

Appendix B

Campus Town Specific Plan



JULY 3, 2019

CAMPUS TOWN Specific Plan

SEASIDE, CALIFORNIA



PREPARED FOR:

City of Seaside

BY:

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Introduction

Overview

The Campus Town Specific Plan is intended to implement the vision of the Fort Ord Reuse Authority Base Reuse Plan and the City's General Plan for a "University Village" adjacent to California State University Monterey Bay. The Campus Town Specific Plan provides the framework for development that will serve residents, visitors, and students in a "new urbanist" community. Key elements of development in the area will be focused on providing arts and entertainment, retail, housing, visitor lodging, and employment space for the University student and faculty population and the broader Seaside community to increase the overall economic opportunity available in the region.



1.1 Specific Plan Organization

This Specific Plan is organized per the following chapters:

Chapter 1- Introduction

This chapter describes the Specific Plan's organization; purpose; goals and policies; authority; relationship to other plans; the Specific Plan Area and scope of development and boundaries; local physical conditions and development limitations; and the public participation process that resulted in the Specific Plan.

Chapter 2 - Form and Character

This chapter describes the Specific Plan vision and the designated sub-areas. This chapter also describes the Form Based Code approach of the Specific Plan.

Chapter 3 - Public Realm Standards and Guidelines

This chapter establishes standards and guidelines for the public realm, including standards and guidelines for thoroughfare networks and types, bicycle networks and facilities, Open Space networks and types, landscape, and streetscape.

Chapter 4 - Private Realm Standards and Guidelines

This chapter identifies the plans that regulate private development in the Specific Plan Area and identifies land use standards, urban standards, and architectural standards that govern private development. This chapter identifies the Maximum Allowable Development in the Specific Plan Area. It also provides conceptual design guidelines.

Chapter 5 - Infrastructure

This chapter provides an overview of the major utility infrastructure improvements needed at buildout of the Specific Plan Area and the public facilities planned to be available.

Chapter 6 - Implementation

This chapter addresses the actions that are necessary to implement the Specific Plan by both the City of Seaside and private developers to achieve the goals of the Specific Plan. It includes a phasing plan and other implementation actions.

Chapter 7 - Glossary

This chapter provides the definitions of defined terms used in this Specific Plan. Defined terms included in the glossary have initial letters capitalized.



1.2 Specific Plan Authority

California State Law authorizes cities to prepare and adopt Specific Plans in accordance with Government Code Sections 65450-65457. Specific Plans are used as a bridge between the General Plan and individual development proposals in a defined area.

This Specific Plan implements the Seaside General Plan for the Campus Town area.

1.3 Specific Plan Hierarchy

This Specific Plan should be interpreted with the following concepts in mind when making subsequent Specific Plan consistency conclusions and implementation decisions. Specific plans attempt to balance a range of competing interests. An agency, therefore, has the discretion to approve a project even though the project is not perfectly consistent with all of a specific plan's policies. (*Sierra Club v. County of Napa* (2004) 121 Cal.App.4th 1490.)

This Specific Plan includes:

- “Standards” are established rules for objective measures to which development must substantially conform, subject to deviations as provided in Chapter 6, Implementation. “Standards” are identified in this Specific Plan by use of the terms “will” or “shall.”
- “Guidelines” are recommended practices that allow greater discretion in their interpretation, implementation, or use. Development in the Specific Plan Area should not be materially inconsistent with Specific Plan Guidelines. “Guidelines” are identified in this Specific Plan by use of the terms “should” or “may.”
- “Conceptual” examples depict one possible design that would substantially conform to the Standards and would be materially consistent with the Guidelines, but are not determinative. The Conceptual examples included in this Specific Plan, including graphic depictions and renderings are Conceptual, including without limitation the location, configuration, and massing buildings, the location and configuration of Streets other than Designated Thoroughfares, the placement and size of Open Space areas, and configuration of public facilities. Features depicted in grey scale on exhibits and renderings within the Specific Plan are Conceptual.
- “Illustrative” examples illustrate one possible design that would substantially conform to the Standards and would be materially consistent with the Guidelines, but are not determinative. The illustrative examples included in this Specific Plan, including photos, are illustrative with respect to all elements depicted therein, including buildings, building location, alignment of streets, placement of Open Space areas and configuration of public facilities.





1.4 Plan Purpose

The purpose of the Campus Town Specific Plan is to implement the Fort Ord Reuse Authority Base Reuse Plan (BRP) and the City of Seaside’s General Plan on the approximately 120 acre Specific Plan Area referred to as the Campus Town area. The area formerly was part of the Fort Ord Army Base, which was closed in 1994 pursuant to Base Realignment and Closure (BRAC) action.

The Specific Plan is a tool authorized by State law that enables a jurisdiction to define a specific vision for the future development of a specified planning area. The Specific Plan contains unique and customized development Standards and Guidelines that enable the City of Seaside to shape the streets and public spaces, and property owners to develop their properties according to the vision of the Specific Plan and consistent with the BRP and the General Plan. For the Campus Town area, the Specific Plan guides public and private investment and construction in a coordinated and integrated way.

The resulting Specific Plan is based upon a “new urbanist” paradigm, characterized by pedestrian amenities, a flexible mix of uses, networked thoroughfares and well-designed public spaces.

Accordingly, the primary purposes of this Specific Plan are to define:

1. A Vision for the future of Campus Town that builds upon the goals and policies of the Fort Ord BRP and the City’s General Plan by providing new opportunities to live, work, and shop in the City of Seaside.
2. Goals and Policies that work in tandem with and refine those of the Fort Ord Reuse Authority’s BRP and its Regional Urban Design Guidelines.
3. Form-based development standards and guidelines, that are customized to deliver development consistent with the community’s vision for its future.
4. Implementation recommendations so that private developers and City and regional officials have a clear understanding of the future context of the Specific Plan Area.

1.5 Plan Goals

The Specific Plan is intended to:

1.5.1 To develop a variety of building types and uses, including entertainment, retail, housing, visitor lodging, and employment space with sufficient resident population in proximity to proposed commercial uses to support a viable Mixed Use Urban Village.

1.5.2 Provide shopping, employment, and housing opportunities for households of various sizes and income levels, in close proximity to one another and the CSUMB campus, to reduce vehicle miles traveled on a per capita basis.

1.5.3 Centrally focus commercial development, typical of historic main streets.

1.5.4 To create a vibrant multimodal transportation network, including improvements which encourage pedestrian and bicycle activity.

1.5.5 To expand the City of Seaside's retail and employment opportunities, including the creation of employment space and live/work space capable of supporting startup businesses.

1.5.6 To create a project, including a land use mix and phasing, that is responsive to market demand and results in an economically viable development that can support the infrastructure investment needed to transform the Plan Area to civilian use.



1.6 Plan Policies

The Specific Plan is intended to:

1.6.1 Create a Master Plan for landscape and public spaces.

1.6.2 Utilize a “complete streets” policy to ensure that all forms of mobility are considered; and that safety for pedestrians and bicyclists is considered alongside safety for vehicle occupants.

1.6.3 Utilize Form-Based Development Standards and Guidelines (Form-Based Code) to address street design, Land Use, Building Types, Frontage Types, architectural qualities, building massing and orientation, parking, fencing, lighting, and signage, among other components.

1.6.4 Use building organization and construction to derive scale and articulation.

1.6.5 Establish a coordinated, mixed use area that supports higher-density housing, shopping, services, jobs, offices, and Open Space.

1.6.6 Create a new employment-designated district in accordance with the terms of the BRP.

1.6.7 Create a development pattern where the majority of residents are within walking distance of a variety of neighborhood-serving uses.

1.6.8 Ensure pedestrian-friendly design through intersection density that averages no less than 235 intersections per square mile, recognizing that intersection density (or block size) directly impacts walkability. See subsection 1.9.8.

1.6.9 Provide for vertically mixed-use buildings in neighborhood centers.

1.6.10 Provide for a variety of Building Types within close proximity to one another, including types that may not yet exist in the area, such as Live/Work or “Maker Spaces.”

1.6.11 Minimize the extent of surface parking lots and garage doors visible from the public space network.

1.6.12 Provide for an east-west street through the Specific Plan Area.



Conceptual Rendering of a Mixed-Use Center
Subject to Change

1.7 Relationship to Other Plans

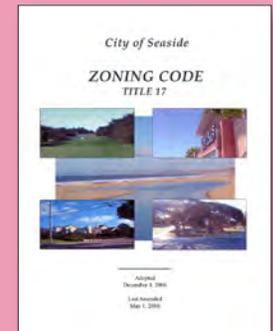
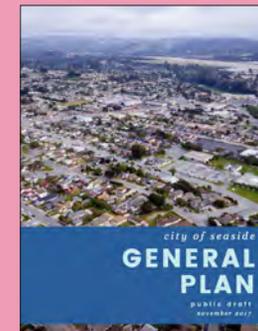
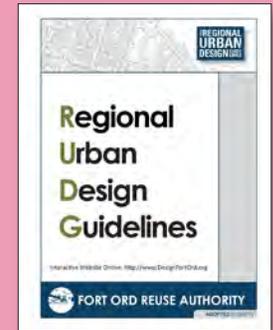
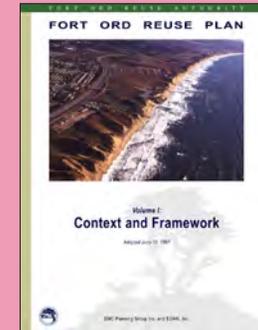
1.7.1 Fort Ord Reuse Authority Base Reuse Plan

The Fort Ord Reuse Authority (FORA) adopted the BRP in 1997 and completed a comprehensive reassessment in 2013. On March 9, 2018 the FORA incorporated text and figure corrections into the BRP, including typographic errors and missing phrases. The BRP primarily designates the Campus Town Specific Plan Area, referred to as “University Village”, as a Planned Development Mixed Use District with Neighborhood Retail. While the boundaries for this Specific Plan are slightly broader than those of the “University Village” planning area in the BRP, the Specific Plan is fully consistent with the BRP designations. It provides that the Specific Plan Area should provide “for market-responsive housing in the University Village District between the CSUMB campus and Gigling Road” and “encourage a vibrant village with significant retail, personal and business services mixed with housing.”

Its specific prescriptions include:

1. Subdivide blocks to promote a mixed-use Urban Village Character.
2. Create a central focus for the Village typical of historic “main streets.”
3. Provide well-designed, pedestrian-oriented streetscapes.
4. Prepare a master landscape plan.
5. Coordinate development within this district with the preparation of a specific plan or other planned development mechanism to achieve the potential integrated design that can be realized in this key mixed-use district.
6. Provide design guidelines to address architectural qualities, building massing and orientation, parking, fencing, lighting, and signage.

While there are several variations between the 2004 General Plan and the BRP, on December 10, 2004 the FORA Board made findings that the 2004 General Plan was consistent with the BRP, and that the differences did not result in conflicts with BRP policies or programs. (FORA Resolution 04-6).



“[Campus Town] is one of the best and most central locations for a neighborhood retail center at the former Fort Ord.”

Base Reuse Plan, p165



1.7.2 Fort Ord Reuse Authority Regional Urban Design Guidelines

The FORA Board adopted the Regional Urban Design Guidelines (RUDG) on June 10, 2016, to provide guidance on matters of visual importance to the former Fort Ord Reuse. The City must consider RUDG when revising proposed land use plans, zoning codes, entitlements and other implementary actions in the Fort Ord.

1.7.3 City of Seaside General Plan

The Campus Town Specific Plan has been designed for the systematic implementation of the City of Seaside's 2004 General Plan and the 2040 General Plan on the approximately 120 acre Specific Plan Area referred to as the Campus Town area. The Specific Plan is a tool authorized by State law that enables a jurisdiction to define a specific vision for the future development of a specified planning area. The Specific Plan contains unique and customized development standards and guidelines that enable the City of Seaside to shape the streets and public spaces, and property owners to develop their properties according to the vision of the Specific Plan and consistent with the General Plan. For the Campus Town area, the Specific Plan guides public and private investment and construction in a highly coordinated and integrated way.

1.7.4 City of Seaside Municipal Code

The Campus Town Specific Plan constitutes the applicable zoning, including development standards and regulations, for development within the Specific Plan Area. Title 17 (Zoning) of the Seaside Municipal Code (SMC) shall apply only to matters not covered in this Specific Plan. If there is any conflict or inconsistency between the provisions of this Specific Plan and the SMC (including without limitation Title 17 (Zoning)), (including any instance in which the Specific Plan is different from or more or less restrictive than the SMC) the provisions of this Specific Plan shall take precedence. By way of example, and not by way of limitation, the Specific Plan shall take precedence over the SMC with respect to the SMC provisions regarding fences, walls, hedges and screening, landscape standards, public improvement requirements, setbacks, parking and loading, signs, and planning approval processes. All applicable state and federal standards and codes also apply to development within the Specific Plan Area.



1.8 Plan Area Boundaries and Scope of Development

1.8.1 Plan Area Boundaries

This Specific Plan is established for the area bounded generally by Lightfighter Drive, Colonel Durham Street, 7th Avenue, Gigling Road, and 1st Avenue, as shown on Figure 1.2 (Specific Plan Area). See Table 1.1 (Specific Plan Area Parcels) for a complete list of parcels included in this Specific Plan.

1.8.2 Scope of Development

The approximately 120-acre Specific Plan Area is intended to be developed with up to 1,485 housing units, 250 hotel rooms, 150,000 square feet of Retail, Dining and Entertainment, and 50,000 square feet of Office, Flex, and Makerspace, and light industrial, as well as park/recreational areas and supporting infrastructure. It is assumed there would be 3.3 persons per equivalent dwelling unit.

APN	
031-151-013 (Portion)	031-151-040
031-151-018	031-151-041
031-151-024	031-151-042
031-151-029	031-151-043
031-151-031	031-151-044
031-151-032	031-151-054
031-151-036 (Portion)	031-151-055
031-151-037	031-151-056
031-151-038	031-261-003
031-151-039	031-261-004

Table 1.1 - Specific Plan Area Parcels



Figure 1.1 - Specific Plan Regional Context



Figure 1.2 - Specific Plan Area

1.9 Local Physical Conditions

1.9.1 Surrounding Assets and Land Uses

The Specific Plan Area is uniquely situated to support all of the three “E’s”: Education, Environment, and Economy that guide base reuse planning and land use decisions per the BRP. The northern boundary of the Campus Town area is the California State University at Monterey Bay. Campus Town is between State Route 1 and the Fort Ord National Monument, providing access to large swaths of land for recreation and conservation. The Specific Plan Area’s proximity to the university and other local developments provides the economic opportunity to truly develop a mixed-use neighborhood that appropriately connects and enhances the existing military family housing communities and services at the southern boundary.

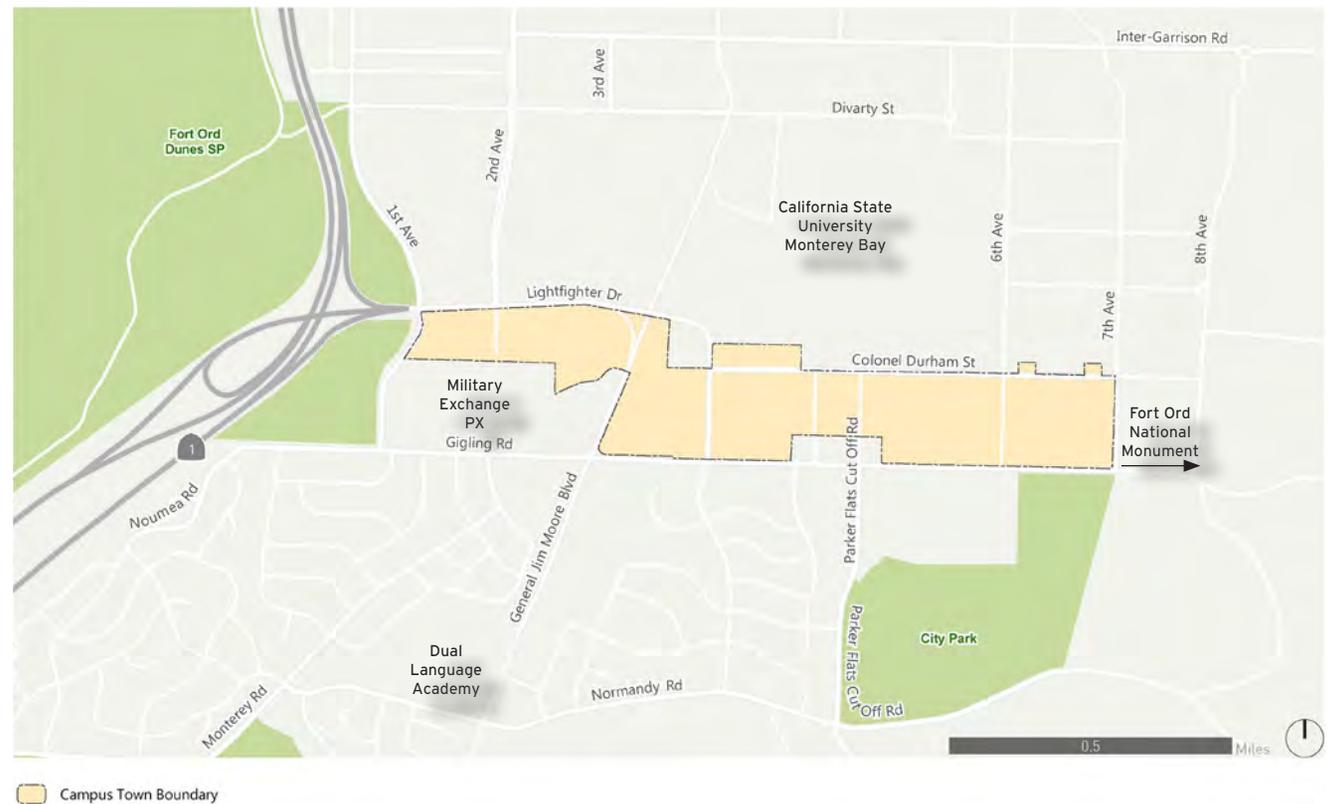


Figure 1.3 - Specific Plan Local Context

1.9.2 Specific Plan Area Elevation Map

An analysis of the Specific Plan Area, Figure 1.4, reveals a gradual elevational change from approximately 170 ft above sea level (asl) at 1st Avenue and Lightfighter Drive at the northwestern tip of the Specific Plan Area to approximately 345 ft asl at 7th Avenue and Gigling Road in the southeast.

