



3.0 PROJECT DESCRIPTION

This chapter of the Environmental Impact Report (EIR) describes the Development Project and Mt. San Jacinto College (MSJC) Entitlements which together comprise the overall action being considered (Project). In conformance with the *CEQA Guidelines*¹ and includes the Project's geographic setting, location, setting and boundaries, relevant General Plan and zoning designations, characteristics, and objectives, along with a description of the intended uses of this EIR, including a list of agencies anticipated to use this EIR and a list of discretionary actions required to implement the proposed Project. To provide accurate and understandable context, this Project Description provides information related to the existing environmental setting² of the property upon which the Project would be developed, including, but not limited to, the physical land use setting and the characteristics of adjacent properties. The Project Description provided in this EIR chapter is used as the basis for analyzing the proposed Project's impacts on the existing physical environment.

To assist the reader pertaining to the nomenclature used in this EIR, **Table 3.A: Standard References** provides a list of terms and phrases that will be used throughout the EIR in describing different Project components.

3.1 OVERVIEW

This EIR is being prepared to evaluate, at a project level, the environmental impacts associated with the proposed commercial and industrial development of a 533.8-acre property (Development Site) located in part in the City of Banning (City) and in part in the City's Sphere of Influence (SOI) in unincorporated Riverside County (County), California, which is proposed to be entitled through, among other things, rezoning and pre-zoning of the Development Site and adoption of the Sunset Crossroads Specific Plan (Specific Plan). This EIR analyzes the physical environmental effects associated with all components of the Development Project, including planning, grading, construction, and ongoing operation. Among other things, this rezoning would result in the reduction of the City's residential capacity by a maximum of 1,146 units. Therefore, this EIR also evaluates, at a programmatic level, the State law requirement set forth in Government Code Section 66300 that the City identify and rezone other land in the City to ensure no net loss of residential capacity. The City has identified an approximately 49.2-acre site located east of Sunset Avenue and south of Westward Avenue (MSJC Site) owned by the Mt. San Jacinto Community College District (MSJCCD)³ for that purpose. A portion of the MSJC Site houses the existing MSJCCD San Gorgonio Campus. To meet its no net loss obligations, the City has initiated an application to amend the general plan designation and rezone the MSJC Site to very high-density residential development, creating capacity for up to 1,181 units. No specific development project has been identified for the MSJC Site, and any plan for development would be subject to the requirement that a specific plan be adopted and further environmental review conducted.

¹ *CEQA Guidelines*, § 15124. This section requires technical, economic, and environmental characteristics of the project being studied.

² *CEQA Guidelines*, § 15125.

³ Section 17, Township 3 south, Range 1 East, SBBM.



Table 3.A: Standard References

| Standard Reference | Definition |
|---|---|
| Banning Electric Utility | The Banning Electric Utility. |
| Banning Electric Utility Electrical Substation or Electric Substation | The Banning Electric Utility (BEU) will develop a future electrical substation on 1 acre of land within the Development Site within Planning Area 7 (PA 7). This facility will be constructed, owned, operated, and maintained by BEU. |
| Development Project Entitlements | General Plan Amendment, Pre-Zoning and Zoning Amendment, and Adoption of Specific Plan; Annexation; Tract Maps; Development Agreement and other requested approvals as may be required from time to time for the Development Project. |
| Development Project | The Sunset Crossroads Specific Plan Project proposed for the Development Site and Development Project Entitlements, as further described in the project-level component of this Environmental Impact Report (EIR). |
| Development Site | The 533.8-acre site that is the location for the Development Project. |
| Reverse osmosis facility | A City built/owned/operated/maintained reverse osmosis facility, which is proposed to be located on an approximately 2.3-acre parcel on the Development Site. |
| Battery Energy Storage Systems (BESS) Facility ¹ | A privately owned and operated 65 megawatt-hours (MWh) battery energy storage facility proposed to be constructed on the Development Site to support storage of energy by the City of Banning Electric Utility. The facility would be a permitted use and may be constructed anywhere within the industrial zoned areas of the Development Site. However, it is anticipated to be located in proximity to the planned electrical substation located in PA 7. No off-site electricity transmission is anticipated from the BESS. |
| Existing natural drainages | The natural drainages on the Development Site, consisting of Pershing Creek, which flows in a northwest to southeast direction through the eastern portion of the Development Site (Planning Areas 13–15), Smith Creek, on the western portion of the Development Site, which flows in a north-south direction through Planning Areas 17 and 18, and Highland Wash, in Planning Area 19, which confluences with Smith Creek on the Development Site near Westward Avenue. Smith Creek continues in Planning Area 16 to the Development Site boundary. |
| Existing ROW | The 110-foot-wide right-of-way (ROW) owned by the City and County, the centerline of which forms the boundary between the land in the City and the land in the City's Sphere of Influence (SOI), which extends from Highland Home Road to Sunset Avenue. |
| Fueling Facility | An auto and commercial fueling station and carwash proposed to be located in the General Commercial portion of the Development Site (Planning Area 1) to include 20 vehicle fueling stations and 6 commercial fueling stations to accommodate recreational vehicles and multi-axle tractor trailers and electric vehicle charging stations. The fueling facility would accommodate four 20,000-gallon underground storage tanks (USTs). |
| Maximum Building Square Footage | The total square footage of all floors in structure as measured from either the interior surface of each exterior wall of the structure or, if the structure does not have walls, from each outer edge of the roof. |
| Maximum Coverage | The percentage of total lot area covered by buildings as measured by the total horizontal area of the building(s) calculated by the total square footage covered divided by the total lot area. |
| MSJC Entitlements | City initiated General Plan Amendment and Zone Change for the Mt. San Jacinto College (MSJC) Site to residential to ensure no net loss of residential capacity from implementation of the Development Project (in compliance with Government Code Section 66300), which are analyzed at a programmatic level in this EIR as no development plan is proposed at this time. |
| MSJC Site | The 49.2-acre site on the southeast corner of Sunset Avenue and Westward Avenue where the City proposes to modify the development standards, policies, and conditions to ensure no net loss in residential capacity as a result of implementation of the Development Project. |
| Northern Portion of Development Site | The 280.1 acres of the Development Site north of the Existing ROW. The Northern Portion of the Development Site is under the jurisdiction of the City. |
| Planning Area | A total of 19 areas established by the Specific Plan within the Development Site to differentiate the land uses on the Development Site for planning purposes. |



Table 3.A: Standard References

| Standard Reference | Definition |
|--|---|
| Project or proposed Project | The overall action, comprised of the Development Project for which the Development Project Applicant is the applicant and the MSJC Entitlements for which the City is the applicant. |
| Project Sites | The Development Site and MSJC Site, collectively. |
| Sphere of Influence or SOI | The plan for the probable physical boundaries and service area of the City of Banning. Land in the SOI is currently under the jurisdiction of unincorporated Riverside County. |
| Southern Portion of Development Site | The 253.7 acres of the Development Site south of the Existing ROW. The Southern Portion of the Development Site is currently under the jurisdiction of Riverside County and in the SOI of the City and is proposed to be annexed into the City as part of the Development Project. |
| Specific Plan | Sunset Crossroads Specific Plan. |
| Sun Lakes Boulevard Extension or SLB Extension | These terms refer to both (1) the approximately 21 acres of land owned by the Applicant and subject to an existing or future right-of-way easement in favor of the City (comprising the Existing ROW and land north of the Existing ROW) that together are the proposed location for the Sun Lakes Boulevard Extension, and (2) the future roadway to be constructed within that easement area, extending Sun Lakes Boulevard from its current terminus at Highland Home Road to the intersection of Westward Avenue and Sunset Avenue. The northern boundary of the SLB Extension forms the southerly boundary of the Northern Portion of the Development Site. The southern boundary of the SLB Extension (also the southern boundary of the Existing ROW) forms the northern boundary of the Southern Portion of the Development Site. |
| Sunset Crossroads or Sunset Crossroads Specific Plan Project | Proposed Name of Development Project. |
| Travel Center Retail Uses | One or more buildings in the Travel Center containing retail/convenience sales and a drive-through fast food restaurant (these uses are included in the 268,400 square feet of commercial square footage). |

Source: Compiled by LSA (July 2023).

¹ The Battery Energy Storage Systems Facility (BESS) use is not included in the Development Site square footage. The as energy storage capacity, rather than square footage, is the basis for measurement of such facilities.

City = City of Banning

County = County of Riverside

Detailed examination of the potential environmental impacts as a result of the Development Project is provided in **Chapter 4.0** of this EIR. In addition, a programmatic level analysis of potential environmental impacts pertaining to the application by the City for adoption of the MSJC Entitlements is provided in **Chapter 5.0** of this EIR.

3.1.1 Development Project and Development Site (Project-Level Review)

NP Banning Industrial, LLC (Development Project Applicant) is a private development entity that proposes to develop the Development Site with commercial and industrial uses. The northerly portion of the Development Site is approximately 280.1 acres (Northern Portion of the Development Site) and is located in the City, while the southerly portion of the Development Site (Southern Portion of the Development Site) is approximately 253.7 acres within the City's SOI in unincorporated Riverside County. Development of the Southern Portion of the Development Site pursuant to the Specific Plan requires annexation of this portion of the Development Site into the City. Development of the Northern Portion of the Development Site can proceed without annexation.



The Sunset Crossroads Specific Plan is proposed to establish City zoning regulations for commercial and industrial uses on the Development Site, including, but not limited to, allowed uses, intensity of use, building heights, setbacks, lot area and dimensions, parking, landscaping, and signage, as well as to identify on-site circulation and infrastructure improvements necessary to support the Development Project. The Specific Plan proposes to establish 19 Planning Areas consisting of the following:

- Up to 268,400 square feet of medical office, professional office, education, recreation, and commercial uses, including Travel Center Retail Uses, a Fueling Facility, and a hotel with 125 rooms (comprised of approximately 90,000 square feet) in a 47.9-acre area within the Northern Portion of the Development Site;⁴
- Up to 5,545,000 square feet of industrial land uses within a 392.0-acre portion of the Development Site including 330,000 square feet of cold storage uses;
- A 65 megawatt-hours (MWh) Battery Energy Storage Systems (BESS) facility comprised of main transformers, disconnect switches, breakers, and approximately 40-foot dead end, and an approximately 85-foot power pole, to be constructed by the Applicant in an industrially zoned portion of the Development Site, likely associated with the planned electrical substation in PA 7;
- 65.6 acres of land designated for Open Space – Resources (53.0 acres) and Open Space – Parks (12.6 acres); and
- Approximately 28.3 acres of internal circulation features.

Including the approximately 90,000-square-foot hotel, the total square footage of the Development Project would be approximately 5,903,400 square feet. As indicated above, the BESS facility is not included in this square footage calculation. The BESS facility would be associated with the planned electrical substation. No building facilities related to the BESS are anticipated; as such, no square footage associated with the BESS is included in the Development Project square footage calculation.

The Development Site is situated in an area previously part of the Stewart Ranch and adjacent to the west of the Barker Ranch; however, studies of adjacent properties indicate that at least a portion of the Barker Ranch extended west onto the Development Site. The results of the records search conducted for the Project identified 68 previously recorded resources (one prehistoric isolate and 67 historic sites, buildings, and features) within one mile of the Development Site. Of the resources identified within the records search, one, P-33-009176, is located within the MSJC Site, at the southeast corner of Sunset and Westward avenues. This site (P-33-009176) originally contained historic buildings associated with a ranch complex tied to the Barker Ranch which extended across Sunset Avenue into the Development Site. The Stewart Ranch complex was removed in the mid-1980s. On-site remnant features of the Barker Ranch were removed in the early 2000s to accommodate the installation of a City wastewater pipeline. The Development Site was previously considered for development in the mid-2000s under the Five Bridges Specific Plan, which envisioned development of a master-planned community of up to 2,160 residential units, a 51.6-acre commercial center and park and open space uses. Although the Five Bridges Specific Plan was not adopted, at

⁴ The 90,000 square feet of hotel use is not counted as part of Planning Area 1's 268,400 Maximum Building Square Footage because traffic analysis for hotels is calculated by number of rooms, not by square footage.



approximately that time, the City rezoned the Northern Portion of the Development Site for commercial and low-, moderate-, and high-density residential and open space uses, while providing an advisory general plan designation with similar uses for the Southern Portion of the Development Site (which is not under City jurisdiction). However, as the Five Bridges Specific Plan was never adopted, the specific changes in land use and the development associated with the Five Bridges Specific Plan were never realized on site.

With the approval of the Project, the City of Banning General Plan and zoning code would be amended to change the Development Site land uses from residential, commercial, and open space to industrial, commercial, and open space, and the City would adopt the Sunset Crossroads Specific Plan and pre-zone the Southern Portion of the Development Site. In addition, the Development Project Applicant will seek to have the Southern Portion of the Development Site annexed into the City of Banning through an action by the Riverside County Local Agency Formation Commission (LAFCO).

Modification of land use and zoning in the Northern Portion of the Development Site to industrial use would require that the City concurrently designate for residential use and rezone other land in the City in order to ensure there is no net loss in residential capacity in the City. Based on the allowed residential uses for the site as of January 1, 2018, the City has identified a maximum residential capacity of 1,146 units at the Development Site that must be concurrently designated elsewhere in the City to avoid a net loss of residential capacity pursuant to Government Code Section 66300 requirements (the Southern Portion of the Development Site is not under City jurisdiction and is not within an affected county as defined in Government Code Section 66300(a)(2)).⁵ A thorough description of the various Development Project components, extent, and intensity of development planned improvements and Development Project features is included below.

3.1.2 MSJC Entitlements

To avoid a net loss to the City of residential capacity that would result from City approval of the Development Project, the City has proposed a general plan amendment and rezoning of the MSJC Site, which adjoins the Development Site to the east, from its current general plan and zoning of Public Facilities to Very High Density Residential, allowing a maximum capacity of not less than 1,146 residential units. The contemplated rezoning would create a maximum capacity of 1,181 units on the MSJC Site. Despite this modification, development of the MSJC Site with residential uses is not currently contemplated by the MSJCCD, and a detailed project description is not available. Accordingly, **Chapter 5** of this EIR evaluates the MSJC Entitlements at a programmatic level.

3.1.3 Related Public Facilities Projects

The City has either previously approved or is in the process of considering various public improvements that may occur on or adjacent to the Development Site. Each of these has either undergone prior review or is under consideration by the City and would proceed with or without development of the Development Project. Accordingly, these public facilities are not analyzed as part

⁵ "Affected county" means a census-designated place, based on the 2013-2017 American Community Survey 5-year Estimates, that is wholly located within the boundaries of an urbanized area, as designated by the United States Census Bureau.



of the Development Project in this EIR but are treated as related projects, and their impacts are addressed cumulatively with those of the Project in **Chapter 6.0** of this EIR.

- **Electric Substation.** An approximately 1-acre area within the Development Site, at the northwest corner of Planning Area 7 (PA 7), has been identified by Banning Electric Utility (BEU) as a potential site for development of an electric substation to be developed by the BEU to support projected long-term growth anticipated by the City's existing General Plan. The Development Project would not require construction of this electrical substation; therefore, implementation of the Development Project is not reliant or dependent on development of this electrical substation. BEU is separately entitling and will develop, own, maintain, and operate the future electrical substation.
- **Potable Water Reservoir.** To serve the water storage requirements of the City and the approved Rancho San Gorgonio development, the City has identified an approximately 7.8-acre portion in Planning Area 3 (PA 3) of the Development Site, south of Westward Avenue, west of Sunset Avenue, and east of Pershing Creek for a proposed 1.5-million-gallon potable water reservoir required to be constructed at a certain elevation to facilitate gravity conveyance of potable water to that project (Potable Water Reservoir). The City and the Rancho San Gorgonio applicant will be responsible for the future development of the potable water reservoir. Based on the size of existing water reservoirs in the City, the 1.5 million gallon potable water reservoir will be approximately 100 feet in diameter and up to 26 feet in height. The need for additional water storage capacity was identified in the City's Integrated Master Plan (2018) and analyzed in the Rancho San Gorgonio (RSG) EIR, but the exact location was not identified at that time.
- **Reverse Osmosis Facility.** A City built/owned/operated/maintained reverse osmosis facility, which is proposed to be located on an approximately 2.3-acre parcel located in Planning Area 12 on the Development Site. The purpose of this facility is to reduce salt and nitrogen in the water prior to recharge of the aquifer.
- **Sun Lakes Boulevard Extension.** The City's General Plan Circulation Element identifies a future Arterial Highway to connect Sunset Avenue to Highland Home Road, bisecting the Development Site. In conformance with the City's existing circulation map, the City is currently planning construction of this four-lane divided roadway with two travel lanes in each direction, separated by a landscaped median, within the approximately 21 acre area (SLB Extension). This area is not within any of the Planning Areas in the Specific Plan and is labeled there as not a part of the development footprint. The City is currently processing approvals for this roadway and its construction as a City project.
- **Sunset Avenue Bridge.** Access on Sunset Avenue across the Pershing Wash drainage, south of Westward Avenue, is currently limited during rain events as Pershing Wash floods and is typically inaccessible by vehicle during flood events. Construction of a future bridge crossing in this location (Sunset Avenue Bridge) has been approved by the City as part of the Rancho San Gorgonio Specific Plan (RSGSP). This construction is under the control of the City and could be constructed by the City or third parties designated by the City.



In addition, at the City's request, the Specific Plan also identifies a site in Planning Area 12 that could be used for a future fire station. However, because neither the City nor the Riverside County Fire Department has considered nor identified a need for a fire station at this location, there is no current plan for development of a fire station and, if desired, the planning, design, and construction of any fire station would occur at an undetermined future date by the City and Riverside County Fire Department. Accordingly, development of a fire station at the Development Site is considered speculative and is not analyzed in this EIR. If the City and County Fire Department elect to proceed with a fire station at a future date, they would be responsible for the future environmental analysis and development of the fire station.

3.2 PROJECT LOCATION

3.2.1 Regional Location

The Development Site is in the City and in a portion of the City's SOI in unincorporated Riverside County. The City lies within the San Gorgonio Pass area, an east-west trending valley situated between the San Bernardino and San Jacinto Mountains. The City straddles Interstate 10 (I-10), a regionally and nationally important east-west transportation facility that connects the City to the greater Los Angeles area, including the Ports of Los Angeles and Long Beach, and to other major metropolitan areas (e.g., Phoenix, El Paso, San Antonio, Houston, Baton Rouge, Mobile, and Jacksonville) to the east. Regional connectivity is further provided by interchanges on I-10 connecting to State Routes (SR) 60, 62, 111, and 243, which provide access to Moreno Valley/Riverside, Yucca Valley/29 Palms, Palm Springs, and Idyllwild, respectively. Banning Municipal Airport is located approximately 3 miles east of the Development Site.

The City comprises a total of 14,823 acres, extending easterly from Highland Springs Avenue to Fields Road and from the San Bernardino County line on the north to Bobcat Road on the south. The City's SOI includes eight separate areas on the north and south ends of the City, totaling 5,436 acres.⁶ Within the San Gorgonio Pass region, the cities of Beaumont and Calimesa are located west of the City, while the unincorporated community of Cabazon and Morongo Band of Mission Indians tribal lands are located to the east. The Coachella Valley metropolitan area (the cities of Palm Springs, Indio, Palm Desert, Rancho Mirage, and others) is located farther east along I-10. The City's regional context is detailed in **Figure 3-1: Regional Location**.

3.2.2 Project Location

The location of the Project Site (Development Site and MSJC Site) is shown on **Figure 3-2: Project Location**.

⁶ City of Banning General Plan Land Use, III-1.



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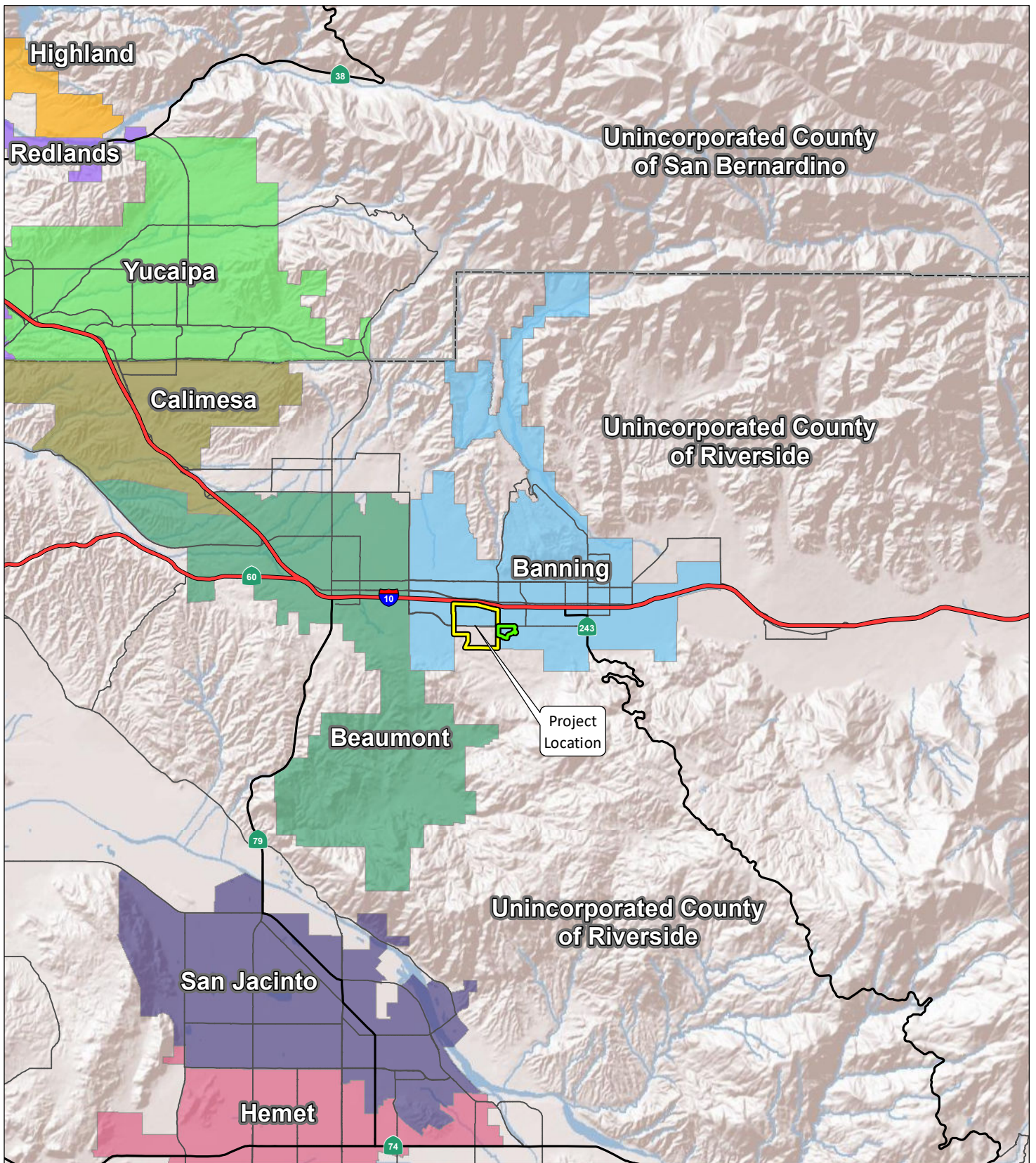
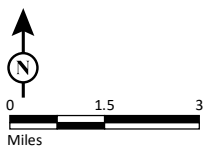


FIGURE 3-1

LSA

LEGEND

- Project Location
- MSJC Boundary



SOURCE: Esri (2021)

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Sunset Crossroads
Regional Location



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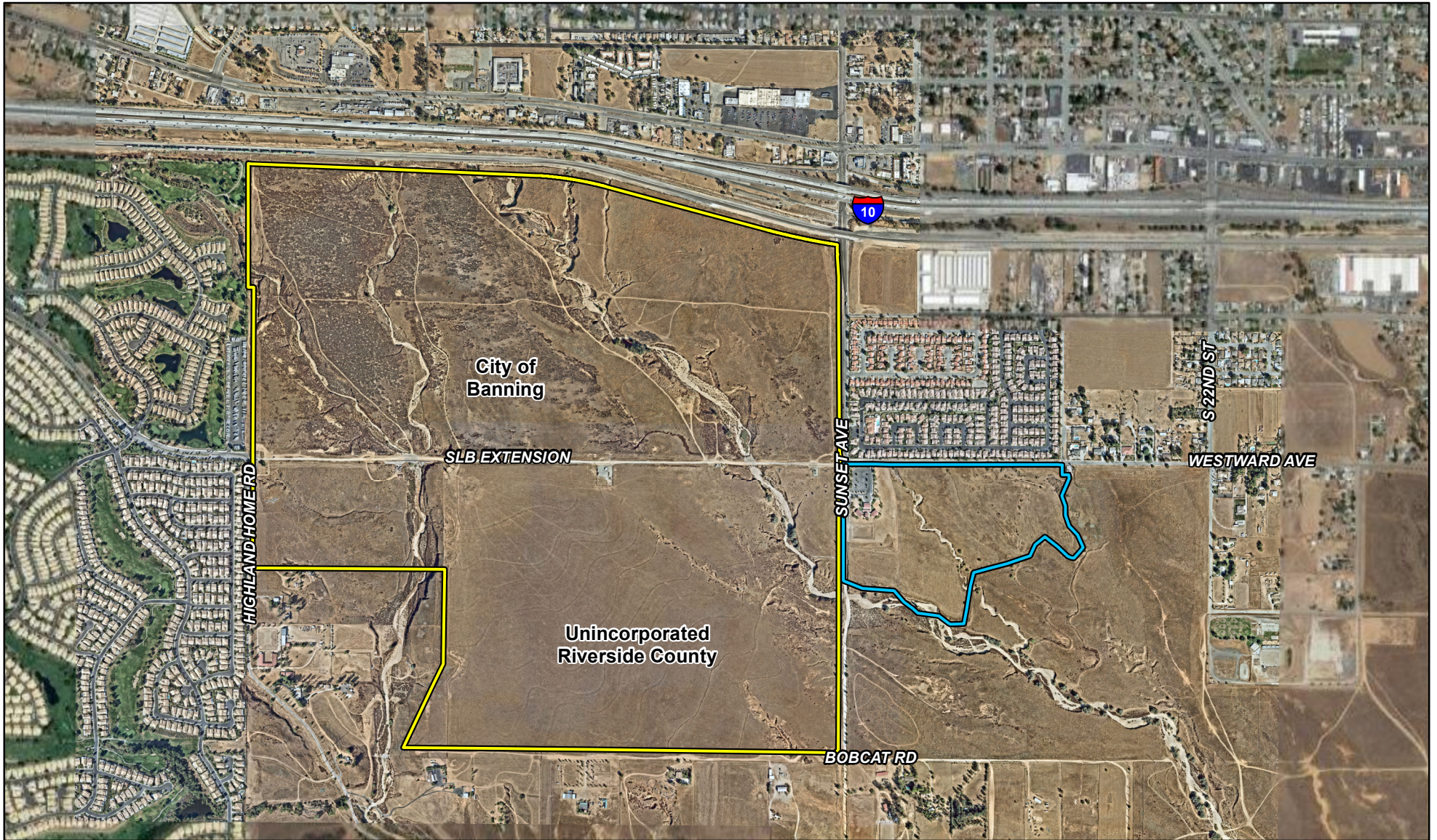


FIGURE 3-2

LEGEND

- Development Site
- MSJC Site



SOURCE: Nearmap Imagery (10/12/2020)

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3.2.2.1 Development Site

The Development Site consists of approximately 533.8 acres located south of I-10 and the Union Pacific Railroad (UPRR), between Highland Home Road and Sunset Avenue and north of Bobcat Road. The 280.1-acre (263.7 acres for development and 16.4 acres for circulation) Northern Portion of the Development Site is in the City of Banning, and the 253.7-acre (241.8 acres for development and 11.9 acres for circulation) Southern Portion of the Development Site is in the City's SOI in unincorporated Riverside County. The dividing line between land within the City and unincorporated County land is the centerline of the 110-foot existing Westward Avenue right-of-way (Existing ROW), which extends from Highland Home Road to Sunset Avenue. The City holds or intends to acquire various right-of-way easements over approximately 21 acres of land owned by the Applicant comprising the Existing ROW and land north of the Existing ROW that together are the proposed location for the Sun Lakes Boulevard Extension. An additional 0.8 acre of land south of the Existing ROW is owned by the City in fee and contains well houses operated by the City's Water Division. These areas, together comprising approximately 21.8 acres of land, are anticipated to have public uses, are not included in the Specific Plan Planning Areas, and are not a part of the Development Site or the Development Project.

