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3997, Civ. C.).
 Lender's Name: _____ Lender's Address: _____

21. I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.
 I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein or the condition of the property or soil upon which such work is performed. (See Sec. 91 0202 LAMC)
 Signed: [Signature] Position: OWNER/DOWNER Date: 7-14-94
 (Owner or agent having property owner's consent)

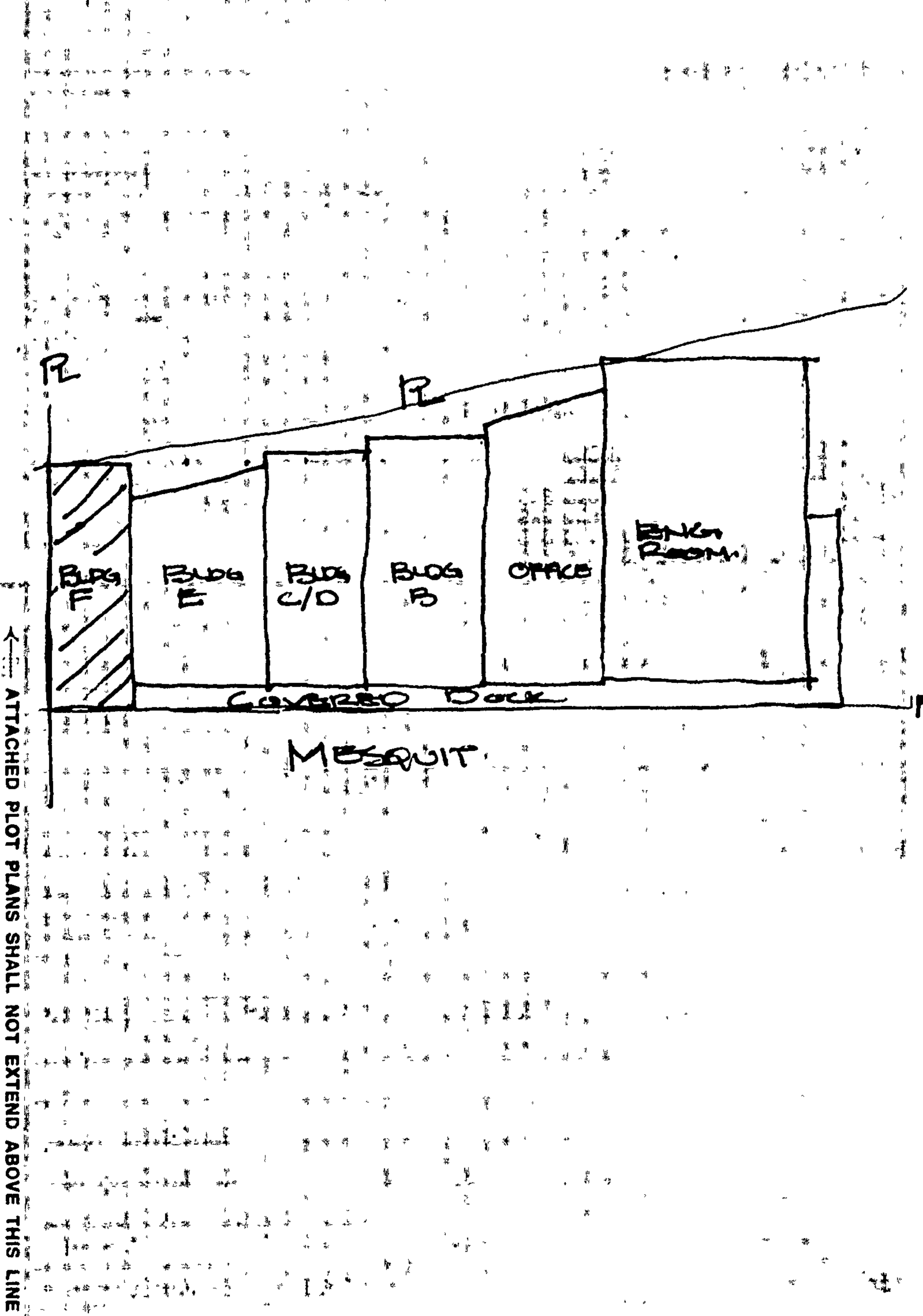
46897100213

Bureau of Engineering		ADDRESS APPROVED	
		DRIVEWAY	
		HIGHWAY	REQUIRED
		DEDICATION	COMPLETED
		FLOOD CLEARANCE	
Public Works Improvement	Required YES <input type="checkbox"/> NO <input type="checkbox"/>	PERMIT #	
SEWERS		SEWERS AVAILABLE	
RES. NO		NOT AVAILABLE	
CERT. NO.		SFC PAID	
	SFC NOT APPLICABLE	SFC DUE	
Grading	PRIVATE SEWAGE SYSTEM APPROVED		
Comm. Safety	APPROVED FOR ISSUE <input type="checkbox"/> NO FILE <input type="checkbox"/> FILE CLOSED <input type="checkbox"/>		
CEQA			
Fire	APPROVED (TITLE 19) (L.A.M.C-S700)		
	APPROVED - HYDRANT UNIT, ROOM 920 CHE		
CRA	APPROVED PER REDEV. PROJECT		
Transportation	APPROVED FOR DRIVEWAY LOCATION		
	APPROVED FOR ORD. #		
Planning	WORK SHEET #		
	APPROVED UNDER CASE #		
	LANDSCAPE / XERISCAPE		
	SIGHT PLAN REVIEW		
Housing	HOUSING AUTHORITY AFFIDAVIT NO		
Construction Tax	RECEIPT NO.	DWELLING UNITS	
Cultural Affairs			
Rent Stabilization Division			

LEGAL DESCRIPTION

Certified copy of 2 affidavits (Tension anchors provided only at adjacent building, per EQ Guideline #4) are pending (but applied for) from County Recorder's Office. Rept # 2026 on 7/14/94 by L. Lander

ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH



CITY OF LOS ANGELES
CALIFORNIA



JAMES K. HAHN
MAYOR

CERTIFICATE OF OCCUPANCY

ADDRESS OF BUILDING: 690 S. MESQUIT STREET

NOTE: Any change of use of occupancy must be approved by the Department of Building and Safety.

- This certifies that, so far as ascertained or made known to the undersigned, the vacant land, building or portion of building described below and located at the address complies with the applicable construction requirements (Chapter 9) and/or the applicable zoning requirements (Chapter 1) of the Los Angeles Municipal Code for the use, or occupancy group in which it is classified.* (Non-Residential Uses)
- This certifies that, so far as ascertained by or make known to the undersigned, the building or portion of building described below and located at the above address complies with the applicable requirements of the Municipal Code, as follows: Ch. 1, as to permitted uses, Ch. 9, Arts. 1,3,4, and 5; and with applicable requirements of State Housing Law-for following occupancies:* (Residential uses)

Permit No. and Year: 99010-10000-03361/00016-10000-17409

NEW 1 STORY, TYPE III-N, 193' X 148' REINFORCED CONCRETE TILT-UP COLD STORAGE BUILDING.

S1 OCCUPANCY

TOTAL PARKING REQUIRED: 6

TOTAL PARKING PROVIDED: 6 = STANDARD: 5 + HANDICAPPED: 1

* ALSO SUBJECT TO ANY AFFIDAVITS OR BUILDING AND ZONING CODE MODIFICATIONS WHETHER LISTED ABOVE OR NOT.

Issued By/Office:

(LA) -VN-WLA-SP-C.D. #:

Bureau:

(BLDG) -BCS:

Division:

GI - (MS) -MSS-EQ-BMI-COMM:

OWNER: RANCHO COLD STORAGE
OWNER'S 670 MESQUIT ST.
ADDRESS: LOS ANGELES, CA 90021

Issued: 03/06/2002

BY:  P.H./D.R.

08-B-95C (R 11/89)



13690600402

Appendix D
Sacred Land File Results



626 Wilshire Boulevard
Suite 1100
Los Angeles, CA 90017
213.599.4300 phone
213.599.4301 fax

www.esassoc.com

February 23, 2018

Gayle Totton
Native American Heritage Commission
1550 Harbor Boulevard, Suite 100
West Sacramento, CA 95691
FAX- 916-373-5471

Subject: SLF Search Request for 670 Mesquit Street Mixed Use Project (D170431.00)

Dear Ms. Totton:

Environmental Science Associates (ESA) is preparing a Cultural Resources Assessment in support of an Environmental Impact Report (EIR) for the 670 Mesquit Street Mixed Use Project (Project). The Project is located in the Arts District of downtown Los Angeles on the east side of Mesquit Street, between Jesse Street and East 7th Street (Project area).