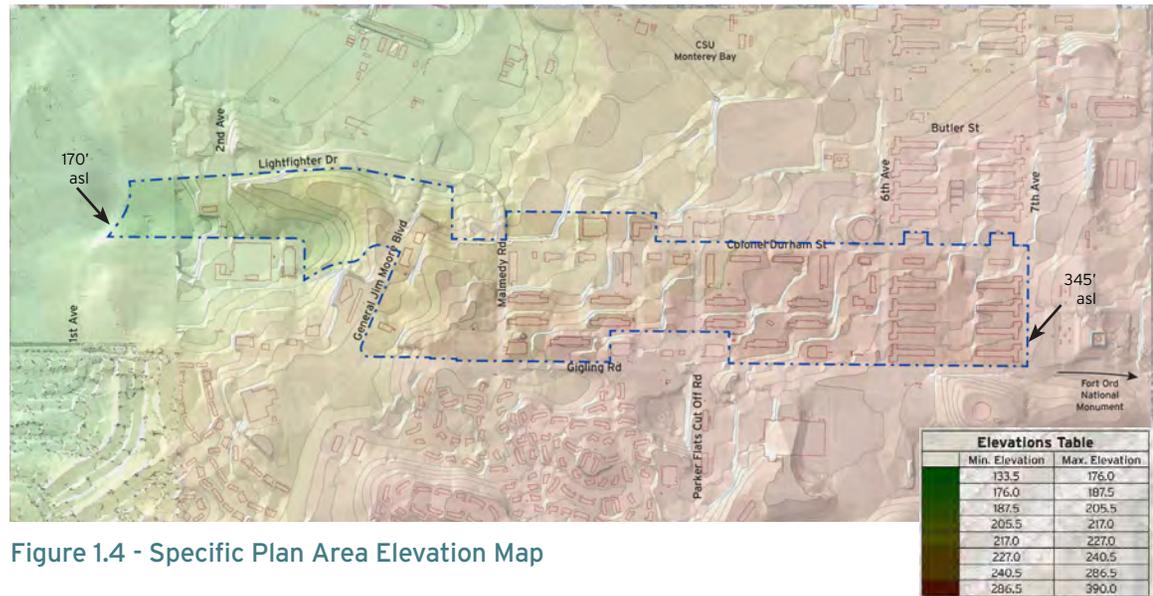


Figure 1.4 - Specific Plan Area Elevation Map

1.9.3 Specific Plan Area Slope Map

The majority of the Specific Plan Area has a slope between 1% and 6%, depicted as yellow in Figure 1.5. For reference, a maximum slope of 5% is considered suitable for accessible ramps (terracing required for steeper slopes). This means the Specific Plan Area is relatively flat and developable for the intended development, provided that flexibility is retained to accommodate landscaped slopes and retaining walls. Orange and brown areas are steeper slopes and are the exception in the Specific Plan Area.



Figure 1.5 - Specific Plan Area Slope Map

1.9.4 Existing Street System

Regional access to the Specific Plan Area is provided by State Route (SR) 1. Primary local access is through 2nd Avenue, General Jim Moore Boulevard, 6th and 7th Avenues on the north side of the Specific Plan Area, Lightfighter Drive and Gigling Road on the west of the Specific Plan Area, General Jim Moore Boulevard and Parker Flats Cut Off Road on the south side of the Specific Plan Area, and Colonel Durham Street and Gigling Road on the east side of the Specific Plan Area. These streets are described below and illustrated in Figure 1.6.

State Route 1 (SR 1) is a state highway within Monterey County, providing access to Watsonville and Santa Cruz to the north via Seaside, Marina, and Castroville, and to San Luis Obispo to the south via Monterey and Carmel. Through its connection to SR 156 in Castroville, it also provides access to US 101 and the greater San Francisco Bay Area. Through Marina and Seaside, SR 1 has a posted speed limit of 65 miles per hour (mph), and provides four lanes north of the Del Monte Boulevard interchange, six lanes south of Del Monte Boulevard interchange to the Fremont Boulevard/Del Monte Boulevard interchange, and returns to four lanes south of the Fremont

Boulevard/Del Monte Boulevard interchange. SR 1 is identified as part of the regional truck network. The highway is intended to move goods efficiently within the cities of Marina and Seaside, between outlying agricultural uses, and packing/distribution centers. Additionally, the highway serves to separate truck traffic from local streets where the larger vehicles may conflict with other uses.

2nd Avenue connects Lightfighter Drive in Seaside with Imjin Parkway in Marina, along the western edge of California State University, Monterey Bay (CSUMB). 2nd Avenue is a north-south arterial street in Marina and Seaside with four lanes from Imjin Parkway to Tenth Street, two-lanes from Tenth Street to Divarty Street, and returns to four lanes south of Divarty Street. 2nd Avenue has right-turn and left-turn channelization on the entire stretch of the street. The thoroughfare has bike lanes north of Divarty Street to Imjin Parkway. The posted speed limit on 2nd Avenue is 35 mph.

General Jim Moore Boulevard is a four-lane arterial with a 35 mph speed limit that extends from Canyon del Rey Boulevard to Lightfighter Drive in Seaside. In Marina, the street is a two-lane arterial from Lightfighter Drive to 5th Street with a posted speed limit of 30 mph.

6th Avenue is a north-south connector that connects the Campus Town Specific Plan Area and the CSUMB campus. 6th Avenue extends from Gigling Road to the south to 8th Street to the north.

7th Avenue is a north-south connector that connects the Campus Town Specific Plan Area and the CSUMB campus.

Parker Flats Cut Off Road is a north-south connector that connects the Campus Town Specific Plan Area and military housing communities to the south. Parker Flats Cut Off Road extends from Colonel Durham Street to Eucalyptus Road.

Lightfighter Drive starts from the SR 1 ramps as an east-west street that continues as the north-south street, Malmedy Road at Colonel Durham Street. From the SR 1 interchange to General Jim Moore Boulevard, the street is a four-lane divided major arterial with a 40 mph speed limit. East of General Jim Moore Boulevard, Lightfighter Drive is a two-lane minor arterial with a 25 mph speed limit.

Colonel Durham Street is a two-lane local street that extends between Lightfighter Drive to the west and 8th Avenue to the east. The street has pedestrian facilities along one or both sides west of 6th Avenue, and although it is a local street, the speed limit is 35 mph along its entirety.

Gigling Road is a two-lane arterial that extends from west of SR 1 to 8th Avenue, and extends past Seaside as Watkins Gate Road that ends at Reservation Road. The street has a 30 mph speed limit.

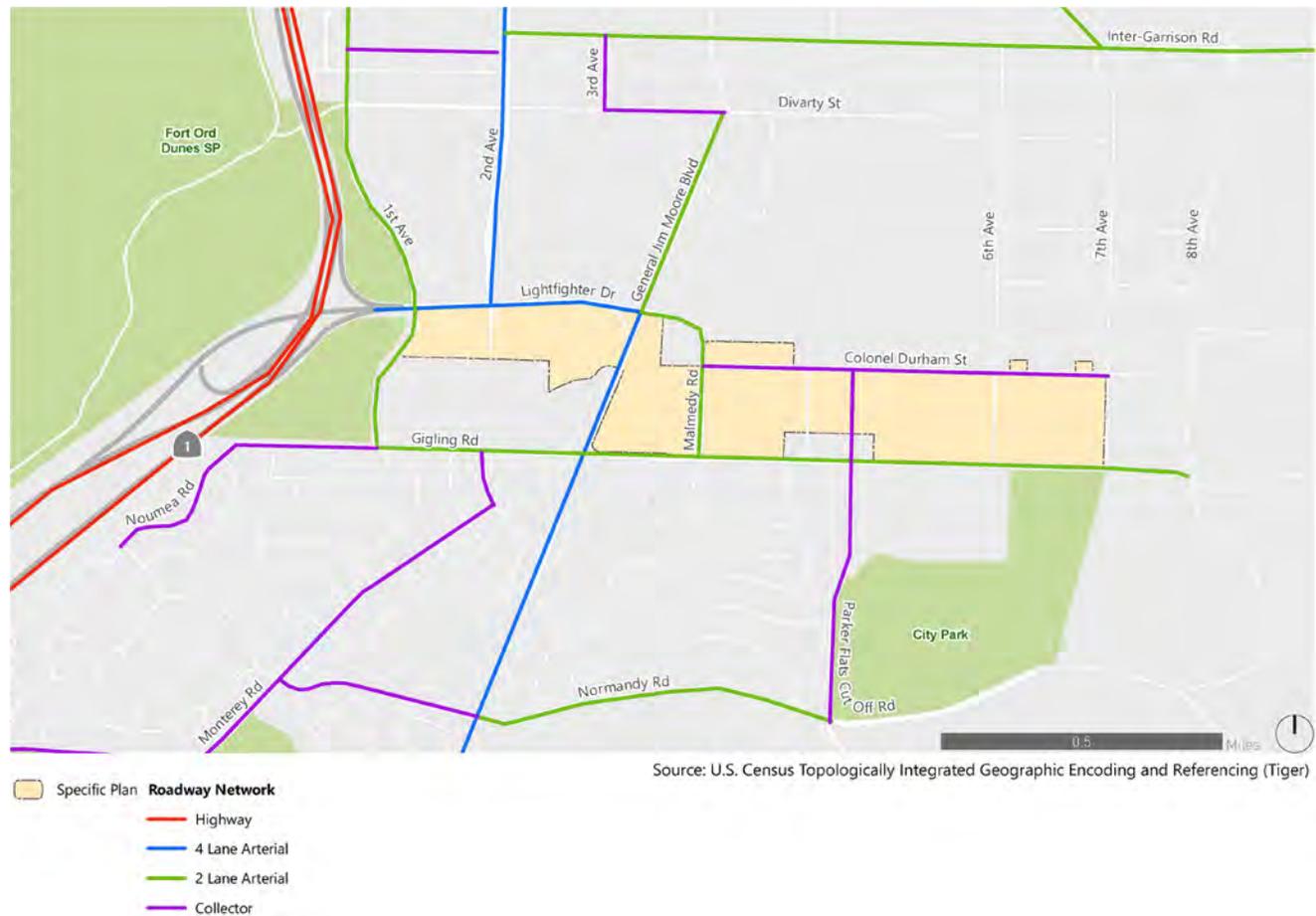


Figure 1.6 - Existing Local Street System

1.9.5 Existing and Previously Proposed Bicycle Network

There are a number of infrastructure improvements that have already been planned separately from this Specific Plan, but are not yet implemented for the Campus Town area. Figure 1.7 shows both existing and previously planned bicycle facilities within and surrounding the Campus Town Specific Plan Area. Extending the bicycle network of CSUMB to the Specific Plan Area and street upgrades that include sidewalk expansions and bicycle lanes will encourage more pedestrian and bicycle activity.

Additionally, FORTAG plans for connections to and through the Campus Town area. These connections are valuable sources for local and regional connectivity.

Bikeway planning and design in California typically relies on guidelines and design standards established by California Department of Transportation (Caltrans) in the Highway Design Manual (Chapter 1000: Bikeway Planning and Design). The City of Seaside uses these guidelines to create four general bikeway facility classifications, as outlined below.

- Class I Bikeways (Multi-Use Paths) provide a completely separate right-of-way and are designated for the exclusive use of bicycles and pedestrians, with vehicle and pedestrian cross-flow minimized. In general, bike paths serve corridors where on-street facilities are not feasible or where sufficient right-of-way exists to allow them to be constructed. Regionally, the Monterey Recreational Trail is a multi-use path that provides north-south connectivity for Seaside and Marina along SR 1. Additionally, a multi-use path exists along 2nd Avenue between Lightfighter Drive and Divarty Street. Class I bikeways on the East side of General Jim Moore Boulevard are currently being planned concurrently with this Specific Plan by a separate agency as an independent project.
- Class II Bikeways (Bicycle Lanes) are dedicated lanes for bicyclists generally adjacent to the outer vehicle travel lanes. These lanes have special lane markings, pavement legends, and signage. Bicycle lanes are at least five (5) feet wide. Adjacent vehicle parking and vehicle/pedestrian cross-flow are permitted. Within the Campus Town Specific Plan Area, bicycle lanes are concurrently being planned by separate agencies as independent projects along Lightfighter Drive/Malmedy Road between General Jim Moore Boulevard and Gigling Road. Surrounding the Specific Plan, bicycle lanes are proposed along Parker Flats Cut Off Road south of Gigling Road, along 7th Avenue north of Gigling Road, and along Monterey Road south of Gigling Road.
- Class III Bikeways (Bicycle Boulevards/Bicycle Routes) are designated by signs or pavement markings for shared use with pedestrians or motor vehicles, but have no separated bike right-of-way or lane striping. Bike routes serve either to: a) provide a connection to other bicycle facilities where dedicated facilities are infeasible, or b) designate preferred routes through high-demand corridors. Bike boulevards give bicycles priority on campus streets and allow them to use the whole lane. Bike boulevards use signs and pavement markings to inform motorists. The draft 2040 General Plan identifies a

future bicycle route along Gigling Road between 2nd Avenue and SR 1.

- Class IV Bikeways (Separated Bikeways) provide a right-of-way designated exclusively for bicycle travel within a street and are protected from other vehicle traffic by physical barriers including, but not limited to, grade separation, flexible posts, inflexible vertical barriers such as raised curbs, or parked cars. Future Class IV facilities along Gigling Road between 2nd Avenue and 7th Avenue and along Lightfighter Drive between General Jim Moore Boulevard and SR 1 are concurrently being planned by a separate agency as independent projects.



Figure 1.7 - Existing and Previously Proposed Bicycle Network

1.9.6 Existing Pedestrian Network

The existing pedestrian network in the Specific Plan Area has many gaps and opportunities for improvements. Some portions, including areas along General Jim Moore Boulevard and Gigling Road, have a favorable walking environment with many destinations within a close walking distance. Other streets within and near the Specific Plan Area lack sidewalks entirely. Figure 1.8 shows where existing sidewalks and sidewalk gaps are located within the Campus Town Specific Plan Area.

Arterial streets such as Lightfighter Drive and Gigling Road currently have inconsistent pedestrian sidewalks where in some areas sidewalks are not provided on both sides of the street. Many local streets within and near the Campus Town Specific Plan Area have no sidewalks resulting in gaps in the pedestrian network. In addition, several local streets have sidewalks only along one side of the street. In some areas, the natural topography results in sidewalks with moderately steep slopes. Some pedestrian crossings at intersections also do not have ADA-accessible curb ramps. Distances between some existing destinations are beyond a 10 minute walk. Coupled with a coastal climate that can sometimes be windy and foggy, these conditions can deter pedestrian movement.



Figure 1.8 - Existing Pedestrian Network

1.9.7 Existing Transit Network

The public transit system that connects the Campus Town Specific Plan Area to the greater Monterey and Salinas area is operated by the Monterey-Salinas Transit District (MST). Five bus routes serve stops in or along the boundary of the Campus Town Specific Plan Area: Routes 12, 18, 67, 74, and 75. Figure 1.9 shows the map of the transit service, and Table 1.2 describes weekday bus route information and route access from the Campus Town Specific Plan Area to major points of interest throughout the region.

Most of the bus stops serving the Specific Plan Area are located along Lightfighter Drive to the north and along Gigling Road to the south. Route 12 has one stop at General Jim Moore Boulevard and Lightfighter Drive and three stops along Gigling Road between General Jim Moore Boulevard and 6th Avenue to the east. Route 18 has one stop at General Jim Moore Boulevard and Lightfighter Drive and three stops along Gigling Road between General Jim Moore Boulevard and Noumea Road, and Route 64 stops at several more bus stops along Gigling Road between General Jim Moore Boulevard and 6th Avenue.

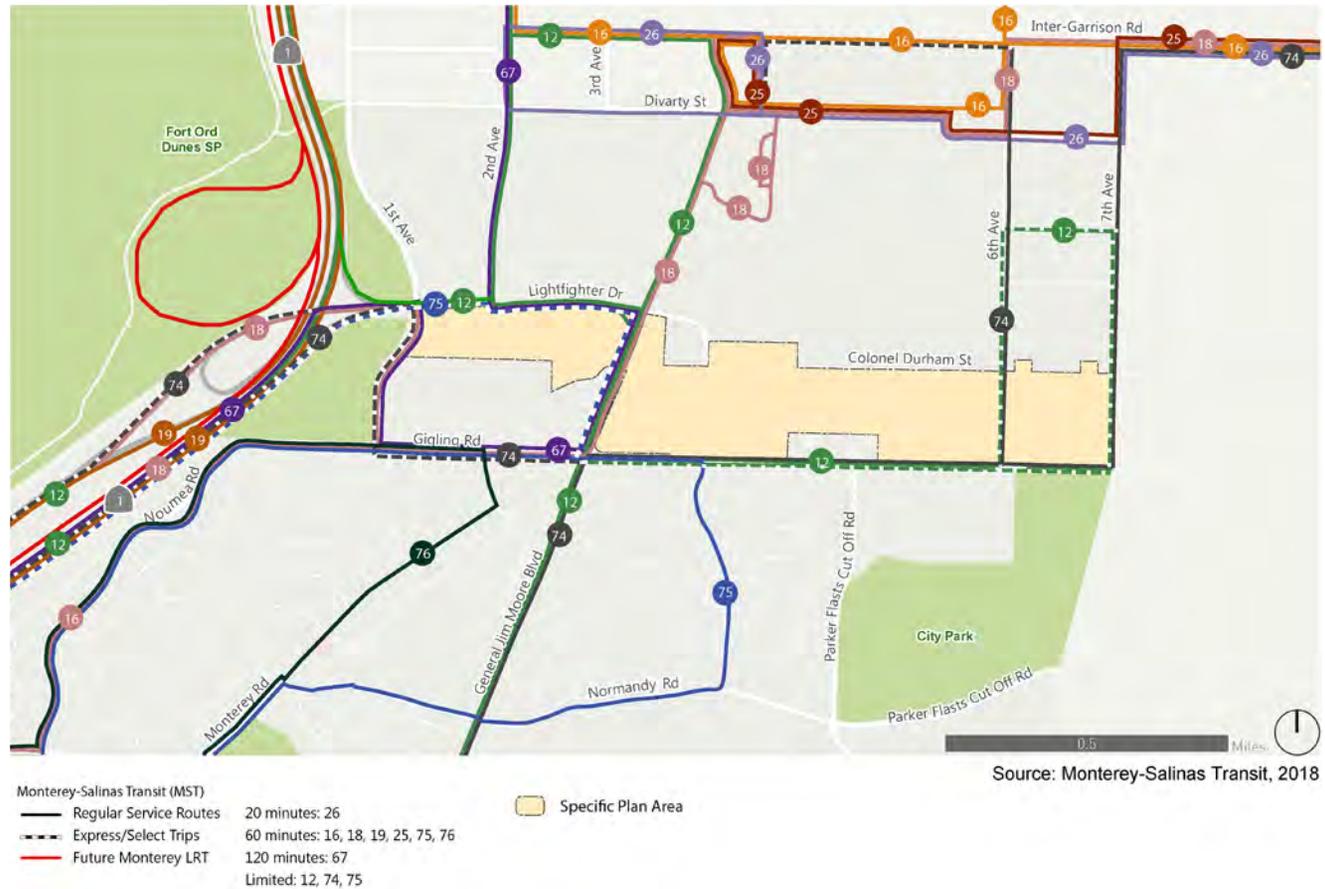


Figure 1.9 - Existing Transit Network

Students, staff, and faculty of CSUMB receive free unlimited access on all MST regular bus routes with their CSUMB Otter ID card. Additionally, all transit users with physical disabilities have access to the MST para-transit program (RIDES). This service operates on a point-to-point basis. Appointments are required to guarantee service, and service is not available on weekends or holidays.