The Development Site is crossed by various natural drainages. Pershing Creek flows in a northwest to southeast direction through the eastern portion of the Development Site (what will be Planning Areas 13–15 of the Specific Plan). On the western portion of the Development Site, Smith Creek flows in a north-south direction through what will be Planning Areas 17 and 18 of the Specific Plan. Highland Wash (what will be Planning Area 19) confluences with Smith Creek near Westward Avenue. Smith Creek continues in what will be Planning Area 16 to the Development Site boundary. Vegetation cover reflects a history of dry farming and cattle grazing disturbances over a majority of the site and consists primarily of non-native grasslands, with patches of Riversidean sage scrub, mixed ornamental woodland, and sandy wash. The sandy wash habitat is associated with two deeply incised drainage courses and a tributary that flow through the Development Site. The existing high point is located at 2,523 feet above mean sea level (amsl) measured in the northwest corner of the Development Site, and the low point is located at 2,399 feet amsl at the Pershing Wash bed in the southeast corner of the Development Site. The Development Site is in portions of Sections 7 and 18 within Township 3 South, Range 1 East on the U.S. Geological Survey's (USGS) 7.5-minute *Beaumont, California* quadrangle.

3.2.2.2 MSJC Site

The MSJC Site is a 49.2-acre site located in the City at the southeast corner of Sunset Avenue and Westward Avenue. Approximately 8.3 acres of the MSJC Site are currently developed with MSJC Campus features. The balance of the MSJC Site is undeveloped, with no existing structures, and vegetation that is a mosaic of Riversidean sage scrub, grassland, and water habitats. Pershing Creek crosses the MSJC Site from the west across Sunset Avenue, entering the southern portion of the MSJC Site.

Table 3.B: Assessor Parcel Numbers identifies the Assessor's Parcel Numbers (APNs) associated with the Development Site and MSJC Site.



Table 3.B: Project Assessor Parcel Numbers

| Development Site | MSJC Site |
|------------------|--------------|
| 537-110-003 | 537-140-004 |
| 537-110-004 | 537-140-006 |
| 537-110-005 | 537-140-007 |
| 537-110-013 | 537-140-008 |
| 537-110-014 | 537-140-009 |
| 537-110-011 | 537-140-010 |
| 537-110-012 | 537-140-011 |
| 537-120-013 | 537-140-012 |
| 537-120-025 | 537-150-001 |
| 537-120-028 | 537-150-003 |
| 537-120-029 | 537-150-004 |
| 537-120-030 | 537-150-008 |
| 537-120-031 | 537-140-001* |
| 537-120-032 | 537-140-002* |
| 537-120-033 | 537-140-003* |
| 537-120-034 | |
| 537-120-035 | |
| 537-120-036 | |

Source: Compiled by LSA (2022).

Notes: * APNs 537-140-001 (1.5 acre), 537-140-002 (5.9 acres), and 537-140-003 (0.9 acre) are the developed areas of the MSJC Site.

APN 537-110-013 was changed from 537-110-007 in 2022.

APN 537-110-014 was changed from 537-110-008 in 2022.

APN = Assessor Parcel Number

MSJC = Mt. San Jacinto College

3.2.3 Existing Circulation System

The Development Site is currently accessed via Westward Avenue, which bisects the Development Site and connects Sunset Avenue to Highland Home Road. Unimproved Bobcat Road to the south also provides access to the Development Site via Sunset Avenue. The MSJC Site is bordered to its north by Westward Avenue and bordered to its west by Sunset Avenue. Access to the MSJC Site can occur via Westward Avenue and Sunset Avenue in the northwest corner of the MSJC Site. The following provides a description of the local roadways that currently serve the Development Site and MSJC Site:

- Sunset Avenue:** Sunset Avenue from Wilson Street (north of the I-10 Freeway) to Lincoln Street is classified as an Arterial Highway by the City and south of Lincoln Street is classified as a Secondary Highway. Arterial Highways typically have a 110-foot right-of-way and an 86-foot curb-to-curb width and are 4-lane divided roadways that may provide on-street parking. These roadways typically direct traffic through major development areas and serve to move larger volumes of inter-city traffic. Secondary Highways are 4-lane roadways that may include a painted median. These roadways typically have an 88-foot right-of-way with a 64-foot curb-to-curb measurement. Sunset Avenue is currently a two-lane unpaved roadway south of Westward Avenue (specifically, south of the southern driveway of the Mt. San Jacinto College San Geronio Pass Campus), a two-lane paved roadway from Westward Avenue to the I-10 Freeway ramp intersections and a four-lane paved roadway north of the I-10 Freeway.



- **Bobcat Road:** This road is classified as a Divided Collector Highway by the City. A Divided Collector Highway typically has a 78-foot-wide right-of-way with a 56-foot curb-to-curb measurement. Divided Collector Highways are designed as two-lane roads that have a center median and provide on-street parking on both sides. They provide connections to secondary streets, arterials, and freeways, with most traffic being through-traffic or intra-city volumes. Bobcat Road is currently a two-lane unpaved roadway throughout the Development Site area.
- **Lincoln Street:** Lincoln Street east of Sunset Avenue is classified as a Major Highway by the City. Major Highways typically have a 100-foot right-of-way and a 76-foot curb-to-curb width. Lincoln Street is currently a 2-lane paved roadway east of Sunset Avenue and does not exist west of Sunset Avenue.
- **Highland Home Road:** Highland Home Road is comprised of two sections of South Highland Home Road. The portion north of Sun Lakes Boulevard is classified as a Secondary Highway by the City of Banning and has the same description as the portion of Sunset Avenue classified as a Secondary Highway, as discussed above. The portion of Highland Home Road south of Sun Lakes Boulevard is classified as a Collector Highway by the City. Collector Highways are designed as two-lane roads that provide on-street parking on both sides. These roadways typically have a 66-foot-wide right-of-way and provide connections to secondary streets, arterials, and freeways, with most traffic being through-traffic or intra-city volumes. Highland Home Road is currently a two-lane paved roadway south of Sun Lakes Boulevard and does not exist north of Sun Lakes Boulevard.
- **Westward Avenue/Sun Lakes Boulevard/Sun Lakes Boulevard Extension:** Westward Avenue is classified as a Collector Highway and has the same description as does the portion of Highland Home Road classified as a Collector Highway, as discussed above. Westward Avenue is currently a two-lane paved roadway that terminates at Sunset Avenue. Westward Avenue continues to the west as a two-lane unpaved roadway between Sunset Avenue and Highland Home Road. Although this is a public roadway, access is currently gated and not open to the public, but the roadway is used and maintained for utility purposes. The City's General Plan Circulation Element identifies a future roadway, referred to in this document as Sun Lakes Boulevard Extension, as an Arterial Highway that is proposed to be constructed by the City along the existing alignment of the Sun Lakes Boulevard. West of Highland Home Road, the roadway continues as Sun Lakes Boulevard, which is also classified as an Arterial Highway and is currently a four-lane paved roadway.

3.3 EXISTING LAND USE, GENERAL PLAN/ZONING

Table 3.C: Existing General Plan and Zoning Designations for the Development Site and MSJC Site outlines the existing General Plan land use designations, which are depicted in **Figure 3-3: Existing General Plan Designations of the Development Site and MSJC Site**.



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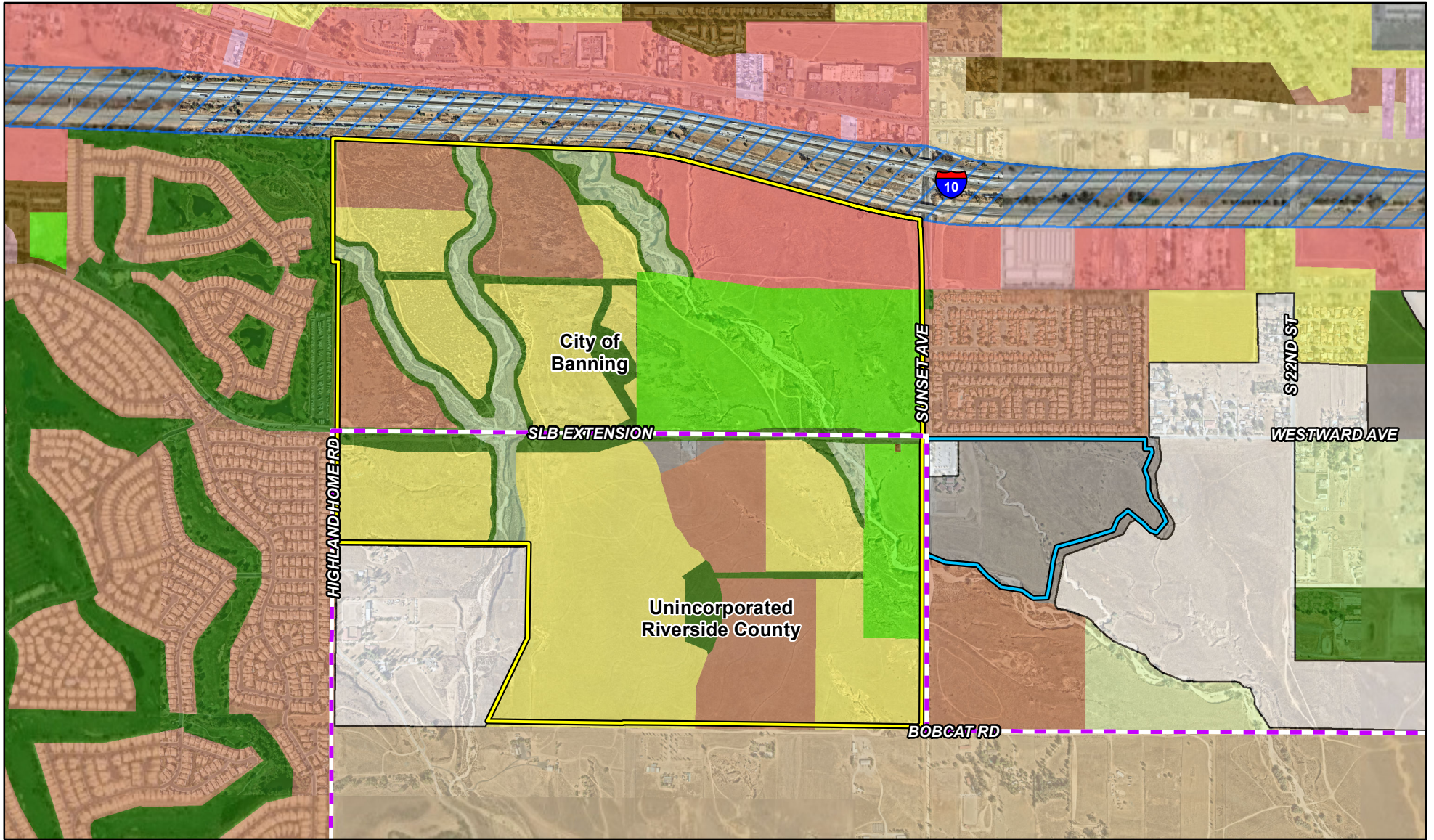


FIGURE 3-3

LEGEND

- Development Site
- MSJC Site
- City Boundary

City of Banning General Plan Land Use Map (2021)

- Very Low Density Residential (0-2 du/ac)
- Low Density Residential (0-5 du/ac)
- Medium Density Residential - (0-10 du/ac)
- High Density Residential - (11-18 du/ac)
- High Density Residential -20/Affordable Housing Opportunity (20-24 du/ac)
- Mobile Home Parks
- General Commercial

- Highway Serving Commercial
- Business Park
- Open Space - Parks
- Open Space - Hillside Preservation
- Public Facilities
- Public Facilities - Railroad/Interstate
- Ranch/Agriculture (10 ac min)
- Rural Residential



SOURCE: Nearmap Imagery (10/12/2020); City of Banning (2021)

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Sunset Crossroads

Existing General Plan Designations
of the Development Site and MSJC Site



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**Table 3.C: Existing General Plan and Zoning Designations
for the Development Site and MSJC Site**

| General Plan Designation | Zoning Designation | Acres |
|--|--------------------------------|--------------|
| Development Site City of Banning (Northern Portion of the Development Site) | | |
| High Density Residential | High Density Residential | 9.0 |
| Medium Density Residential | Medium Density Residential | 64.5 |
| Low Density Residential | Low Density Residential | 67.7 |
| General Commercial | General Commercial | 49.8 |
| Open Space – Parks | Open Space – Parks | 49.2 |
| Open Space – Resources | Open Space – Resources | 33.7 |
| Public Facilities – Government | Public Facilities – Government | 6.2 |
| City of Banning: Total Acres | | 280.1 |
| Development Site County of Riverside (Southern Portion of the Development Site) | | |
| Low Density Residential | Light Agriculture (A-1-10) | 253.7 |
| County of Riverside: Total Acres | | 253.7 |
| Total Development Site under Existing General Plan/Zoning Designations | | 533.8 |
| MSJC Site | | |
| Public Facilities | Public Facilities | 49.2 |

Source 1: City of Banning General Plan Land Use Map and Zoning Map, Website: https://www.ci.banning.ca.us/DocumentCenter/View/4051/Banning_General-Plan-Map2016?bidId= (accessed March 15, 2022).

Source 2: Riverside County Information Technology GIS, Website: <https://rcitgis-countyofriverside.hub.arcgis.com/> (accessed March 15, 2022).

MSJC = Mt. San Jacinto College

3.3.1 Development Site

The Development Site is currently undeveloped and has been historically used for agricultural practices (dryland farming and grazing). Grazing no longer occurs in the Northern Portion of the Development Site but does occur intermittently in the Southern Portion of the Development Site. There are no plans to permit grazing on site following Project approval. Vegetation communities located within the Development Site consist of non-native grassland, California buckwheat scrub, and Riversidean alluvial fan sage scrub (RAFSS).

The Northern Portion of the Development Site is within the jurisdiction of the City. The General Plan Designations and Zoning Classifications on the Northern Portion of the Development Site include a mix of commercial, residential, and open space.⁷ The Southern Portion of the Development Site is located within the City's SOI; however, Riverside County is the governing jurisdiction of this area until the land is annexed into the City of Banning. The Southern Portion of the Development Site is designated in the County General Plan as Low Density Residential and zoned as A-1, Light Agriculture. As the Southern Portion of the Development Site is within the City's SOI, the City has given this area the following advisory General Plan designation and pre-zoning: Low Density Residential, Medium Density Residential, High Density Residential, Open Space – Parks, Open Space – Resources, and Public Facilities – Government. The City identifies zoning in the City with the same designation as the General Plan; accordingly, these advisory General Plan designations also could be seen to establish pre-zoning of the Southern Portion of the Development Site by the City. The Development Project would modify these General Plan designations and provide revised pre-zoning for this portion of the Development

⁷ It should be noted that the City's General Plan Land Use Designations coincide with the City's Zoning Classifications.



Site. Pre-zoning of the Southern Portion of the Development Site would not be effective until this area is annexed into the City (see Government Code §65859[a]).

An existing 12-inch sewer line in the Existing ROW flows until it reaches an existing lift station also located within the Existing ROW, where wastewater is conveyed through an existing 10-inch force main toward the existing manhole at the intersection of Sunset Avenue and the Existing ROW. A petroleum pipeline traverses the Northern Portion of the Development Site and the Existing ROW. This pipeline is marked by signs, flags, and spray paint.

A natural gas pipeline also crosses the Northern Portion of the Development Site, marked by signs and exposed where the pipeline crosses the existing natural drainages. City water facilities (well houses) operate on parcels south of the Existing ROW. The City does not intend to relocate these utilities as part of the construction of the SLB Extension; therefore, the Development Project would connect to these utilities in their current location.

Overhead and underground utility lines are present along the perimeter of the Development Site. No standing structures are currently located within the Development Site, which is currently fenced along its perimeter.

3.3.2 MSJC Site

The MSJC Site is directly to the east of the Development Site on the east side of Sunset Avenue. Approximately 8.3 acres of the MSJC Site is developed with a surface parking lot and five buildings associated with the MSJC Campus including an administration building, two classroom buildings, and a facility building. The remainder of the MSJC Site is undeveloped and currently contains natural vegetation and natural topography. Pershing Creek crosses the MSJC Site from the west across Sunset Avenue, entering the southern portion of the MSJC Site.

The MSJC Site is in the City and has a General Plan and Zoning designation of Public Facilities. A programmatic-level analysis of the MSJC site action is included in this EIR in **Chapter 5.0**.

3.4 ADJACENT PROPERTIES AND LAND USES

3.4.1 Land Uses Adjacent to the Development Site

3.4.1.1 North

The UPRR and associated ROW are located directly north of the Development Site. Sunset Avenue is grade separated from the UPRR and maintains access with I-10 through a full interchange. Commercial uses are located along W. Ramsey Street (between Sunset Avenue and Highland Home Road) north of I-10. General Plan and Zoning designations to the north include Public Facilities – Railroad/ Interstate and General Commercial (north of I-10).

3.4.1.2 West

West of the Development Site is the Sun Lakes Specific Plan area (SLSP), a 975-acre area generally bounded by I-10 and UPRR ROW to the north, Highland Springs Road to the west, Highland Home Road to the east, and vacant land to the south. The Sun Lakes Community is a master-planned, age-restricted residential development centered on the Sun Lakes Country Club. Within the SLSP are



single-family and multifamily residences (duplexes and townhomes), golf courses, and recreational facilities. Within the SLSP, an assisted living facility is located north of Sun Lakes Boulevard and a recreational vehicle storage area is located directly west of the Development Site. The SLSP was recently amended to allow approximately 6.8 acres Retail/Service, approximately 30 acres Business/Warehouse, and approximately 10 acres Office/Professional uses on a portion of the SLSP between I-10 and Sun Lakes Boulevard. General Plan and Zoning designations to the west in the SLSP include General Commercial (including the approximately 20-acre Sun Lake Village Shopping Center in the northwest quadrant of the SLSP), Business Park, High Density Residential, Open Space – Parks, Medium Density Residential, Professional Office, and Open Space –Resources.

3.4.1.3 East

Business park and industrial and storage uses intermingled with larger lot residential uses are located east of Sunset Avenue and north of Lincoln Street. Several single-family residential neighborhoods have developed east of Sunset Avenue, south of Lincoln Street. The MSJCCD San Gorgonio Campus is located east of Sunset Avenue and south of Westward Avenue. Scattered rural residential uses are located farther west of the MSJC property. The Rancho San Gorgonio Specific Plan (RSGSP), an approximately 831-acre master-planned community, is located east of Sunset Avenue across from the Southern Portion of the Development Site. General Plan and Zoning designations to the east include Rancho San Gorgonio Specific Plan, Public Facilities, Medium Density Residential, and General Commercial.

3.4.1.4 South

Large-lot residential uses and agricultural operations (ranching/grazing) are located south of Bobcat Road. Riverside County General Plan and Zoning designations to the south include Agricultural (AG) and A-1, Light Agriculture, respectively.

3.4.2 Land Uses Adjacent to the MSJC Site

3.4.2.1 North

Westward Avenue is directly north of the MSJC Site, and a single-family residential neighborhood is located north of Westward Avenue. Land to the north of the MSJC Site has a General Plan and Zoning designation of Medium Density Residential.

3.4.2.2 West

Sunset Avenue is located west of the MSJC Site, and to the west of Sunset Avenue is the Development Site.

3.4.2.3 East

Land to the east of the MSJC Site is not developed and is occupied by natural vegetation and natural topography. A 0.11-acre area of land to the east of the MSJC Site is occupied by a well and water container that is fenced on its perimeter. Land to the east of the MSJC Site has a City of Banning General Plan and Zoning designation of Very Low Density Residential.



3.4.2.4 South

Land to the south of the MSJC Site is undeveloped and is occupied by natural vegetation and natural topography. The bed of Pershing Creek is also located south of the MSJC Site. Land to the south of the MSJC Site is located in the City and has a General Plan and Zoning designation of Medium Density Residential.

The existing land use patterns on the Development Site and MSJC Site and adjacent properties are depicted in **Figure 3-4: Existing Adjacent Land Use Patterns**.

3.5 DEVELOPMENT PROJECT COMPONENTS

The following subsections detail the Development Project components and Development Project Entitlements.

3.5.1 General Plan Land Use Amendment

The Development Project would include a General Plan Amendment that would amend the City's General Plan and Land Use Map to change the land use designations of the Development Site to General Commercial, Industrial, Open Space – Parks, and Open Space – Resources.

3.5.2 Zoning/Pre-Zoning and Sunset Crossroads Specific Plan

With implementation of the Development Project, the Northern Portion of the Development Site would be rezoned while the Southern Portion of the Development Site would be pre-zoned, with the pre-zoning becoming effective upon annexation of the Southern Portion of the Development Site into the City.⁸ Zoning regulations and development standards for the Development Site would be established in and governed by the Specific Plan, which would provide a means of implementing and detailing the City's General Plan and tailoring its policies to the Development Site.

3.5.3 Project Components

3.5.3.1 Sunset Crossroads Specific Plan

The Specific Plan would guide development within the 533.8-acre Development Site by establishing the distribution, location, and extent of the uses of land, including open space, within the area covered by the plan, land use designations, infrastructure plans, development standards, and design guidelines that address building placement, architectural style, landscaping materials, and other design elements for permitted uses.

Applications for implementing entitlements for the Development Project, including, without limitation, for infrastructure improvements, individual maps, buildings, or phases, that are in substantial conformance with the Specific Plan shall not require additional review under the California Environmental Quality Act (CEQA), provided that none of the conditions are present that require further environmental review under CEQA, in particular, Public Resources Code Section 21166.

⁸ California Government Code Section 65859(a).