The Project will construct a new mixed-use development totaling approximately 1,792,103 square feet of floor area on approximately 5.45 acres. The development would include creative office space, multi-family residential housing, hotel; and a range of commercial uses including retail uses (grocery and farmer's market); restaurants; studio/event/gallery space and a potential museum; and a gym.

The Project would also include at- and above-grade landscaped open space, including recreational amenities. Four levels of below-grade parking spanning the Project area, and at and above grade parking, containing a total of approximately 2,000 vehicle parking spaces and 930 bicycle parking spaces. A rooftop heliport is also proposed for emergency and occasional use incidental to residential and office uses, providing an amenity for the Project's residents, hotel guests, office workers, and visitors.

The Project could also include a cantilevered pedestrian deck (Deck) over a portion of the railway property (Railway Property) to the east of the Project area.

The attached map depicts the Project area located in a portion of the Los Angeles 7.5-minute USGS quadrangle, Township 1S and 2S, Range 13W.

In an effort to provide an adequate appraisal of all potential impacts to cultural resources that may result from the proposed Project, ESA is requesting that a records search be conducted for sacred lands or traditional cultural properties that may exist within the Project area.



626 Wilshire Boulevard
Suite 1100
Los Angeles, CA 90017
213.599.4300 phone
213.599.4301 fax

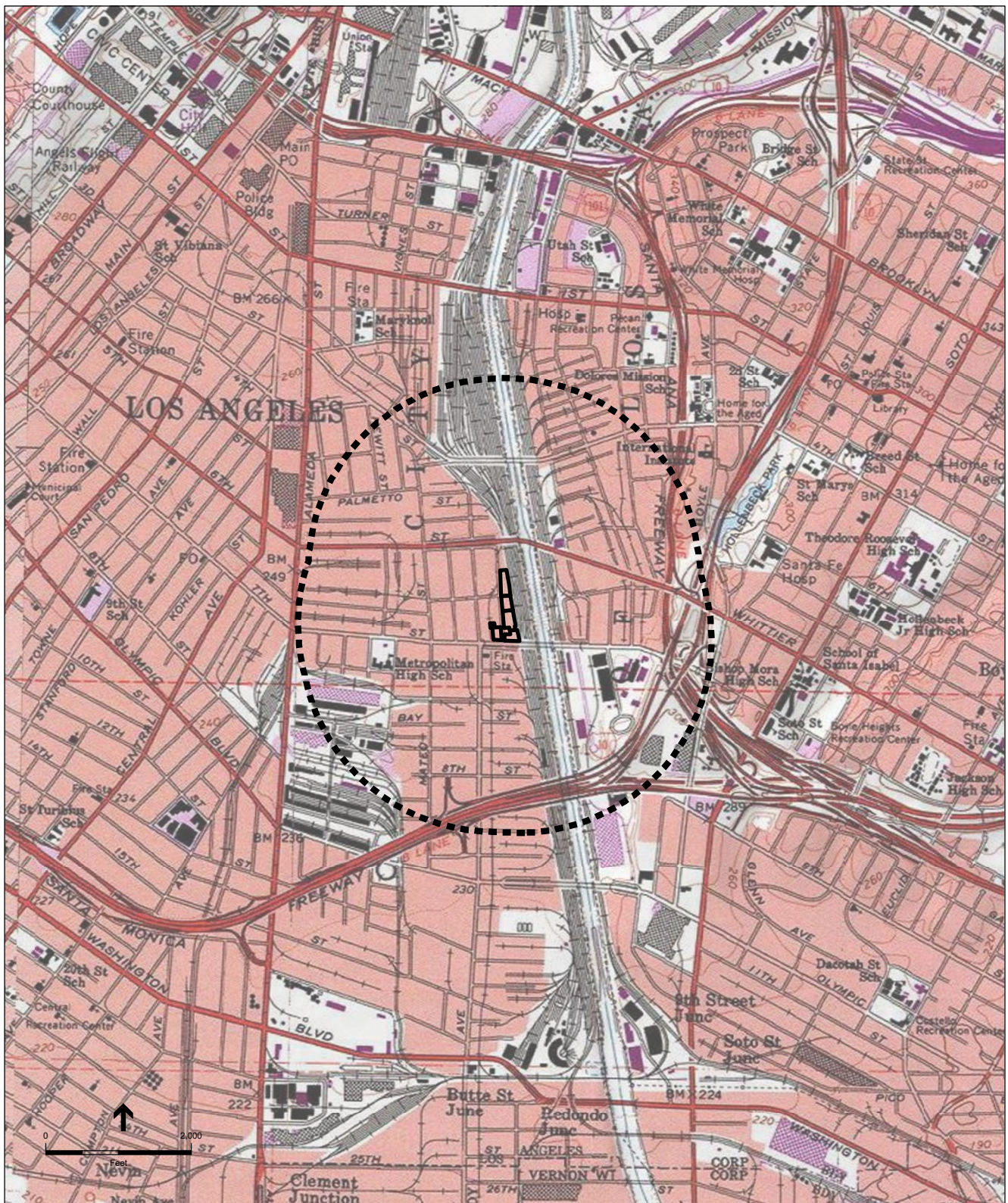
www.esassoc.com

Thank you for your time and cooperation regarding this matter. To expedite the delivery of search results, please fax them to 213.599.4301. Please contact me at 323.246.6289 or vortiz@esassoc.com if you have any questions.

Sincerely,

A handwritten signature in blue ink that reads "Vanessa Ortiz". The signature is written in a cursive, flowing style.

Vanessa Ortiz, M.A., RPA
Cultural Resources Specialist



NATIVE AMERICAN HERITAGE COMMISSION

Cultural and Environmental Department
1550 Harbor Blvd., Suite 100
West Sacramento, CA 95691
(916) 373-3710



February 26, 2018

Vanessa Ortiz
Environmental Science Associates

Sent by E-mail: vortiz@esassoc.com

RE: Proposed 670 Mesquit Street Mixed Use (D170431.00) Project, City of Los Angeles; Los Angeles USGS Quadrangle, Los Angeles County, California

Dear Ms. Ortiz:

A record search of the Native American Heritage Commission (NAHC) *Sacred Lands File* was completed for the area of potential project effect (APE) referenced above with negative results however the area is sensitive for cultural resources. Please note that the absence of specific site information in the *Sacred Lands File* does not indicate the absence of Native American cultural resources in any APE.

Attached is a list of tribes culturally affiliated to the project area. I suggest you contact all of the listed Tribes. If they cannot supply information, they might recommend others with specific knowledge. The list should provide a starting place to locate areas of potential adverse impact within the APE. By contacting all those on the list, your organization will be better able to respond to claims of failure to consult. If a response has not been received within two weeks of notification, the NAHC requests that you follow-up with a telephone call to ensure that the project information has been received.

If you receive notification of change of addresses and phone numbers from any of these individuals or groups, please notify me. With your assistance we are able to assure that our lists contain current information. If you have any questions or need additional information, please contact via email: gayle.totton@nahc.ca.gov.

Sincerely,

Gayle Totton, M.A., PhD.
Associate Governmental Program Analyst
(916) 373-3714

CONFIDENTIALITY NOTICE: This communication with its contents may contain confidential and/or legally privileged information. It is solely for the use of the intended recipient(s). Unauthorized interception, review, use or disclosure is prohibited and may violate applicable laws including the Electronic Communications Privacy Act. If you are not the intended recipient, please contact the sender and destroy all copies of the communication.

Native American Heritage Commission
Native American Contact List
Los Angeles County
2/26/2018

**Gabrieleno Band of Mission
Indians - Kizh Nation**

Andrew Salas, Chairperson
P.O. Box 393 Gabrieleno
Covina, CA, 91723
Phone: (626) 926 - 4131
admin@gabrielenoindians.org

**Gabrieleno/Tongva San Gabriel
Band of Mission Indians**

Anthony Morales, Chairperson
P.O. Box 693 Gabrieleno
San Gabriel, CA, 91778
Phone: (626) 483 - 3564
Fax: (626) 286-1262
GTTribalcouncil@aol.com

Gabrielino /Tongva Nation

Sandonne Goad, Chairperson
106 1/2 Judge John Aiso St., Gabrielino
#231
Los Angeles, CA, 90012
Phone: (951) 807 - 0479
sgoad@gabrielino-tongva.com

**Gabrielino Tongva Indians of
California Tribal Council**

Robert Dorame, Chairperson
P.O. Box 490 Gabrielino
Bellflower, CA, 90707
Phone: (562) 761 - 6417
Fax: (562) 761-6417
gtongva@gmail.com

Gabrielino-Tongva Tribe

Charles Alvarez,
23454 Vanowen Street Gabrielino
West Hills, CA, 91307
Phone: (310) 403 - 6048
roadkingcharles@aol.com

This list is current only as of the date of this document. Distribution of this list does not relieve any person of statutory responsibility as defined in Section 7050.5 of the Health and Safety Code, Section 5097.94 of the Public Resource Section 5097.98 of the Public Resources Code.