The Association of Monterey Bay Governments (AMBAG) adopted a Sustainable Communities Strategy in June of 2018. This strategy outlines priority areas for High Quality Transit Corridors. While the existing routes in this area do not universally meet the criteria, the area is defined as a high quality transit priority corridor in the Sustainable Communities Strategy.

A high quality transit priority corridor as defined as “Sustainable Communities Opportunities Area” is an area within 1/2 mile of an existing or planned “high quality transit corridor” (per definition in California Public Resources Code Section 21155(a)) or “major stop” (per California Public Resources Code Section 21064.3) that has the potential for transit oriented development including mixed use. High quality transit is service with headways of 15 minutes or less during peak period or rail service. Appendix I of the AMBAG Sustainable Communities Strategy shows the area of Campus Town and Main Gate adjacent to the CSUMB campus as a potential “Opportunity Area” typified by Town Center and Neighborhood Mixed Use land uses.

Route	Description (To/From)	Hours of Operation	Average Weekday Headway
12	The Dunes - NPS	6:45 AM to 4:10 PM	Limited*
18	Monterey - The Dunes	6:00 AM to 10:40 PM	Every 60 minutes
67	Presidio - Marina	Fridays from 2:15 PM to 10:10 PM Weekends from 10:15 AM to 10:10 PM	Every 120 minutes on weekdays, Every 60 minutes on weekends
74	Presidio - Toro Park	6:30 AM to 6:00 PM	Limited*
75	Presidio - Marshall Park Express	6:00 AM to 10:10 PM	Every 60 minutes

* Headways for Route 12 range between 60 to 120 minutes. Route 74 runs twice in the AM and once in the PM. Sources: Monterey Salinas Transit, 2018.

Table 1.2 - Existing Local Weekday MST Service



1.9.8 Existing Block Structure

There is an existing local street network throughout the entire Specific Plan Area, with most of the streets in need of maintenance. Conceived to primarily serve the automobile, the pedestrian and bicycle amenities are lacking. The existing block sizes are large and contribute to unfavorable conditions for the non-motorist. The existing Specific Plan Area is approximately 120 acres with only 15 intersections which is 80 intersections per square mile. A walkable neighborhood should have at least 150 intersections per square mile at a minimum. Many of the Monterey Bay area's more treasured neighborhoods approach and sometimes exceed 250 intersections per square mile.

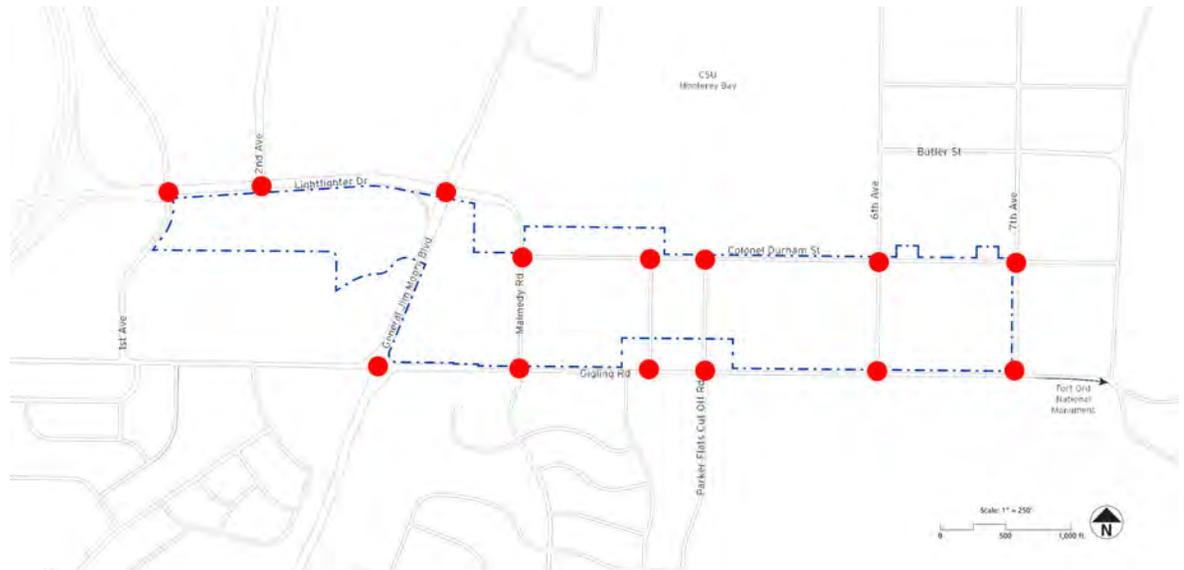


Figure 1.10 - Existing Block Structure

1.9.9 Existing Buildings

Portions of the Specific Plan Area are currently undeveloped, see Figure 1.11. Most of the existing buildings were originally a part of the Fort Ord base and will be demolished by FORA. At the time of this writing, FORA has removed most of the buildings in the Specific Plan Area that had been identified for demolition, with the exception of eight buildings.

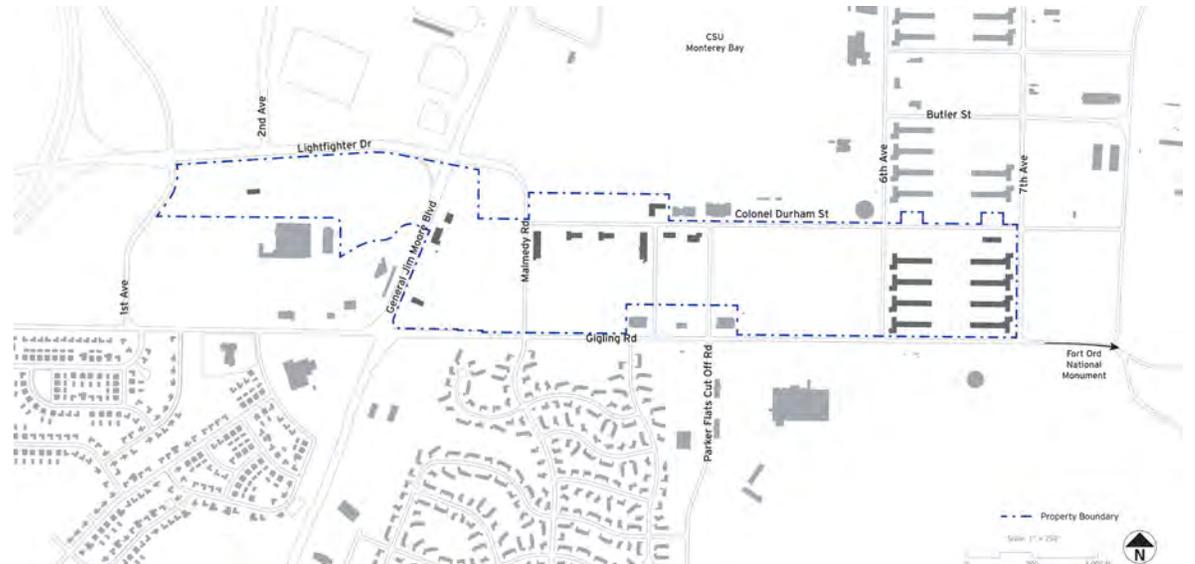


Figure 1.11 - Existing Buildings

1.9.10 Existing Utilities

Throughout the development of the Fort Ord base and its subsequent closure, a patchwork of utility systems have been installed to serve the Specific Plan Area, see Figure 1.12.

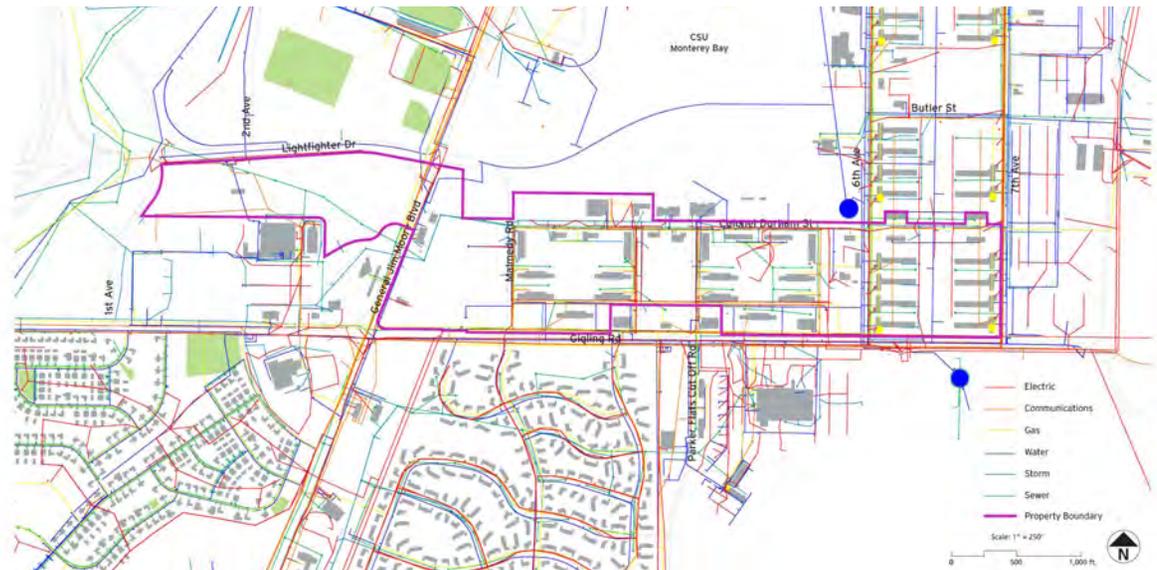


Figure 1.12 - Existing Utilities

1.9.11 Existing and Previously Proposed Roadway Improvements

Figure 1.13 shows where street redesigns and the incorporation of roundabouts have been previously proposed by FORA, though not yet implemented. There is a roadway widening for Gigling Road that has been previously proposed by FORA and has an EIR.



Source: FORA Reuse Plan Fee Reallocation Study: Deficiency Analysis and Fee Reallocation, 2017. FORA Regional Urban Design Guidelines, 2016. Fort Ord Reuse Plan, 1997. Fort Ord Reuse Authority Capital Improvement Program 2017/18 to 2027/28. City of Marina CIP, 2016-2017.

Figure 1.13 - Existing and Previously Proposed Roadways

- Roadway Network Improvements**
- Signalization
 - Signalization or Roundabout
 - New Roadway
 - Roadway Widening
 - Specific Plan Area



1.10 Public Participation

This Specific Plan is a result of a community-based process that involved various forums for public participation. Community input from residents, CSUMB students and faculty, and local stakeholders coalesced into the driving vision for this Specific Plan.

1.10.1 Community Meeting - Project Introduction

The first community meeting was held on September 14, 2017 to introduce the public to the Campus Town Specific Plan and the process. After opening remarks by project team leaders, attendees were invited to share their vision for the Campus Town area. Of the many comments expressed, environmental sustainability and interest in mixed-use development with equal consideration for pedestrians and bicyclists were the most common.

1.10.2 Design Charrette

A five-day design workshop, *charrette*, was held from October 30th through November 3rd, 2017 at the Soper Field Community Center. The event was kicked-off with a Monday night presentation that introduced the Specific Plan and charrette process. Throughout the week, designers and project team members collaborated with members of the public to develop a framework that appropriately reflects the community's vision for the Specific Plan Area. Stakeholder meetings also took place during the week in order to comprehensively address the various interests of local agencies, institutions and community organizations. The Friday night presentation at the end of the week presented the group with a draft plan that would guide the evolution this Specific Plan.



Community Engagement

Public workshops, periodic meetings, and a project website were a few of the ways that the public was encouraged to share their thoughts. Community comments were actively referenced by the project team throughout the process.





1.10.3 CSUMB Community Presentation

Given Campus Town's proximity to the CSUMB campus, consideration of and active engagement with the student body, faculty, staff, and administration played a pivotal role in the creation of this Specific Plan. To that end, the Specific Plan was presented on campus by invitation of a university professor committed to drawing the connection between academia and professional practice. Student organizers created an outreach campaign to solicit student body input and shared their efforts with the project team to ensure that student concerns and ideas were appropriately addressed.



Community-Based Vision

The participating public directly interacted and influenced the vision and design of the draft Plan. The conceptual sketch (left) drawn in situ with extensive community input at the charrette guided the evolution of this Specific Plan.

- NON-FAST FOOD OPTIONS
- HOW WILL EXISTING INSTITUTIONS INTEGRATE WITHIN THE SITE
- CONSIDERATIONS FOR ENVIRONMENTAL PRACTICES & SYSTEMS
 - LEED CERTIFICATIONS
- SIGLING & 8TH CONNECTION TO FOOT DES NATIONAL MONUMENT
- US DEPT. OF DEFENSE NEIGHBOR
- EXISTING TREES?
- AFFORDABLE CHILDCARE
- WATER AUTHORITY-(PURPLE PIPE) CONSERVATION
- BIAPPERED BIKE LANES
- HAVE GROUND LEVEL VIEW OF WHAT'S HERE NOW-(BROWN FIELD)



1.10.4 Project Information Desk - Community Outreach

A project team member specifically tasked to answer questions and receive feedback was available on a bi-weekly basis for three months at Seaside Creates, the City of Seaside's satellite office for Economic Development and Planning Departments. Interested stakeholders visited to learn about the project and share their vision for Campus Town.





Form & Character

2.1 Vision and Plan

Envisioned as a walkable, mixed-use community that serves the needs of residents and visitors alike, Campus Town is designed to capitalize on the positive momentum of existing and emerging local developments throughout the former Fort Ord and proximity to CSUMB campus. As reflected in the Conceptual Specific Plan (Figure 2.2), a framework of Open Spaces (Figure 3.17) and Thoroughfare Types (Figure 3.1) are fixed in location and general configuration, but all other elements of the Specific Plan are conceptual and subject to change consistent with the Standards of this Specific Plan for flexibility and development feasibility.

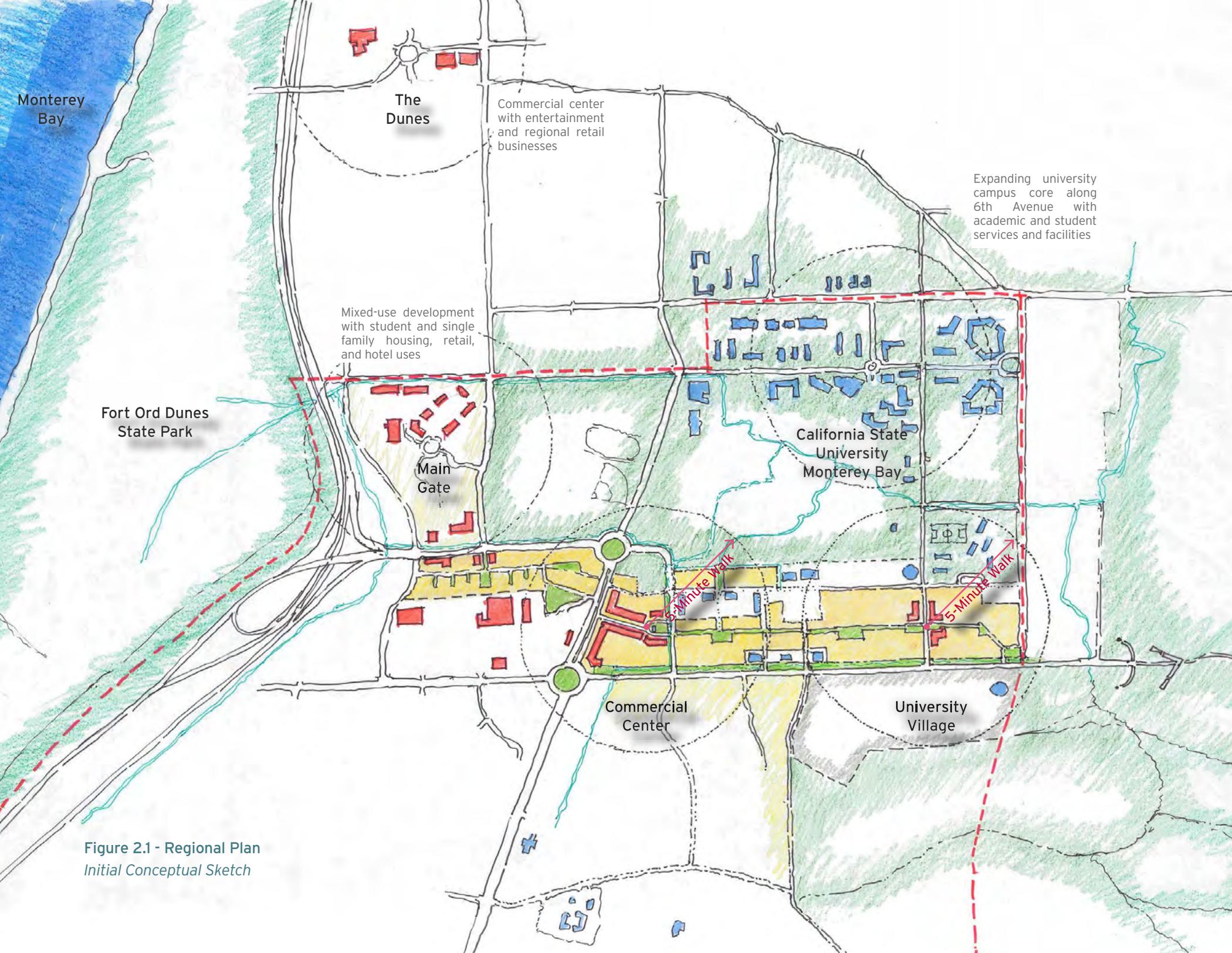


Figure 2.1 - Regional Plan
Initial Conceptual Sketch

2.1.1 Conceptual Plan

This FORA's Regional Urban Design Guidelines identify two Town and Village Centers (Commercial Center and University Village) within the Campus Town Area to serve the CSUMB community and local residents. At the western end of the Plan Area, the Commercial Center infuses the Specific Plan Area with economic stimulus by tapping into a busy regional artery, General Jim Moore Boulevard, and drawing patrons into the businesses along the main street.