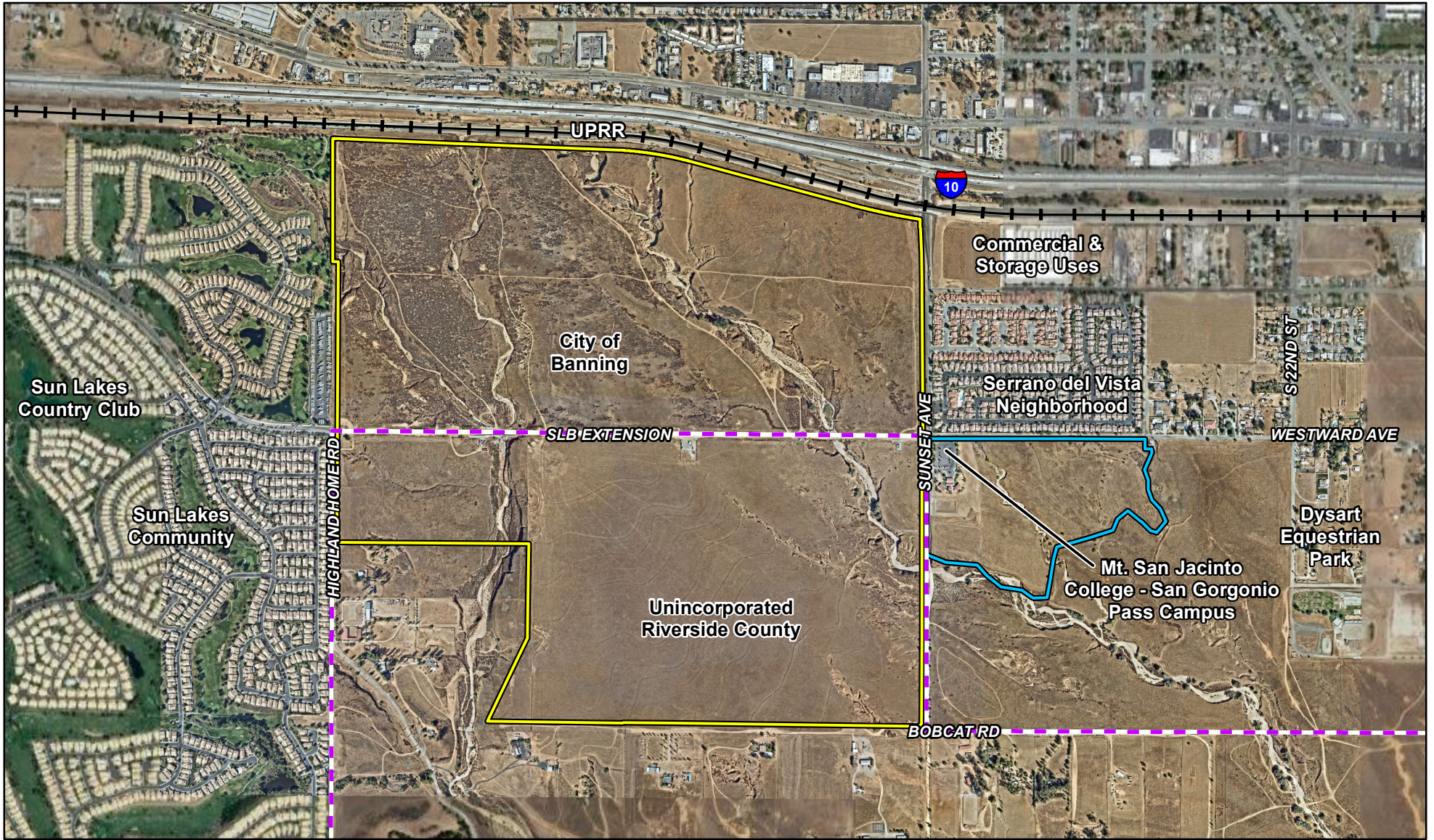
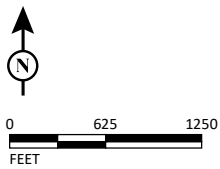


FIGURE 3-4

- LEGEND
- Development Site
 - MSJC Site
 - City Boundary



SOURCE: Nearmap Imagery (10/12/2020)
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In addition to roads, the Development Project’s master-planned on-site infrastructure improvements include potable water, wastewater, reclaimed water lines, and stormwater management, including water quality management/flood control systems, dry utilities, roadways and sidewalks, landscaping, and a park. The Specific Plan ensures that necessary roads, utilities, landscape elements, and other infrastructure facilities are installed when needed to accommodate the needs of the Specific Plan as it builds out.

The Specific Plan establishes four General Plan Land Use Designations: General Commercial (GC), Industrial (I), Open Space – Parks (OS-P), and Open Space – Resources (OS-R). Within the Development Site, 19 Planning Areas are geographic areas to which identified Implementation Regulations, Development Standards, and Design Guidelines described in the Specific Plan are applied. The Planning Areas would accommodate commercial, industrial, and open space uses. **Figure 3-5: Specific Plan Land Use and Planning Area Map** identifies the 19 Planning Areas established by the Specific Plan while **Table 3.D: Specific Plan Land Use Summary** provides a summary of the land uses established by the Specific Plan.

Commercial (GC). The approximately 47.9-acre commercial district of the Development Site (Planning Area 1) is bordered on the east by Sunset Avenue and stretches along the northeast portion of the Development Site between Lincoln Street and the site boundary. This EIR analyzed the following uses for Planning Area 1:

- A maximum of 268,400 square feet of commercial uses, which may include leisure, retail, medical office, professional office, education, wellness, recreation, entertainment, restaurant, and other retail uses, operating out of a number of buildings aligned along an internal road (yet to be named) that would provide central access from Sunset Avenue. This portion of the Development Project is designed to be a regional commercial and entertainment attraction, providing multigenerational destinations such as activity and wellness-based retail uses including, but not limited to, stationary wave surfing, indoor skydiving, indoor go-karting, obstacle gyms and a large indoor/outdoor climbing wall, yoga studios, dance studios, and other athletic activities. Sit-down restaurants (with patio areas), a food hall, and specialty retail uses that would orient inwards to central open space areas are proposed to allow for outdoor activities ancillary to the on-site leisure/recreation uses (i.e., walking paths, grass areas, outdoor displays, benches) or for use by on-site vendors/concessionaires and as connections between uses along with an internal system of pedestrian walkways. The proposed commercial uses include Travel Center Retail Uses, comprising retail and convenience sales and a drive-through fast food restaurant proposed to be located adjacent to the Fuel Facility.
- A freeway accessible 125-room (approximately 90,000-square-foot) hotel that would include a fitness room, hotel lobby, common area, office/management/engineering area, laundry facilities, and outdoor or indoor pool.



Table 3.D: Specific Plan Land Use Summary

| Planning Area | Land Use Designation | Development | Building Square Footage | Acres |
|---|------------------------|--|---|--------------------------|
| 1 | General Commercial | Commercial uses; Travel Center Retail Uses; hotel with maximum 125 keys (approximately 90,000 sf); and Fueling Facility (each detailed below in the Commercial subheading). | 268,400 sf | 47.9 |
| SUBTOTAL: COMMERCIAL DEVELOPMENT | | | up to 268,400 sf plus 125-room hotel¹ plus Fueling Facility | 47.9 |
| 2 | Industrial | Building 5 Building 6 | 326,000 sf 152,000 sf | 33.8 |
| 3 | Industrial | Building 4 | 44,000 sf | 7.8 |
| 4 | Industrial | Building 1 Building 2 Building 3 | 1,420,000 sf 1,386,000 sf 575,000 sf | 194.3 |
| 5 | Industrial | Building 7 | 896,000 sf | 66.3 |
| 6 | Industrial | Building 8 | 250,000 sf | 28.1 |
| 7 | Industrial | Building 9 | 274,000 sf | 25.8 ² |
| 8 | Industrial | Trailer parking/storage | Trailer parking/storage | 16.2 |
| 9 | Industrial | Trailer/RV storage | Trailer and/or RV storage | 3.3 |
| 10 | Industrial | Building 10 | 222,000 sf | 16.4 |
| SUBTOTAL: INDUSTRIAL DEVELOPMENT³ | | | up to 5.545 million sf | 392.0 |
| DEVELOPMENT SUBTOTAL | | | 5.8134 million sf | 439.9 |
| 11 | Open Space – Parks | Planning Area 11 includes approximately 5.0 acres of passive park uses (detailed in Section 3.5.3.1). The balance of the Planning Area will be retained as passive open space. | | 12.6 |
| 12 | Open Space – Resources | Passive open space, which may include trails. Planning Area 12 includes 2.3 acres for the City's reverse osmosis facility (detailed in Section 3.5.4). | | 12.5 |
| 13 | Open Space – Resources | Planning Areas 13–19 accommodate existing natural drainages within the Development Site, with potential for construction of roadway and utility crossings at Lincoln Street. | | 7.5 |
| 14 | Open Space – Resources | | 7.1 | |
| 15 | Open Space – Resources | | 6.0 | |
| 16 | Open Space – Resources | | 4.8 | |
| 17 | Open Space – Resources | | 4.1 | |
| 18 | Open Space – Resources | | 3.3 | |
| 19 | Open Space – Resources | | 7.7 | |
| SUBTOTAL: OPEN SPACE | | | | 65.6 |
| Street Dedication/Circulation | | | | 28.3 |
| TOTAL | | | | 533.8⁴ |

Source: T&B Planning, Inc. *Sunset Crossroads Specific Plan No. 20-20000002*, City of Banning, May 2023.

¹ Planning Area 1 is anticipated to include a 125-room limited-service hotel (approximately 90,000 square feet). The approximately 90,000 square feet of hotel use is not counted as part of the General Commercial's 268,400 Building Square Footage because the project's traffic analysis for the commercial site estimates traffic based on square footage, while the traffic analysis for hotels is based on the number of rooms. With the hotel's approximately 90,000 square feet, the approximate total square footage in Planning Area 1 would be 358,400 square feet. Building Square Footage is defined as follows: Total square footage of all floors in structure as measured from either the interior surface of each exterior wall of the structure or, if the structure does not have walls, from each outer edge of the roof. Exterior areas may constitute gross floor area.

² The western portion of Planning Area 7 would be retained as open space, buffering uses west of the Development Site.

³ Per the Specific Plan, in the Industrial Planning Areas the building square footage in each Planning Area may increase/decrease in each Planning Area by up to 20 percent, but the total square footage of buildings in the Industrial Planning Areas may not exceed 5.545 million square feet. The Development Project would include approximately 197,500 square feet of general light industrial, 490,500 square feet of warehousing, and up to 330,000 square feet of cold storage uses.

⁴ The future extension of Sun Lakes Boulevard (approximately 21.0 acres) and existing City-owned well sites (0.8 acre) are Not a Part (NAP) of the Project.

RV = recreational vehicle

sf = square feet

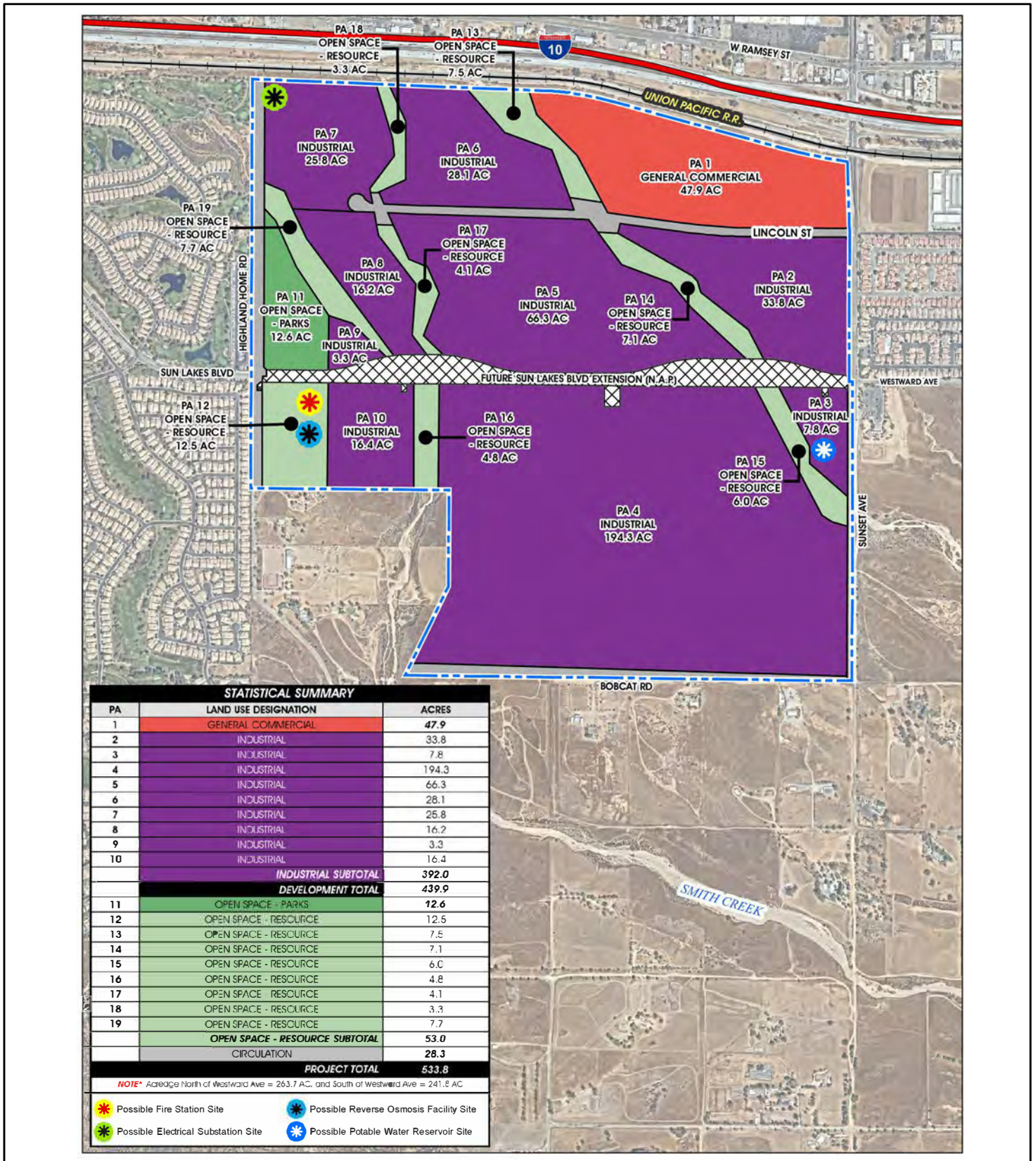


FIGURE 3-5

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SOURCE: NearMap Imagery / 2022), RCTLMA / 2021)

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- A Fueling Facility, consisting of an auto and commercial fueling station and carwash with 20 vehicle fueling stations and 6 commercial fueling stations to accommodate recreational vehicles and multi-axle tractor trailers and electric vehicle charging stations. The fueling facility would be accessed from Sunset Avenue or Lincoln Street and as analyzed would include four 20,000-gallon underground storage tanks (USTs).

Direct access to Planning Area 1 would be provided from the extension of Lincoln Street along the southern portion of the Planning Area via four driveways as well as a driveway off Sunset Avenue. The various buildings and activity areas would further be connected by an internal system of roadways and pedestrian walkways. See **Figure 3-6: Commercial Area of Specific Plan**.

Development of Planning Area 1 will be subject to the applicable Infrastructure Improvement Standards, Development Standards, and Planning Area Standards set forth in the Specific Plan. Because the buildings or parcels in the commercial areas of the Development Site may be leased to third parties, it is not possible to specifically define the uses at this time, and therefore the Specific Plan provides for a range of allowable uses. While the uses described above may be developed under the Specific Plan, the Specific Plan does not require these uses to be implemented, and any of the commercial land uses listed are consistent with the Specific Plan and allowed uses so long as the City determines either that such uses do not increase the Development Project's environmental impacts evaluated in the Sunset Crossroads Specific Plan EIR (or in additional supplemental environmental analysis previously performed by the City) or the City performs supplemental analysis as required by CEQA. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses in Planning Area 1 is provided in Chapter 3, Development Standards, of the Specific Plan.

The City, in considering an application for a permitted use, would evaluate whether there are significant environmental impacts resulting from the proposed use not addressed in the EIR or any then prepared subsequent environmental documentation. If the environmental impacts of the proposed use exceed the impacts disclosed in this EIR or subsequent environmental documentation approved by the City, the City will undertake additional environmental review in compliance with CEQA prior to approving any such use.

Industrial (I). Planning Areas 2 through 10 provide for development of up to 5,450,000 square feet of industrial land uses on approximately 392.0 acres within the Development Site. Based on its proximity to the Sunset Avenue interchange at I-10, the Development Project would facilitate the development of warehousing, distribution, manufacturing, and other industrial uses. To accommodate future tenants, the Specific Plan allows the size of individual buildings to vary within an established range (increasing or decreasing by up to 20 percent); however, the overall square footage of buildings in the industrial Planning Areas is capped at 5.545 million square feet. The Development Project considered in this EIR analyzes industrial buildings ranging in size from 44,000 square feet up to 1,420,000 square feet and accommodating uses such as general industrial, manufacturing, parcel hub, warehouse/storage, truck/trailer parking and storage, recreational vehicle (RV) storage, high cube warehouse, cold storage warehouse (up to 330,000 square feet proposed for Phase 2 [Buildings 5 and 6]), fulfillment center, and e-commerce operations. A proposed 65 MWh BESS facility would be a permitted use in any of the industrial use planning areas. However, it would likely be associated with



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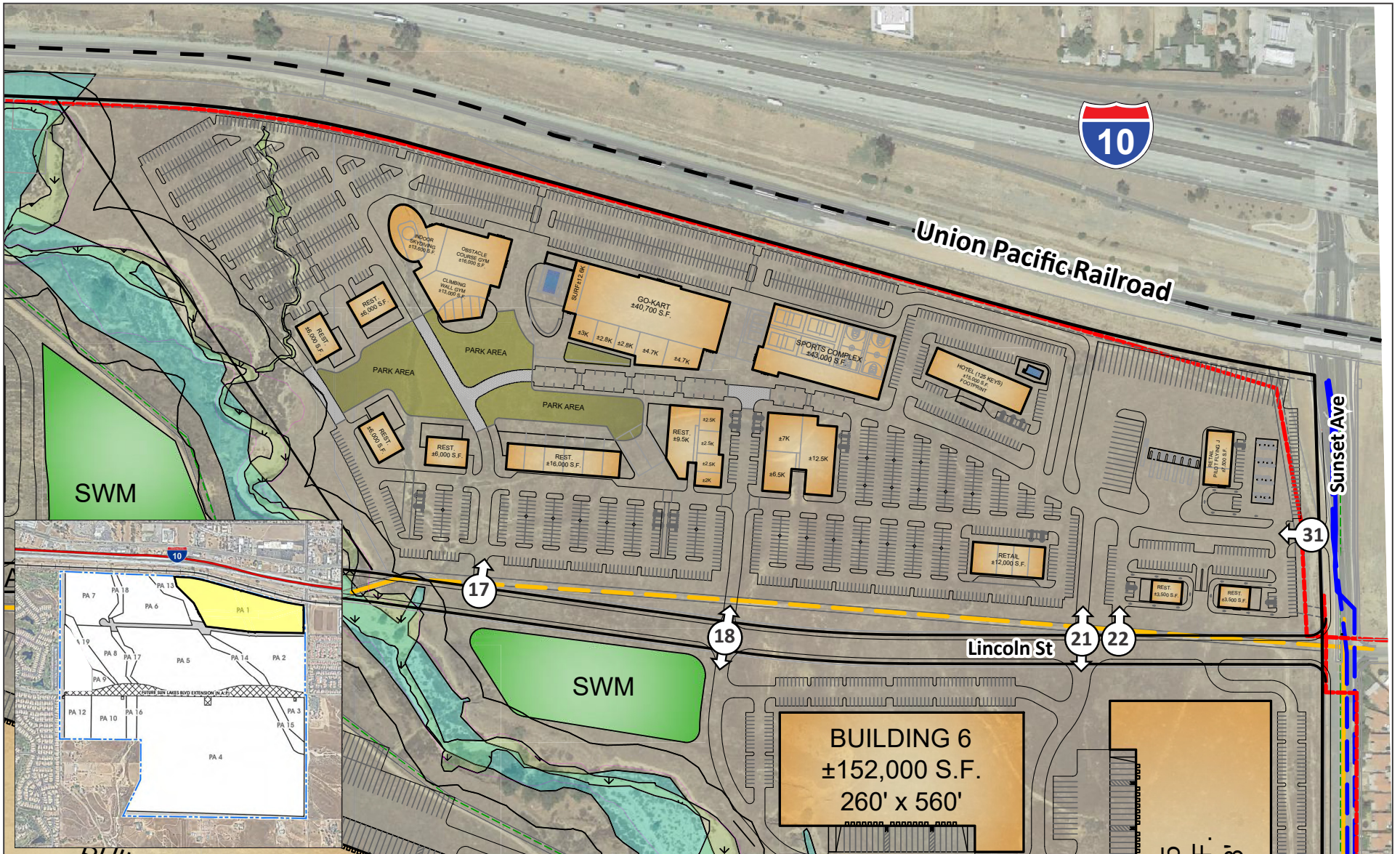


FIGURE 3-6

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LEGEND

→ Site Access Locations



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SOURCE: North Point Development

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Sunset Crossroads
Commercial Area of Specific Plan



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the planned electrical substation in PA 7. Direct access to the industrial Planning Areas would be provided from Sunset Avenue via Lincoln Street, Westward Avenue and a variety of internal driveways, and from Highland Home Road, the SLB Extension (upon completion), and Bobcat Road. **Figure 3-7: Industrial Area of Specific Plan** provides a generalized site plan for the Project's industrial area.

The industrial building sites within the Development Project may include outdoor employee break areas with tables affixed to the ground to provide employees with a location to eat, gather, and enjoy being outside. Shading of these areas may be achieved through a combination of shade trees, umbrellas, or fabricated shade structures. Other open space amenities within the industrial areas may include pedestrian walkways, seating areas, overhead structures, and open space areas. Development of the industrial Planning Areas shall be subject to the applicable Infrastructure Improvement Standards, Development Standards, and Planning Area Standards set forth in the Specific Plan.

Because the buildings or parcels in the industrial areas of the Development Site may be leased to third parties, it is not possible to specifically define the uses at this time, and therefore the Specific Plan provides for a range of allowable uses.

While the uses described above may be developed under the Specific Plan, the Specific Plan does not require these uses to be implemented, and any of the industrial land uses listed in Specific Plan Table 3-1 are consistent with the Specific Plan and allowed uses so long as the City determines either that such uses do not increase the project's environmental impacts evaluated in the Sunset Crossroads Specific Plan EIR (or in additional supplemental environmental analysis previously performed by the City) or the City performs supplemental analysis as required by CEQA. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses in Planning Areas 2 through 10 is provided in Chapter 3, Development Standards, of the Specific Plan.

The City, in considering an application for a permitted use, would evaluate whether there are significant environmental impacts resulting from the proposed use not addressed in this EIR or any then prepared subsequent environmental documentation. If the environmental impacts of the proposed use exceed the impacts disclosed in the EIR or subsequent environmental documentation approved by the City, the City shall undertake additional environmental review in compliance with CEQA prior to approving any such use.

Open Space – Resources (OS-R). The area designated OS-R totals 53.0 acres and would be maintained by the Master Property Owner Association (MPOA). Planning Areas 13 through 19 (totaling 40.5 acres) are designed to retain three deeply incised existing natural drainages (from east to west: Pershing Wash, Smith Creek, and Highland Wash) and associated features within the Development Site.

No grading or development would be permitted within Planning Areas 13 through 19 other than temporary activities required for the construction of roadway crossings (Lincoln Street and Highland Home Road) and utility infrastructure; however, any land disturbance beyond these features would be returned to substantially pre-construction conditions to maintain the natural vegetation and topography of the Open Space – Resources areas.





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FIGURE 3-7

LSA



- LEGEND**
-  Planning Areas
 -  Site Access Locations

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SOURCE: North Point Development

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Sunset Crossroads
Industrial Area of Specific Plan



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Planning Area 12 consists of 12.5 acres designated as Open Space – Resources and is intended to serve as a buffer for the Sun Lakes residential community to the west of the Development Site. Planning Area 12 includes passive open space areas, which may contain trails as well as a City-constructed reverse osmosis facility on 2.3 acres of land. Additionally, a future City-constructed fire station would be a permissible use in accordance with the Specific Plan.

Open Space – Parks (OS-P). Planning Area 11 has been designated Open Space – Parks. Within this 12.6-acre Planning Area 11, 5 acres are identified for the development of a public passive park, which may include features such as, but not limited to, a tot lot playground, picnic tables, trails, walking paths, surface parking lot, and restrooms. The public park would include security lighting to illuminate restrooms, the surface parking lot, tot lot, and walkways. No lighted fields (i.e., lighted soccer fields or baseball/softball diamonds) would be provided in the public passive park. The balance of the Planning Area would be retained as passive open space, which may include trails. This Planning Area would provide buffering to residential uses west of the Development Site. The conceptual design of the proposed passive park is depicted in **Figure 3-8: Conceptual Design of Passive Park in Planning Area 11**.

Additionally, a minimum 100-foot-wide open space buffer is required at the time of Development Project implementation along the western portion of Planning Area 7, to reduce the effect of its industrial uses on the existing residential community to the west. Planning Area 2 would be buffered from the Rancho San Gorgonio residential development, and Planning Area 3 would be buffered from the off-site school site to the east by a landscaped buffer. The building planned for development in Planning Area 3 would be accessed from Sunset Avenue via two driveways.

3.5.3.2 Access and Circulation

The Development Site is located along the south side of I-10, and access to this regional transportation component from the Development Site would be provided at the Sunset Avenue interchange, at the northeast corner of the Development Site. The Development Project's proposed roadway improvements planned for the Development Site are detailed below and in **Figure 3-9: Circulation Improvements of the Specific Plan**. Development of the circulation system would be governed by the circulation improvement standards set forth in Chapter 2 of the Specific Plan. Within the Development Site, approximately 28.3 acres are identified for installation of internal roadways and improvement of circulation features to the existing surrounding roadways.⁹

Planning Areas that take access from the SLB Extension, Bobcat Road, or Highland Home Road are expected to be developed substantially concurrently with the development of either the SLB Extension, Lincoln Street, or Sunset Avenue so that one of these roadways will be in place to provide access upon building completion in each such Planning Area. Until completion of construction by the City or third parties of the SLB Extension, access to the Development Site would be from Lincoln Street, Sunset Avenue and other internal roadways, with existing Westward Avenue and Bobcat Avenue available as secondary access to Sunset Avenue. Following completion of construction of the SLB Extension, primary access would be available from Highland Home Road to Sunset Avenue along the SLB Extension and internal roadways.