This list is only applicable for contacting local Native Americans with regard to cultural resources assessment for the proposed 670 Mesquit Street Mixed Use Project, Los Angeles County.

Appendix E
DPR 523 Forms

PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code _____

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 14 *Resource Name or #: (Assigned by recorder) 639 S. Mesquit Street, Los Angeles

P1. Other Identifier: _____

*P2. Location: Not for Publication Unrestricted

*a. County Los Angeles and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad _____ Date _____ T _____; R _____; _____ of _____ of Sec _____; _____ B.M.

Address 639 S. Mesquit Street City Los Angeles Zip 90021

UTM: (Give more than one for large and/or linear resources) Zone 11S, 386533.42 mE/ 3766958.39 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, decimal degrees, etc., as appropriate)

APN: 5164-018-009

*P3a. **Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) 689 S. Mesquit Street, is a rectangular lot improved with a utilitarian industrial structure completed in 1963. The building has an irregular L-shaped footprint with multiple access points along the north elevation, including a pedestrian entrance and vehicle loading bays. Exterior walls are constructed of a combination of concrete blocks and reinforced concrete. The subject building has a flat combination roof with parapets extending above the roofline at various heights. The main entrance is located at the northeast corner of the primary (north) elevation. The building occupies half of the lot, with the other half reserved for vehicular parking and loading.

The primary (north) elevation is dominated by a sunken truck loading dock featuring three loading bays. Behind the loading dock is a large open warehouse used as a staging area for goods being shipped. To the left of the loading bays is the subject building's primary entrance. [See continuation sheets]

*P3b. **Resource Attributes:** (List attributes and codes) HP8 (Industrial building)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P4. **Resources Present:**

Building Structure Object
 Site District Element of District
 Other (Isolates, etc.)

P5b. Description of Photo: (view, date, accession #) View of the primary entrance on the north elevation, view south.

*P6. **Date Constructed/Age and Source:** Historic Prehistoric
 Both

1963/Los Angeles County Assessor

*P7. **Owner and Address:**

RCS VE LLC, 319 Lafayette Street, Suite 133, New York, NY 10012

*P8. **Recorded by:** (Name, affiliation, and address) ESA, 626 Wilshire Boulevard, #110, Los Angeles, CA, 90017

*P9. **Date Recorded:** December 2021

*P10. **Survey Type:** (Describe) Intensive Pedestrian

*P11. **Report Citation:** (Cite survey

report and other sources, or enter "none.")

ESA, 670 Mesquit Project, City of Los Angeles, California: Cultural Resources Assessment Report, Prepared for RCS VE LLC, September 2021.

*Attachments: NONE Location Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

*Resource Name or # (Assigned by recorder) 639 S. Mesquit Street, Los Angeles *NRHP Status Code 6Z
 Page 2 of 14

B1. Historic Name: _____
 B2. Common Name: 639 S. Mesquit Street, Los Angeles
 B3. Original Use: Industrial B4. Present Use: Industrial

*B5. Architectural Style: Mid-Century Modern

*B6. Construction History: (Construction date, alterations, and date of alterations)
 The subject parcel is currently improved with an existing concrete industrial manufacturing building at 689 Mesquite Street that was completed in 1963 for the West Coast Quilting Company and remodeled for use as cold storage warehouse in or by 2006.

In 1963, construction of the building currently within the Survey Area #1, 689 S. Mesquit Street, was completed for the West Coast Quilting Company, which used the factory for offices and manufacturing purposes. A Certificate of Occupancy (No. LA 4024-61) was issued June 20, 1963 to the West Coast Quilting Company, for a one-story, type III-B, 120-foot by 178-foot office and manufacturing facility with 41 parking spaces. [See Continuation Sheets]

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features:

B9a. Architect: G. H. Wyman b. Builder: N/A
 *B10. Significance: Theme Industrial Development Context, 1850-1980; Sub-Context Manufacturing for the Masses, 1887-1965; and Industrial-Manufacturing-Factory "factory" Property Type, 1887-1980 Area Central City North Community Plan Area

Period of Significance 1963-1980 Property Type Industrial Applicable Criteria 1,3
 (Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

. The West Coast Quilting Company factory (Rancho Cold Storage) building at 689 S. Mesquite Street. was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 and C/3/3 for its association with the Sub-Context Manufacturing for the Masses (1887-1965) as applied to the Factory Property Type (1887-1990). Originally constructed for the West Coast Quilting Company, the existing building currently does not retain any characteristic features that convey its original factory use. Furthermore, research results do not indicate that the West Coast Quilting Company was a historically significant manufacturer in Los Angeles. The original industrial manufacturing building was substantially altered when it was converted from a factory to a cold storage warehouse in or by 2006 and it does not currently have intact character-defining features and integrity to convey its original function as a manufacturing facility. [See Continuation Sheets]

B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:
 [See Continuation Sheets]

B13. Remarks:

*B14. Evaluator: ESA
 *Date of Evaluation: December 2021

(This space reserved for official comments.)



CONTINUATION SHEET

Property Name: 639 S. Mesquit Street, Los Angeles
Page 3 of 14

*P3a. Description (continued):

The main entry, which leads to the building's office spaces, exhibits some minor decorative articulation characteristic of the of Mid-Century Modern Style that was popular at the time of the building's construction in 1963. This portion of the façade features a decorative parapet wall, made up of stacked brick (painted over), extending from the ground to beyond the building's roofline. A flat porch canopy extends from the building's façade, supported by three large projecting L-shaped metal posts and beams (later alteration).

The posts and beams connect the building's façade with a small landscape feature (concrete planter). Together, the porch, canopy, planter, posts, and beams create a small outdoor foyer or patio. The main entry consists of a fully glazed aluminum framed door with sidelights and transoms and is fortified with metal security bars and screens (alteration). The main entry is the only part of the building that is articulated in a decorative manner, as it was obviously intended to serve as the main point of contact with the public; the rest of the building is articulated in a utilitarian manner.

On the right side of the primary (north) elevation is a protruding mass extending northward, giving the building its L-shaped footprint. While the majority of the building is constructed out of reinforced concrete, the protruding mass is made up of concrete blocks. This section of the building houses the main cold storage areas. Along the east side of the mass is a metal canopy constructed out of steel I-beams with a corrugated steel roof (alteration). The canopy provides shade and shelter for a concrete path leading to two industrial size openings, one of which leads to the interior of the concrete block mass, while the other opening leads to the interior of the reinforced concrete warehouse. A metal railing (alteration) separates the concrete path from the adjacent sunken loading bays. Fenestration on the mass includes three small industrial style windows with metal security bars and screens (alterations).

The east elevation features tilt-up concrete construction with decorative textured concrete closer to the building's main entry (north end of the east elevation). The building possesses few architectural features along the east façade, exhibiting its industrial utilitarian design. Secondary entrances to the building are located near the south end of the east elevation and include a pedestrian entry with a single metal door and a larger vehicle entry with a metal roll-up door. Fenestration along the east elevation consists of three moderate sized window openings with security screens (alteration). The window openings have been boarded up (alteration), most likely due to the building's conversion from a factory to a cold storage warehouse.

The building's south elevation does not contain any significant architectural features. It is clad with concrete and there are no entryways or fenestration. The west elevations were not accessible during the site survey. This elevation adjoins the neighboring lot, which is private property and admission to that property was not granted during the survey.

Originally constructed as factory for a quilt manufacturing company, the building shows signs of alteration in the interior, converting it for its current use as a cold storage warehouse. The interior is dominated by a large open area where goods preparing to be shipped are staged. The open area has been divided by

CONTINUATION SHEET

Property Name: | 639 S. Mesquit Street, Los Angeles |
Page | 4 | of | 14 |

new interior partitions, creating office spaces and cold storage areas. The construction of these partitions is not documented in building permits. The ceiling of the interior has been insulated to accommodate the building's use as for cold storage.