A green spine, "Central Street," connects the Commercial Center with the University Village to create a pedestrian-oriented, walkable, amenity-rich neighborhood that provides a framework for enriching experiences. Pocket parks, plazas and diverse building types enliven the streetscape contributing to the creation of an engaging sense of place. The green link defines and orients the public space network.

Anchoring the eastern portion of the Plan Area, the University Village draws on the growing hub on campus at 6th Avenue down to the Plan Area with a mixed-use, multi-modal environment that embraces a range of experiences and activities. Providing spaces for students and residents to engage and take ownership of the public realm fosters the inclusive and diverse community envisioned for Campus Town.

Figure 2.2 provides a conceptual buildout under this Specific Plan pursuant to the development regulations contains herein. This concept should not be construed as a development limit.

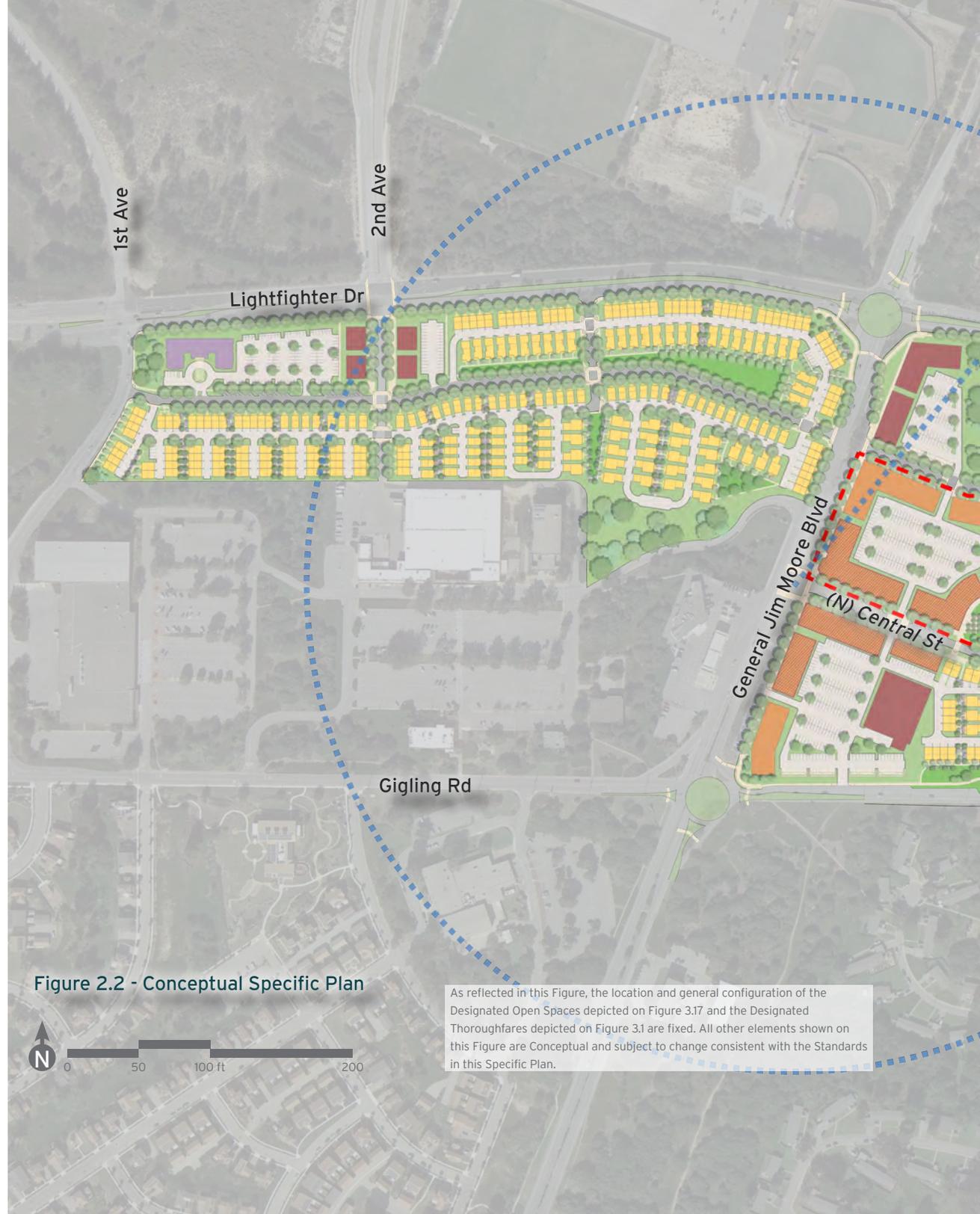
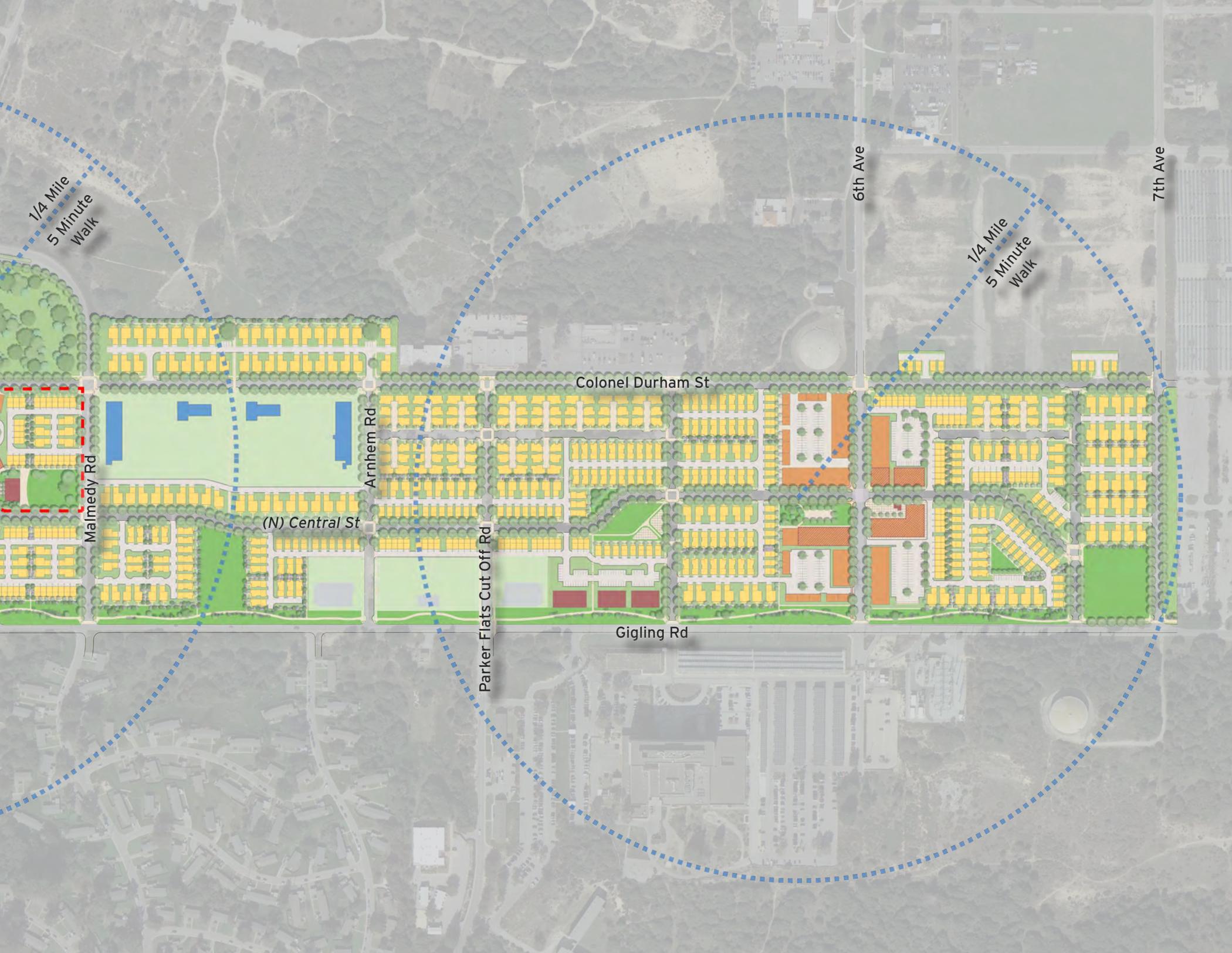


Figure 2.2 - Conceptual Specific Plan

As reflected in this Figure, the location and general configuration of the Designated Open Spaces depicted on Figure 3.17 and the Designated Thoroughfares depicted on Figure 3.1 are fixed. All other elements shown on this Figure are Conceptual and subject to change consistent with the Standards in this Specific Plan.





1/4 Mile
5 Minute
Walk

1/4 Mile
5 Minute
Walk

6th Ave

7th Ave

Colonel Durham St

Arnhem Rd

(N) Central St

Malmey Rd

Parker Flats Cut Off Rd

Gigling Rd



Figure 2.3 - Local Plan
Initial Conceptual Sketch

2.1.3 Conceptual Figure Ground Plan Diagram

This conceptual diagram illustrates how the physical environment of the Specific Plan Area may develop by depicting built structures such as buildings and Thoroughfares (i.e. figures) against the white background of the ground plane. The conceptual size, location, and number of figures; and their relationship to each other, reveal that Campus Town will be decidedly urban in character as demonstrated by the compact configuration of the figures - building intensity - and their various forms - building diversity - thus, fostering the development of a walkable and mixed-use community.

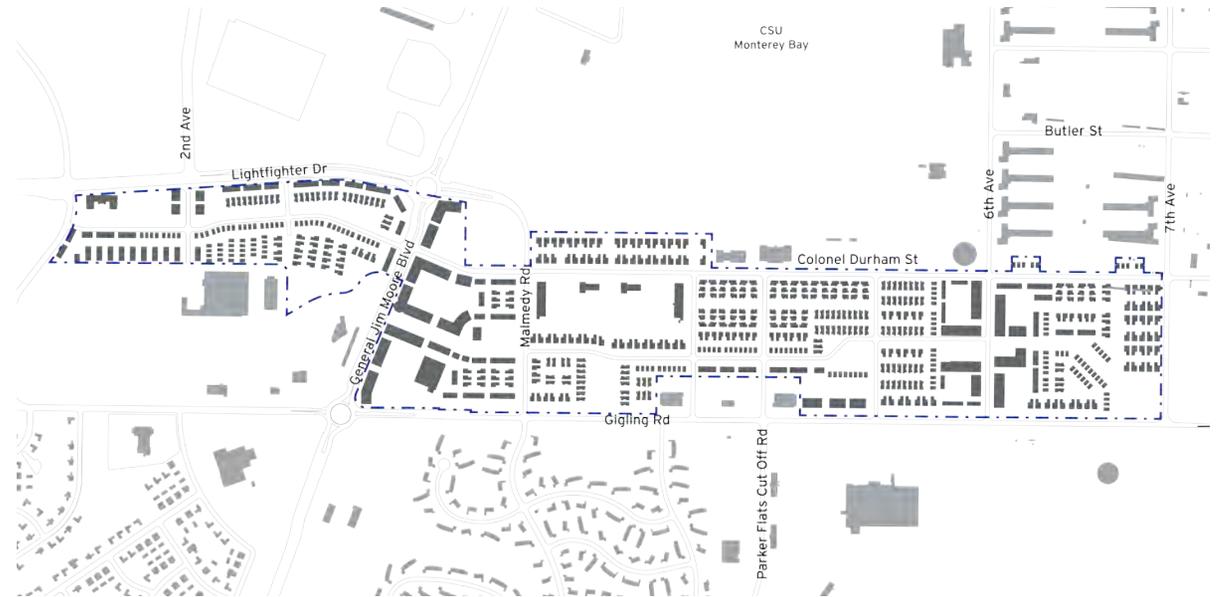


Figure 2.4 - Conceptual Figure Ground Plan Diagram

2.1.4 Conceptual Reverse Figure Ground Plan Diagram

This conceptual diagram flips the emphasis of the Conceptual Figure Ground Plan Diagram (Figure 2.4) by highlighting the areas between the built forms (figures) created by their disposition. The public and semi-public spaces define the urban network by creating a sequence of outdoor rooms and corridors that are bounded and characterized by the built forms that envelop them. The diagram illustrates the role that the thoroughfares and Open Spaces play in shaping and connecting the public realm.

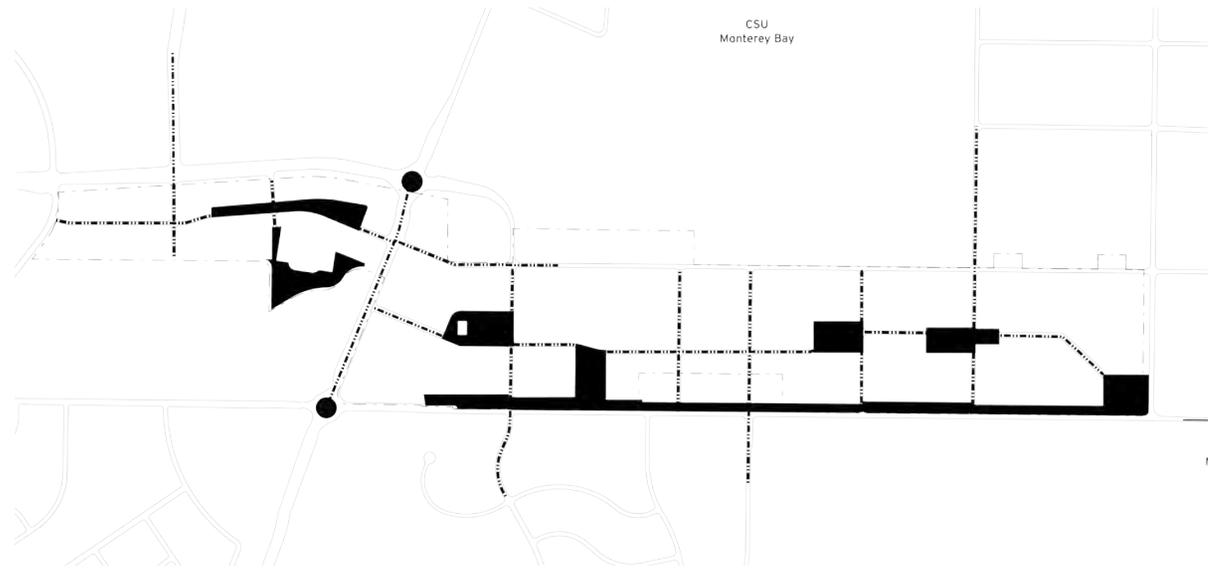


Figure 2.5 - Conceptual Reverse Figure Ground Plan Diagram

2.1.5 Conceptual Open Space Plan Diagram

The Conceptual Open Space Plan Diagram peels a layer off the Conceptual Reverse Figure Ground diagram (Figure 2.5) and focuses solely on the Open Spaces of the urban sequence. While the thoroughfares (corridors) are also public spaces, the designated Open Spaces particularly serve as community gathering spaces and neighborhood living rooms, highly influencing the character and utility of the public realm.



Figure 2.6 - Conceptual Open Space Plan Diagram

2.1.6 Conceptual Intersection Spacing Plan Diagram

Intersection density is an objective method of assessing and predicting the walkability of a community. High intersection density often corresponds to a more walkable and therefore health-promoting environment. Research indicates that higher street intersection density has environmental benefits. People living in neighborhoods with higher street intersection density tend to drive less and walk and take transit more. The decision to walk is more often influenced by intersection density than street connectivity. In addition, higher street intersection density is associated with less per capita air pollution from vehicle emissions, which benefits human health and the mitigation of climate change. Finally, additional studies of medium-sized cities in California provided a correlation between cities (or areas of cities) exhibiting higher intersection density and fewer vehicular related fatalities.

The Seaside General Plan requires that the former Fort Ord lands achieve a motorized intersection density of at least 150 intersections per square mile. The Campus Town Specific Plan exceeds that standard and provides 238 intersections per square mile, see Figure 2.7.

The Seaside General Plan requires that the former Fort Ord lands achieve a combined motorized and non-motorized intersection density of at least 300 intersections per square mile. The Campus Town Specific Plan exceeds that with 540 intersections per square mile, see Figure 2.8.