⁹ Highland Home Road: 9.2 acres, Lincoln Street: 8.7 acres, Sunset Avenue: 2.7 acres, Bobcat Road: 3.0 acres, and internal roadways: 4.7 acres.



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SOURCE: Hunter Landscape (05-25-2021)

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FIGURE 3-8

Sunset Crossroads

Conceptual Design of Passive Park in Planning Area 11



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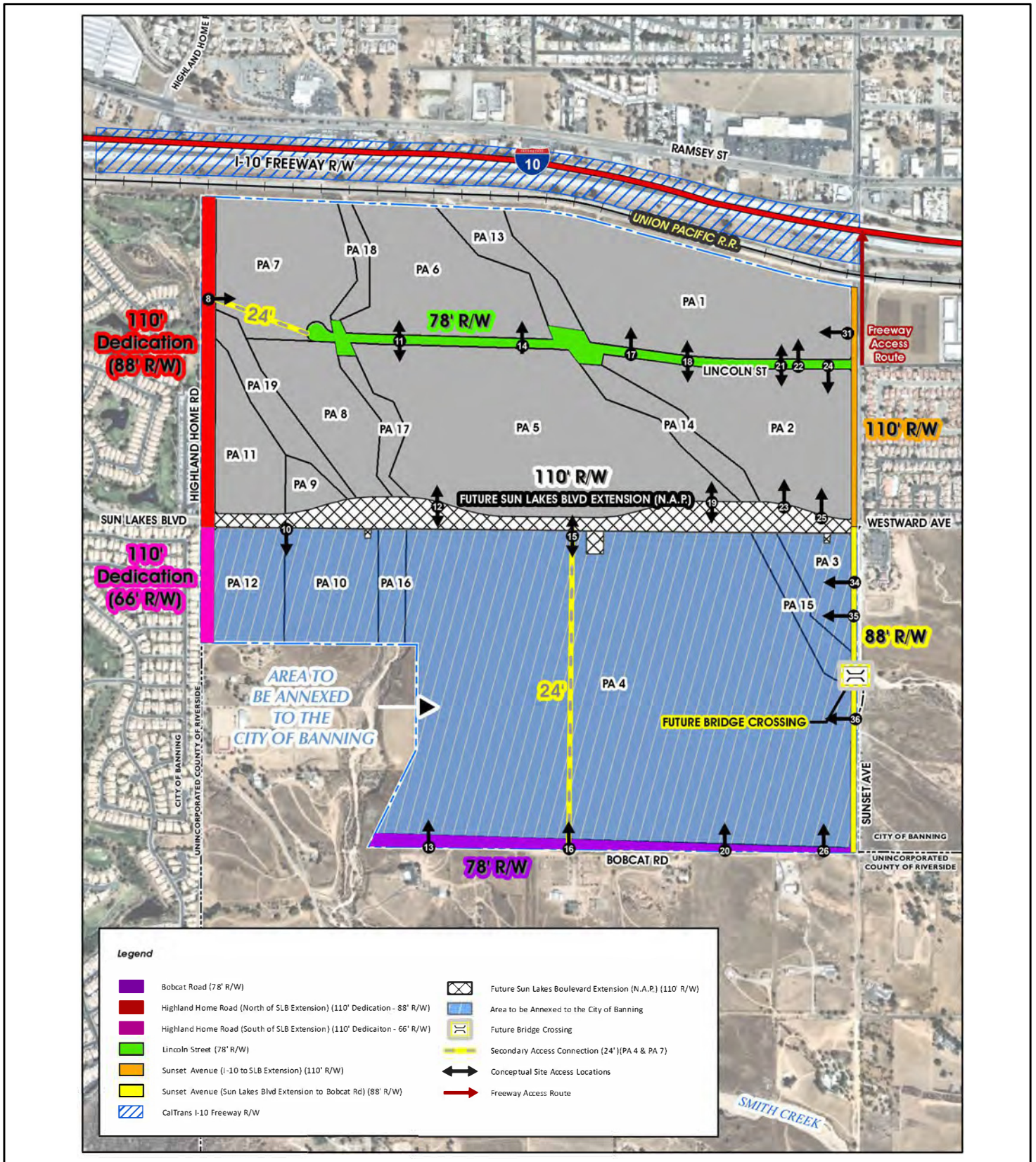


FIGURE 3-9
Page 1 of 2

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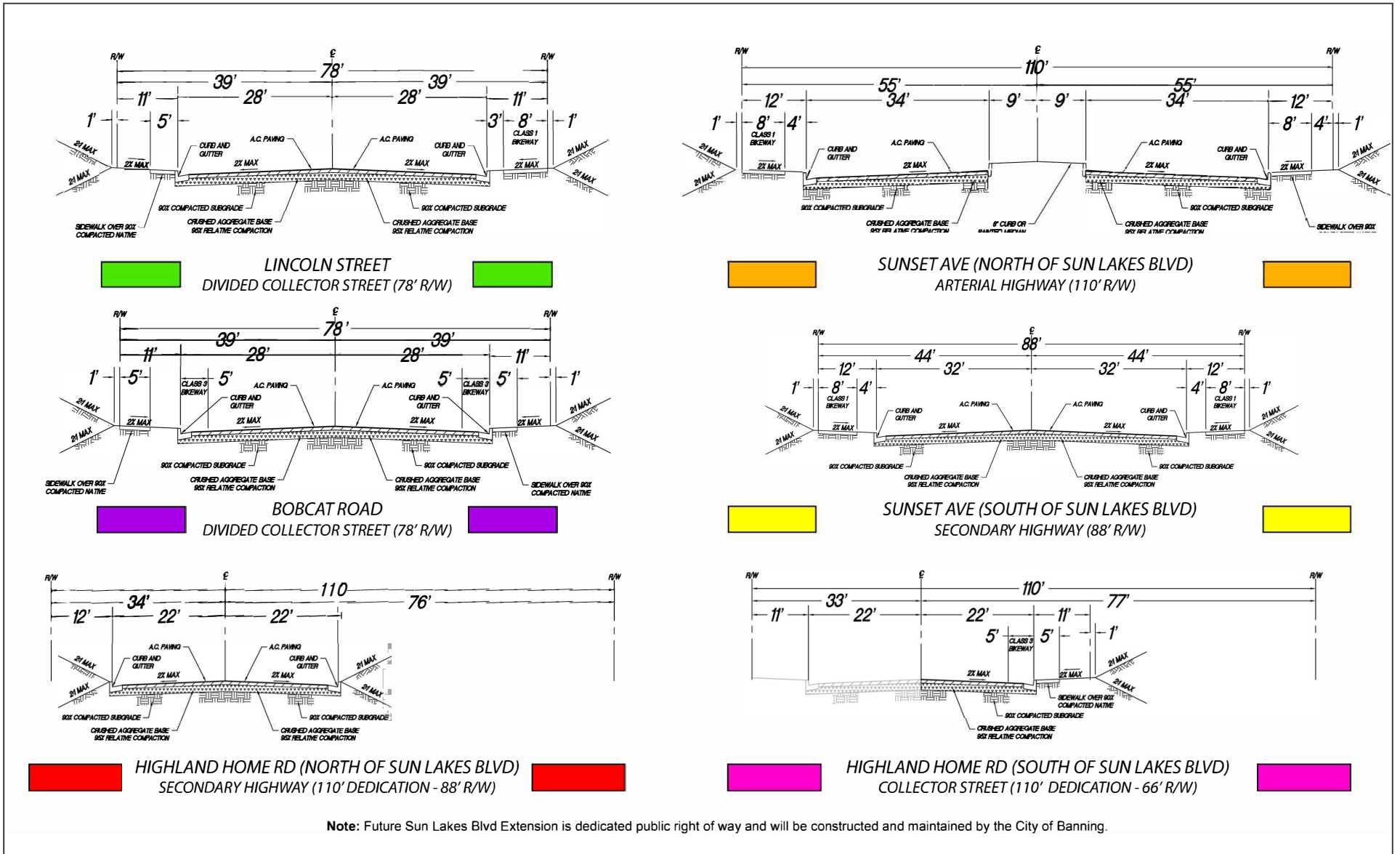


SOURCE: ESRI, Nearmap /2022), RCTLMA /2021) Proactive Engineering Consultants (10-19-2022)

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SOURCE: Proactive Engineering Consultants (01-19-2023)

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FIGURE 3-9
Page 2 of 2



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Access to the Specific Plan's commercial area in Planning Area 1 would be provided by internal roads (yet to be named) and Lincoln Street, which would be developed as part of the Development Project as an east-west roadway within the Development Site. Direct access to Planning Areas 2, 3, and 4 would be provided by Sunset Avenue. Planning Area 4 would be accessible from the SLB Extension on the north, Bobcat Road on the south, and Sunset Avenue to the east. Access to Planning Areas 6 and 7 would be provided from the south by Lincoln Street. Access to Planning Areas 2, 5, and 8 would be provided from the north by Lincoln Street and by the SLB Extension to the south. Planning Areas 9 and 10 would be accessed from the future SLB Extension. Planning Areas 11 and 12 would be accessible from the SLB Extension and Highland Home Road. The following describes improvement actions associated with the Development Project to provide adequate circulation and access to the Development Site.

Sunset Avenue, I-10 to Sun Lakes Boulevard Extension. This section of roadway is designated as Arterial Highway with 110-foot right-of-way, consisting of an 18-foot-wide center raised or painted median, with a 4-foot-wide curb-adjacent landscaped parkway and 8-foot-wide parkway-adjacent Class I bikeway on one side of the street, and an 8-foot-wide curb-adjacent sidewalk and 4-foot-wide landscaped parkway on the other side of the street. The Project Applicant would dedicate and construct western half-width lanes including the median plus 10-foot-wide improvements along this segment of Sunset Avenue along the Development Site's frontage (Planning Areas 1 and 2). The Specific Plan and future applications submitted within this Specific Plan will dedicate and construct the western half-width, full median, plus 10-foot improvements along the project frontage to the segment of Sunset Avenue north of the Existing ROW. The Development Project Applicant would also provide for full right-of-way expansion west of the current Sunset Avenue alignment, including bike lanes. This would be accommodated through designed setbacks of buildings, parking areas, and other improvements.

Sunset Avenue, Sun Lakes Boulevard Extension to Bobcat Road. Sunset Avenue is classified as a Secondary Highway and would have an 88-foot right-of-way. The Development Project Applicant would dedicate and construct the western half-width plus 10-foot-wide improvements along the Development Site's frontage (Planning Areas 3 and 4) on this segment of Sunset Avenue, except for the Sunset Avenue Bridge.

Lincoln Street. Lincoln Street east of Sunset Avenue is classified by the City as a Major Highway. West of Sunset Avenue, Lincoln Street would be constructed as a Divided Collector Highway and would have a 78-foot-wide right-of-way with a 56-foot curb-to-curb measurement. Divided Collector Highways are designed as two-lane roads that have a center median and provide on-street parking on both sides. They provide connections to secondary streets, arterials, and freeways, with most traffic being through-traffic or intra-city volumes. The Development Project Applicant would construct full-width improvements of Lincoln Street on the Development Site and the crossings of Pershing Wash (Planning Areas 13 and 14) and Smith Creek (Planning Areas 17 and 18) drainages via a 10-foot by 10-foot reinforced concrete box (RCB) culvert. Lincoln Street would provide a Class I Bikeway on the north side of the right-of-way. In Planning Area 7, a 24-foot-wide fire access connection would link Lincoln Street to Highland Home Road via a private roadway. The Specific Plan and future applications submitted within the Specific Plan will construct full-width improvements along the length of Lincoln Street on the Development Site.



Bobcat Road. Bobcat Road is designated as a Divided Collector Highway and will be improved to include a 78-foot right-of-way. The Development Project Applicant would dedicate and construct half-width plus 10-foot-wide improvements along the Development Site's frontage of Bobcat Road along the southern portion of Planning Area 4. The northerly portion of the right-of-way consists of 22 feet of paving, with a 6-foot-wide landscaped parkway and 5-foot-wide curb-adjacent sidewalk. Bobcat Road also provides a Class III Bikeway on each side of the road. The Specific Plan and future applications submitted within this Specific Plan will dedicate and construct the northern half-width plus 10-foot improvements of Bobcat Road along the project frontage. The existing roadway would become the southerly (east-bound) lanes. The trees, shrubs, and groundcovers to be planted in the landscape zones along Bobcat Road are in accordance with Table 4-1, *Plant Palette*, from the Specific Plan. The centerline of Bobcat Road will be relocated a maximum of 70.5 feet north from its current alignment to no longer conflict with the Southern California Edison (SCE) power poles that are encroached upon by the current road alignment.

Highland Home Road. Highland Home Road (north of the Existing ROW) is classified as a Secondary Highway by the City of Banning and is designed as a four-lane road within an 88-foot right-of-way. Highland Home Road (south of its intersection with the Existing ROW) is classified as a Collector Street by the City and is designed as a two-lane road that provides on-street parking on both sides within a 66-foot right-of-way. The entirety of the Highland Home Road right-of-way (north and south of its intersection with the Existing ROW) is within a 100-foot dedication. Highland Home Road is currently a two-lane paved roadway south of Sun Lakes Boulevard and does not exist north of Sun Lakes Boulevard. The Development Project Applicant would construct Highland Home Road to its ultimate half-width as a Secondary Highway (88-foot right-of-way) with a 12-foot-wide buffer and 44 feet of paving to accommodate two-way traffic from the northern boundary to the southern boundary of the Development Project.

Internal Road Network. Internal roads (yet to be named) would provide access from Sunset Avenue through the Development Site's commercial area (Planning Area 1). The final alignment, design, and width of the internal road (yet to be named) would be determined when buildings are oriented and designed as part of implementing buildings within Planning Area 1.

No on-site or off-site roadway improvements are currently proposed for the MSJC Entitlements. Such improvements would be conditioned and analyzed when an application to the City for a specific project for the MSJC Site is submitted to the City.

3.5.3.3 *Drainage and Water Quality*

The Development Site lies within the San Gorgonio Pass sub-basin in the northwest region of the Whitewater River Watershed in a grassland valley, with three natural drainage courses and a tributary traversing the Development Site, accounting for approximately 40.5 acres of the Project Site. The Development Site is subject to a local land use authority on-site retention standard (required by the Riverside County Flood Control and Water Conservation District per the Riverside County Watershed Protection Program for the Whitewater Region), which requires that the Development Project retain 100 percent of the 100-year, 3-hour storm event. Existing drainage courses include off-site areas north of I-10 that begin in the San Bernardino Mountains and flow south under I-10 and the UPRR corridor via an existing 8-foot by 7-foot arch box culvert and existing 12-foot by 10-foot arch box culvert, and outlets southerly to the Development Site's on-site natural drainages. Pershing Creek flows in a



northwest to southeast direction through the eastern portion of the Development Site (Planning Areas 13 through 15). On the western portion of the Development Site, Smith Creek flows in a north-south direction through Planning Areas 17 and 18. Highland Wash (Planning Area 19) confluences with Smith Creek near the future SLB Extension. Smith Creek continues in Planning Area 16 to the Development Site boundary. All natural drainage courses eventually confluence outside of the Development Site’s limits, and these natural drainages continue to flow east until they confluence with the San Gorgonio River and ultimately join the Whitewater River.

To preserve the natural drainage courses on the Development Site with implementation of the Specific Plan, Planning Areas 13 through 19 would be designated for Open Space – Resources and restricted from development other than for construction of trails in Planning Area 12 and the City-owned reverse osmosis facility and any future fire facility, and construction of utilities and Lincoln Street in Planning Areas 13 through 19. The flow path of the three existing drainage courses and the tributary within the Development Site are within the 100-year flood zone, but no portion of the Development Site is to be developed within a flood zone. Buildout of the Specific Plan would increase the impervious surface with street, access drives, paved parking areas, and building footprints. The drainage system routes the runoff from the proposed impervious surfaces to stormwater treatment and mitigation basins, and each basin provides stormwater treatment and peak flow mitigation for their respective tributaries via reinforced concrete pipes (RCP). Landscaped slopes and parking medians would be incorporated into each Planning Area to reduce the overall amount of impervious surface. The Development Site has been segmented into 16 Drainage Management Areas (DMA) as detailed below and as identified in **Figure 3-10: DMAs of the Specific Plan**.

The Development Project’s conceptual drainage plan consists of storm drainpipes, RCPs ranging in size from 12 to 42 inches, and 15 on-site Water Quality Management Plan (WQMP) basins. Data for the proposed WQMP basins are provided in **Table 3.E: Proposed On-Site WQMP Basins**.

Table 3.E: Proposed On-Site WQMP Basins

| DMA/Basin | 100-Year, 3-Hour Flood Volume (ft³) | Proposed Basin Volume (ft³) | Maximum Basin Depth (ft) | Basin Bottom Elevation (ft) |
|-----------|-------------------------------------|-----------------------------|--------------------------|-----------------------------|
| A | 191,430 | 263,102 | 7 | 2,410 |
| B | 298,654 | 616,810 | 5 | 2,447 |
| C | 610,875 | 818,928 | 7 | 2,419 |
| D | 299,723 | 813,701 | 6 | 2,483 |
| E | 80,077 | 118,919 | 6 | 2,248 |
| F | 316,048 | 439,085 | 6 | 2,459 |
| G | 76,388 | 131,551 | 6 | 2,460 |
| H | 14,089 | 104,544 | 5 | 2,455 |
| I | 113,906 | 164,657 | 5 | 2,436 |
| J | 119,538 | 309,712 | 5 | 2,410 |
| K | 133,929 | 286,625 | 5 | 2,468.5 |
| L | 211,113 | 520,542 | 5 | 2,410 |
| M | 445,382 | 605,048 | 7 | 2,450 |
| N | 102,363 | 112,032 | 5 | 2,435 |
| O | 9,122 | 10,344 | 5 | 2,465 |
| P | 39,989 | 40,547 | 6 | 2,482 |

Source: Project-Specific Preliminary Water Quality Management Plan for Sunset Crossroads (Proactive Engineering Consultants West, Inc. 2023).

DMA = Drainage Management Area ft³ = cubic feet
 ft = foot/feet WQMP = Water Quality Management Plan



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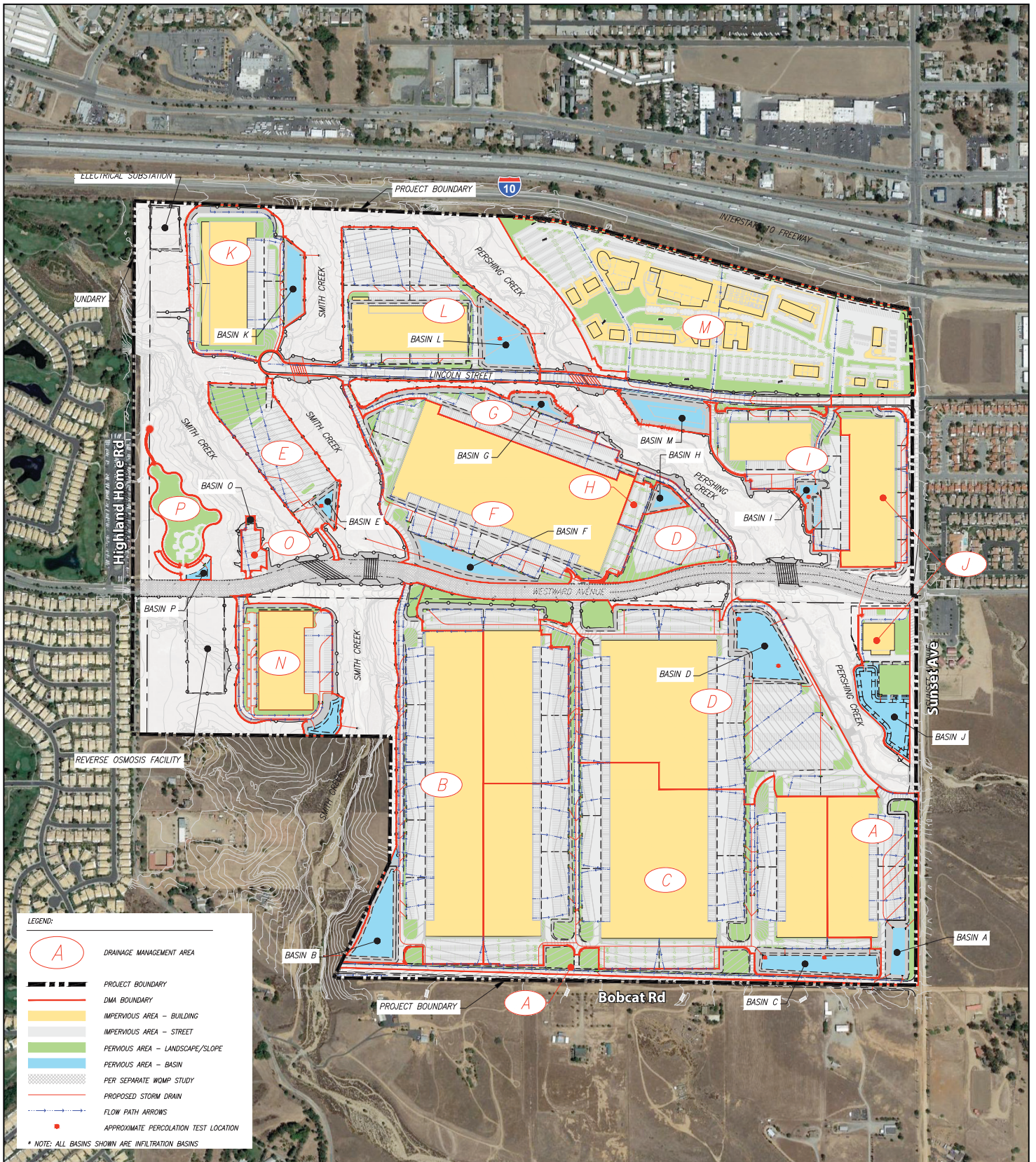
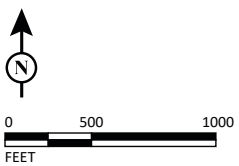


FIGURE 3-10





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Each of DMAs A, B, C, D, F, I, J, K, L, and N consists of a warehouse building footprint, access drives, paved parking areas, landscaped slopes, and landscaped medians. Runoff for these areas sheet flow until intercepted by catch basins where the captured runoff is conveyed to the infiltration basin with the same letter designation as the DMA. DMAs E, G, H, and O each consists of access drives, paved parking areas, landscaped slopes, and landscaped medians. Runoff for these areas sheet flow until intercepted by catch basins where the captured runoff is conveyed to the infiltration basin with the same letter designation as the DMA. DMA M consists of commercial building footprints, access drives, paved parking areas, landscaped slopes, and landscaped medians. Runoff for these areas sheet flow until intercepted by catch basins where the captured runoff is conveyed to the infiltration basin M. DMA P consists of a park site and paved parking areas. Runoff for these areas sheet flow until intercepted by catch basins where the captured runoff is conveyed to the infiltration basin P.

In summary, the WQMP basins are distributed across the Development Site as follows:

- Infiltration Basins I and M in Planning Area 2;
- Infiltration Basin J in Planning Area 3;
- Infiltration Basins A through D in Planning Area 4;
- Infiltration Basins F through H in Planning Area 5;
- Infiltration Basin L in Planning Area 6;
- Infiltration Basin K in Planning Area 7;
- Infiltration Basin E in Planning Area 8;
- Infiltration Basin N in Planning Area 10;
- Infiltration Basin O in Planning Area 9; and
- Infiltration Basin P in Planning Area 11.

An overflow outlet would be installed in each basin and would discharge excessive runoff to existing drainages. Sediment transport design features would be included in each infiltration basin on the Development Site. These features would be inspected annually and within 72 hours after significant rainfall, to scrape the bottom as needed to restore the infiltration basin cross section and infiltration rate. The Development Site is subject to a local on-site retention requirement. Per City of Banning Ordinance #1415, the Development Project (at buildout) is required to retain 100 percent of a 100-year, 3-hour storm event. The proposed infiltration basins would be sized to contain the design storage volume (VBMP), as well as retain the 100-year, 3-hour storm event.

Two 10-foot by 10-foot RCB facilities are proposed for the Lincoln Street crossings of the Pershing Wash and Smith Creek drainages.