P5a. Photographs (continued):



Primary (north) elevation, view south



Closer view of the entryway on the primary (north) elevation, view south

CONTINUATION SHEET

Property Name: | 639 S. Mesquit Street, Los Angeles |
Page | 5 | of | 14 |



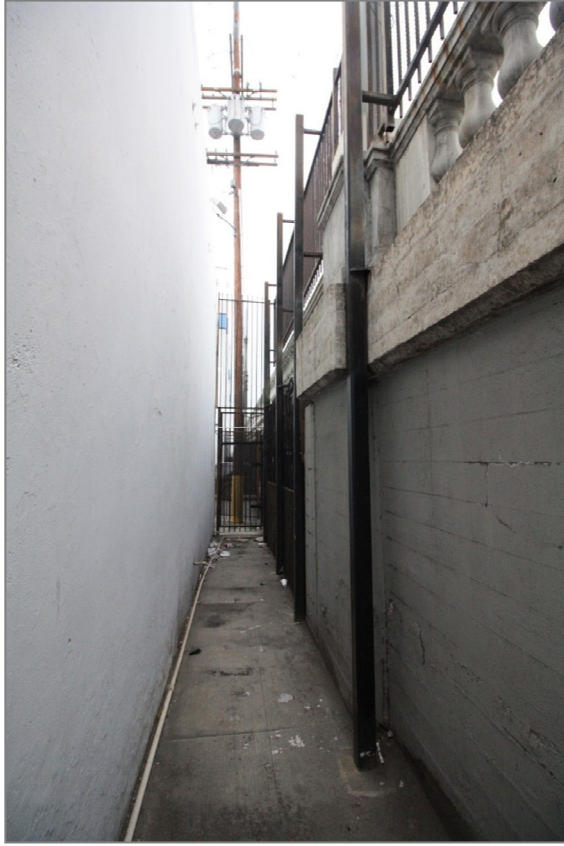
View of the building's projecting mass, north elevation, view south



Secondary (east) elevation, view southwest

CONTINUATION SHEET

Property Name: | 639 S. Mesquit Street, Los Angeles |
Page | 6 | of | 14 |



Overview of the South Elevation, view east

***B6. Construction History (continued):**

Further research on the history of the West Coast Quilting Company did not reveal any specific information about the company or the products they produced. On June 2, 2006, a building permit was issued to owner Frank H. Gallo and applicant Stuart Baltz, for removal of the existing roof materials and installation of a new spray polyurethane foam and elastomeric coating roofing system, providing insulation supporting the building's current use as a cold storage warehouse. The building was likely modified for use as a cold storage warehouse at or by this time (2006).

***B6. Significance (continued):**

The building is presently used by Rancho Cold Storage as a staging area for frozen and refrigerated goods. There is no manufacturing occurring in the building at this time. It is a common example of a utilitarian industrial factory building that has some minor articulation of the front entry to the building in a Mid-Century Modern style. Substantially altered, it is not considered an excellent example of its property type, nor is the building a representative example of a particular style since it is largely utilitarian—save for a few decorative elements at its entry. Furthermore, it does not appear to be the work of a master architect or builder. Therefore, due to lack of significance and integrity, the existing utilitarian industrial building at 689 S.

CONTINUATION SHEET

Property Name: | 639 S. Mesquit Street, Los Angeles |
Page | 7 | of | 14 |

Mesquite Street in Survey Area #1 (APN 5461-018-009) appears to be ineligible as a historical resource under CEQA.

***B12. References (continued):**

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CONTINUATION SHEET

Property Name: 639 S. Mesquit Street, Los Angeles
Page 8 of 14

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City of Los Angeles. 1994. Seventh Street Viaduct Over Los Angeles River Seismic Retrofit. W.O. E6000371. "As-Built" plans, including crack and spall repair details and structural details. Clark W. Robins, Div. Dist. Engineer, L. Lawrence Lewis, Deputy Engineer, Robert S. Horn, City Engineer, approved 10/28/1994.

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CONTINUATION SHEET

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Page 9 of 14 |

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State of California The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code _____

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2 *Resource Name or #: (Assigned by recorder) 2143 E. 7th Street, Los Angeles

P1. Other Identifier: _____

*P2. Location: Not for Publication Unrestricted

*a. County Los Angeles and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad _____ Date _____ T ____; R ____; ____ of ____ of Sec ____; ____ B.M.

Address 2143 E. 7th Street City Los Angeles Zip 90021

UTM: (Give more than one for large and/or linear resources) Zone 11S, 386579.75 mE/ 3766707.27 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, decimal degrees, etc., as appropriate)

APN: 5164-017-008

*P3a. **Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) 2143 E. 7th Street [APN 5164-017-008] contains a cold storage warehouse built in 2002. The structure measures 193' x 148' and is constructed of reinforced tile-up concrete and a flat roof. The building does not have windows and includes a loading dock in the northeast portion.

*P3b. **Resource Attributes:** (List attributes and codes) HP8 (Industrial building)

*P4. Resources Present:

Building Structure Object Site District Element of District
 Other (Isolates, etc.)

P5b. Description of Photo: (view, date, accession #) 2143 E. 7th Street, GoogleMaps 2021, view northeast

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P6. **Date Constructed/Age and Source:** Historic Prehistoric

Both 2002/LADBS

*P7. **Owner and Address:** RCS VE LLC, 319 Lafayette Street, Suite 133, New York, NY 10012

*P8. **Recorded by:** (Name, affiliation, and address) ESA, 626 Wilshire Boulevard, #110, Los Angeles, CA, 90017

*P9. **Date Recorded:** December 2021

*P10. **Survey Type:** (Describe) Intensive Pedestrian

*P11. **Report Citation:** (Cite survey report and other sources, or enter "none.") ESA, ESA, 670 Mesquit Project, City of Los Angeles, California: Cultural Resources Assessment Report, Prepared for RCS VE LLC, September 2021.

*Attachments: NONE Location Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

*Resource Name or # (Assigned by recorder) 2143 E. 7th Street, Los Angeles *NRHP Status Code 6Z
 Page 2 of 2

B1. Historic Name: _____
 B2. Common Name: 2143 E. 7th Street
 B3. Original Use: Industrial B4. Present Use: Industrial
 *B5. Architectural Style: Contemporary Industrial

*B6. Construction History: (Construction date, alterations, and date of alterations)
 The most recent building constructed on the Project Site is a cold storage warehouse built in 2002 located in Survey Area #2, according to a Certificate of Occupancy (COA) on file with the Los Angeles Department of Building and Safety (LADBS). Aerial images from 1994 and 2002 confirm the construction date presented in the COA.

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____
 *B8. Related Features:

B9a. Architect: None b. Builder: None

*B10. Significance: Theme N/A Area -
 Period of Significance N/A Property Type Industrial Applicable Criteria N/A
 (Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The National Register requires that properties be at least 50 years of age to be eligible, while the California Register recommends evaluation for properties 45 years or older. Since this building was constructed in 2002, it does not meet the age thresholds for consideration as a historic resource under the National Register (50 years) or California Register (45 years), and therefore is not eligible for listing on either register. Also, as stated in the Regulatory Setting, pursuant to the National Park Service Criteria Consideration G, properties of less than 50 years of age may be eligible for listing on the National Register if it can be clearly established that they are of exceptional significance. However, the building in Survey Area 2 is a utilitarian cold storage warehouse and does not possess exceptional significance to qualify it under Criteria Consideration G. Therefore, the existing cold storage warehouse building at 2143 E. 7th Street does not meet the age or significance requirements for designation and is found ineligible as a historical resource under CEQA.

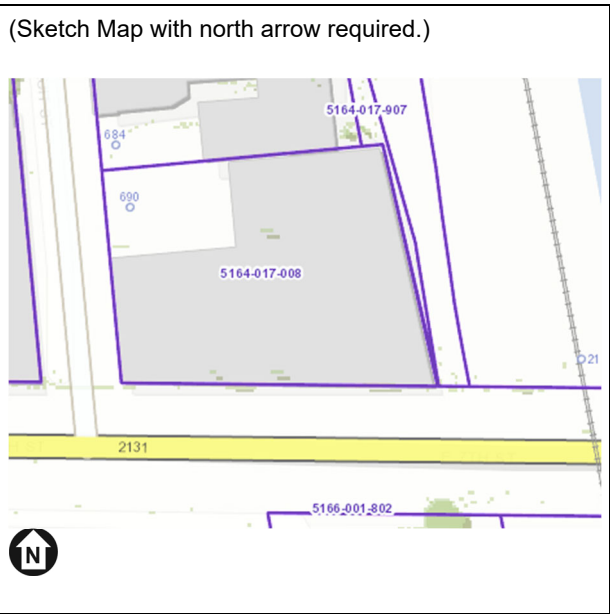
B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:
LADBS building permits and COA documents; aerial photographs 1994, and 2002

B13. Remarks:

*B14. Evaluator: ESA
 *Date of Evaluation: December 2021

(This space reserved for official comments.)



State of California The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code _____

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 16 *Resource Name or #: (Assigned by recorder) 642 and 674 S. Mesquit Street

P1. Other Identifier: _____

*P2. Location: Not for Publication Unrestricted

*a. County Los Angeles and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad _____ Date _____ T _____; R _____; _____ of _____ of Sec _____; _____ B.M.