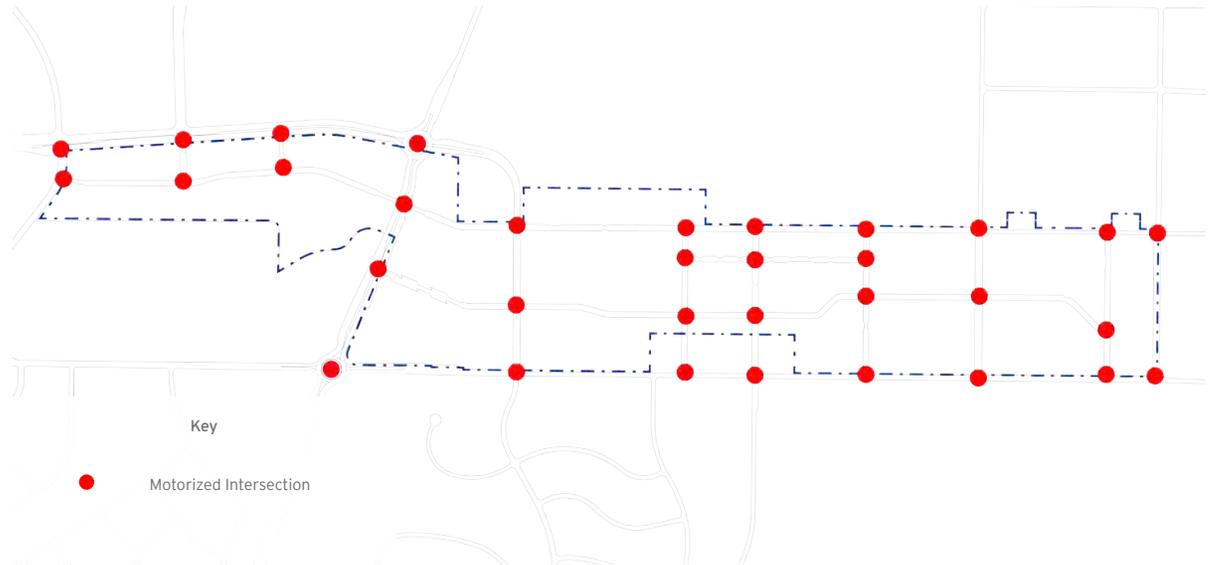


Figure 2.7 - Conceptual Motorized Intersection Spacing Plan Diagram



Figure 2.8 - Conceptual Motorized and Non-Motorized Intersection and Walkway Spacing Diagram

2.1.7 Conceptual Bike and Trails Plan Diagram

Campus Town will be fully integrated into the overall bicycle and trails network. Connections to existing and proposed trails will ensure that seamless connections to and through the Specific Plan Area effectively provide access to the greater community. FORTAG trail spurs and separately planned bicycle infrastructure improvements will connect with the proposed bikeways within the Specific Plan Area.

Proposed bikeways will be located within the Thoroughfares. As described for Figure 3.1, locations of Designated Thoroughfares are fixed. The locations of other Thoroughfares are conceptual and subject to change.

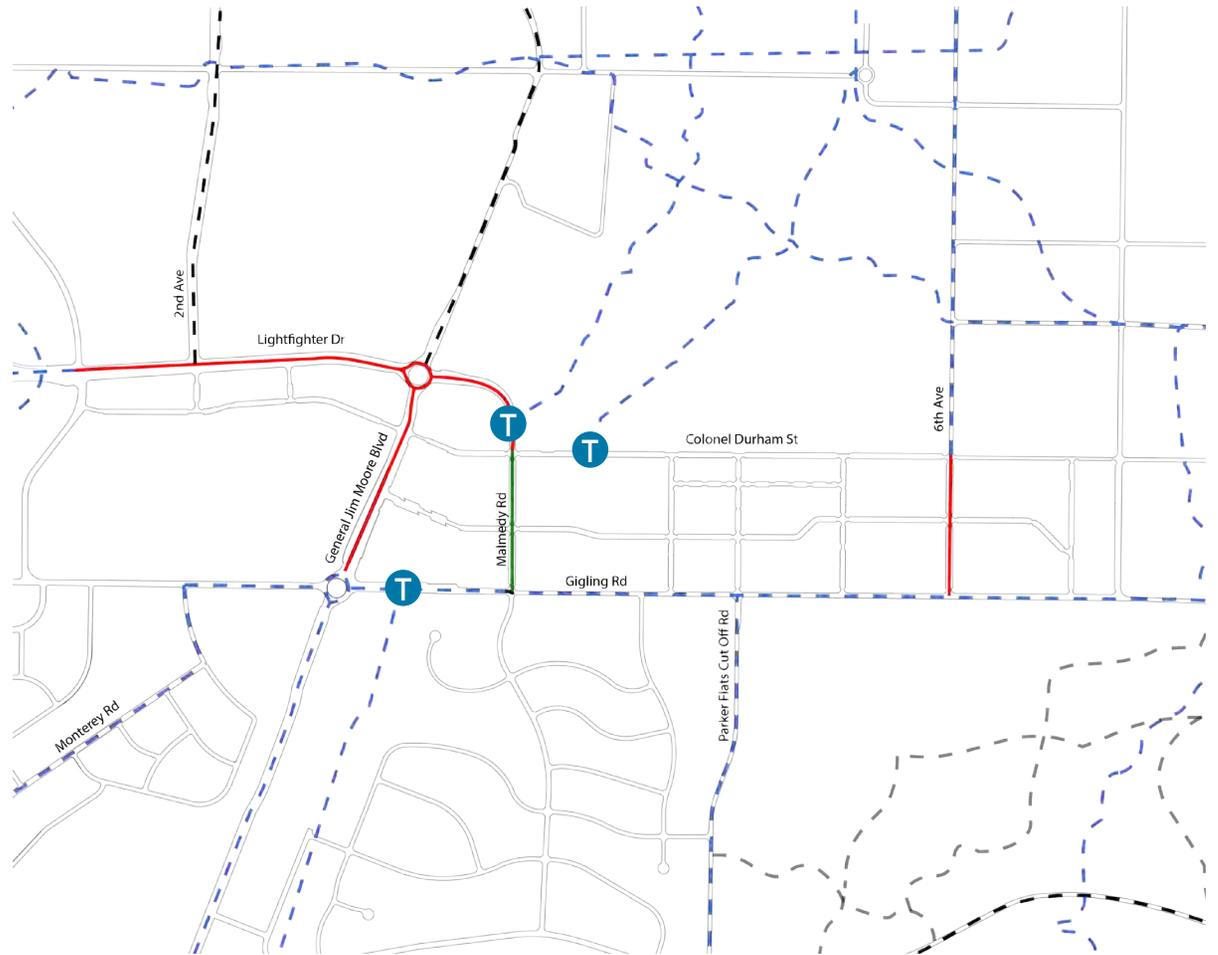
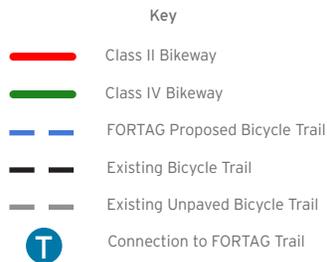


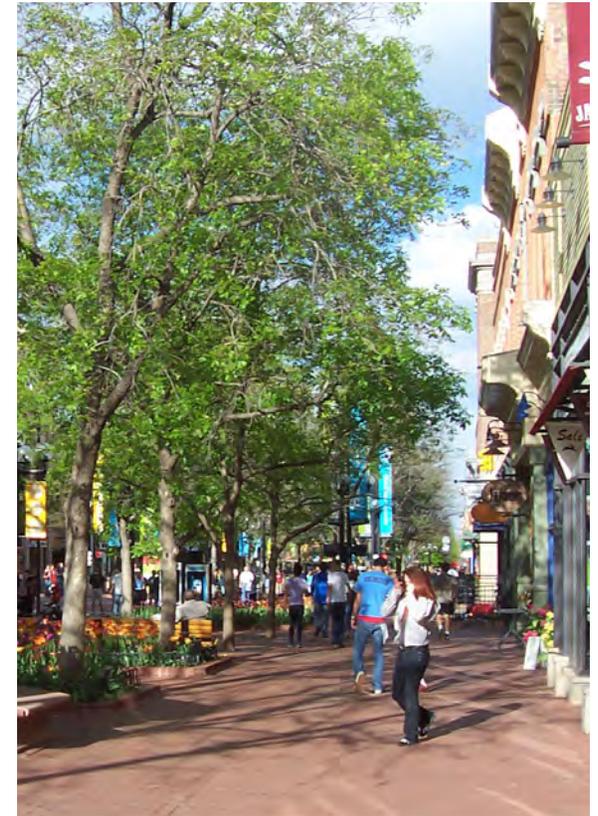
Figure 2.9 - Conceptual Bicycle and Trails Network Diagram



2.2 Form-Based Code

In lieu of traditional zoning standards, the Specific Plan utilizes a Form-Based Code, which provides a land development regulation that fosters predictable built results and a high-quality public realm by using physical form (rather than separation of uses) as the organizing principle for the code. Using the Form-Based Code Standards provided in the Specific Plan, developers are able to realize different urban contexts with the functions and intensities appropriate to their locations within their sub-area.

Pursuant to the Form Based Code, the primary elements that will define the character of the Campus Town Specific Plan Area and of each sub-area are: **Thoroughfare (Street) Types, Frontage Types, Building Types and Open Space Types**. Allowable land-uses enjoy flexibility within limits described in this Specific Plan. In addition to these elements the Specific Plan provides Standards and Guidelines for buildings, Façades, lighting, signage, service, and landscape.



Walkable Urbanism (Illustrative)

Appropriate landscaping, activated ground-floors and human-scaled architecture enhance the pedestrian experience and public realm.



Thoroughfare Types (Section 3.3):

Thoroughfares are the principal structuring devices of urbanism. They are the matrix of mobility, accommodating both pedestrians and various forms of transportation. Thoroughfares are also a major part of the public realm, typically occupying between 20 - 40% of the urbanized land area. Perhaps most importantly, the street and road network subdivides the land into blocks, fundamentally determining the urban development pattern of the town and the types of buildings that can be accommodated. For all these reasons, roads and streets have an enormous effect on community and the character is as important as the capacity to move vehicles.



Building Types (Section 4.6.2): The character of a place is substantially defined by the position of the building on its site, the configuration of the building, and also its function. The building's position and configuration together determine type (eg. Single Family Dwelling, Townhouse, etc.) which supports the intended uses and defines the degree of urban life. Building typology is an essential element of urbanism.



Frontage Types (Section 4.6.3): A Frontage is the privately held area between the Façade of a building and the Lot Line. The variables of Frontage are the dimensional depth of the Front Yard and various architectural elements (eg. fences, Stoops, etc.) that define the transition from the public realm of the Street and the private realm of the building. The combination of the private Frontage, the public streetscape, and the capacity of the Thoroughfare defines the character of the public realm.

2.3 Designation of Sub-Areas

The Campus Town Specific Plan is comprised of six Sub-Areas: two mixed-used village centers and four residential neighborhoods. Consistent with the RUDG, the nodes of concentrated activity are methodically located to serve and induce multi-modal transit and cultivate a diversity of uses and users. Mixed-use development in the village centers provides for pedestrian activity and economic vitality.

Each of the six Sub-Areas exhibits a distinct character informed by its development concept. The following descriptions of the individual Sub-Areas outline how the vision for the Specific Plan Area is physically accomplished in the varying settings that make up Campus Town.

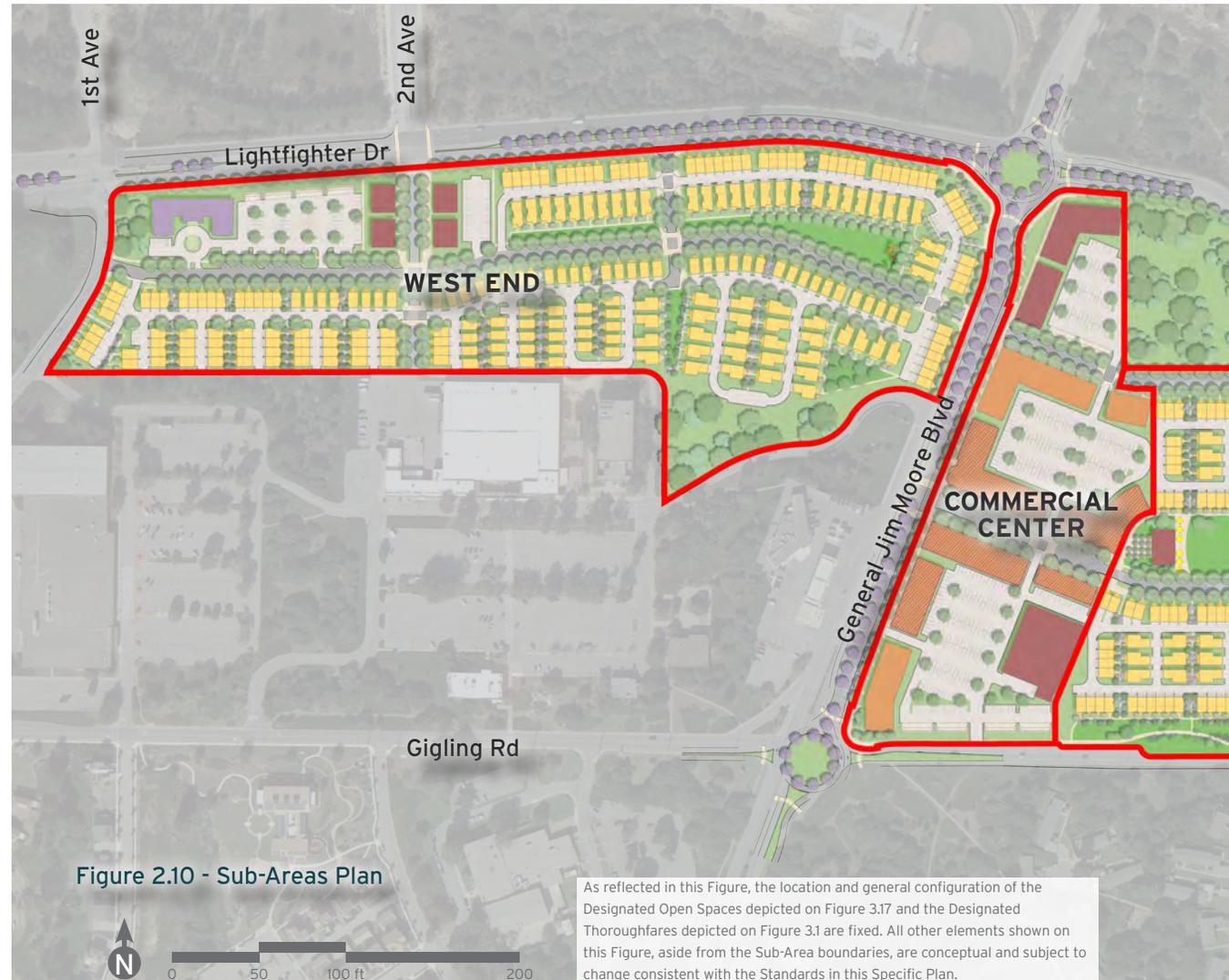
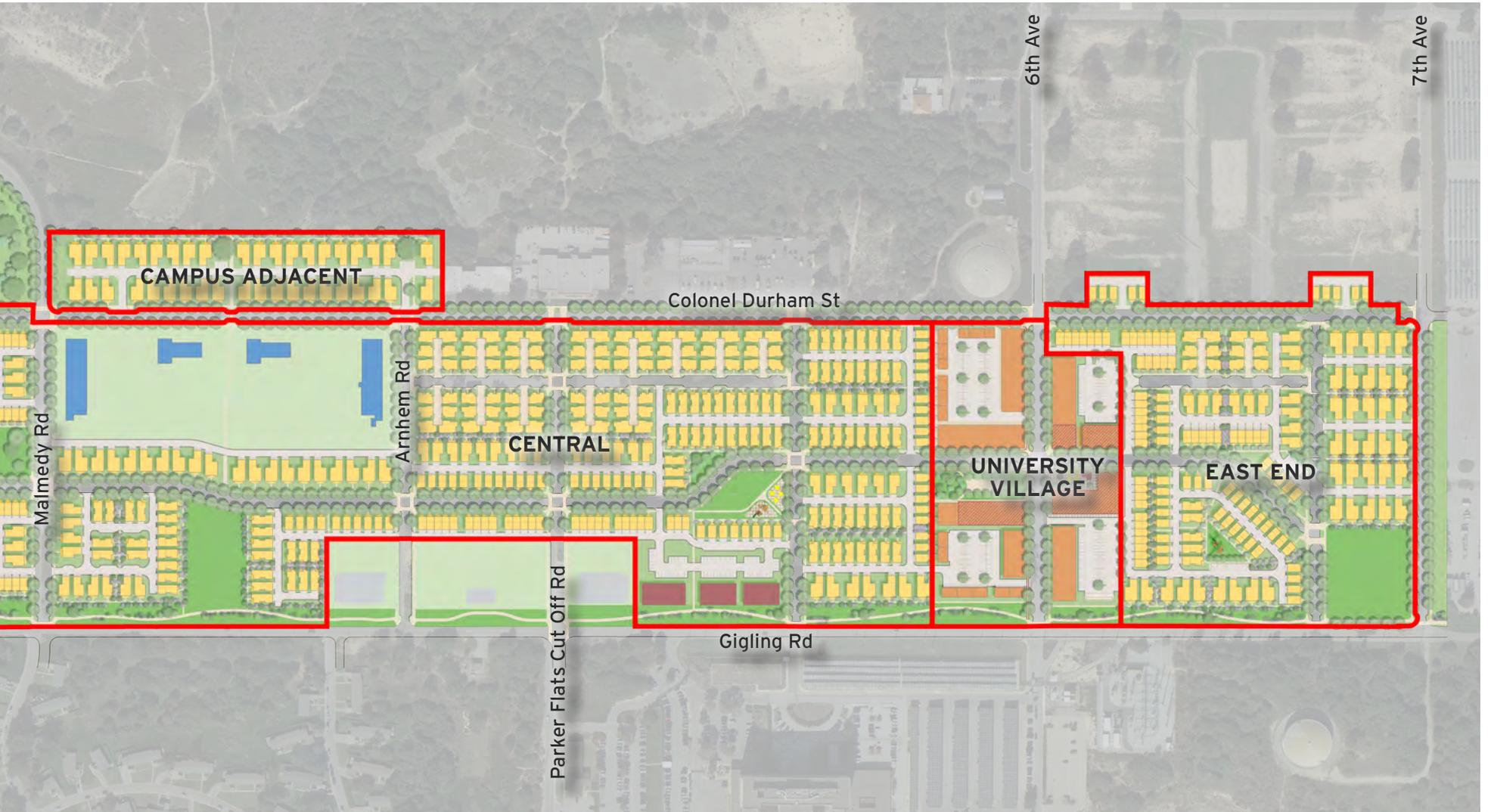


Figure 2.10 - Sub-Areas Plan

As reflected in this Figure, the location and general configuration of the Designated Open Spaces depicted on Figure 3.17 and the Designated Thoroughfares depicted on Figure 3.1 are fixed. All other elements shown on this Figure, aside from the Sub-Area boundaries, are conceptual and subject to change consistent with the Standards in this Specific Plan.