3.5.3.4 Utilities

Buildout of the Specific Plan would require the installation of public utilities on the Development Site and in roadways adjacent to the Development Site to service the intended commercial and industrial uses. Construction of all wet utility improvements would be subject to the Implementation Standards set forth in the Specific Plan. The public utilities plan in the Specific Plan is considered conceptual in that precise alignments and sizing of potable and recycled water facilities and wastewater facilities would be determined at the Design Review, Conditional Use Permit, and/or final map stages of Specific Plan implementation in order to conform to Code and governmental agency requirements



and final project design. Construction of these wet utility lines would be linked to the construction of each phase of Specific Plan development and would be linked to the construction of roadways in which the lines are located, and the need to service individual buildings as they become operational and occupied. Development of the utilities systems would be governed by the improvement standards set forth in the Specific Plan. Proposed wet utility improvements include:

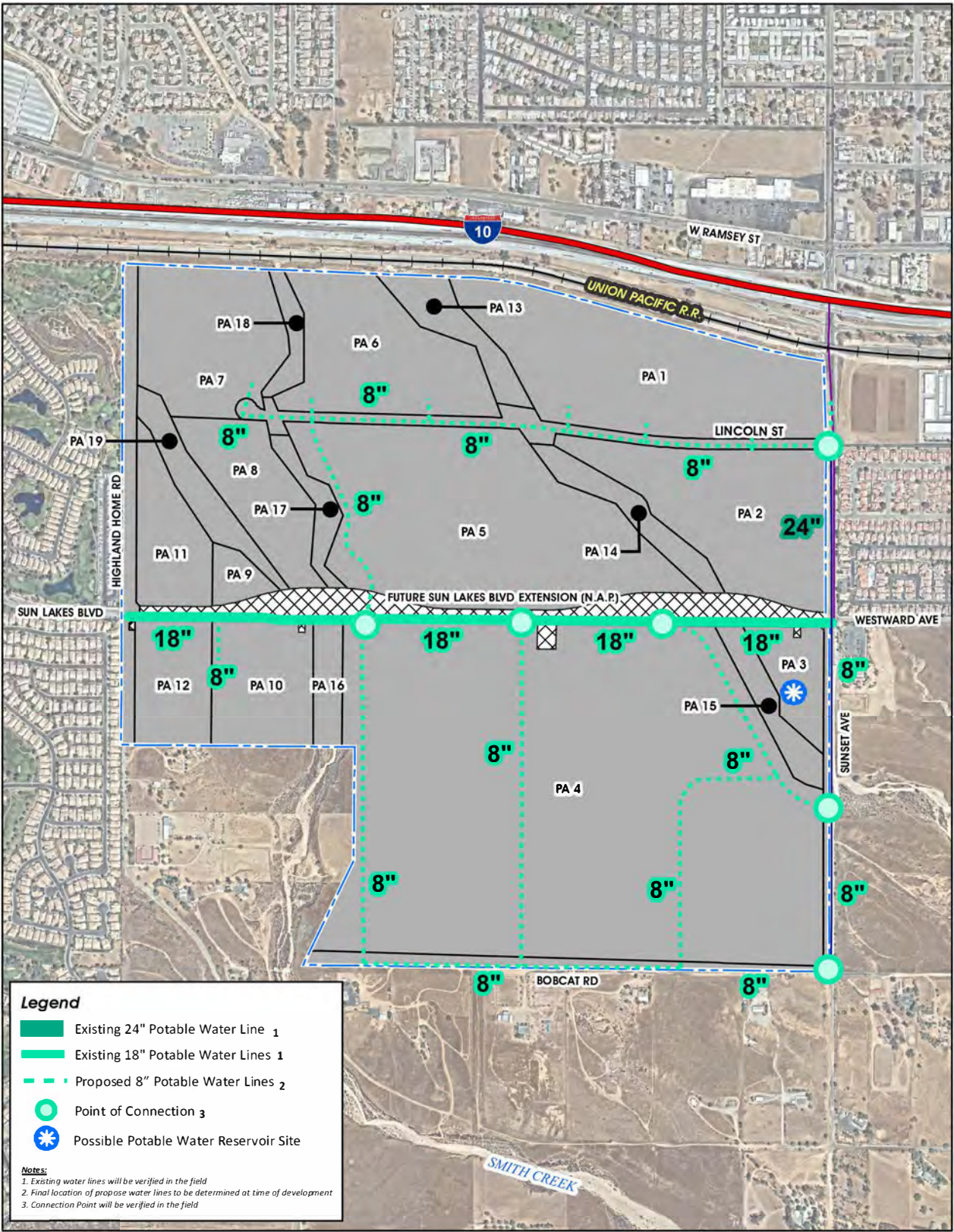
Potable Water. Existing potable water lines within Pressure Zone 2721 exist in Sunset Avenue (24-inch) and the Existing ROW (18-inch). The Specific Plan's proposed Potable Water Plan creates a looped system to serve the Development Site by installing the following improvements:

- A proposed 8-inch water line located in Lincoln Street to serve the industrial and general commercial uses located in the Northern Portion of the Development Site. This proposed 8-inch water line connects to the existing 24-inch water line located in Sunset Avenue.
- A proposed 8-inch water line located in Planning Area 5 connecting the proposed 8-inch line in Lincoln Street to the existing 18-inch water line located in the Existing ROW, forming a looped system.
- A proposed 8-inch water line located in Bobcat Road with a point of connection to the proposed 8-inch line in Sunset Avenue.
- Proposed 8-inch water lines located within Planning Areas 3, 4, and 10 to connect the existing 18-inch water line in the SLB Extension to the proposed 8-inch line in Sunset Avenue and to the proposed 8-foot water line in Bobcat Road.

Proposed potable water lines would be installed underground, and all potable water lines and associated facilities would be designed and installed in accordance with the requirements of the City. The proposed potable water distribution system would provide adequate flows for fire protection in accordance with requirements of the Riverside County Fire Department. **Figure 3-11: Potable Water Improvements of the Specific Plan** shows the potable water infrastructure that would be installed as part of the Development Project.

Recycled Water. Recycled water lines currently exist in Sunset Avenue (24-inch) from the proposed Lincoln Street to the Existing ROW and in Existing ROW between Sunset Avenue and Highland Home Road. The Development Project would connect to the two existing non-potable wells on Westward Avenue for recycled water needs. The proposed recycled water facilities required to serve the Development Site are detailed in **Figure 3-12: Recycled Water Infrastructure of the Specific Plan**. The Conceptual Recycled Water Plan anticipates the installation of the following improvements:

- A proposed 8-inch recycled water line in Lincoln Street would connect to the existing 24-inch recycled water line in Sunset Avenue and would service the uses in the Northern Portion of the Development Site.



Legend

- Existing 24" Potable Water Line 1
- Existing 18" Potable Water Lines 1
- Proposed 8" Potable Water Lines 2
- Point of Connection 3
- * Possible Potable Water Reservoir Site

Notes:

1. Existing water lines will be verified in the field
2. Final location of propose water lines to be determined at time of development
3. Connecton Point will be verified in the field

FIGURE 3-11

LSA



SOURCE: ESRI, Nearmap /2022), Proactive Engineering Consultants /01-2023)

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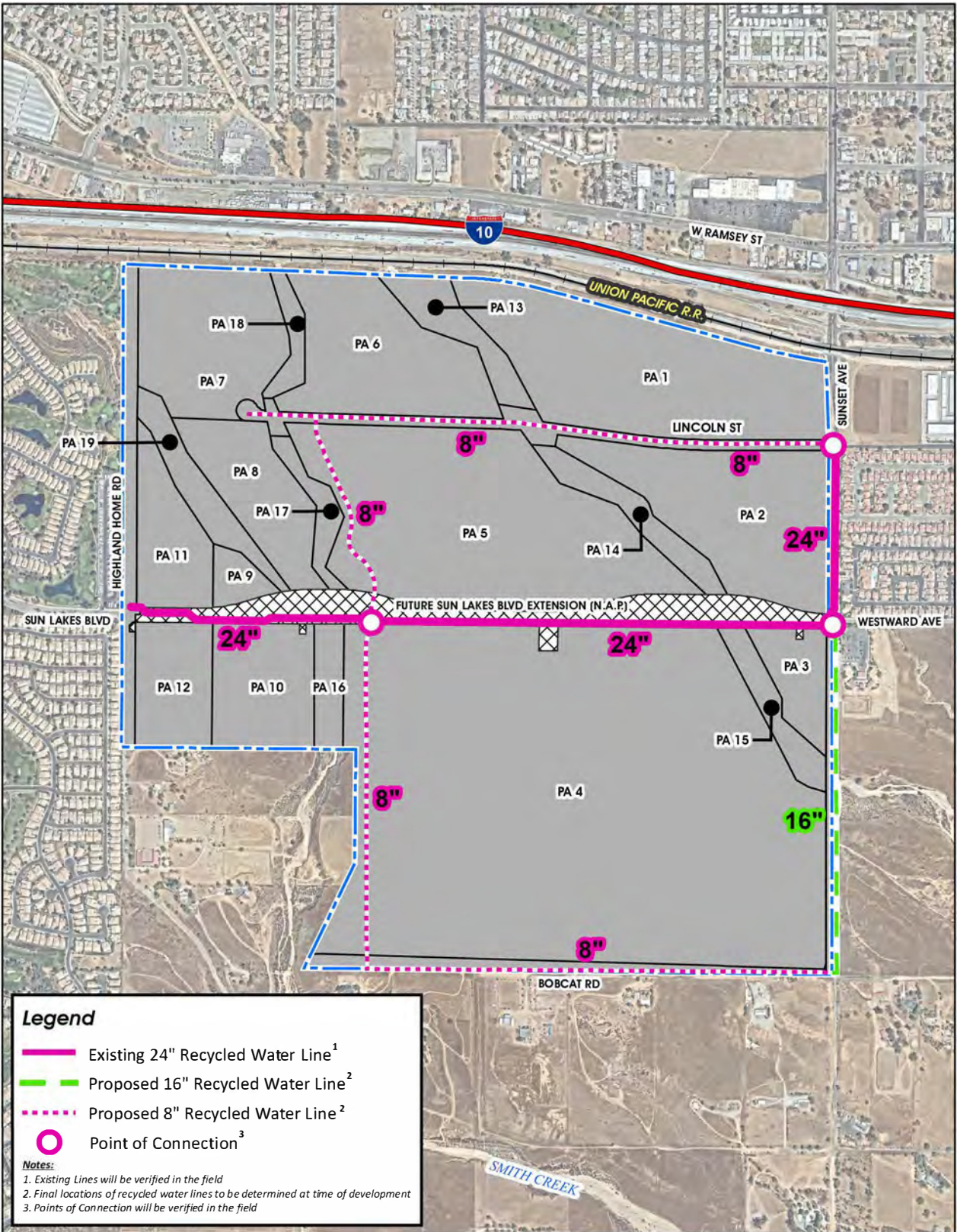


FIGURE 3-12

LSA



SOURCE: ESRI, Nearmap /2022), Proactive Engineering Consultants /01-2023)

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- A proposed 8-inch recycled water line located in Planning Area 5 would connect to the proposed Lincoln Street line. The proposed Lincoln Street line would connect to the existing 24-inch recycled water line in the Existing ROW to create a closed loop system.
- A proposed 8-inch recycled water line along the western portion of Planning Area 4 would connect to the existing 24-inch line in the Existing ROW with proposed 8-inch lines in Bobcat Road and Sunset Avenue (between Bobcat Road and the Existing ROW).
- Development in the Southern Portion of the Development Site would connect to the existing 24-inch recycled water line in the Existing ROW, the proposed 8-inch line in Bobcat Road, or the proposed 8-inch line in Sunset Avenue.

Proposed recycled water lines would be installed underground, and all recycled water lines and associated facilities would be designed and installed in accordance with the requirements of the City.

Wastewater. An existing 12-inch sewer line is in the Existing ROW where effluent flows until it reaches the existing Lift Station within the Existing ROW, where the wastewater is then conveyed through an existing 10-inch force main toward the existing manhole at the intersection of Sunset Avenue and Westward Avenue in the Existing ROW. An existing 15-inch sewer line crosses the Development Site from the northwest to the southeast through Planning Areas 5, 6, and 14. As identified in **Figure 3-13: Conceptual Wastewater Plan of the Specific Plan**, the Development Project includes the installation of the following wastewater conveyance features:

- A proposed 8-inch gravity sewer line would connect to a proposed 12-inch gravity sewer line within Lincoln Street.
- A proposed 12-inch gravity sewer line along Sunset Avenue from Lincoln Street to just south of the Existing ROW at Westward Avenue would connect to the existing sewer system at the existing manhole at the Sunset Avenue/Westward Avenue intersection. There is no connection between this proposed line and the existing 15-inch line along Planning Areas 13 and 14.
- Development in the Planning Areas north of Lincoln Street (Planning Areas 1, 6, and 7) and Planning Areas 2 and 3 would connect to the proposed 8-inch and 12-inch lines in Lincoln Street.
- Development in Planning Area 5 would connect to the existing 15-inch line along the western boundary of Planning Areas 13 and 14.
- Development in Planning Areas in the Southern Portion of the Development Site (i.e., in Planning Areas 3, 4, and 10) would connect with 8-inch lines to the existing 12-inch line in the Existing ROW.

Proposed sewer lines would be installed underground, and all sewer lines and associated facilities would be designed and installed in accordance with the requirements of the City.



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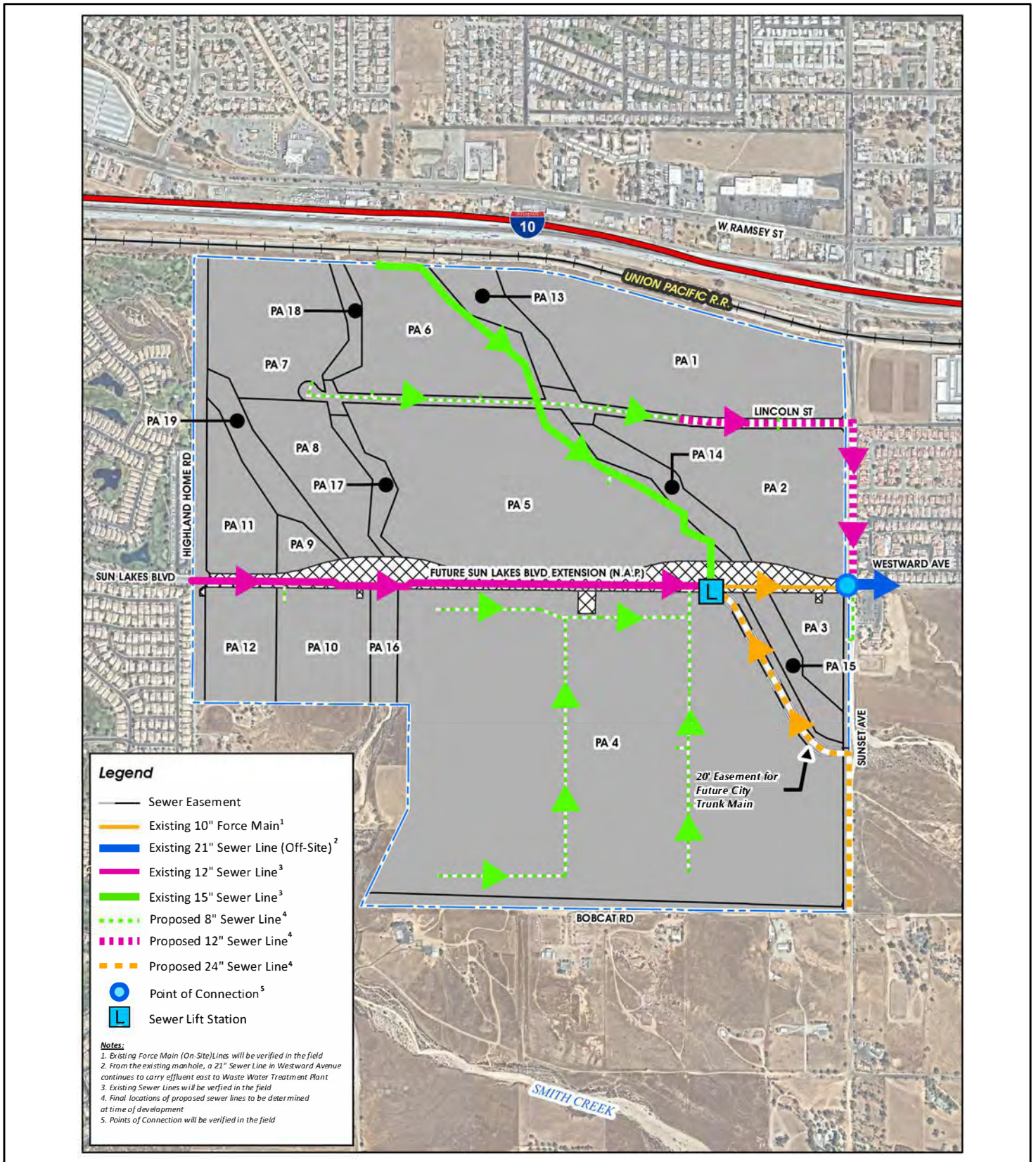


FIGURE 3-13

LSA



SOURCE: ESRI, Nearmap /2022), Proactive Engineering Consultants /01-2023)

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Electricity. The BEU would service the Development Site with as many circuits as the load demands from the existing substation facilities within the City and ultimately via the future City-owned substation that would be located in the northwest corner of the Development Site. The City has proposed to create sufficient capacity at the future substation to service the Development Project at buildout conditions along with other planned development in the City. The initial connection point for the Development Site (prior to development of the City-owned substation) would come from circuit 22 (on poles) near the Lincoln Street/Sunset Avenue intersection, but ultimately the load would transition to the new substation when it becomes operational. All on-site electrical facilities would be undergrounded on the Development Site. City streetlights on the Development Site would be powered by conduit/transformers that would be spaced every 150 feet (depending on photometrics of the Development Site).

Natural Gas. The Southern California Gas Company (SoCalGas) supplies natural gas to the City of Banning. Total natural gas supplies available to SoCalGas are forecast to slightly decrease from 2,462 to 2,103 million cubic feet per day (MMCF/day) between 2020 and 2035 during average temperature years. Total peak demand for SoCalGas natural gas between 2020 and 2026 is expected to slightly decrease from 3,460 to 3,329 MMCF/day.¹⁰ A natural gas pipeline crosses the northern half of the Development Site and is marked by signs and exposed where the pipeline crosses the natural drainages that exist on the site.

3.5.3.5 Landscaping

The Specific Plan establishes landscape principles and standards that apply to all Planning Areas to ensure that plant materials, hardscapes, entries, monuments, streetscapes, and other amenities are compatible with the overall design theme. A combination of drought-tolerant plant material including evergreen and deciduous trees, low shrubs, and masses of groundcovers would be installed throughout the Development Site to create a cohesive and inviting environment for visitors, pedestrians, and passing motorists. Prominent landscape focal points would be installed at most prominent street corners, along roadways, at building entrances, and in passenger car parking lots. Monumentation landscaping featuring colorful accent trees, shrubs, and groundcover would be installed at key corners and driveway entries. In general, the Master Landscape Plan and Conceptual Landscape Zones and Plant Palette, as shown in **Figure 3-14: Master Landscape Plan** and **Figure 3-15a–b: Conceptual Landscape Plan and Conceptual Plant Palette**, respectively, recognize the following categories of landscape within the Development Site with implementation of the Specific Plan:

- **Zone A – Entrance Planting:** Zone A consists of plant material designed to create a strong opening statement. These plants would be arranged for viewing from public roadways such as Sunset Avenue, Lincoln Street, and I-10.

¹⁰ California Gas and Electric Utilities. 2020. *2020 California Gas Report*. Website: https://www.socalgas.com/sites/default/files/2020-10/2020_California_Gas_Report_Joint_UTILITY_Biennial_Comprehensive_Filing.pdf (accessed December 3, 2021).



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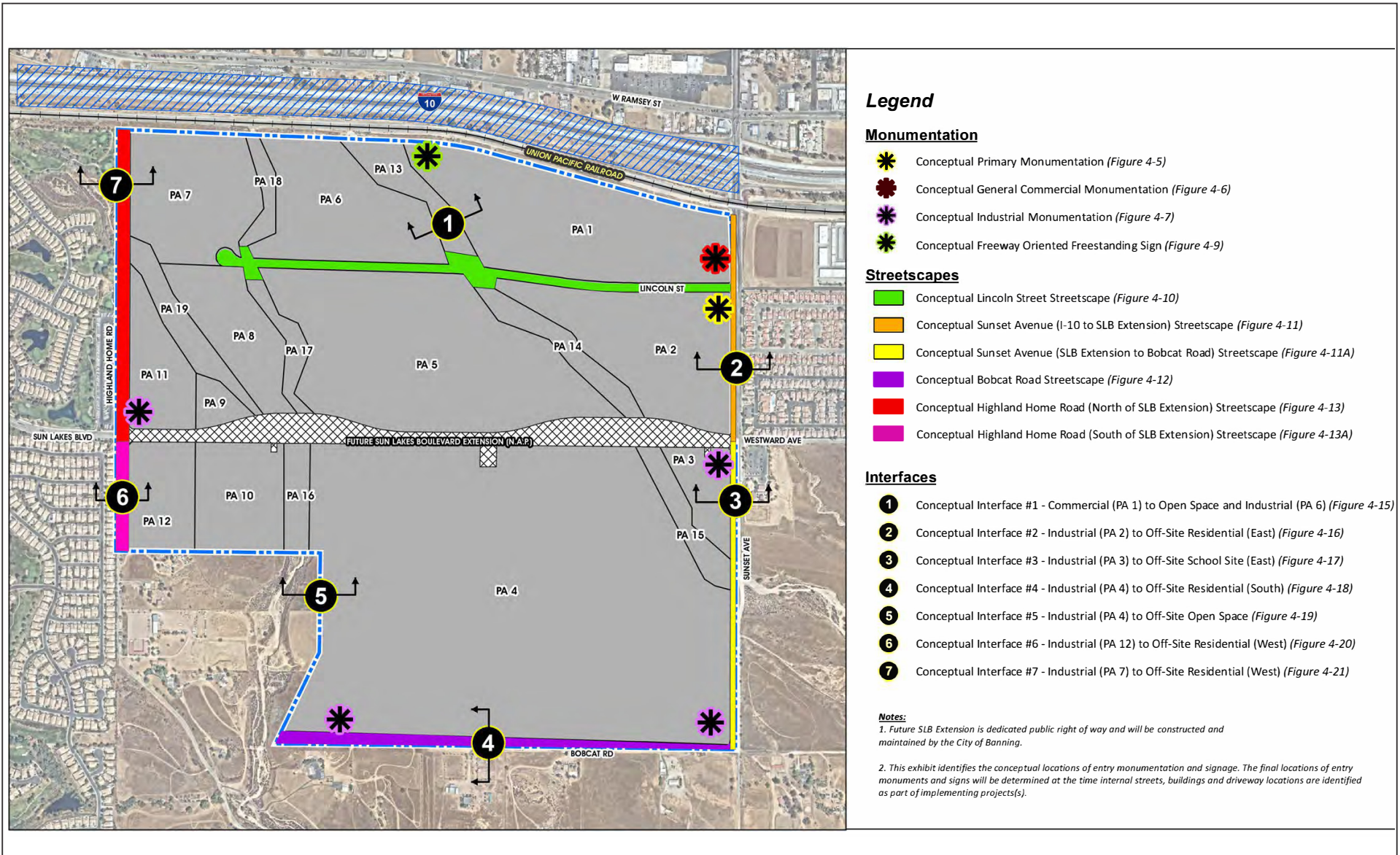


FIGURE 3-14

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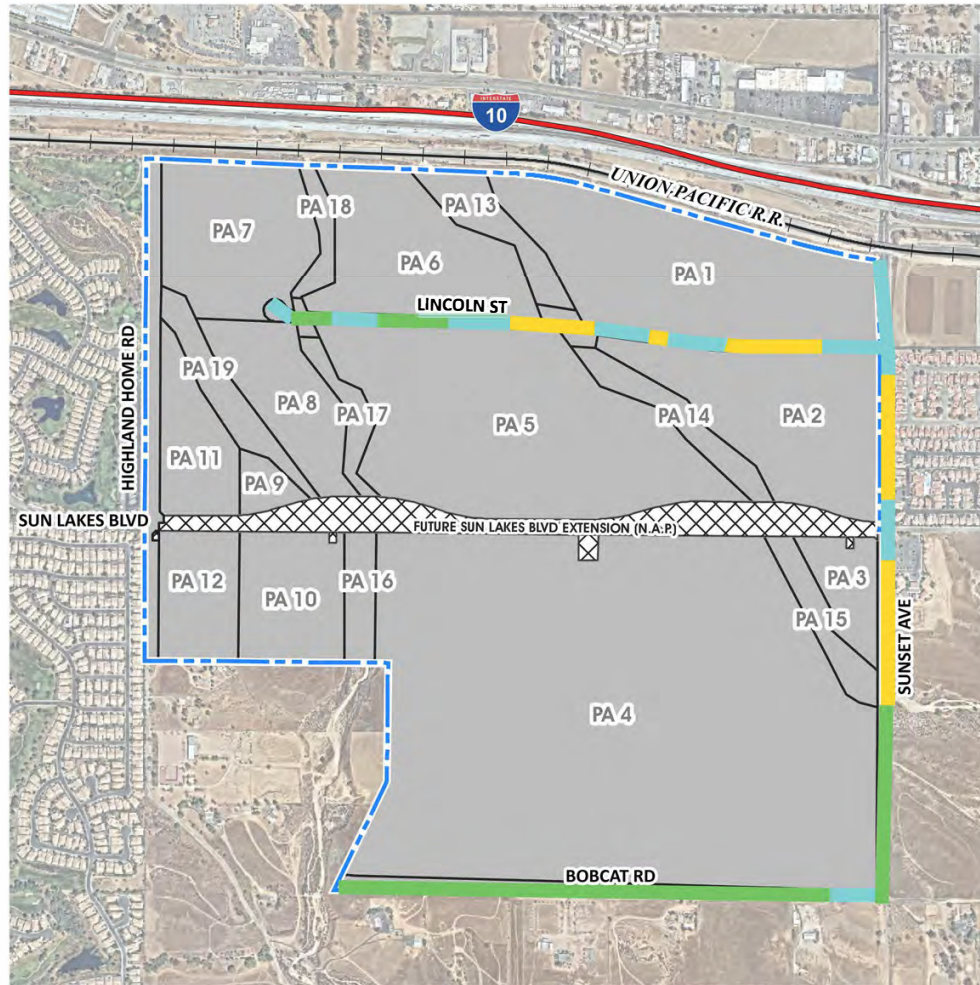


SOURCE: ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023), Hunter Landscape (2021)

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Zone A: Entrance Planting

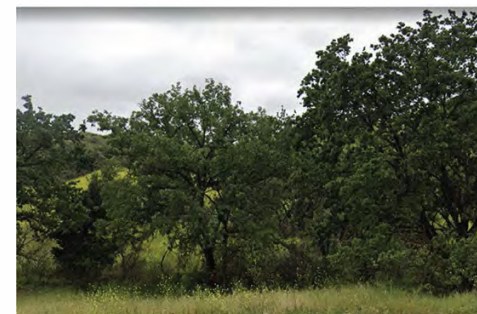
Zone A consists of plant material designed to create a strong opening statement. These plants will be arranged for viewing from roadside as well as for viewing from the highway and Sunset arterial to create intrigue.