Address 642 and 674 S. Mesquit Street City Los Angeles Zip 90021

UTM: (Give more than one for large and/or linear resources) Zone 11 S, 386558.82 mE/ 3766813.76 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, decimal degrees, etc., as appropriate)

APN: 5164-017-003, 5164-017-002, and 5164-017-006

*P3a. **Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) Encompassing an irregular lot spanning three parcels, 642 and 674 S. Mesquit Street (APNs:5164-017-003, 5164-017-002, and 5164-017-006), are occupied by a complex of 10 buildings constructed between 1908 and 1985 with later alterations and additions that now appear as a single building. Sections of the building range between one-, two-, and three-stories in height and form an irregular rectangular footprint. The building has been divided into multiple cold storage warehouses, which have been connected at various points in the site's history. The period during which an addition was constructed and the function of each space dictate the associated outward appearance, including such features as wall heights and materials. Exterior walls are constructed of concrete blocks or reinforced concrete. The building has several roof types, including flat and arched roofs with parapets extending above the roofs at various heights. [See Continuation Sheets]

*P3b. **Resource Attributes:** (List attributes and codes) HP8 (Industrial building)

*P4. **Resources Present:**

Building Structure Object
 Site District Element of District
 Other (Isolates, etc.)

P5b. Description of Photo: (view, date, accession #) Primary (west) elevation, overview of the loading bays and primary entry into the company offices, view southeast

*P6. **Date Constructed/Age and Source:** Historic Prehistoric

Both
1908/LADBS Permits

*P7. **Owner and Address:** RCS VE LLC, 319 Lafayette Street, Suite 133, New York, NY 10012

*P8. **Recorded by:** (Name, affiliation, and address) ESA, 626 Wilshire Boulevard, #110, Los Angeles, CA, 90017

*P9. **Date Recorded:** July 2021

*P10. **Survey Type:** (Describe) Intensive Pedestrian

*P11. **Report Citation:** (Cite survey

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



report and other sources, or enter "none.")

ESA, 670 Mesquit Project, City of Los Angeles, California: Cultural Resources Assessment Report, Prepared for RCS VE LLC, September 2021.

*Attachments: NONE Location Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

BUILDING, STRUCTURE, AND OBJECT RECORD

*Resource Name or # (Assigned by recorder) 642 and 674 S. Mesquit Street *NRHP Status Code 7SQ; 6Z
Page 2 of 16

B1. Historic Name: _____

B2. Common Name: 642 and 674 S. Mesquit Street

B3. Original Use: Industrial B4. Present Use: Industrial

*B5. Architectural Style: Utilitarian Industrial

*B6. Construction History: (Construction date, alterations, and date of alterations)

On January 2, 1908 a permit (No. 9) was issued for construction of a new storage building for the Los Angeles Ice and Cold Storage Company at 674 to 678 Mesquit Street designed by Hunt, Eager & Burns architect, and constructed for a cost of \$15,000; the 80-foot by 96-foot building was four-stories tall and had a concrete foundation, blue brick front, brick cornices, iron stairways, wood columns, and a composition roof. On April 11, 1908, a permit (No. 1984) was issued to the Los Angeles Ice and Cold Storage Company to raise the ceiling and roof ten feet and replace burned portions of the one-story wood-frame 55-foot by 110-foot ice storage building at 672-674 Mesquit Street for a cost of \$3,000. On July 22, 1927, a permit (No. 20768) was issued to the Los Angeles Ice and Cold Storage Company to replace old wood platforms with concrete platforms and construct a steel canopy covered with galvanized iron for the existing 78-foot by 94-foot three-story cold storage building at 678 S. Mesquit Street for a cost of \$1,500. [See Continuation Sheets]

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features:

B9a. Architect: Hunt, Eager & Burns b. Builder: _____

*B10. Significance: Theme Los Angeles Ice and Cold Storage Company was evaluated under the Industrial Development Context (1850-1980); Theme, Agricultural Roots (1850-1945), Sub-Theme, From Farm to Market (1900-1960). Area Central City North Community Plan Area

Period of Significance 1900-1945 Property Type Industrial, Cold Storage Warehouse Applicable Criteria 1,3

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

One historic architectural resource located within the Project Site was included in SurveyLA Central City North Survey but found ineligible for designation due to lack of integrity: the Los Angeles Ice and Cold Storage Company (Rancho Cold Storage), 674-686 S. Mesquit Street. The property was assigned an 7SQ status code: Not eligible; individual property assessed for significance in accordance with SurveyLA but does not meet eligibility standards (SurveyLA, 2016; HistoricPlacesLA.org). ESA conducted further intensive-level survey and research on the cold storage building complex including the Los Angeles Ice and Cold Storage Company. The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) and the former fruit juice factory (now cold storage facility) in Survey Area #5 (APN: 5164-017-006) are attached structures that appear as one large building. [See Continuation Sheets]

B11. Additional Resource Attributes: (List attributes and codes) _____

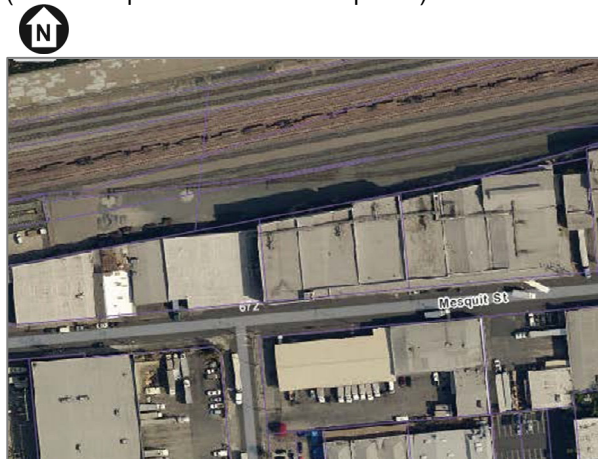
*B12. References:
[See Continuation Sheets]

B13. Remarks:

*B14. Evaluator: ESA
*Date of Evaluation: December 2021

(This space reserved for official comments.)

(Sketch Map with north arrow required.)



CONTINUATION SHEET

Property Name: 642 and 674 S. Mesquit Street

Page 3 of 16

*P3a. Description (continued):

The primary (west) elevation is an amalgamation of 10 buildings, and because of this the materials and roof height and type varies. According to LADBS permits and building plans provided by the property owner, a majority of the primary elevation was re-clad with textured concrete blocks (alteration) as part of a project to comply with the City of Los Angeles Division 88 ordinance.¹ These alterations occurred through a series of projects dating from 1989 through 1994 and resulted in a uniform appearance across seven of the 10 buildings.

The primary elevation is dominated by a partial length loading dock, covered by a long flat wood canopy supported by metal posts (alteration). The building's main entrance, which leads to the company's office spaces, is recessed beneath the canopy, where a set of concrete steps lead to the elevated loading dock and doorway. The main entrance consists of a single wood door with partial glazing. Additional points of entry consisting of large openings with metal roll-up doors (alterations) can be found along the loading dock on the east elevation. These opening provide access to the various cold storage spaces throughout the complex.

At the north end of the east elevation, there is a series of structures that have been combined through a series of alterations and additions. These portions of the building are constructed out of a variety of materials including tilt-up concrete, plaster, metal paneling, concrete block, and poured-in-place concrete. Roof profiles along this portion of the building vary, including pitched, flat, and shed roof types. A pedestrian entrance provides access to the cold storage area at the north end of the east elevation. Additional points of entry along this portion of the building include multiple vehicle loading bays.

The north elevation features a tilt-up concrete structure built in 1985 (alteration). This portion of the building also has a concrete block addition that is not documented in the LADBS records but appears to be a newer addition. An elevated concrete walkway, accessed via four concrete steps, leads to secondary entrances along the north elevation. The secondary entrances consist of a pedestrian entry with a single wood door and a large industrial entrance with a metal roll-up door. Additional access points near the north elevation consist of multiple vehicle loading bays.

The south elevation features a combination of concrete block and poured-in-place concrete construction. The wooden canopy that dominates the east elevation, wraps around to the south façade (alteration), providing shelter to two additional industrial scale openings. Extending from the south elevation is a concrete block garage area where equipment maintenance is performed (alteration). The south end of the building houses equipment to power the refrigeration needs of the cold storage facility. Due to the use of these spaces, the south elevation features multiple openings providing ventilation.

CONTINUATION SHEET

Property Name: 642 and 674 S. Mesquit Street

Page 4 of 16

The rear (east) elevation contains no specific architectural features. Historic Sanborn maps show the east side of the building fronting a rail spur; however, the rail spur is no longer present and any aspect of the building that interacted with the adjacent railyard has been altered. The east elevation is clad with a combination of concrete block and poured-in-place concrete. A new concrete wall (alteration) and metal fencing (alteration) separate the property from the adjacent railyard. There is a single point of entry, featuring a metal roll-up door on the east elevation (alteration).