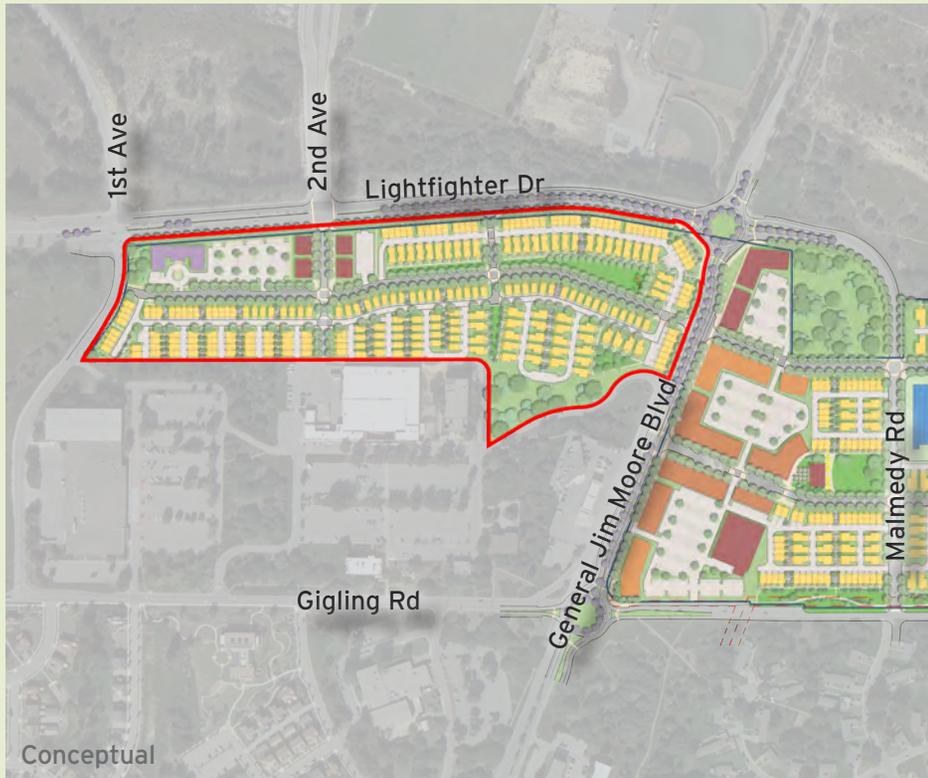


Figure 2.11 - West End Sub-Area

Sub-Area WE: West End

At the nexus of State Route 1, CSUMB, and the future Main Gate, the western most portion of Campus Town at Lightfighter Drive functions as a gateway to Seaside and CSUMB. An emphasis on connectivity and permeable urban fabric that fosters walkability and healthy lifestyles is integral to the design of the Specific Plan and drives the various connections to and through the Campus Town.

The Plan extends 2nd Avenue from the north through the Plan area and introduces an internal street from 1st Avenue to General Jim Moore Boulevard. The West End primarily serves as a residential neighborhood with local serving retail at 2nd Avenue. Two significant green spaces are provided, one is a neighborhood green and the other area is set aside for the preservation of a tree grove containing native oak trees.

A hotel is proposed at the intersection of 1st Avenue and Lightfighter Drive drawing visitors to Fort Ord Dunes State Park and the Main Gate development to the north.

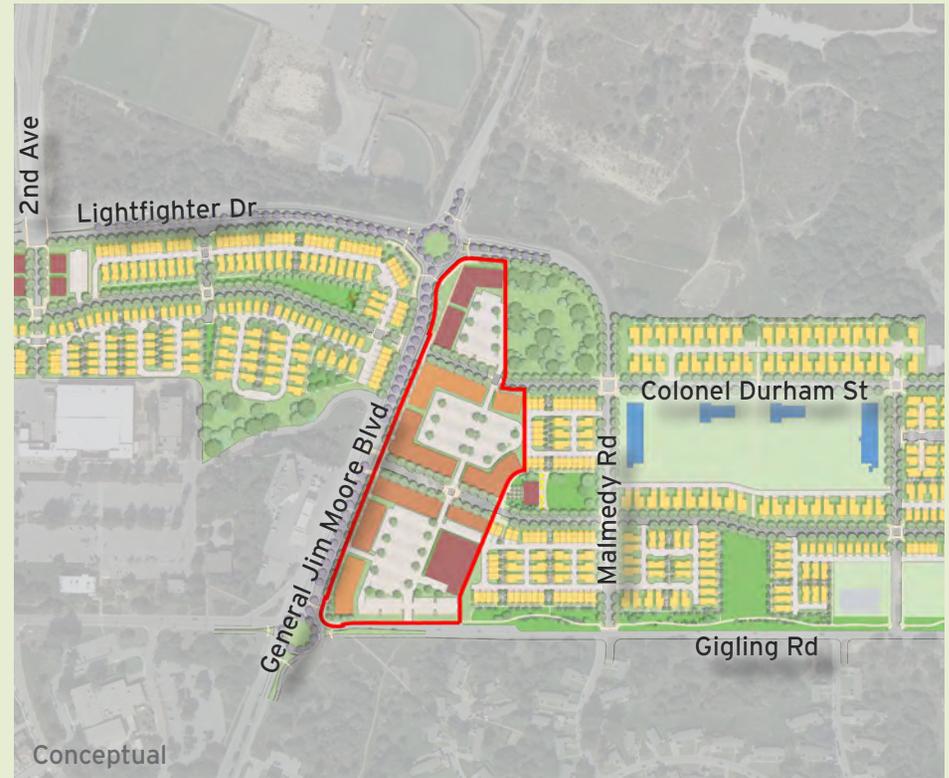


Figure 2.12 - Commercial Center Sub-Area

Sub-Area CC: Commercial Center

Just east of General Jim Moore Boulevard between Lightfighter Drive and Gigling Road, the Commercial Center Sub-Area at Campus Town is defined by a typical main street with street-facing retail and surface parking lots located at the interior of the blocks.

Pedestrian-scaled Frontages on the large tenant buildings foster walkable and lively streetscapes while still accommodating the needs of large footprint retailers. The Commercial Center is located to take advantage of the regional connections in Seaside to CSUMB from State Route 1 and General Jim Moore Boulevard.



Figure 2.13 - Campus Adjacent Sub-Area

Sub-Area CA: Campus Adjacent

At the intersection of Lightfighter Drive and Colonel Durham Street, the campus adjacent Sub-Area is a small residential block that abuts the CSUMB campus. Given its boundaries, this Sub-Area is envisioned as a residential liner with an internal alley so that both the street and campus Frontages are appropriately defined. A common walkway lines the natural reserve to the north at CSUMB and the adjoining homes to link the university with the amenities at the Commercial Center.

Existing and proposed bike routes are also accommodated through the Specific Plan Area to provide choices and opportunities for multi-modal transportation. Two FORTAG trail spurs provides access to this Campus Town Sub-Area, linking the greater Monterey bay region to Campus Town.

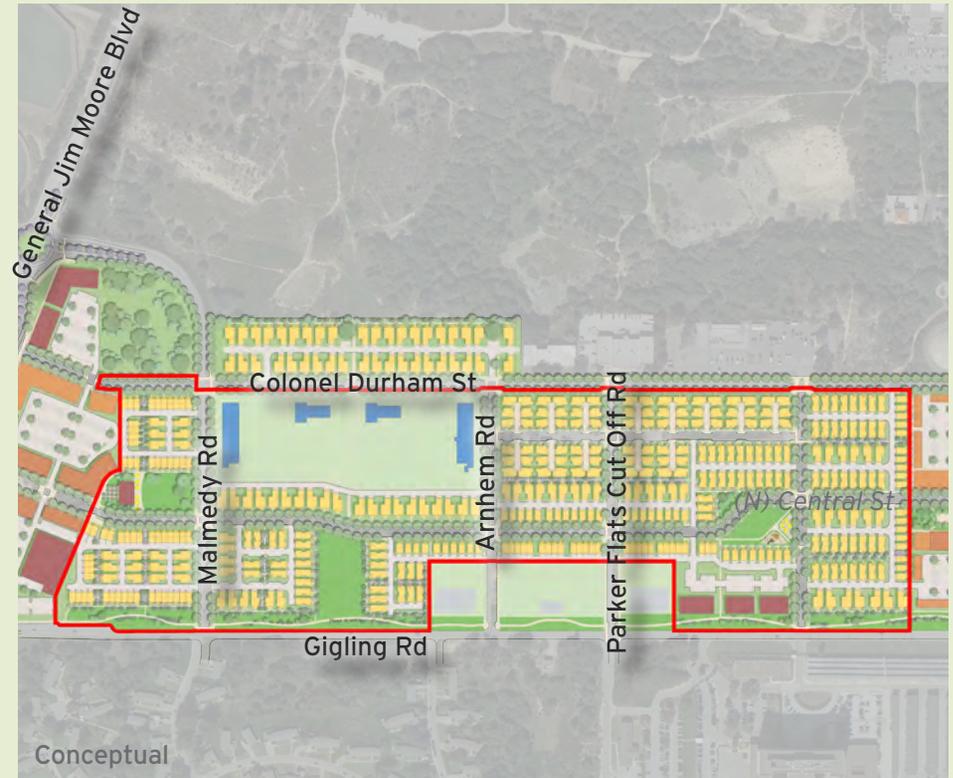


Figure 2.14 - Central Sub-Area

Sub-Area CE: Central

Linking the village centers, Central Street is the spine of Campus Town that runs from General Jim Moore Boulevard in the west to 7th Avenue in the east. A Central Square with the potential to hold a freestanding restaurant or cafe, will serve as community gathering space and provides a visual terminus at the end of the main street to define the retail core and serve as a landmark to help orient visitors and residents alike. The Central Sub-Area provides the greatest concentration of residential units.

Although continuous, Central Street is not a straight line through Campus Town, but rather, an episodic route punctuated by parks, plazas, and terminated vistas that orient, define, and enliven the Campus Town. Given the street's length and location, it serves the greatest variety of Building Types and uses in all of Campus Town to ensure a rich and vibrant urban experience.



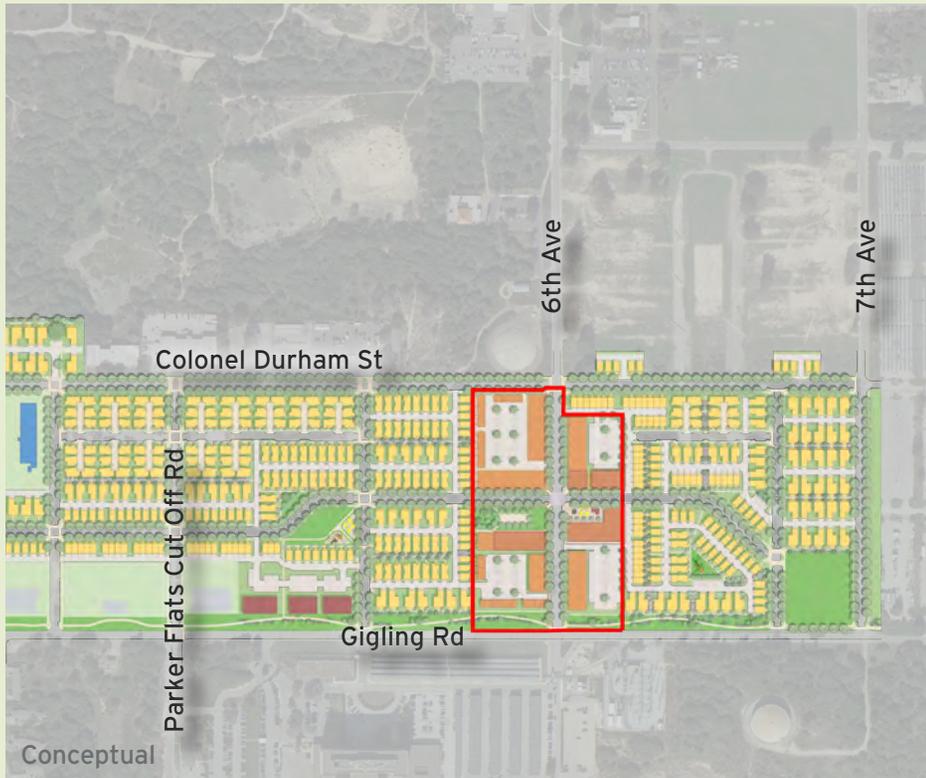


Figure 2.15 - University Village Sub-Area

Sub-Area UV: University Village

The University Village Sub-Area is envisioned as primarily serving the CSUMB community. By focusing development on student, faculty, and staff amenities the increasingly important 6th Avenue spine on campus is extended off campus to engage and interact with the community at large.

The development has the potential for student and faculty housing; office; and research and development space over ground-floor retail; eating establishments; and entertainment venues. The Central Plaza facilitates the engagement between the transitory student body and the local permanent residents to foster a spirit of neighborly cohesion and community pride. A youth hostel is planned for the University Village Sub-Area.

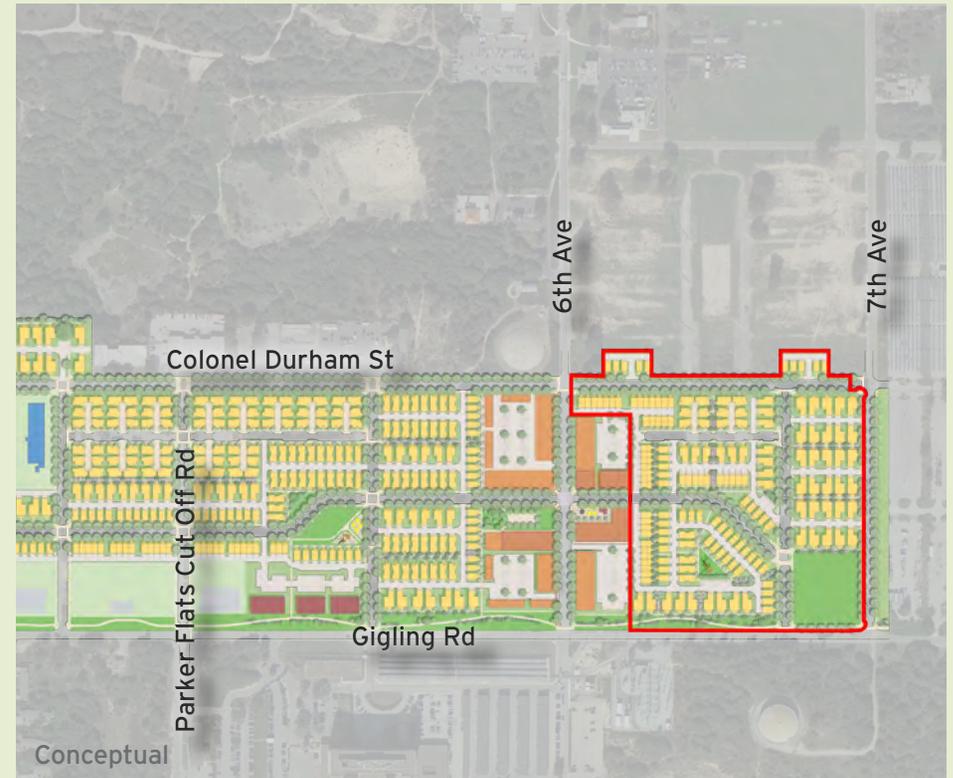


Figure 2.16 - East End Sub-Area

Sub-Area EE: East End

The East End Sub-Area is primarily defined by Central Street's diagonal termination at a park sufficiently sized to provide a little league baseball field. The variety of Building Types in the East End and its proximity to University Village will provide a diversity of users that will activate the park and streetscape. The ending of this primary axis, Central Street, with a large park denotes the boundary of the Campus Town by serving as a green transition from the compact built environment to the open natural expanse beyond. A gateway feature will also promote the Fort Ord National Monument.

Conceptual Rendering of a Mixed-Use Center
Subject to Change





Public Realm Standards and Guidelines

This Chapter sets forth the Standards and Guidelines for development of the Public Realm in accordance with FORA's Regional Urban Design Guidelines for Complete Streets and Seaside's commitment to environmentally-conscious city building. New developments within the Specific Plan Area shall substantially conform to the Standards set forth in this Chapter 3, and should be generally consistent with the Guidelines set forth in this Chapter 3, as provided in Chapter 6, Implementation.

3.1 Purpose and Applicability

In order to effectively implement the community's vision for Campus Town, these Public Realm Standards and Guidelines ensure that the Public Realm serves the needs of the various functions required of an enjoyable, efficient, and resilient infrastructure network. Composed of public rights of way and private front Yards, the Public Realm is the communal social setting of urban life. Consistent with the Form-Based Code, the primary aspects of the Public Realm that serve to organize the built environment are Thoroughfares and Open Spaces. The appearance of front Yards of private Lots play a vital role in the success of the Public Realm, and therefore Standards and Guidelines for the initial construction of such Yards are included in this Chapter (rather than in Chapter 4). The character of front Yards and how they are detailed are dependent upon the Thoroughfare Type and the ground-floor frontage, whereas the setback distance is regulated in the Setback Plan (Figure 4.2). Together, these features not only provide mobility and recreation, but when properly designed, also nurture local economies, healthy lifestyles, and community cohesion.