Zone B: Native California Palette

Zone B will make up the majority of the aesthetic landscape surrounding the storefront. These plants will make a satisfying backdrop to any visitor's stay.

These plants are native, drought tolerant, and perform admirably in Banning's climate.



Zone C: Industrial Screen Planting

Zone C will be made up of tall, large evergreen trees and thick groundcover all along the periphery slopes of the project. This will screen the industrial buildings and any truck traffic passing through.

LSA



NOT TO SCALE

SOURCE: Hunter Landscape (02-10-2021)

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FIGURE 3-15a



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TREES (SELECTED)



Cercidium 'Desert Museum'
Blue Palo Verde



Pinus eldarica
Afghan Pine



Rhus lancea
African Sumac

SHRUBS (SELECTED)



Salvia greggii
Autumn Sage



Ligustrum i 'texanum'
Texas Privet



Lavandula 'Goodwin Creek Grey'
Lavender

ACCENTS (SELECTED)



Agave parryi
Parry's Agave



Hesperaloe parviflora
Red Yucca



Echinocactus grusonii
Barrel Cactus

GROUNDCOVER (SELECTED)



Acacia r. 'Low Boy'
Acacia



Sesleria autumnalis
Moor Grass



Trachelospermum jasminoides
Star Jasmine

LSA



NOT TO SCALE

SOURCE: Hunter Landscape (09-2021)

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FIGURE 3-15b

Sunset Crossroads
Conceptual Plant Palette



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- **Zone B – Native California Palette:** Zone B would make up the majority of the aesthetic landscape surrounding the entrances and fronts of the buildings within the General Commercial and Industrial areas. Zone B would contain plants that are native and/or drought tolerant.
- **Zone C – Industrial Screen Planting:** Zone C would be made up of tall, large evergreen trees and thick groundcover along the perimeters of Development Site. Landscaping within Zone C is also intended to screen industrial buildings and any truck traffic passing through the Development Site.

The Development Project would incorporate standard streetscape landscaping along Specific Plan roadways and would provide a variety of standard ‘interfaces’ that would provide buffering between the on-site commercial, industrial, and open space uses, as well as between on-site and adjacent off-site uses.

All landscaped areas would be equipped with a permanent, automatic, underground irrigation system conforming to City of Banning requirements. The irrigation system would consist of drip irrigation systems designed to apply water slowly, allowing plants to be deep soaked and to reduce runoff. As deemed feasible and available at the time of installation, irrigation systems would be connected to the on-site recycled water system.

3.5.3.6 Streetscapes

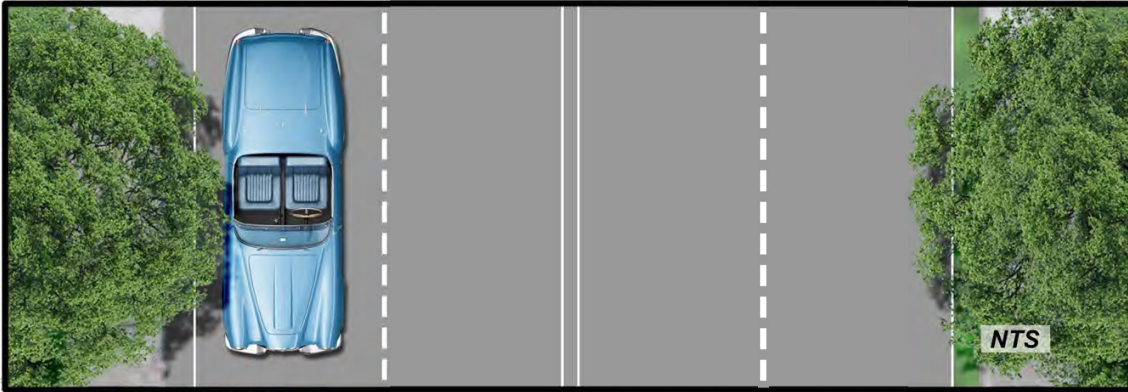
The Specific Plan, as described above, would have an internal circulation system that would be designed with various streetscapes. The streetscapes would be implemented to create a sense of place and to maintain the Development Site’s visual characteristics and theme. The streetscapes of the Specific Plan would also serve the functional purposes of softening and screening components of the uses developed as part of the Specific Plan. Streetscapes throughout the Specific Plan would be planted with a combination of street trees, shrubs, and large masses of groundcover. Landscape treatments would also be incorporated as part of the streetscapes and may include elements such as sidewalks, pedestrian paths, and parkway trees to enhance roadway appearances. **Figures 3-16a–f: Conceptual Streetscapes** show the conceptual streetscapes for Lincoln Street, Sunset Avenue between I-10 and the Westward Avenue, Sunset Avenue between Westward Avenue and Bobcat Road, Bobcat Road, and Highland Home Road in the Specific Plan.

3.5.3.7 Fire Management and Fuel Modification

The entire Development Site has been designed with fire protection as a key objective. The Development Site improvements are designed to facilitate emergency apparatus and personnel access throughout the site. Driveway and road improvements with fire apparatus turnarounds would provide access to the sides of every building. Water availability and flow would be consistent with requirements including fire flow and hydrant distribution required by local and State codes. The Development Site’s Fuel Modification Plan identifies a 100-foot fuel modification zone (FMZ), including a 50-foot-wide paved/irrigated Zone 1 extending out 50 feet from the proposed structures and a 50-foot-wide paved/irrigated Zone 2 extending 50 feet beyond Zone 1 (50 to 100 feet). For those areas where the full 100 feet cannot be met, the reduced FMZ is justified by the minimal width of exposure, low fuel loads, low calculated flame lengths, and the type of construction proposed.



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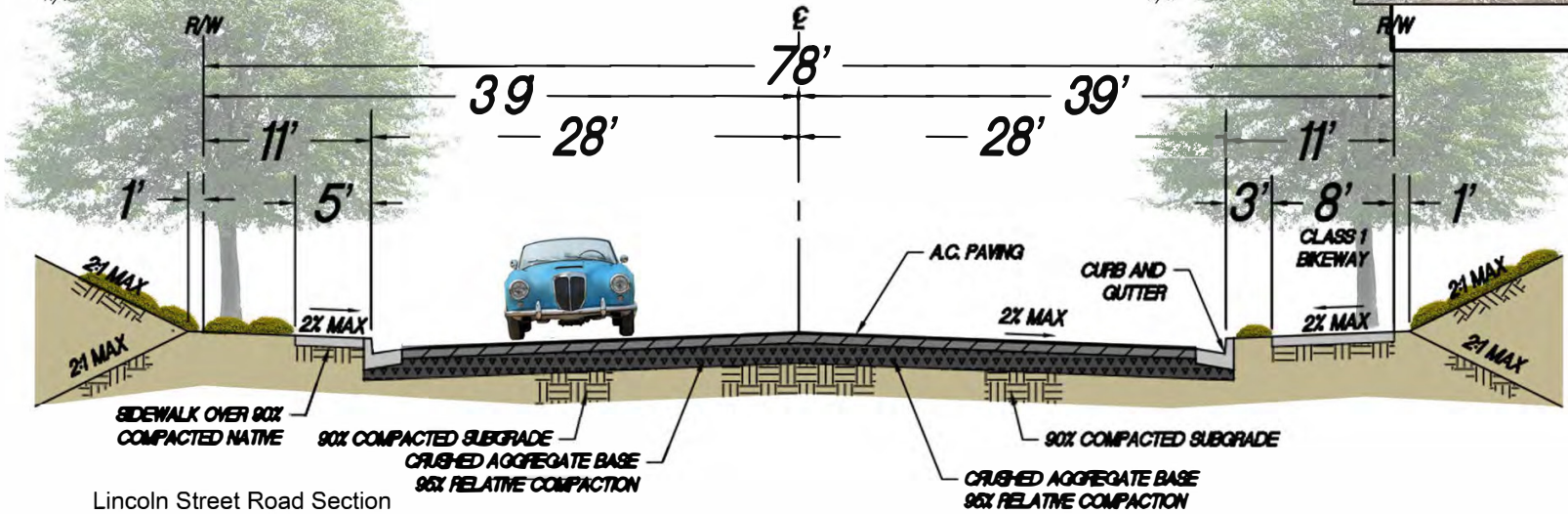


SOUTHERNLY
R/W

NORTHERNLY
R/W



KEY MAP 



LSA

FIGURE 3-16a



NOT TO SCALE

SOURCE: Hunter Landscape (12-02-2022)

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Sunset Crossroads
Conceptual Streetscapes

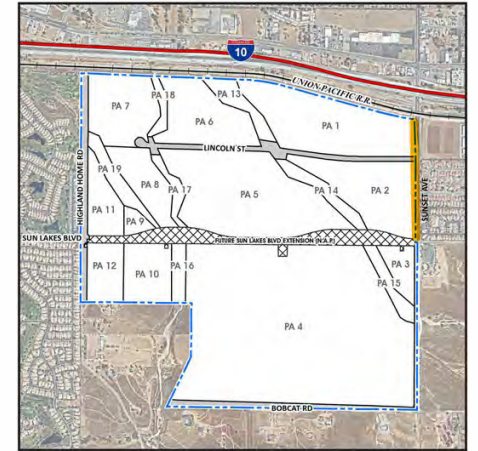



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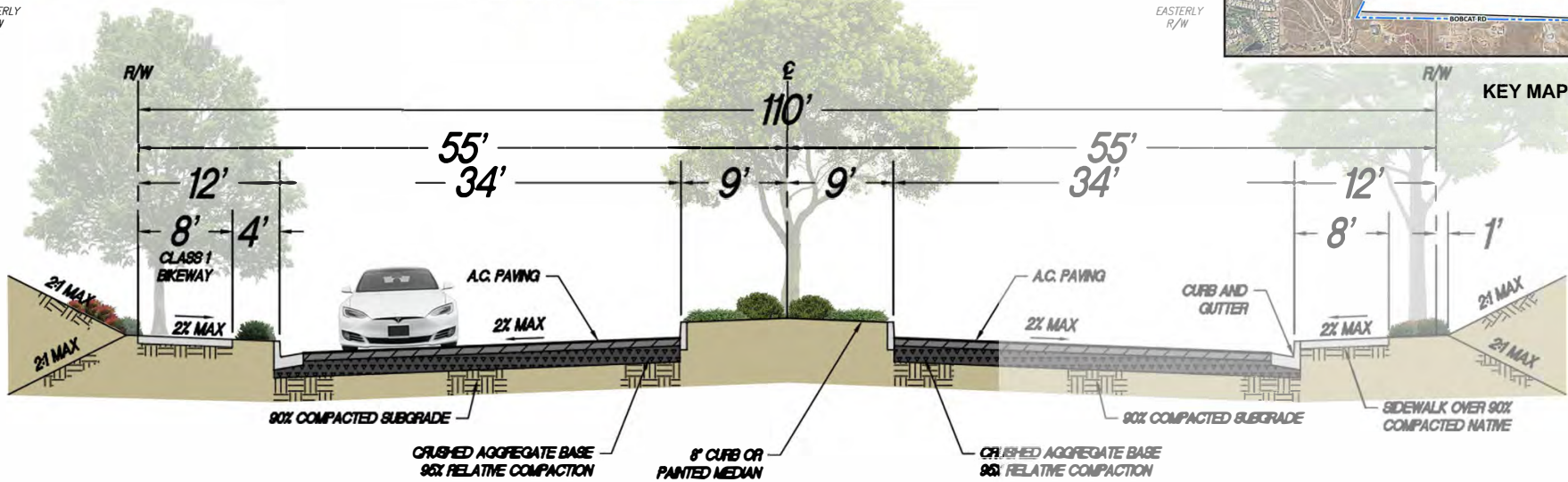


WESTERLY
R/W

EASTERLY
R/W



KEY MAP 



LSA

FIGURE 3-16b



NOT TO SCALE

SOURCE: Hunter Landscape (12-02-2022)

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Sunset Crossroads
Conceptual Streetscapes



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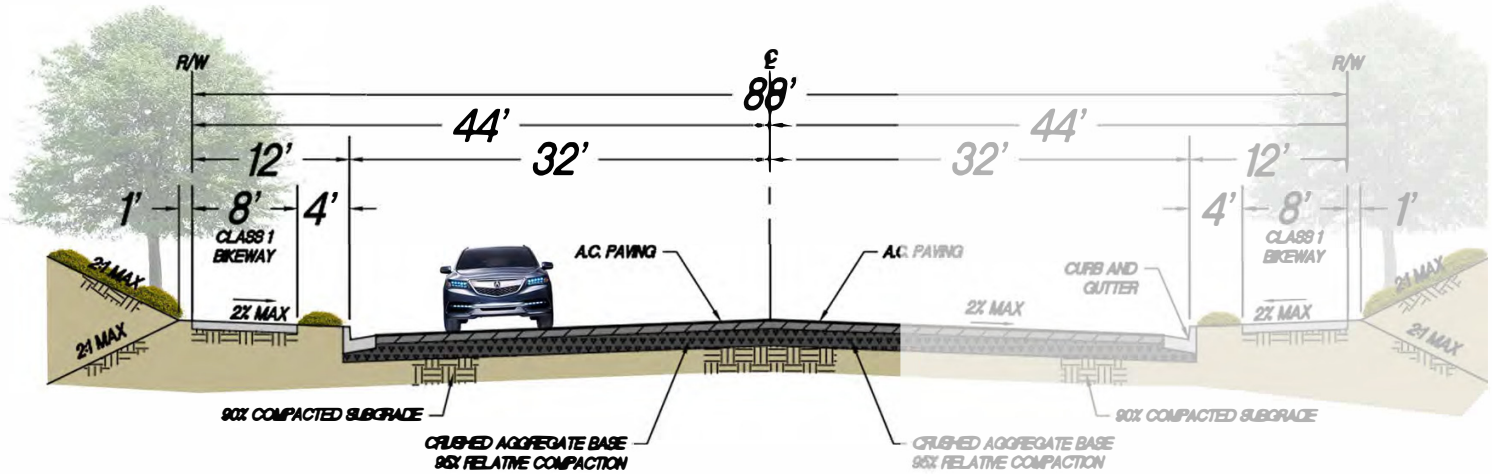


ON-SITE

OFF-SITE



KEY MAP 



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NOT TO SCALE

SOURCE: Hunter Landscape (12-02-2022)

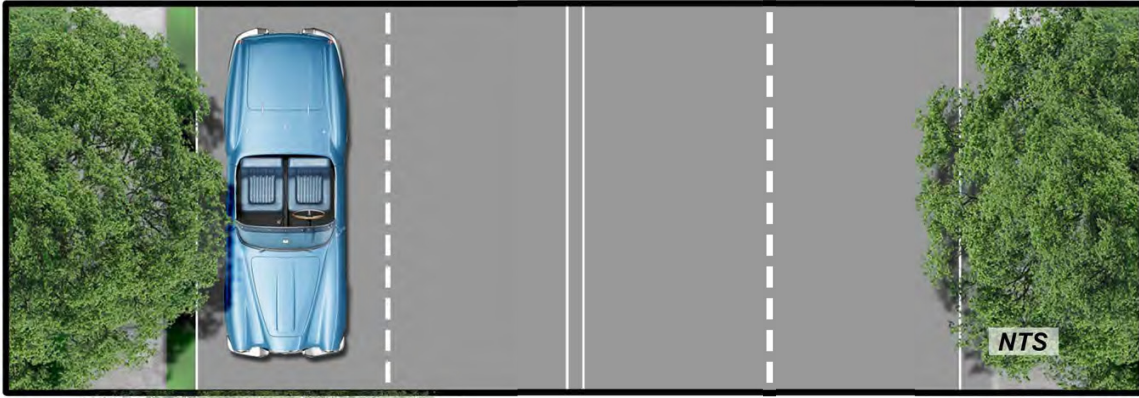
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FIGURE 3-16c

Sunset Crossroads
Conceptual Streetscapes

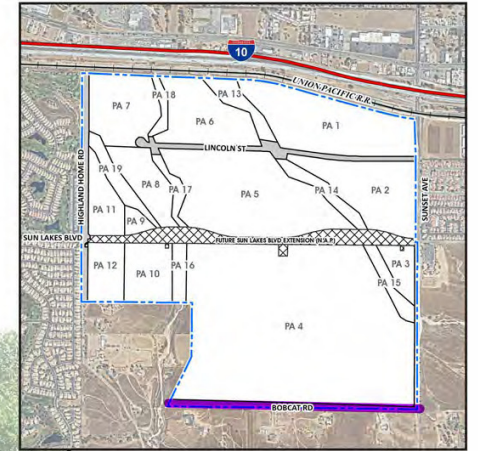


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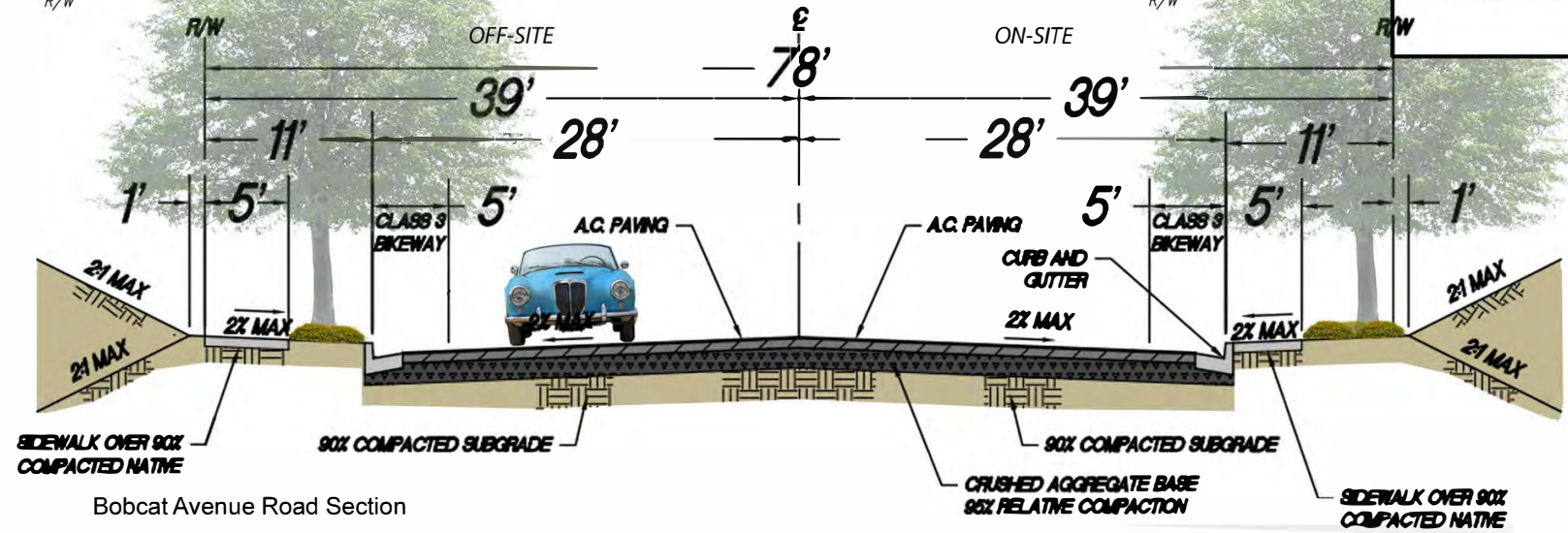


SOUTHERNLY
R/W

NORTHERNLY
R/W



KEY MAP 



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FIGURE 3-16d



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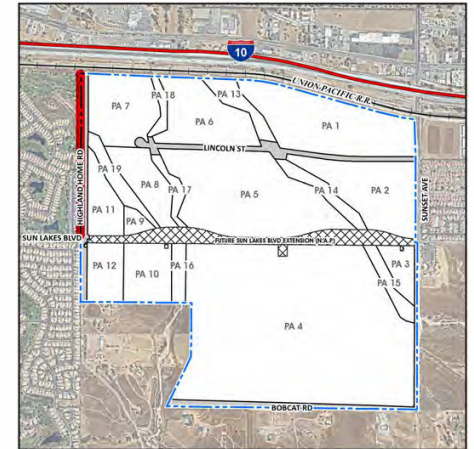
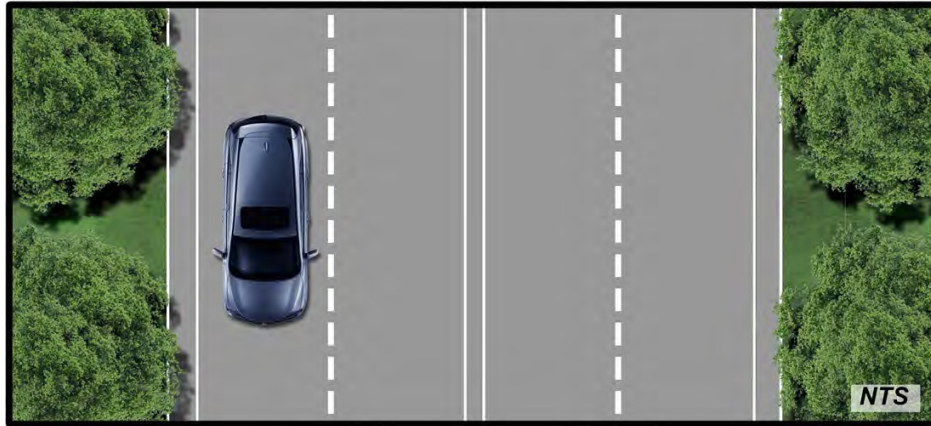
SOURCE: Hunter Landscape (12-02-2022)

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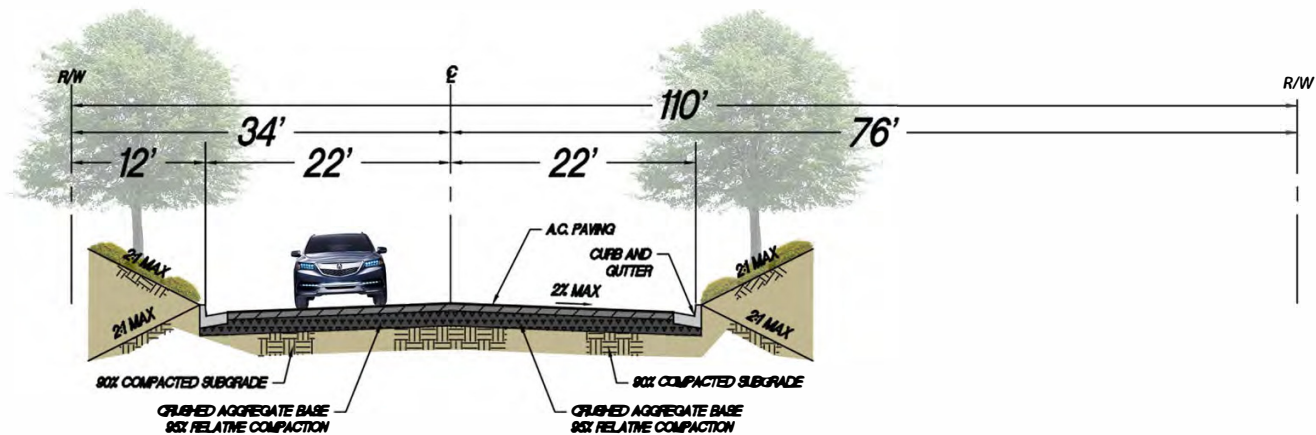
Sunset Crossroads
Conceptual Streetscapes



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KEY MAP 



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SOURCE: Hunter Landscape (12-02-2022)

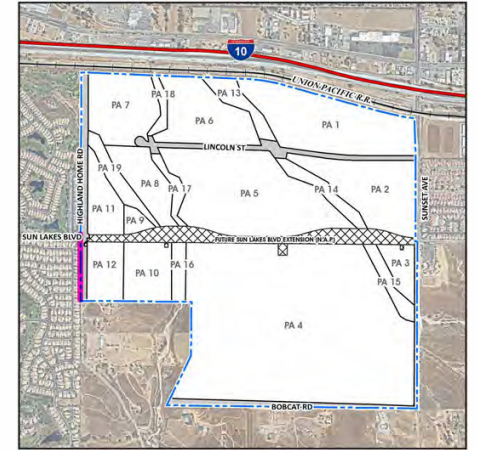
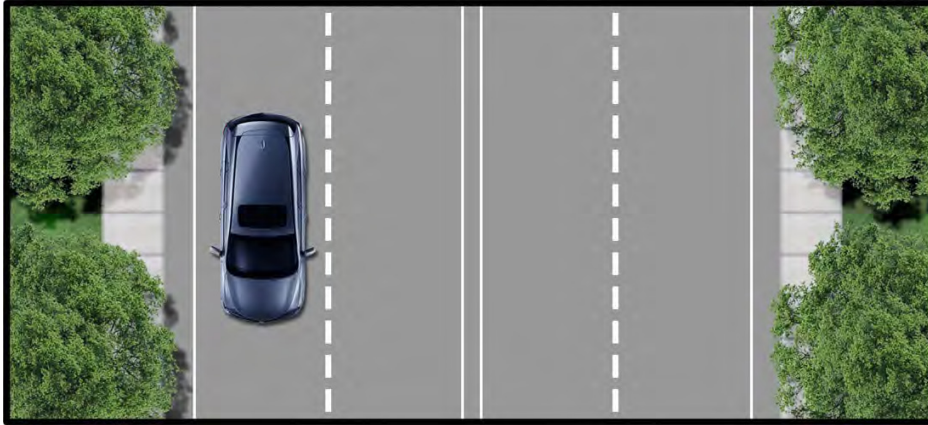
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FIGURE 3-16e

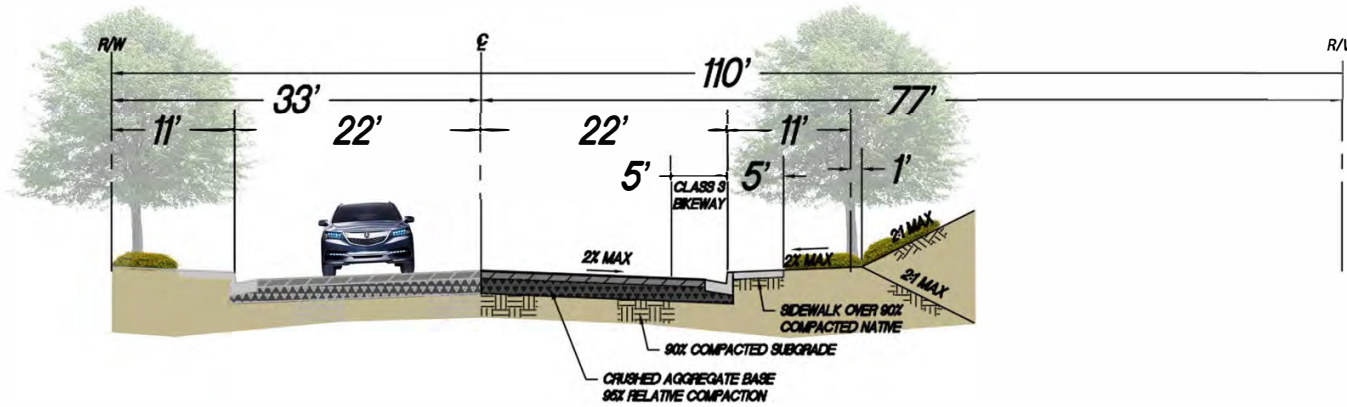
Sunset Crossroads
 Conceptual Streetscapes



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KEY MAP 



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NOT TO SCALE

SOURCE: Hunter Landscape (1-27-2023)

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FIGURE 3-16f

Sunset Crossroads
Conceptual Streetscapes



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In these areas, exterior building construction would be further enhanced to provide a 1-hour to 2-hour rated exterior wall with no openings, or with fire rated and protected door openings, based on requirements and approval of the Riverside County Fire Department (RCFD), and/or a non-combustible wall at the top of slope may be incorporated as a fire protection feature. The Development Project is not required to adhere to the ignition resistant construction requirements of the California Building Code (CBC), Chapter 7A because the Development Site is not located within an area statutorily mapped as a fire hazard severity zone. Regardless, the construction type for the Development Project would comply with Chapter 7A with the possible exception of the building roofs, which would comply with CBC Section 1505.1.3, which requires a minimum Class C. The Fuel Modification Plan of the Specific Plan is detailed in **Figures 3-17a–f: Fuel Modification Plan**.