The building's interior is where the most evidence of the building's construction history can be observed. Portions of the building's interior spaces contain the original wood framing and unreinforced masonry. Other sections of the building demonstrate more modern construction techniques that have evolved throughout the building's existence. In one section of the cold storage facility, columns made of poured-in-place concrete support heavy concrete ceilings. This area may be the result of a building permit from 1932 that describes efforts to increase the load of the upper floors.

P5a. Photographs (continued):



Primary (west) elevation, view of additional loading bays, view northeast

CONTINUATION SHEET

Property Name: | 642 and 674 S. Mesquit Street |
Page | 5 | of | 16 |



Northern section of the building's primary (west) elevation, view northeast



Tilt-up concrete addition constructed in 1985, view southeast

CONTINUATION SHEET

Property Name: 642 and 674 S. Mesquit Street

Page 6 of 16



The building's south elevation featuring wrap around canopy, view northeast



Rear (east) elevation with concrete block exterior and new concrete wall to the east, view north

***B6. Construction History (continued):**

On July 22, 1927, a permit (No. 20769) was issued to the Los Angeles Ice and Cold Storage Company for a new concrete floor for the existing three-story, three-room, 50-foot by 110-foot cold storage building at 680 Mesquit Street for a cost of \$800. On March 28, 1933, a permit (No. 4113) was issued to the California Consumers Corp. to "tear down then replace fire walls facing Mesquit St. and one section of fire wall on east end – wall will extend 2 feet above roof line – quake damage" of the present factory at 680 Mesquit Street at a cost of \$1,000, completed by Nance Construction Company, contractor. On April 4, 1933, a permit (No. 4605) was issued to California Consumers Corp. to demolish and replace 30 feet of

CONTINUATION SHEET

Property Name: 642 and 674 S. Mesquit Street

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present front wall at the south end of the building and install new posts under the ends of two trusses for the one-story brick storage building at 680 Mesquit Street for a cost of \$500. On August 29, 1944, a permit (No. 6394) was issued to California Consumers Corp. for the re-roof of the building at 680 Mesquit Street by Owen Roofing Co. On March 6, 1951, a permit (No. 2182) was issued to California Consumers Corp. to replace a portion of the cracked masonry wall of the 133-foot by 160-foot one-story ice manufacturing, storage, and office building at 682 Mesquit Street, by engineer Geo. J. Fosdyk, and G. O. Gartz Construction Co. In July 1952, a demolition permit (No. 38819) was issued to California Consumers Corp. for El Rancho Lumber Company to remove the wood frame and corrugated iron 45-foot by 75-foot warehouse at 678 S. Mesquit Street, for a cost of \$270. On February 2, 1954, and permit (No. 79024) was issued to California Consumers Corp. and contractor G. O. Gartz Construction Co. to close two doorways in a masonry wall, resurface floor, add office partitions and ceiling, add toilet rooms, and add air conditioning for a 63-foot by 145-foot brick warehouse at 674 Mesquit Street for a cost of \$6,200. On July 15, 1965, a permit (No. 99824) was issued to Rancho Cold Storage c/o Harley R. Tulpis for parapet correction for a one-story cold storage building at 684 S. Mesquit Street, for a cost of \$300. Also on July 15, 1965, a second permit (No. 99827) was issued to Rancho Cold Storage for parapet correction for a cold storage building at 678-82 S. Mesquit Street, Building "A". On July 15, 1965, a third permit (No. 99828) was issued to Rancho Cold Storage for parapet correction for a cold storage building at 676 S. Mesquit Street. On August 18, 1967, a permit (No. 51570) was issued to Jack Levine for a parapet correction for the existing water cooling tower at 684 S. Mesquit Street, a 30-foot by 55-foot one-story brick building. On February 21, 1990, a permit (No. 4582) was issued to Don Gallo for full compliance Division 88, RCIII-B (structural reinforcement) for the cold storage/office at 678-682 Mesquit Street, and Building A, 164-foot by 150-foot, by engineer Merdad Houriani, for a cost of \$280,000. On April 3, 1990, a permit (No. 5425) was issued to Don Gallo for demolition by hand wrecking of a 35-foot by 55-foot unreinforced masonry (URM) warehouse at 684 Mesquit Street for a cost of \$7,700. On April 10, 1990, permit (No. 54366) was issued to Don Gallo to construct a new 35-foot by 55-foot one-story concrete-block cold storage building for a cost of \$100,000 at 684 Mesquit Street by engineer Ining Lu and architect Frank Gallo. On August 17, 1990, a permit (No. 61752) was issued to Don Gallo to change the roof system to Kim truss for the cold storage building at 684 Mesquit Street by engineer Ining Lu and architect Frank Gallo, owner-builder. On January 15, 1991, a permit was issued to Don Gallo for full compliance with Division 88 (change of engineer) for a cost of \$280,000, for a 164-foot by 150-foot warehouse/office at 682 Mesquit and Building A by engineer Chester Schultz and architect Frank Gallo. On May 2, 1991, a permit (No. 12329) was issued to Don Gallo for change of roof design (85-foot by 48-foot) for the 164-foot by 150-foot URM cold office/storage at 682 Mesquit Street Building A by engineer Chester Schultz, architect Frank Gallo, and contractor Gangi Builders for a cost of \$30,000. On October 29, 1992, a permit (No. 20055) was issued to Rancho Cold Storage, Inc. to replace the east URM wall at 682 Mesquit Street with a new concrete block wall and replace south wood wall with steel frame and wood infill for a cost of \$35,000 by engineer Chester Shultz, architect Frank Gallo and contractor Gangi Builders.

LADBS Permits for Survey Area #4 (APN: 5164-017-002) begin in April of 1908 (Table C-4 in Appendix C of this report). The earliest permit on file was for lifting the roof of an existing building by 10 feet, similar to work completed at the same time in Survey Area #3. In 1912, the building was further modified, raising the second floor 4 feet and 4 inches to make room for the addition of a new floor between the first and second levels. In 1932, the building's structure and floor was strengthened to support greater loads. In the 1970s, a series of permits were filed to repair fire damage. The greatest alterations to structures in Survey

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Area #4 occurred between 1989 and 1994 when the building underwent structural upgrades in compliance with Division 88. The alterations replaced the original unreinforced masonry exterior with new textured concrete block. Like the buildings in Survey Area #3, these alterations completely rendered the existing buildings unrecognizable. Furthermore, the buildings in Survey Area #4 cannot be differentiated from the buildings in Survey Area #3 due to the new concrete block exterior walls and Division 88 modifications.

Improvements in Survey Area #5 (APN: 5164-017-006) began as early as 1909 with the construction of a single-family residence owned by the Atchison, Topeka, and Santa Fe (AT&SF) Railroad (Table C-5 in Appendix C of this report). The residence had been demolished by 1922 when a concentrated fruit juice factory was built by the Hyland Stanford Company on the site. The Hyland Stanford Company made significant contributions to the fruit processing industry through multiple patents registered in the company's name. Throughout the 20th century, the facilities were modified by subsequent owners including South Coast Packing Co and Rancho Cold Storage; the buildings were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold -storage facility that was constructed in 1985.

The buildings in Survey Area #3, #4, and #5 are examples of utilitarian cold storage warehouse facilities that are substantially renovated and no longer retain their original appearance. The firm of Kysor, Morgan, and Walls designed the original warehouses for the Los Angeles Ice and Cold Storage Company in 1887-1888. However, the warehouses were destroyed in a fire and the facility was reconstructed, therefore the existing improvements are not representative of this firm's work. After the original Los Angeles Ice and Cold Storage facilities were destroyed by fire in 1908, the company hired the firm of Hunt, Eager, and Burns to design their new facility. However, the concrete materials and construction techniques of the existing buildings on the Project Site are from (or have alterations from) the 1950s and 1960s, and as such their construction methods and appearance are characteristic of the modern post-World War II period. However, substantial alterations in 1990 including demolition of unreinforced masonry sections, full compliance Division 88 structural reinforcement improvements, and new construction have resulted in renovation of the entire complex.

***B6. Significance (continued):**

The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) associated with the Los Angeles Ice and Cold Storage Company was evaluated under the Industrial Development Context (1850-1980) for its potential eligibility under Criteria A/1/1 in association with the Theme, Agricultural Roots (1850-1945) and its associated Sub-Theme, From Farm to Market (1900-1960), and under Criteria C/3/3 as an example of an associated Property Type, Cold Storage Warehouse (1900-1945).

The former fruit juice factory in Survey Area #5 (APN: 5164-017-006) associated with the Hyland Stanford Company was found ineligible as a historical resource under CEQA because the factory was destroyed by fire and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use.