Illustrative Multi-Modal Public Realm

Well-designed thoroughfares enhance the pedestrian experience with wide sidewalks, active ground-floors and attractive landscaping.

3.2 Thoroughfare Network

3.2.1 Description of the Specific Plan Thoroughfare Network

Within Campus Town, multimodal design will allow vehicles, bicyclists, and pedestrians to safely travel from location to location. Several planned improvements to the Campus Town Specific Plan Area include:

- Complete streets: Complete streets include multimodal facilities that allow for multiple modes to travel safely and comfortably along the Thoroughfare, such as bike lanes, comfortable pedestrian sidewalks, transit stops with shelters, and multi-use paths.
- Roundabouts: Two roundabouts will provide access for all modes of travel, while calming and slowing vehicular traffic. As a project with a multimodal focus, these roundabouts will improve vehicular circulation without significantly impeding bicycle and pedestrian travel.
- Traffic Signal: The Specific Plan minimizes the use of signalized intersections to one traffic signal at General Jim Moore Boulevard and the new Central Street. This single traffic signal will serve as the access point into the Commercial Center.
- HAWK Beacon: A **H**igh-Intensity **A**ctivated cross**W**alk Beacon, or HAWK Beacon, is a traffic control device that stops road traffic to allow for protected pedestrian crossings. A HAWK Beacon will be installed across General Jim Moore Boulevard at Colonel Durham Street. A curb bump out and a median refuge area also contribute to a safe and comfortable pedestrian crossing.
- Multiuse paths: Wide sidewalks for pedestrians and bike lanes on selected streets provide separate space for non-vehicular modes. Separating these modes both reduces conflicts with drivers and increase the likelihood that travelers will bike or walk instead of drive through the area. In addition, slow design speeds will allow bikes to travel along other streets with sharrows.





3.2.2 Bicycling in Campus Town

This Specific Plan proposes a simple approach to bicycling in Campus Town: every street is designed to safely accommodate bike traffic. The majority of the new streets are designed for slow-moving traffic with one travel lane in each direction. Class II bicycle lanes are also provided on key streets including Lightfighter Drive, 6th Avenue, Gigling Road, and General Jim Moore Boulevard, to connect to existing and planned bicycle routes in the surrounding area. A class IV separated bikeway along Malmedy Road will connect to a FORTAG spur for a seamless ride to and through Campus Town. Along all other streets in the Specific Plan Area, bicycles and vehicles will share the Roadway.

3.2.3 Emergency Vehicle Considerations

While the Specific Plan utilizes roundabouts and streets with 10 and 11-foot travel lanes, which are beneficial for bicyclists and pedestrians, care has been taken to ensure access for emergency services vehicles. The use of 10-foot travel lanes means that there is a total of 20 feet clear, providing the necessary width for vehicles to pull to the side and permit emergency services to pass.

3.2.4 Delivery Considerations

The Specific Plan includes retail and office land uses, both of which will likely use delivery vehicles. In addition to the street circulation considerations for transit and emergency vehicles, delivery vehicles also require proper on-site circulation and loading/unloading zones. The Specific Plan includes provision for on-site circulation access for larger delivery vehicles where appropriate.

3.2.5 Transit in Campus Town

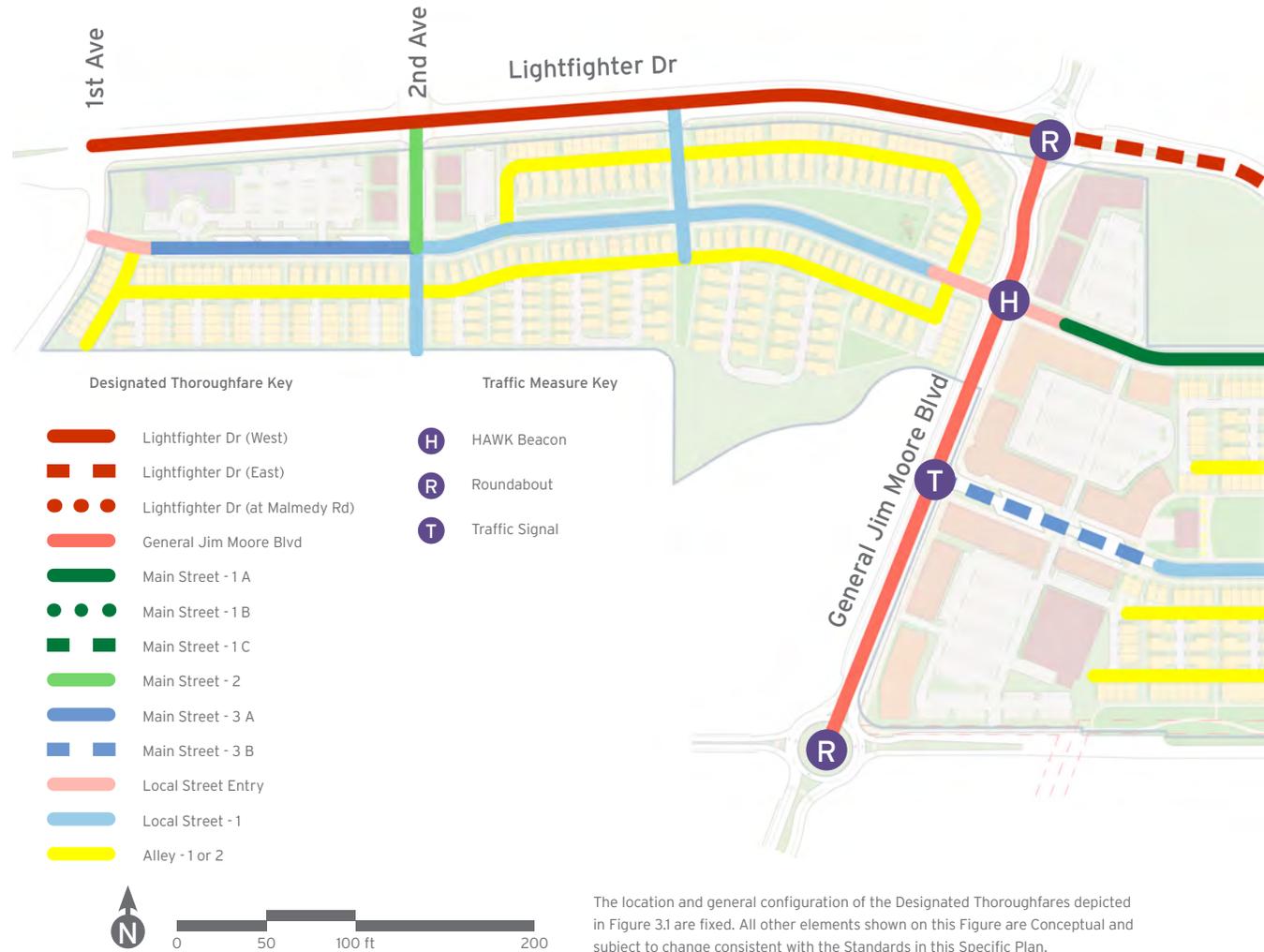
The Campus Town Specific Plan was designed to create a transit oriented corridor at Lightfighter Drive and General Jim Moore Boulevard and at 6th Avenue and Gigling Road. Additionally, the Fort Ord Base Reuse Plan contemplates a transit center on the border of the City of Seaside and the City of Marina at Second Avenue near Lightfighter Drive. Between these Transit Oriented Development areas the entirety of the Campus Town project meets the criteria outlined in California Public Resources Code Section 21155(a) as “high quality transit corridor.”

3.3 Thoroughfare Types and Standards

The Campus Town street design strategy anticipates that a mix of uses will line the streets though it does not prescribe or predict exactly what those uses will be. Instead, the street design strategy fosters human-scaled environments, the success of which is largely independent of the uses fronting a particular street.

This section depicts the proposed Road and sidewalk sections within the Specific Plan Area. Figure 3.1 provides a key to the individual Thoroughfare Types. The Thoroughfare Types and sections are designed to accommodate the expected volumes of traffic associated with new development in Campus Town; the Thoroughfares' posted speeds are also their design speed so that the built infrastructure itself contributes to the safety and efficiency of the network. In the mixed-use nodes, Commercial Center and University Village, the curb radii shall be 20 feet, in all other locations within the Specific Plan Area, the curb radii shall be 15 feet.

Any streets not identified in Figure 3.1 are not fixed and shall conform to the requirements of Local Streets 1 or 2. Any Alleys not identified in Figure 3.1 are not fixed and shall conform to the requirements of Alleys 1 or 2.



The location and general configuration of the Designated Thoroughfares depicted in Figure 3.1 are fixed. All other elements shown on this Figure are Conceptual and subject to change consistent with the Standards in this Specific Plan.



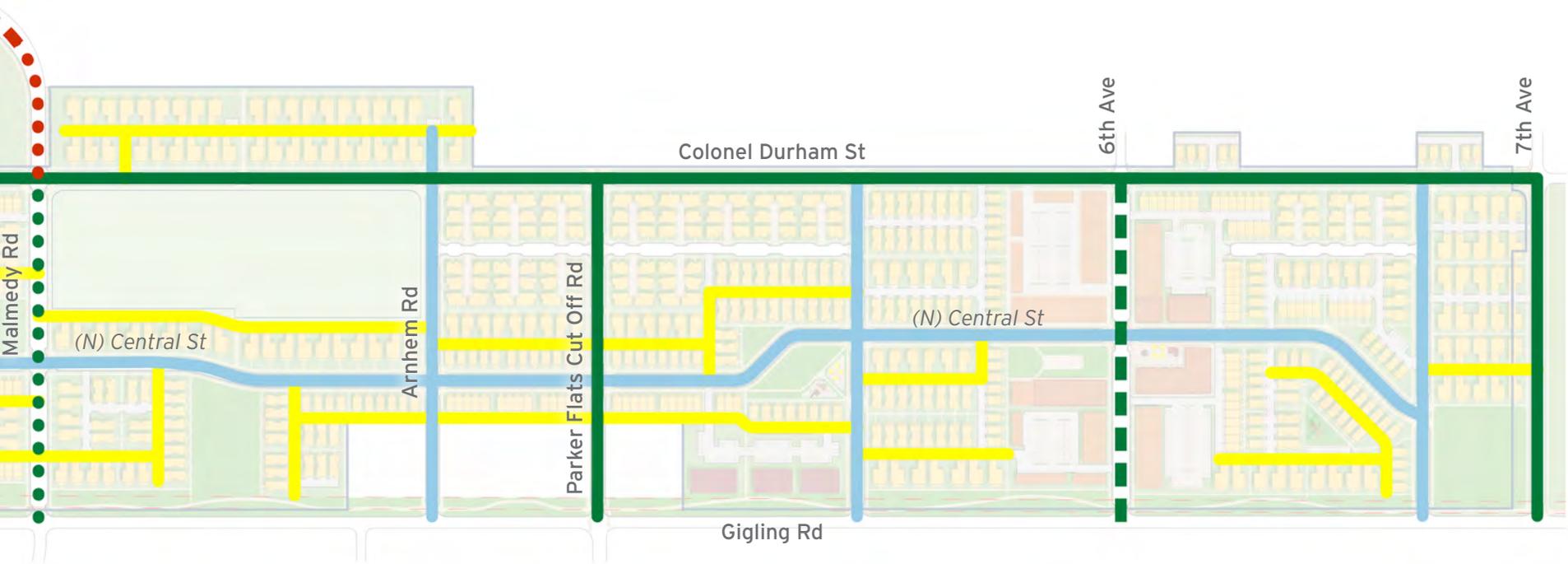


Figure 3.1 - Thoroughfare Types Plan

3.3.1 Lightfighter Drive

West of General Jim Moore Boulevard

A. Design Intent

The southern Street edge of Lightfighter Drive, a multi-lane arterial, is the northern face of the West End Sub-Area of Campus Town. The existing Roadway configuration provides challenges for designing an urban edge. In response to those challenges, the new sidewalk/ shared path design creates a safe pedestrian-oriented environment with building Frontages lining Lightfighter Drive. Regularly planted street trees in grates separate pedestrians from the curb-side bike lane and adjacent travel lane. Finally, per the Setback Plan (Figure 4.2), a 10-foot minimum setback buffers ground-floor uses facing this busy street.

Figure 3.2 - Lightfighter Dr West Section
Conceptual Diagram (right)
Location Legend (below)



SOUTHERN SIDE

NORTHERN SIDE

SBK	10' SWK	8' BIKE	11' TRAVEL	11' TRAVEL	E.T.R. MEDIAN	E.T.R. TRAVEL	E.T.R. TRAVEL	E.T.R. SWK & PKWY
93' PUBLIC R.O.W.								

- E.T.R. Existing to Remain
- PKWY Parkway
- R.O.W. Right of Way
- SBK Setback
- SHDR Shoulder
- SWK Sidewalk

Type Standards



B. Sidewalk Paving

The sidewalk/shared path shall be paved with poured, scored concrete (see Section 3.6.1).

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkways. Trees shall be planted in pits with 5' x 5' minimum tree grates, separated from the face of curb by a one-foot step out strip.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required.

F. Front Setbacks

1. Non-residential ground-floor Frontages.
 - a. Walks to building entries, outdoor dining areas, and Setbacks fronting retail areas shall be paved (see Section 3.6.1).
 - b. Front Setbacks shall be at-grade or raised landscape planters enclosed by a wall or hedge no more than 18

inches in height, measured from the adjacent Sidewalk Grade.

2. Residential ground-floor Frontages.
 - a. Planters should be planted with decorative plants which may include small trees and low shrubs.
 - b. Raised landscape planters, walls, and/or landscaped slopes to accommodate site grading/conform conditions may be appropriate at some residential ground floors.

G. Outdoor Dining

Outdoor dining is permitted within the front Setback adjacent to the operating ground-floor use. Outdoor dining areas may encroach into the public right of way and along a paved parkway provided that a minimum five foot pedestrian path is accessible. They shall be enclosed by removable barriers when barriers are required by State licensing regulations. Outside of business hours, furniture should be stored indoors or may be stacked and secured at the back of the Setback area.

H. On-Street Parking

Not applicable.



Raised Landscape Planters (Illustrative)
Raised planting beds provide occupants of the building with privacy.



Green Hedge (Illustrative)
Hedges provide a green buffer between the road and the residential ground-floor.

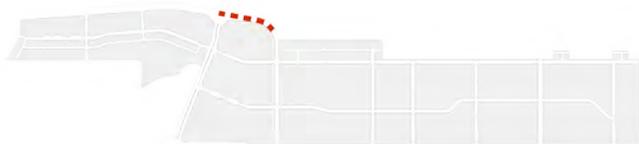


3.3.2 Lightfighter Drive
East of General Jim Moore Boulevard

A. Design Intent

Lightfighter Drive, east of General Jim Moore Boulevard, is located at the northern boundary of the Specific Plan Area. It is intended as a verdant transition from the Specific Plan Area to the CSUMB campus. As an existing Street, no change to the travelway is anticipated along this Street with the exception of any modifications necessary to connect it with the new roundabout at General Jim Moore Boulevard. However, a paved sidewalk and parkway with street trees are proposed for the southern side of this portion of Lightfighter Drive, while a new sidewalk is proposed for the northern side of the street adjacent to CSUMB. There is no regulated Setback on the south side of this street.

Figure 3.3 - Lightfighter Dr East Section
 Conceptual Diagram (right)
 Location Legend (below)



E.T.R. Existing to Remain
 PKWY Parkway
 R.O.W. Right of Way
 SHDR Shoulder
 SWK Sidewalk

Type Standards



B. Sidewalk Paving

The sidewalk/shared path shall be paved with poured, scored concrete (see Section 3.6.1).

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkway. Parkway shall be continuous planters. Wherever possible, parkways shall be designed as infiltration planters with appropriate plant material.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required.

F. Front Setbacks

Not regulated.

G. Outdoor Dining

Not permitted.

H. On-Street Parking

Not applicable.