3.5.3.8 Grading

Three deeply incised existing natural drainages and associated features are located within the Development Site. The existing high point is located at 2,523 feet amsl measured in the northwest corner of the Development Site, and the low point is located at 2,399 feet amsl at the Pershing Wash bed in the southeast corner of the Development Site. The conceptual grading design identifies approximately 2,266,112 cubic yards (CY) of cut and approximately 2,118,698 CY of fill.¹¹ Maximum daily disturbance would be approximately 10,000 CY and would have an average daily disturbance ranging from 7,500 to 9,500 CY. As further described in **Section 3.5.3.9** of this EIR, grading would occur over the entire Development Site in two phases, including the areas where the reverse osmosis facility would be developed by the City. However, except as warranted for the construction of the Lincoln Street crossings of Pershing Wash and Smith Creek, no Development Project associated grading would occur within Planning Areas 13 through 19 (which accommodate the existing natural drainage features). Grading would be governed by the grading improvement standards set forth in Section 2.7.1 of the Specific Plan.

The natural open space located in Planning Area 12 could be graded for trails or related passive park improvements as well as for site development associated with a City-constructed 2.3-acre site for a reverse osmosis facility. Planning Area 11 would include a publicly accessible 5-acre passive park, natural open space, fuel modification areas, trails, and landscaping.

¹¹ The conceptual grading design provides an overall balanced earthwork condition with approximately 2,266,112 cubic yards (CY) of cut and approximately 2,118,698 CY of fill. The grading of the Specific Plan is anticipated to occur in two (2) phases: 1) north of SLB Extension, and 2) south of SLB Extension. No import or exporting of earthwork is anticipated; however, earthwork may be transferred between the two (2) grading phases.



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FIGURE 3-17a

LSA



SOURCE: DUDEK

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Sunset Crossroads
Fuel Modification Plan



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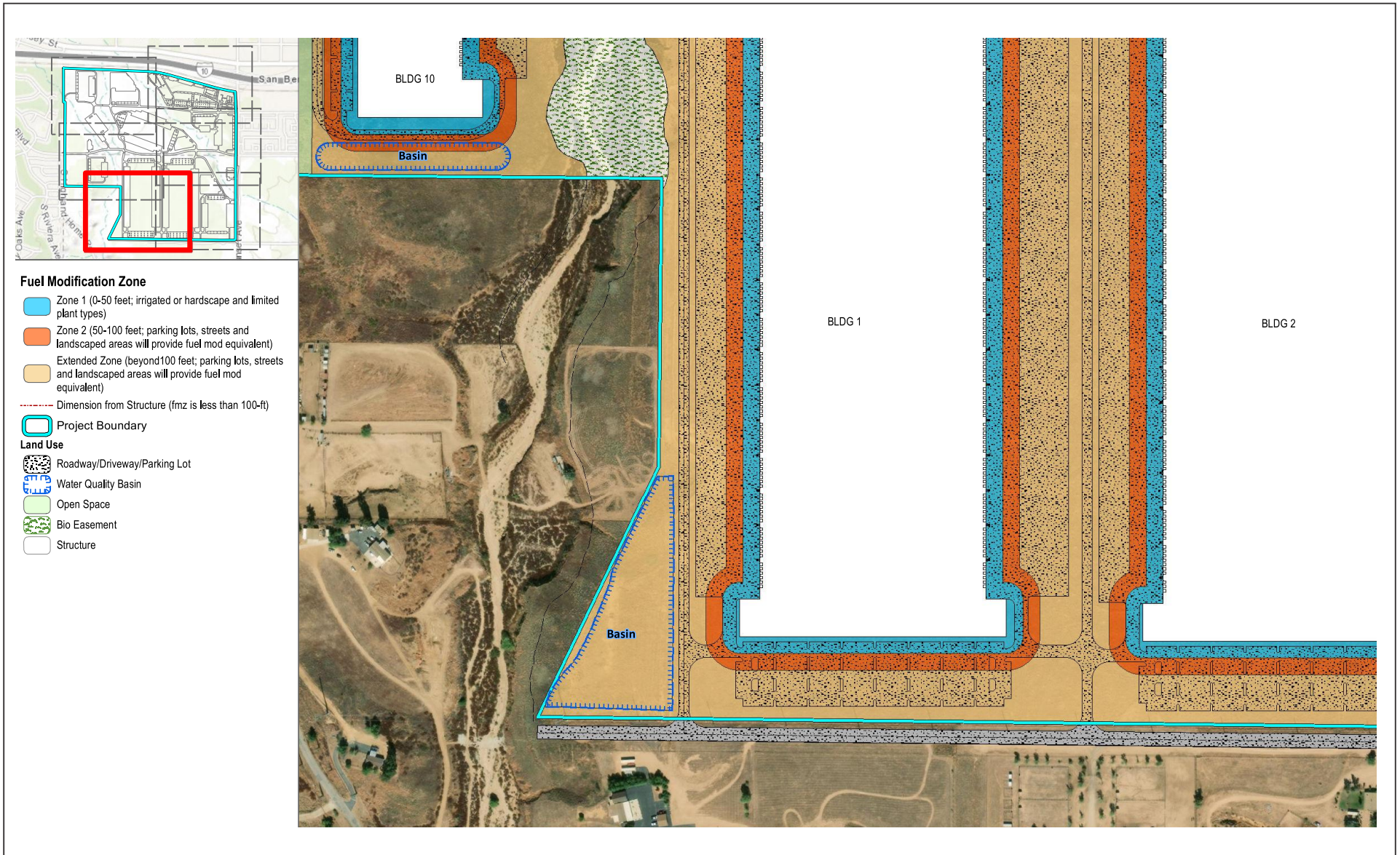


FIGURE 3-17b

LSA



SOURCE: DUDEK

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Sunset Crossroads
Fuel Modification Plan



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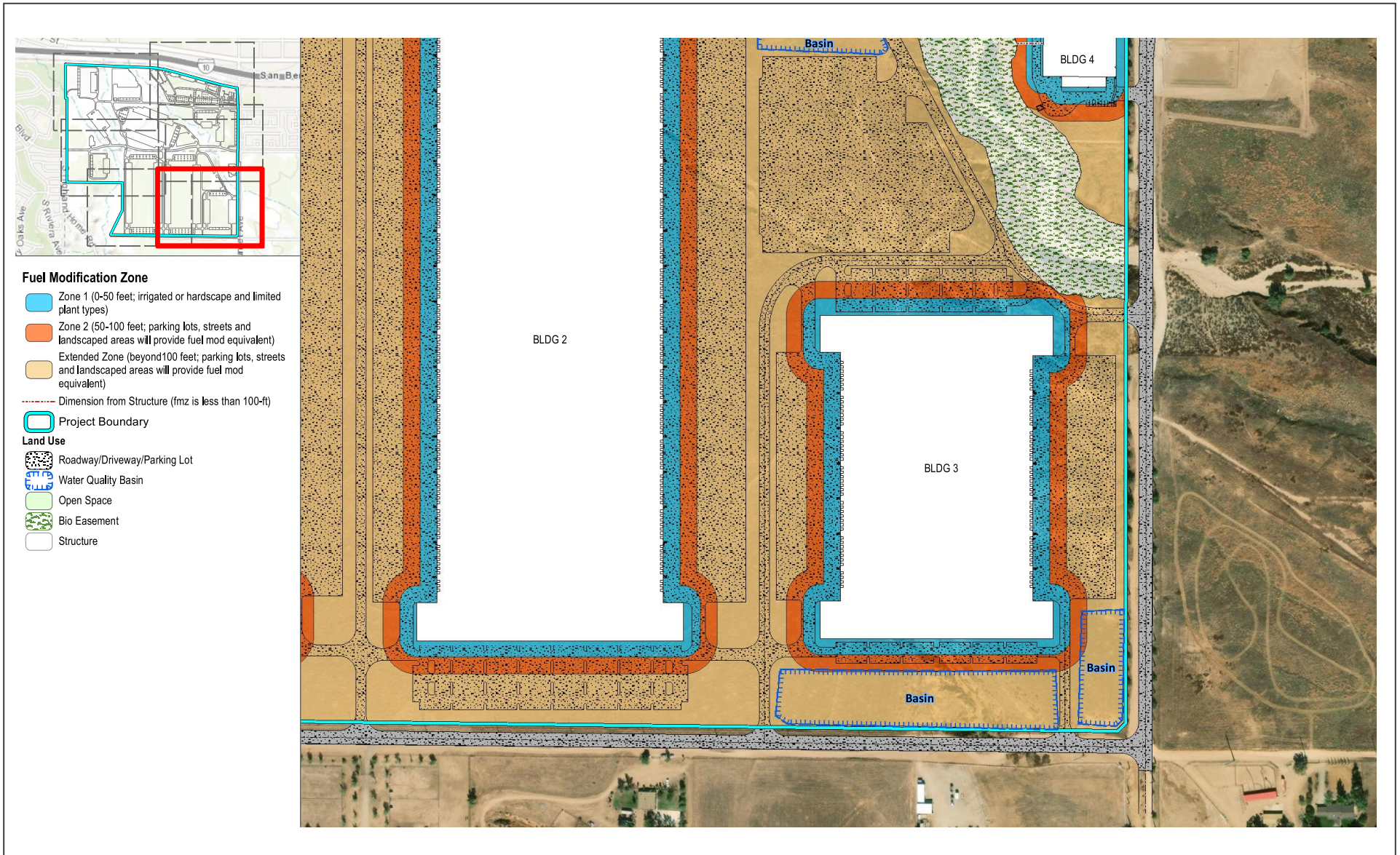


FIGURE 3-17c

LSA



SOURCE: DUDEK

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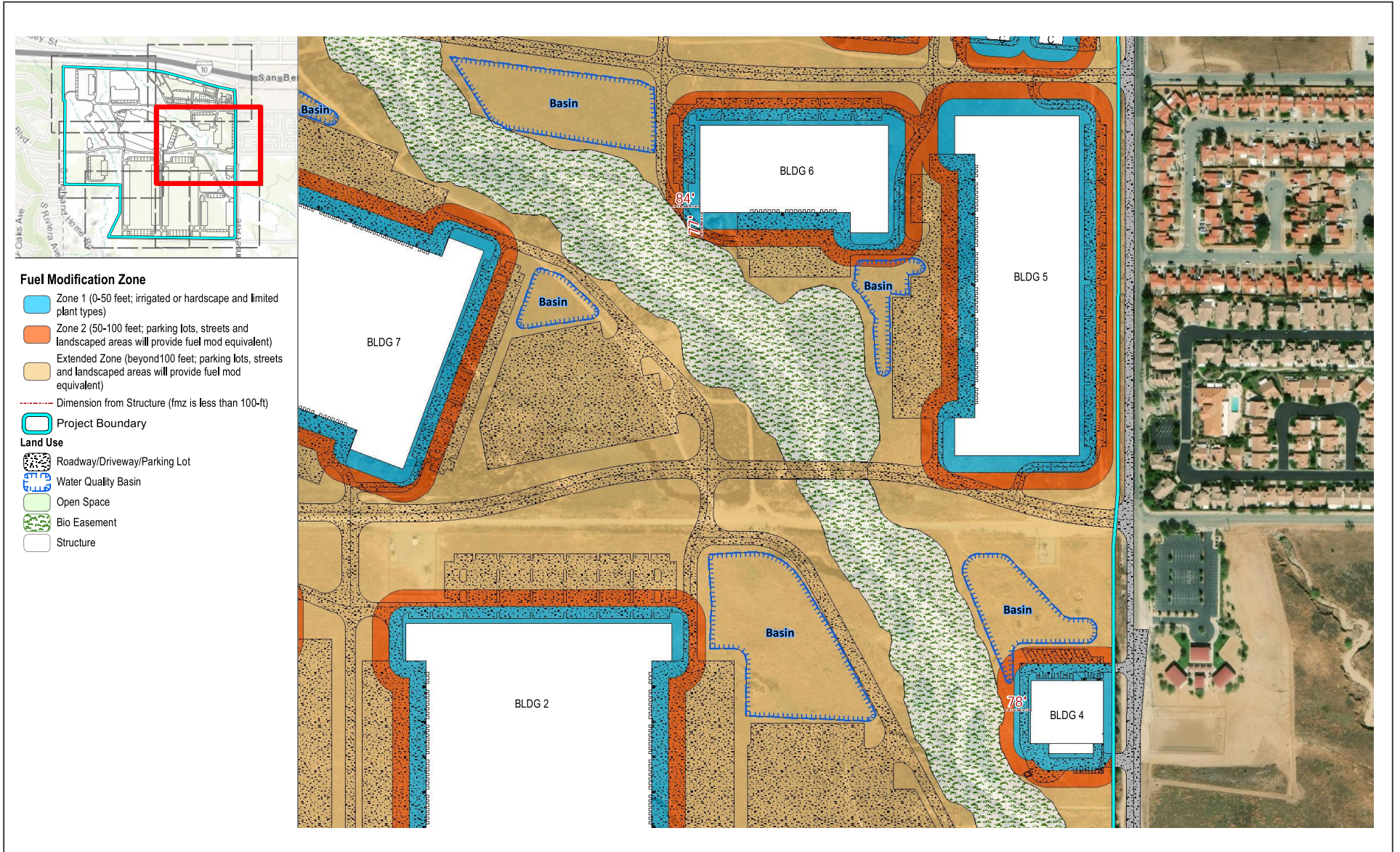


FIGURE 3-17d

LSA



SOURCE: DUDEK

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Sunset Crossroads
Fuel Modification Plan



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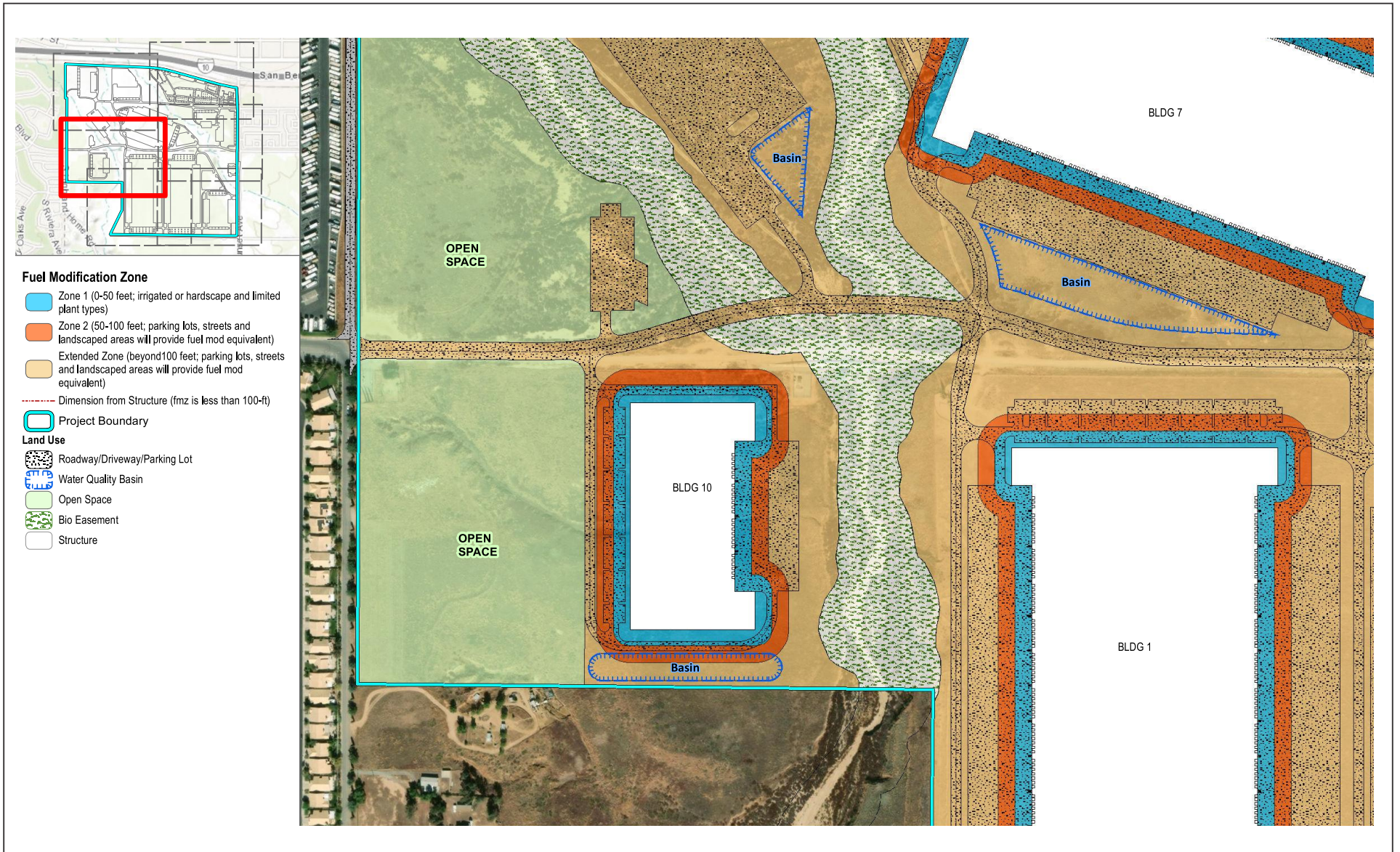


FIGURE 3-17e

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SOURCE: DUDEK

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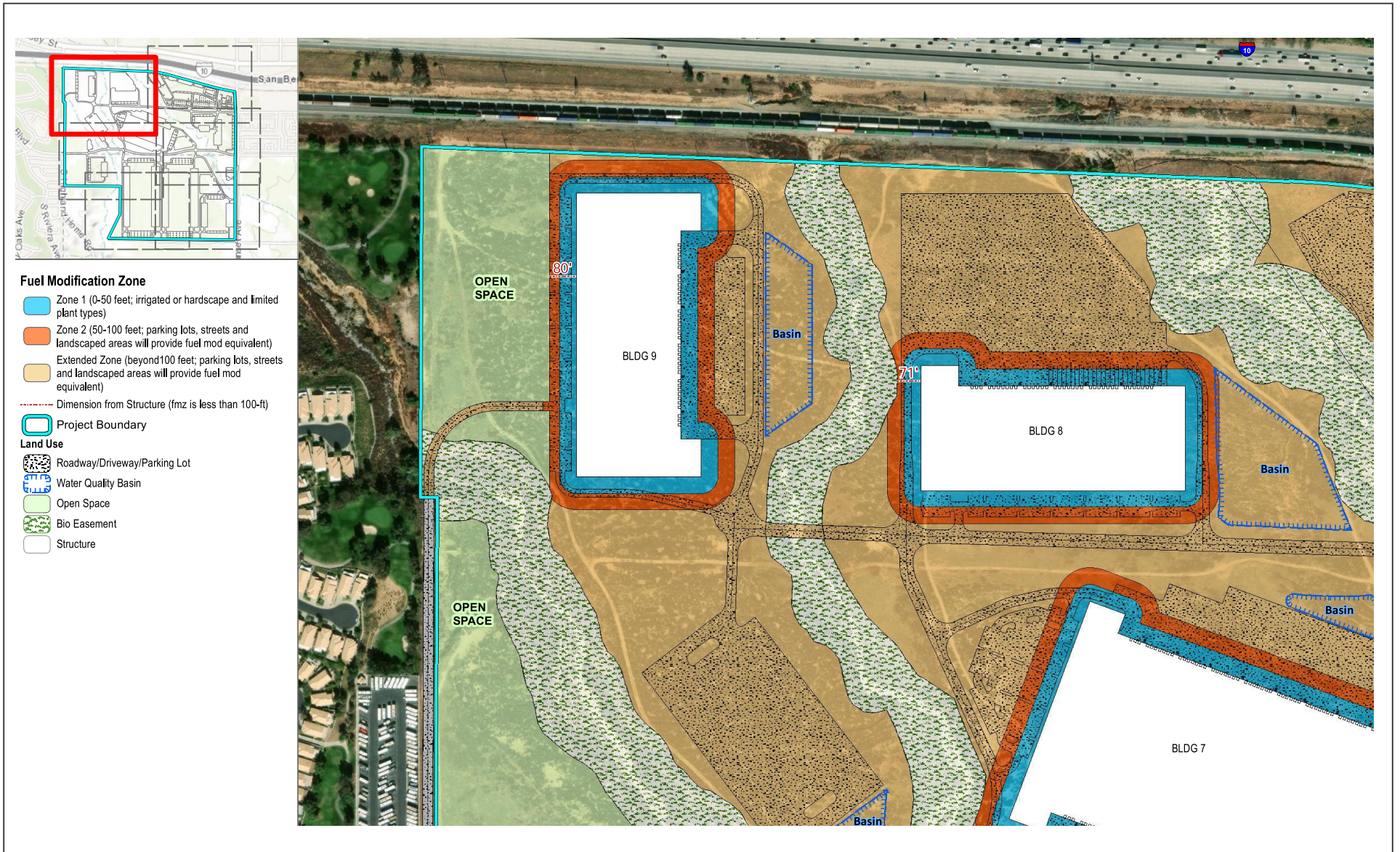


FIGURE 3-17f

LSA



SOURCE: DUDEK

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Sunset Crossroads
Fuel Modification Plan



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3.5.3.9 Grading, Construction, and Phasing of Development Site

Grading of the Development Site is anticipated to occur in two phases, divided between the Northern Portion of the Development Site and the Southern Portion of the Development Site. Although phasing of grading could occur in either order, for purposes of this analysis, a conceptual approach was developed that accelerated the larger buildings within the Development Project into the first phase, and it is therefore assumed that development (including grading) of the Southern Portion of the Development Site would proceed first. This is a more conservative analysis as the construction anticipated for the Southern Portion of the Development Site is more extensive than for the Northern Portion of the Development Site.

As shown in **Figure 3-18: Estimated Construction Schedule**, grading of the Development Site is anticipated to commence in 2024. The first step of the process would involve mass grading of the first phase of the Development Project. While no import or export of earthwork is anticipated, material may be transferred between the two grading phases. Access to the Development Site for grading equipment would be from Sunset Avenue. The balance of the Development Site would be graded to accommodate large generally flat pads suitable for building and parking construction. Except as required to balance earthwork between these two phases of grading, no off-site hauling on City roadways would be required. Upon completion of grading, elevations on the Development Site would range from 2,515 feet amsl in the northwest portion (Planning Area 7) to 2,415 feet amsl at the bottom of the WQMP Basin A located at the southeast corner of the Development Site (Planning Area 4).