***B12. References (continued):**

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Appendix F
**SurveyLA Registration
Requirements**

Industrial Development, 1850-1980

CONTEXT:	Industrial Development, 1850-1980
SUB CONTEXT:	No Sub-context
THEME:	Agricultural Roots, 1850-1965
SUB THEME:	From Farm to Market, 1900-1960
PROPERTY TYPE:	Industrial
PROPERTY SUB TYPE:	Cold Storage Warehouse
GEOGRAPHIC LOCATION	Along historic railroad alignments, with a particular concentration on the east side of Downtown; later examples in Westchester/LAX.
AREA OF SIGNIFICANCE	Agriculture, Commerce
CRITERIA	A/1/1
PERIOD OF SIGNIFICANCE	1900 - 1945
ELIGIBILITY STANDARDS:	More research needed
	No Eligibility Standards checked
	Represents a key link between the collection of agricultural goods from farms, fisheries, and ranches and their distribution to produce markets and food processors
	Resource does not meet Eligibility Standards
	Was constructed between 1900 and 1945 as a cold storage warehouse
CHARACTER DEFINING/ASSOCIATIVE FEATURES:	Few or no window openings
	Frontage on a wide street for efficient truck circulation
	Generously sized loading dock, truck bays and vehicle turnaround space
	Loading dock(s) on one or more sides
	May have "dummy" windows to maintain continuity with the streetscape
	Property setting includes produce markets and/or fish processors (within a block)
	Retains most of the essential physical and character-defining features from the period of significance
	Utilitarian reinforced concrete building, 1-6 stories in height
INTEGRITY CONSIDERATIONS:	New refrigeration equipment may have replaced older equipment as long as it does not significantly alter the appearance of the property
	Should retain integrity of Location, Design, Setting, Materials, Feeling and Association

Industrial Development, 1850-1980

CONTEXT:	Industrial Development, 1850-1980
SUB CONTEXT:	Manufacturing for the Masses, 1883-1989
THEME:	Factories, 1887-1980
SUB THEME:	No SubTheme
PROPERTY TYPE:	Industrial
PROPERTY SUB TYPE:	Factory
GEOGRAPHIC LOCATION	Citywide, concentrations in SE LA, Downtown, Boyle Heights, Lincoln Heights, Atwater Village, Venice, Westchester, N. Hollywood, Van Nuys, Canoga Park, Sun Valley, Pacoima, Sylmar
AREA OF SIGNIFICANCE	Industry, Commerce, Social History
CRITERIA	A/1/1
PERIOD OF SIGNIFICANCE	1887 - 1980
ELIGIBILITY STANDARDS:	Constructed between 1887 and 1980 as a manufacturing plant
	Is a representative example of industrial design as defined in the Industrial Design and Engineering theme
	Is not a factory associated within the other themes relating to this sub-context
	More research needed
	No Eligibility Standards checked
	Resource does not meet Eligibility Standards
	Was a key factory for a company whose branding and/or products had a significant impact on 20th century social history (e.g. new technology, household name)
	Was closely associated with the early manufacture of new technologies in the late 19th/early 20th centuries (e.g. neon, plastic)
CHARACTER DEFINING/ASSOCIATIVE FEATURES:	For the National Register, a property must possess exceptional importance if less than 50 years of age
	May have programmatic elements on the facade that denote what was manufactured at the plant
	May possess branding or company logos on the building exterior
	May retain distinctive equipment or building elements that reflect a particular kind of manufacturing process
	More research needed
	No CDFs/Associative Features checked
	One or more related utilitarian industrial buildings
	Resource does not retain sufficient CDFs/Associative Features
	Retains most of the essential physical features from the period of significance
INTEGRITY CONSIDERATIONS:	Original use may have changed
	Setting may have changed from time of construction

Industrial Development, 1850-1980

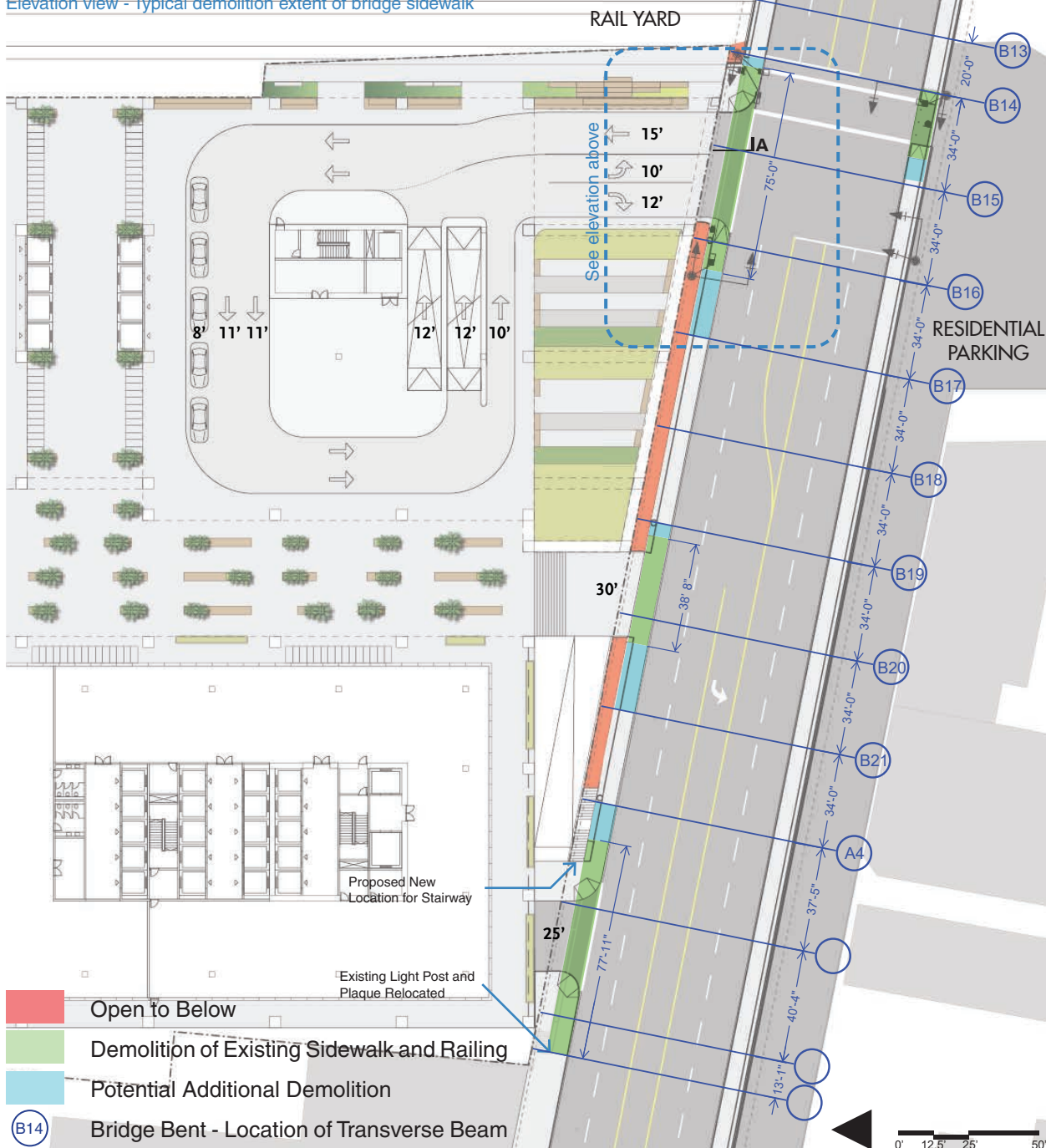
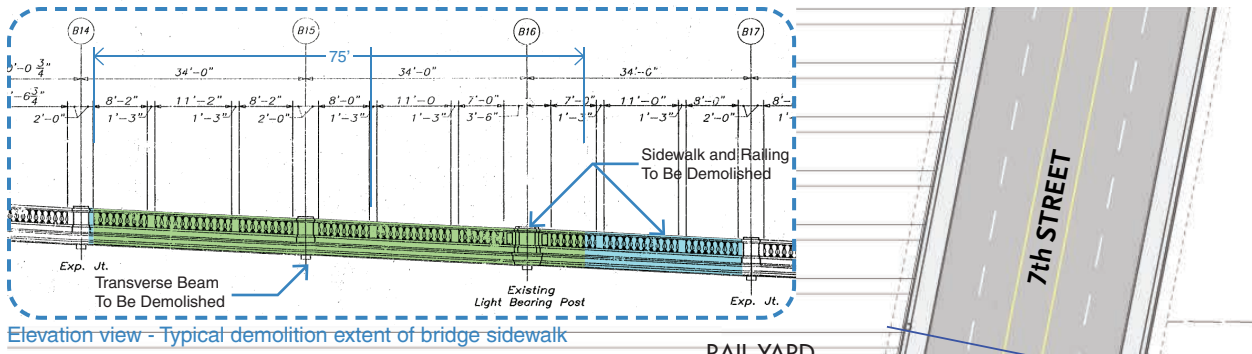
	Should retain integrity of Location, Design, Feeling, Materials and Association
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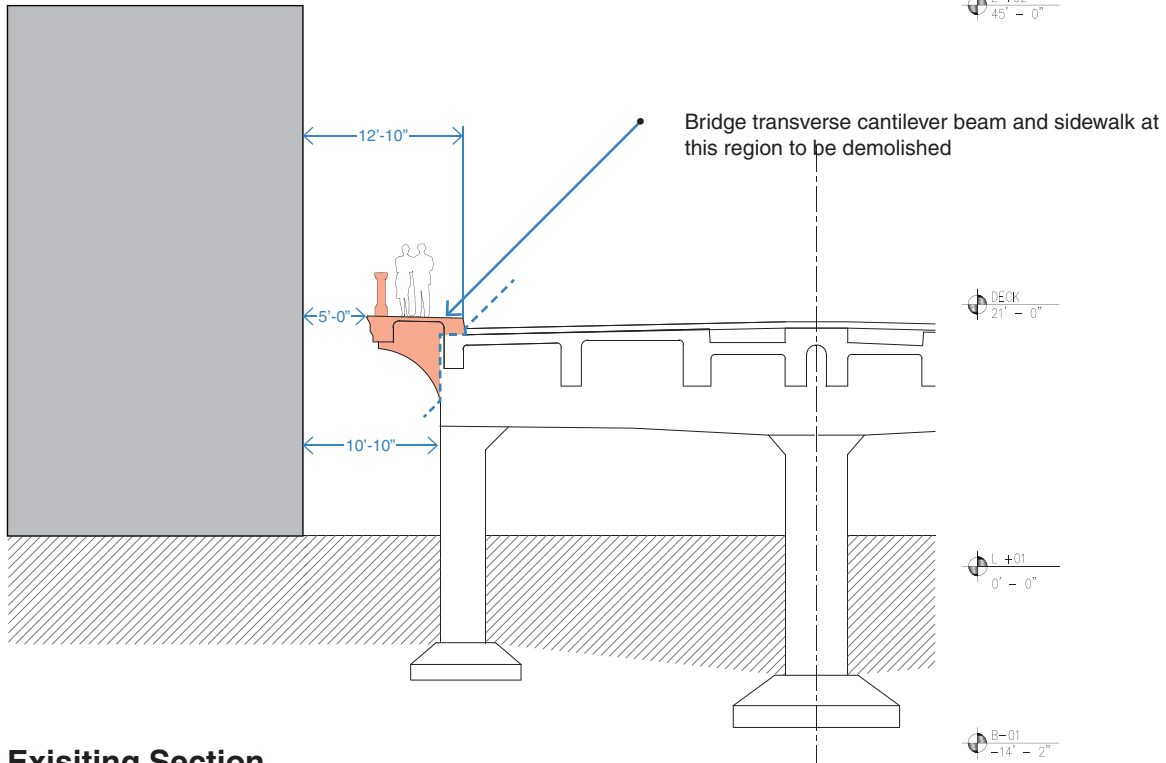
Architecture and Engineering, 1850-1980

CONTEXT:	Architecture and Engineering, 1850-1980
SUB CONTEXT:	L.A. Modernism, 1919-1980
THEME:	Post-War Modernism, 1946-1976
SUB THEME:	Mid-Century Modernism, 1945-1970
PROPERTY TYPE:	Industrial
PROPERTY SUB TYPE:	No Sub-Type
GEOGRAPHIC LOCATION	Citywide - known concentrations in Silverlake, Hollywood; Hollywood Hills; Brentwood, and communities largely developed in the post-World War II era, such as those in the South Bay (Playa del Rey, Westchester) and the San Fernando Valley
AREA OF SIGNIFICANCE	Architecture
CRITERIA	C/3/3
PERIOD OF SIGNIFICANCE	1945 - 1970
ELIGIBILITY STANDARDS:	Exhibits quality of design through distinctive features
	More research needed
	No Eligibility Standards checked
	Resource does not meet Eligibility Standards
	Retains the essential character defining features of Mid-Century Modernism from the period of significance
	Was constructed during the period of significance
CHARACTER DEFINING/ASSOCIATIVE FEATURES:	Direct expression of the structural system, often wood or steel post and beam
	Flat roof, at times with wide overhanging eaves
	Floor-to-ceiling windows, often flush-mounted metal framed
	For the National Register, property must possess exceptional importance if less than 50 years of age
	Horizontal massing
	If Expressionistic: sculptural forms intersecting with geometric volumes
	If Expressionistic: curved, sweeping wall surfaces
	If Expressionistic: dramatic roof forms, such as butterfly, A-frame, hyperbolic paraboloid, folded plate or barrell vault
	More research needed
	No CDFs/Associative Features checked
	Resource does not retain sufficient CDFs/Associative Features
	Simple, geometric volumes
	Unornamented wall surfaces
INTEGRITY CONSIDERATIONS:	Original garage doors may have been replaced

Appendix G

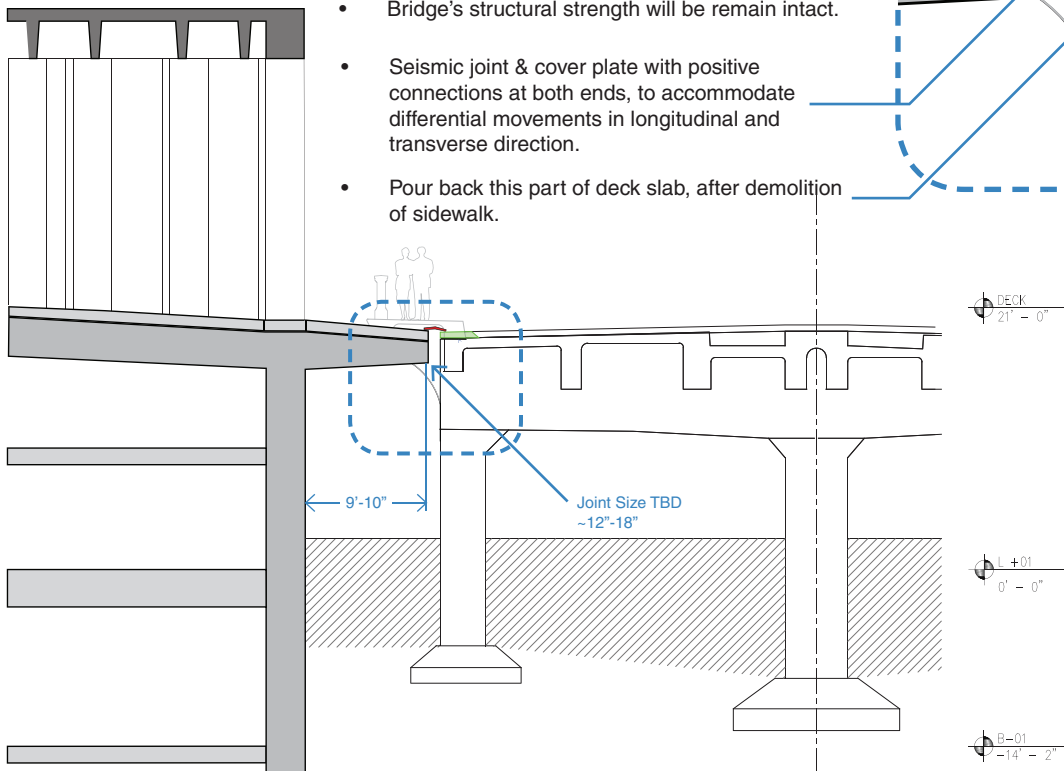
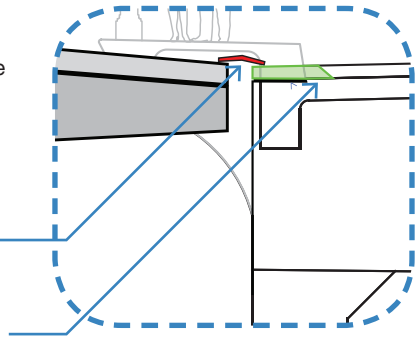
Bridge Connection Plans





Existing Section

- All additional traffic loads are applied to the building only, not to the bridge.
- The seismic joint decouples the movement of the two structures during a seismic event and thus prevents force transfer.
- Bridge's structural strength will remain intact.
- Seismic joint & cover plate with positive connections at both ends, to accommodate differential movements in longitudinal and transverse direction.
- Pour back this part of deck slab, after demolition of sidewalk.



Proposed Section