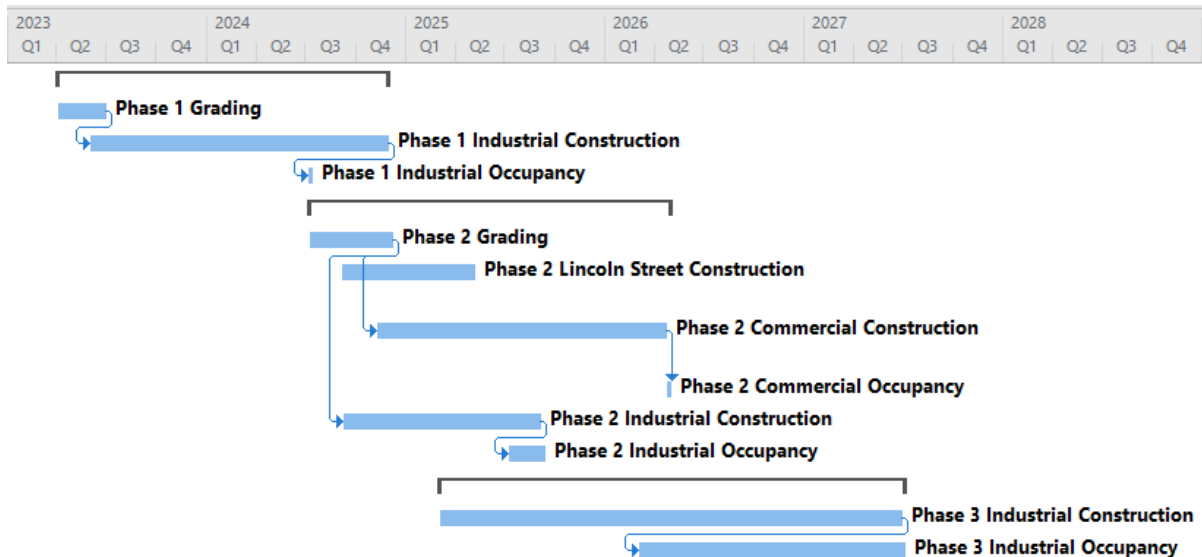


Figure 3-18: Estimated Construction Schedule



Figure 3-19: Conceptual Grading Plan depicts the grading plan for the Development Site. The grading work for the Southern Portion of the Development Site (Phase 1 Grading) is expected to take up to 3 months. This grading would include pad grading for construction of the first building in the Southern Portion of the Development Site, which is expected to be Building 1 as indicated on the site plan.

The proposed construction schedule identifies development on the Development Site in four (4) phases (each a Development Phase). As Development Phase 1, Buildings 1, 2, and 3 could be built in any order but would be expected to start 2 to 4 months apart in 2024, depending on market demand. It is anticipated that Buildings 1 through 4 would be completed and occupied by 2025. Access to these buildings would be provided via the SLB Extension or via Sunset Avenue to Bobcat Road (upon completion of the Sunset Avenue Bridge).

Mass grading of the Northern Portion of the Development Site (referred to herein as Phase 2 Grading) would commence in 2025. The Phase 2 Grading is expected to be completed in 5 months. Phase 2 includes construction of Lincoln Street, which is expected to be completed in 8 months. Construction would move from east to west, allowing access to become available to portions of the Phase 2 development as it is completed (commercial and industrial).

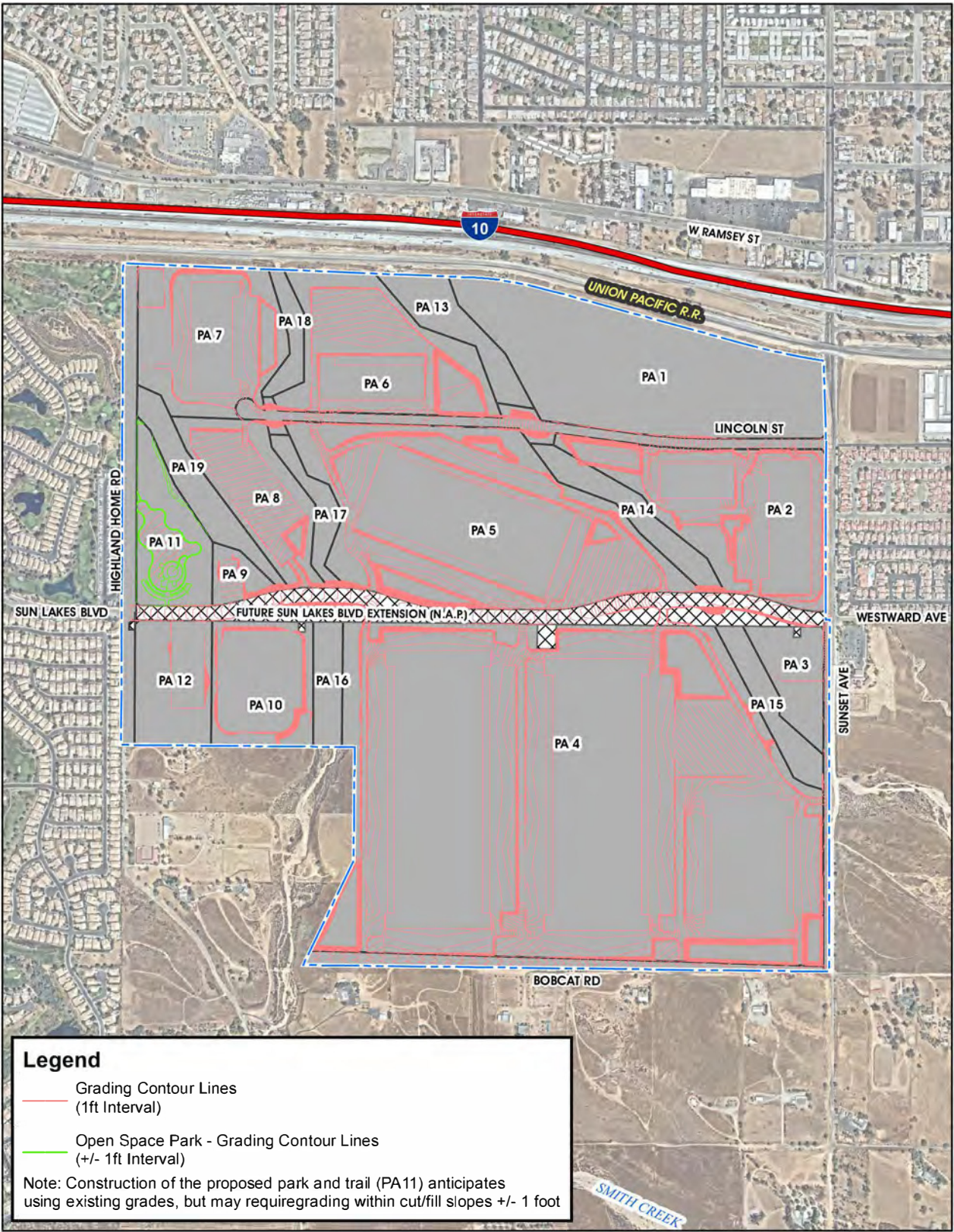
Development Phase 2 includes pad grading for individual lots and buildings in the commercial portion of the Development Project, which is expected to start in 2025, with an anticipated full buildout over the following 18 months, with pace dictated by market demand. Development Phase 2 also includes construction of industrial Buildings 5 and 6 and is anticipated to follow construction of the commercial buildings with occupancy expected in 2026, once access from Lincoln Street is available.

Development Phase 3 includes the buildout of industrial Buildings 7 and 10, with construction starting during 2025 and construction completion and occupancy in 2026. Construction of Development Phase 4, including the remaining two buildings of the Development Project (Buildings 8 and 9), would commence in 2026, with completion in 2027.

3.5.3.10 Sustainability and Energy Efficiency Features

The Development Project is designed with energy efficiency and sustainability features that include those specified in the Specific Plan. The Specific Plan provides that the Project will be consistent with or exceed the applicable criteria from the California Building Code, Title 24 standards, which is updated every 3 years, and which the City has adopted by ordinance. The Specific Plan further specifies that the Development Project may implement new technologies and methodologies if they achieve at least as much environmental protection and do not result in new or greater significant environmental impacts than the technologies or methodologies specified in the following criteria:

- Energy Efficient Structures
 - Enhanced Insulation shall be provided via methods such as rigid wall insulation R-15, roof/attic R-30, etc.
 - Greatly Enhanced Window Insulation with 0.28 or less U-factor, 0.22 or less Solar Heat Gain Coefficient (SHGC), etc. shall be provided.



Legend

- Grading Contour Lines (1ft Interval)
- Open Space Park - Grading Contour Lines (+/- 1ft Interval)

Note: Construction of the proposed park and trail (PA11) anticipates using existing grades, but may require grading within cut/fill slopes +/- 1 foot

FIGURE 3-19



SOURCE: ESRI, Nearmap /2022), Proactive Engineering Consultants (January 2023)

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- Modest Cool Roofs with CRRC Rated 0.15 aged solar reflectance, 0.75 thermal emittance, etc. shall be provided.
- Building roofs shall be solar ready in compliance with California Building Code, Title 24 standards
- Energy Efficient Heating and Cooling (HVAC)
 - Improved Efficiency HVAC (Energy Efficiency Ratio [EER] 14/78% Annual Fuel Utilization Efficiency [AFUE] or 8 Heating Seasonal Performance Factor [HSPF]) shall be provided.
- Energy Efficient Potable Water
 - Improved Efficiency Water Heater (0.72 Energy Factor) shall be provided.
 - Water Efficient Showerheads (1.8 gpm) shall be provided.
 - Water Efficient Toilets/Urinals (1.28 gpm) shall be provided.
 - Water Efficient Faucets (0.5 gpm for lavatories; 1.8 gpm for kitchens) shall be provided.
 - Water Efficient Dishwasher (20% water savings) shall be provided.
- Energy Efficient Appliances
 - Energy efficient lights shall be provided.
 - Energy Star Commercial Refrigerators and Commercial Dishwashers shall be provided.
- Energy Efficient Landscaping
 - Only low water using plants shall be used.
 - Weather based irrigation control systems combined with drip irrigation (demonstrate 20% reduced water) shall be used.
 - Graywater (purple pipe) irrigation system shall be provided on site.
- Energy Efficient Transportation
 - A Car/vanpool program with preferred parking shall be provided within Sunset Crossroads.
 - Bike lockers, secure racks, and/or storage shall be provided.
 - Development shall provide reserved preferential parking spaces for car-share, carpool, and ultra-low or zero emission vehicles.
 - EV charging stations shall be installed in employee garages/parking areas.

In addition to the Specific Plan energy efficiency and sustainability development criteria, the Development Project includes, among other provisions, project design features, and incorporates extensive measures from the County of Riverside 2019 Climate Action Plan (CAP), a regulatory plan to reduce greenhouse gas (GHG) emissions designed to achieve regional GHG reductions from the land use and transportation sectors, even though the 2019 CAP does not apply to the entire project. The sustainability and energy efficiency project design features include the following:

- Occupant sensing lighting that dims to at least 50 percent when unoccupied shall be within the interior areas of warehouses and offices.



- Office space heating within warehouses must utilize heat pumps.
- Development Project street improvements will include sidewalks.
- Secure bicycle storage racks or bicycle lockers, and employee lockers will be provided within the industrial land uses of the Development Project (included in SP).
- Larger parking spaces will be provided that can accommodate vans used for ride-sharing programs and reserve them for vanpools and include adequate passenger waiting/loading areas.
- Provide adequate areas for on-site truck parking, on-site truck queuing, and truck check-in point.
- Post signs clearly showing the designated entry and exit points from the public street to the designated on-site truck check-in and truck parking areas.
- Post signs indicating that all parking and maintenance of trucks must be conducted within the designated on-site areas and not within the surrounding community or public streets.
- Development Project installed traffic signals shall be smart signals that can be synchronized and connected to an ITS system.

3.5.3.11 Off-Site Improvements Required of the Development Project

As part of the Development Project, off-site improvements in the form of adjacent street improvements, utility relocation, utility trenching, and electrical infrastructure undergrounding would need to occur. All off-site improvements would occur within and adjacent to the public roadways and rights-of-way surrounding the Development Site, including but not limited to, street improvements along Sunset Avenue, street improvements and utility relocation along Bobcat Road, roadway improvements and utility trenching at Lincoln Street, and roadway improvements, utility undergrounding, and utility trenching at Highland Home Road. The off-site improvements identified for this Development Project are included in the analysis of this EIR.

3.5.4 Entitlements

3.5.4.1 Development Project Entitlements

General Plan Amendment and Zone Change. The Project includes a General Plan Amendment amending the land use designation for the Development Site from High Density Residential, General Commercial, Medium Density Residential, Open Space – Resources, and Open Space – Parks to the following:

- 392.0 acres of Industrial (LI)
- 47.9 acres of General Commercial
- 53.0 acres of Open Space – Resources (OS-R)
- 12.6 acres of Open Space – Parks (OS-P)

The existing General Commercial land use designation at the northeast corner of the Development Site, would remain generally in its current location. However, a General Plan Amendment and Zone



Change may be required to conform the mapping for the existing General Plan with the Specific Plan, due to variations in mapping techniques over time. The proposed General Plan designations of the Specific Plan on the Development Site are shown in **Figure 3-20: Proposed General Plan Designations of Development Site**.

Annexation. Approximately 253.7 acres of the Development Site are currently located outside the current City boundary within the City’s SOI. As part of the Project, an application to LAFCO would be submitted for annexation of the Southern Portion of the Development Site into the City. The Development Site, including the portions within the SOI, is within the service area for:

- Southern California Edison (SCE)
- Natural Gas (Southern California Gas Company [SoCalGas])
- Solid Waste (Waste Management, Inc.)
- Wastewater (City Water and Wastewater Utilities Department [WWUD])
- Potable Domestic Water (WWUD)
- Storm Drainage (WWUD)
- Banning Electric Utility (Member of the Southern California Public Power Authority [SCPPA])

3.6 MSJC SITE AND MSJC ENTITLEMENTS

As indicated above, the City of Banning has initiated a general plan amendment and rezoning of the MSJC Site to address the loss of residential capacity (identified by the City as a maximum of 1,146 residential units) on the Development Site that would occur if the Development Project is approved by the City. As this is a component of the proposed Project, this EIR (**Chapter 5.0**) analyzes on a programmatic level the reasonably foreseeable impacts associated with the modification of development standards, policies, and conditions applicable to the MSJC Site to achieve no net loss of residential capacity. The MSJC Site is currently assigned a General Plan Land Use designation and zoned “Public Facilities-Schools” and therefore would require a General Plan Amendment/Zoning Change to accept the allocation of residential density.

3.6.1 MSJC Site

The MSJC Site is owned by the Mt. San Jacinto College (MSJC) – Community College District (“District”) and serves as a satellite campus for the District. The current campus facilities, one administration building, two classroom buildings, and one ancillary building, and surface parking are located on three parcels collectively encompassing 8.3 acres¹². The MSJC Site is zoned Public Facilities (PF) with the School (S) suffix, as identified through Chapter 17.16 of the Banning Municipal Code (BMC).

¹² APNs 537-140-001 (1.5 acre), 537-140-002 (5.9 acres), and 537-140-003 (0.9 acre).



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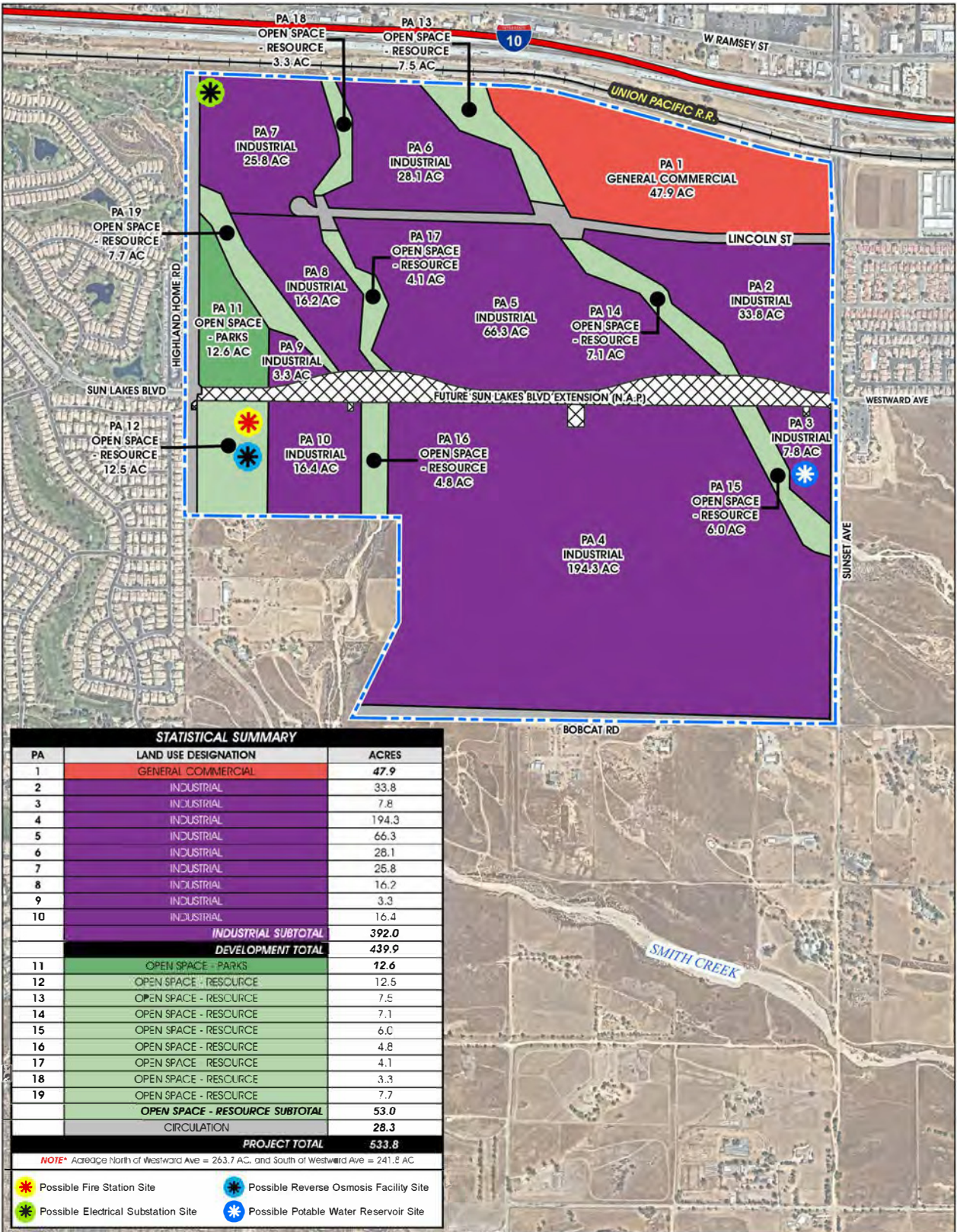


FIGURE 3-20

LSA



0 600 1,200
FEET

SOURCE: NearMap Imagery /2022), RCTLMA /2021)

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Areas outside the current District facilities encompass undeveloped land that generally slopes from northwest to southeast, following the typical drainage characteristics of the City. There are two drainage features that traverse the MSJC Site from the northwest, primarily from Westward and Sunset Avenues. Smith Creek is the primary drainage course that crosses the southern portion of the MSJC Site. The secondary, and smaller, drainage course enters the MSJC Site from a storm drain outlet located approximately 400 linear feet east of the intersection of Sunset and Westward Avenues and is tributary to Smith Creek.

3.6.2 MSJC Entitlements

The City has initiated a General Plan Land Use Amendment (GPA) and a change to the Official Zoning Map (ZC), respectively, on the MSJC Site which will change the land use designation and zoning from PF-S (Public Facilities-Schools) to VHDR (Very High Density Residential), with a density range of 18–24 dwelling units per acre (18–24 DU/AC). The City’s VHDR land use district allows condominiums and townhomes, as well as apartments with the provision of common area amenities and open space. The clustering of condominiums and townhomes is appropriate with the provision of common area amenities and open space. To ensure that the college facilities and any future residential development are compatible, and to provide for the clustering allowed by the City’s Municipal Code, the City will establish a Specific Plan Zoning Overlay (SPZO) (“Overlay”) coterminous with the MSJC Site boundary. The Overlay would require preparation of a Specific Plan, pursuant to Chapter 17.96 of the BMC prior to development of residential uses on the MSJC Site.

While the proposed GPA and ZC will convert the entirety of the MSJC Site to the VHDR zone, no residential development is envisioned at this time. The Overlay will ensure the MSJC Site is developed in a cohesive manner and accounting for the provision of adequate public infrastructure (i.e., roads, storm drain, sewer, potable and recycled water availability) and would provide the opportunity for clustering of residential development, to provide for a mixed-use school facility and residential development on the property. Refer to **Section 3.6.2** of this EIR for further discussion of the proposed entitlements.

3.7 OBJECTIVES

The following objectives of the proposed Project are based on the City’s Vision Statement and Goals from various General Plan elements:

- Establish a functional and balanced pattern of land use that maximizes economic opportunity and provides needed public improvements for City residents.
- Establish land uses for properties in the City’s sphere of influence that will create positive fiscal impact to the City and provide sufficient fiscal benefit to permit annexation of the property upon which the project is proposed into the City.
- Promote job creating uses that reduce the need for City residents to commute outside of the City for employment, thereby improving the City’s jobs to housing ratio.



- Locate industrial and commercial uses that rely on transportation efficiency in areas with convenient access to the local and regional transportation network, thereby minimizing truck traffic on local streets and reducing vehicle miles traveled in the region to the extent feasible.
- Address a need in the City for commercial and industrial land uses that accommodate a variety of modern industrial, business, hospitality, and commercial activities.
- Provide commercial development that allows for a diversified economy, complements existing uses, provides a range of employment opportunities, and promotes a safe and enjoyable shopping experience for residents and visitors.
- Use comprehensive planning tools to create a master-planned development that will be marketable to users, establish an aesthetically pleasing environment, and minimize impacts to adjoining uses.
- Increase City sales and property tax revenues by establishing commercial and industrial uses in the City that can increase City revenues and assist in offsetting public services costs incurred by the City in development and maintenance of housing and public facilities.
- Assist in managing supply and demand for electric services to maintain and increase the existing renewables portfolio standard while minimizing costs to rate payers.
- Assist the City in developing roadway and utility infrastructure to support the anticipated growth requirements of the City and to improve accessibility in areas of the City and the City's sphere of influence that currently have limited infrastructure to serve the needs of local residents and businesses.
- Conserve natural drainage features and open space to provide a balance between the built and natural environment.
- Minimize the demand for water resources and other public services by creating drought tolerant landscaping and encouraging use of recycled water.

3.8 REQUIRED ACTIONS

As established in *CEQA Guidelines* Section 15124(d)(2), "If a public agency must make more than one decision on a project, all its decisions subject to CEQA should be listed." To implement the Development Project as proposed, the following actions are required:

- **Riverside Local Agency Formation Commission (Responsible Agency).**
 - Annexation of the approximately 252.9-acre Southern Portion of the Development Site from unincorporated Riverside County into the City of Banning (following approval of the Project by the City).



- **City of Banning Planning Commission**

- Recommendation to the City Council regarding Certification of the Project EIR No. 2021020011
- Recommendation to the City Council regarding Adoption of Findings of Fact, Statement of Overriding Considerations, and Mitigation Monitoring and Reporting Program
- Recommendation to the City Council regarding General Plan Amendment No. 20-2504
- Recommendation to the City Council regarding the adoption by Ordinance of Zoning and Pre-Zoning of Specific Plan No. 20-20000002
- Recommendation to the City Council regarding the adoption of a Development Agreement
- Recommendation to the City Council regarding approval of Subdivision Map(s)
- Recommendation to the City Council regarding the Annexation of the Specific Plan into the City of Banning
- Recommendation to the City Council regarding General Plan Amendment No. 22-2502 for MSJC Site
- Recommendation to the City Council regarding the adoption by Ordinance of Zoning Change No. 22-3502 for MSJC Site

- **City of Banning City Council**

- Water Supply Assessment Approval
- Certification of the Project EIR No. 2021020011
- Adoption of Findings of Fact, Statement of Overriding Considerations, and Mitigation Monitoring and Reporting Program
- Adoption by Resolution of General Plan Amendment No. 20-2504
- Adoption by Ordinance of Zoning and Pre-Zoning of Specific Plan No. 20-20000002
- Approval by Ordinance of a Development Agreement
- Approval of Subdivision Map(s)
- Approval of Resolution of Annexation of Southern Portion of the Development Site into the City of Banning
- Adoption by Resolution of General Plan Amendment No. 22-2502 for MSJC Site
- Adoption by Ordinance of Zoning Change No. 22-3502 for MSJC Site

Subsequent discretionary actions may include, but not be limited to: Final Maps, Conditional Use Permits, Design Reviews, Subdivision Maps, Grading Permits, Variances, water and sewer system approvals, and encroachment permits.

3.8.1 Other Agencies (as required)

CEQA Guidelines Section 15124(d)(1) further requires the City, to the extent the information is known, to include a list of the agencies that are expected to use the EIR in their decision-making processes, a list of permits and other approvals required to implement the project, and a list of related environmental review/consultation requirements established by federal, State, or local law, regulation, and/or policy. Based on the Project as proposed, the following agencies may require the permits referenced below:



- Regional Water Quality Control Board (Clean Water Act, Section 401)
- California Department of Fish and Wildlife (Streambed Alteration Agreement, Section 1602)
- U.S. Fish and Wildlife Service (404 Permit)
- U.S. Army Corps of Engineers (Clean Water Act, Section 404)
- South Coast Air Quality Management District (Authority to Construct)
- Western Riverside County Regional Conservation Authority (action)
- Riverside County Flood Control and Water Conservation District (action)