Infiltration Planter Parkway (Illustrative)



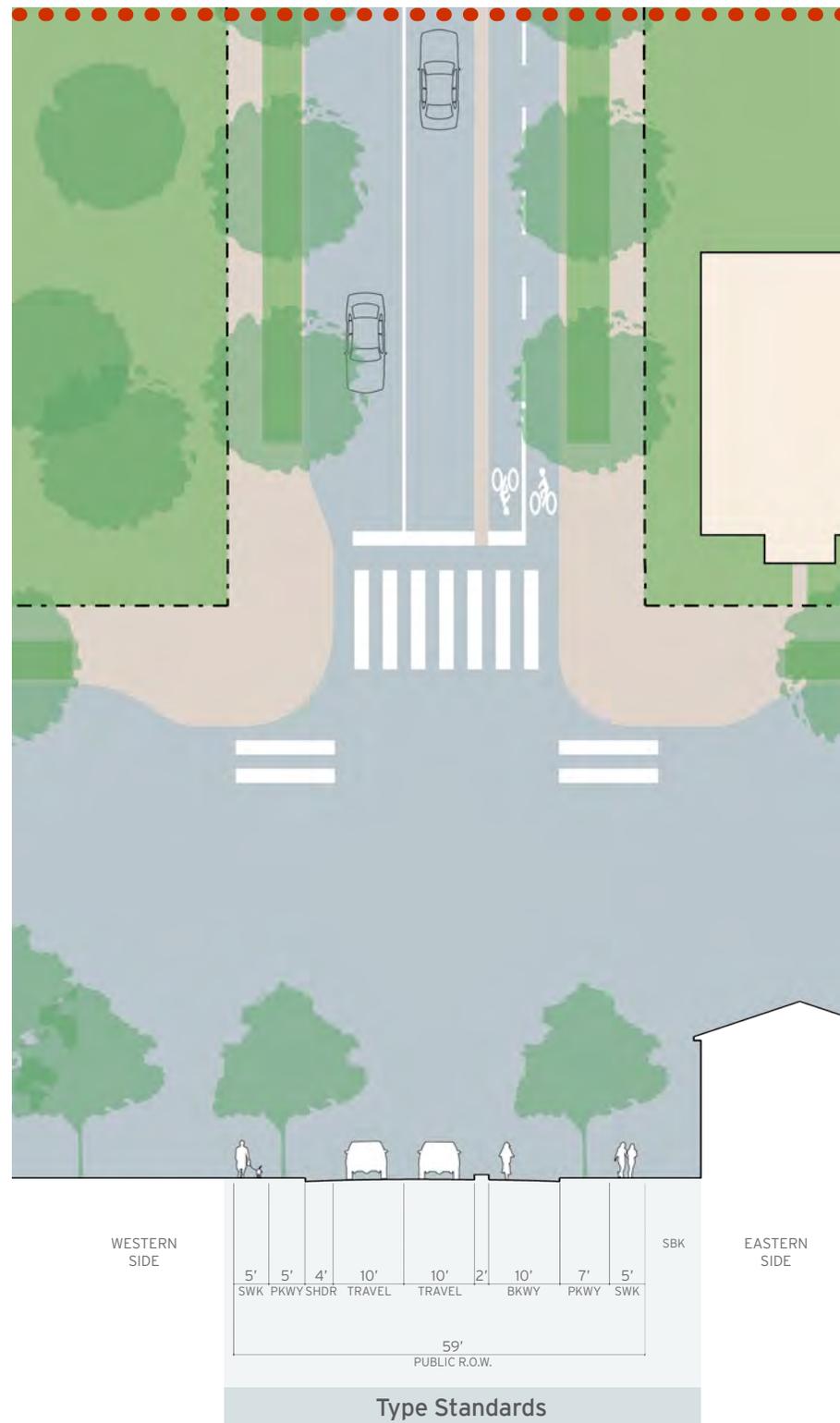
Landscaped Parkway (Illustrative)

3.3.3 Lightfighter Drive at Malmedy Road

A. Design Intent

Lightfighter Drive at Malmedy Road marks the transition of Lightfighter Drive from an Arterial street with a median to a neighborhood street with a bikeway. As an existing street, no change to the width of the travelway is anticipated, but the incorporation of a Class IV bikeway provides a link to the FORTAG network. Paved sidewalks and parkways with street trees are proposed along both sides of the street to further reinforce the multi-modal character of Campus Town - vehicles, bicycles, and pedestrians are all considered. There is no regulated Setback on this street.

Figure 3.4 - Lightfighter Dr Malmedy Section
Conceptual Diagram (right)
Location Legend (below)



B. Sidewalk Paving

Sidewalks shall be paved with poured, scored concrete (see Section 3.6.1).

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkway. Parkway shall be continuous planters. Wherever possible, parkways shall be designed as infiltration planters with appropriate plant material.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required.

F. Front Setbacks

Not regulated.

G. Outdoor Dining

Not permitted.

H. On-Street Parking

Not applicable.



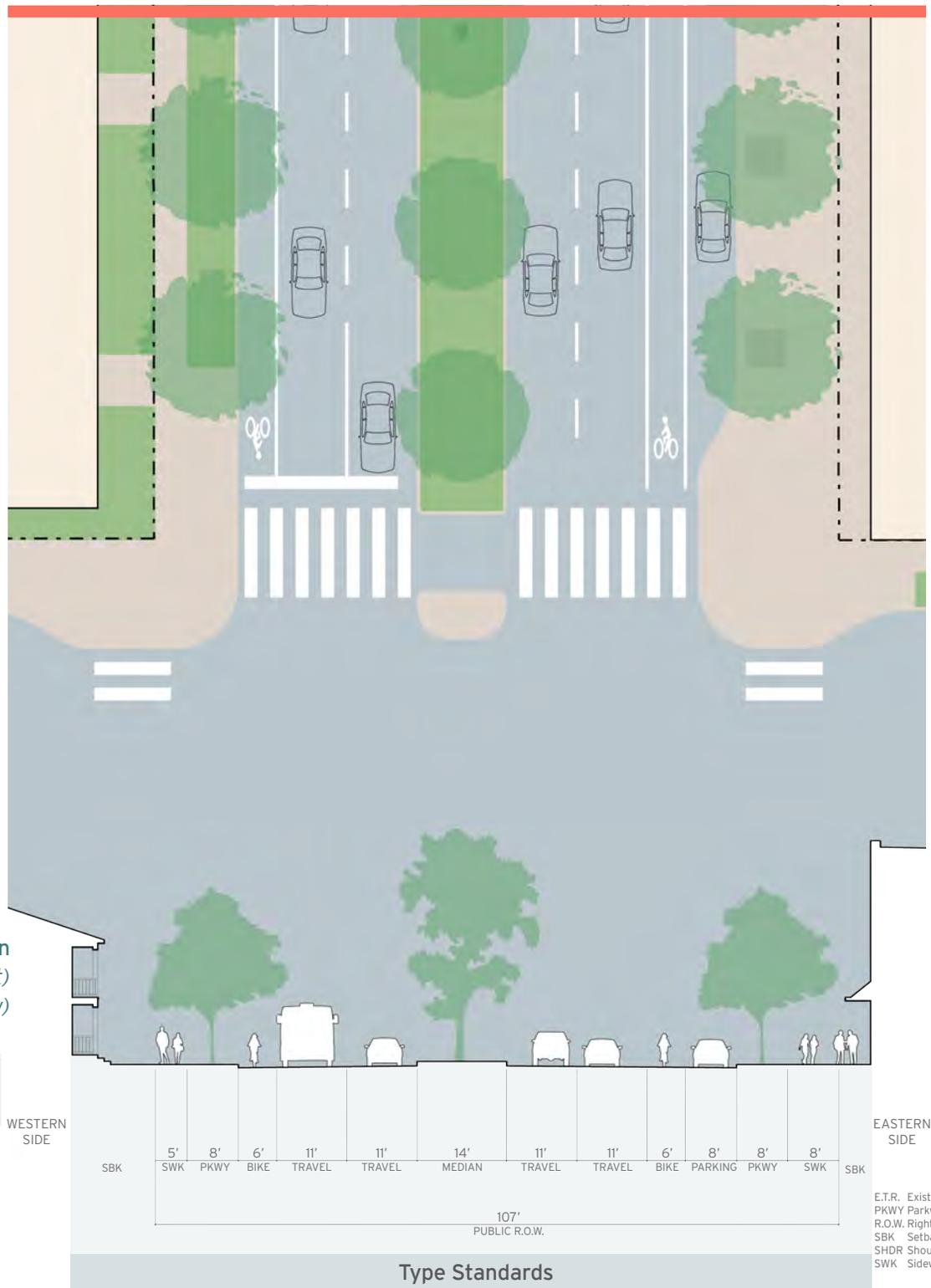
Continuous Parkway (Illustrative)

3.3.4 General Jim Moore Boulevard

A. Design Intent

General Jim Moore Boulevard is a multi-lane Arterial Street that bisects the Specific Plan Area. In order to calm traffic and signal to drivers that this area of General Jim Moore Boulevard is intended as a slower-moving, urban Street, two roundabouts are proposed along the Street, one at the intersection with Gigling Road and the other at Lightfighter Drive. The eastern Street edge of General Jim Moore Boulevard is the face of Campus Town’s Commercial Center Sub-Area. In order to provide for a pedestrian-friendly urban edge, the Roadway has been expanded to accommodate Class II bike lanes, and on-street parallel parking adjacent to the Commercial Center. The sidewalk is designed to accommodate a high volume of pedestrians at the retail core with room for pedestrians, amenity areas, shop displays, and outdoor dining. Curb extensions reduce the crossing distance at intersections and provide space for amenity areas.

Figure 3.5 - General Jim Moore Boulevard Section Conceptual Diagram (right) Location Legend (below)



E.T.R. Existing to Remain
 PKWY Parkway
 R.O.W. Right of Way
 SBK Setback
 SHDR Shoulder
 SWK Sidewalk



B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1), or decorative paving, with integral color.

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkways.
 - a. Western side. Parkway shall be continuous planters. Where feasible, parkways shall be designed as infiltration planters with appropriate plant material.
 - b. Eastern side. Trees shall be planted in pits with 5' x 5' minimum tree grates, separated from the face of curb by a one-foot step out strip. Tree planters shall be placed so that they match the street tree spacing, see Figure 3.6.
3. Curb Extensions. Landscape planters shall extend into curb extensions and separate sidewalk amenity zones from the Roadway.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required. Curb extensions should be furnished with pedestrian or bicycle amenities or both.

F. Front Setbacks

1. Paving.
 - a. Where adjacent to retail or Shopfronts, Setbacks shall be at-grade and paved with poured, scored concrete to match the public sidewalk (see Section 3.6.1).

Where grading conditions make this infeasible, Shopfronts may be raised raised no more than 18 inches in height, measured from the adjacent Sidewalk Grade, provided there is ADA accessibility into the space and subject to the review and approval of the Zoning Administrator.
 - b. Where adjacent to ground-floor residential, Setbacks shall be at-grade or raised landscape planters enclosed by a wall no more than 18 inches in height, measured from the adjacent Sidewalk Grade. Planters should be planted with decorative



Tree Grate Parkway (Illustrative)



Wide, Active Sidewalk (Illustrative)

plants which may include small trees and low shrubs. Walks to building entries shall be paved (see Section 3.6.1).

2. Landscaping.

- a. Where adjacent to retail or Shopfronts, small shrubs and trees in movable pots are permitted in the paved Setback areas fronting retail spaces. Landscaped planters or Yards are not permitted in those areas.
- b. Where adjacent to ground-floor residential, raised landscape planters, walls, and/or landscaped slopes to accommodate site grading/conform conditions may be appropriate at some residential ground floors.

3. Movable Signs and Displays. Movable signs and outdoor merchandise displays in conformance with Sign Standards and Guidelines of Section 4.8 are permitted. All such signs and displays shall be approved by the City, as part of the landscape plan submittal during the development review described in Section 6.2.

4. Furniture. Outside of business hours, furniture shall be removed from the Setback and stored indoors or may be

stacked and secured at the back of the Setback area.

G. Outdoor Dining

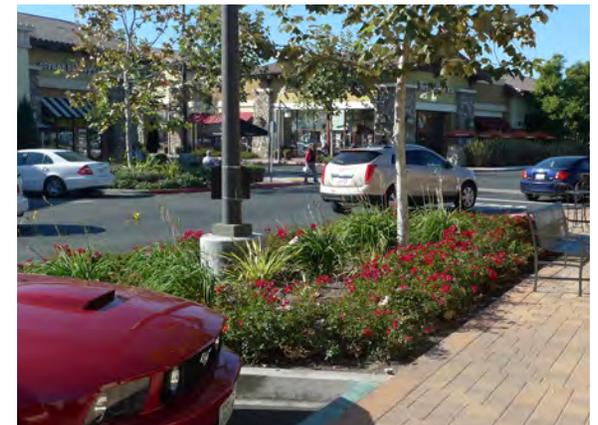
Outdoor dining is permitted within the front setback adjacent to the operating ground-floor use. Outdoor dining areas may encroach into the public right of way and along a paved parkway provided that a minimum five foot pedestrian path is accessible. They shall be enclosed by removable barriers when barriers are required by State licensing regulations. Outside of business hours, furniture shall be stored indoors or may be stacked and secured at the back of the setback area.

H. On-Street Parking

Parallel parking shall be provided along the eastern side of the street.



Vertical Green (Illustrative)
Small shrubs and trees buffer the busy road from the ground-floor uses.



Landscaped Curb Extension (Illustrative)

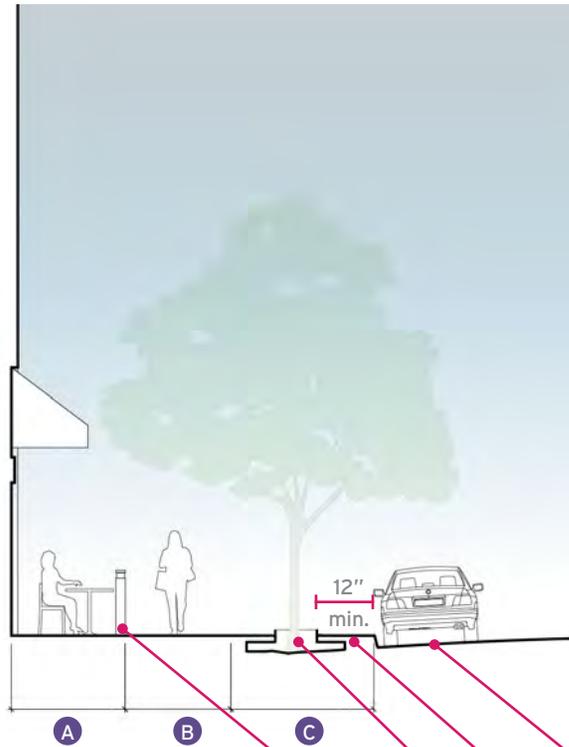


Figure 3.6 - Tree Planter Diagram (Conceptual)

- Pedestrian Amenities Key
- A** Private Setback - Furnishing Zone
 - B** Public R.O.W. - Sidewalk
 - C** Public R.O.W. - Parkway

- Parallel Curbside Parking
- Step-Out Area
- Tree Planting Pit below Tree Grate
- Barrier at Perimeter of Dining Area



Ground-floor Retail (Illustrative)

Stepped-slab ground floors allow pedestrians on heavily sloped sidewalks to access businesses at grade without the need for ramps or steps.



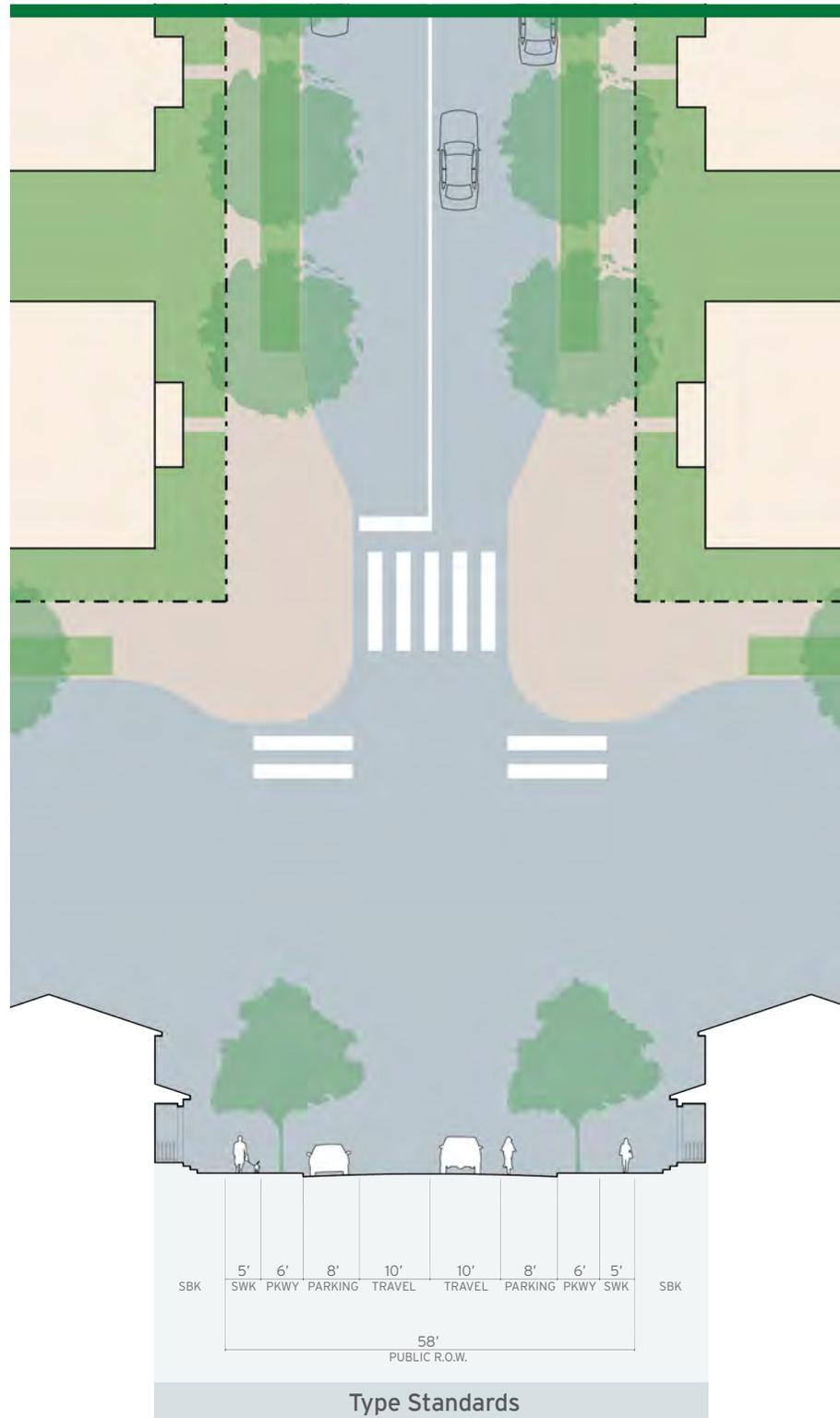
3.3.5 Main Street - 1 A

Colonel Durham Street, Parker Flats Cut Off Road, and 7th Avenue

A. Design Intent

These local Streets are existing and will be improved upon. They serve pedestrians, bicycles, and slow-moving vehicles to access various uses and destinations. The Roadways are designed for slow traffic speeds with shared use traffic lanes that accommodate bicycles. They have one travel lane in each direction and curbside parking lanes on both sides. Sidewalks provide ample room for pedestrians. Streets are landscaped with street trees and continuous parkways with paved pass-throughs to the sidewalk. Setback Standards allow buildings to be set between five and 15 feet from the Lot Line. This variation makes for a livelier street Frontage. The Setback areas are paved or landscaped per the building Frontage Type Standards.

Figure 3.7 - Main Street - 1A Section
Conceptual Diagram (right)
Location Legend (below)



E.T.R. Existing to Remain
PKWY Parkway
R.O.W. Right of Way
SBK Setback
SHDR Shoulder
SWK Sidewalk



B. Sidewalk Paving

Sidewalks shall be paved with poured, scored concrete (see Section 3.6.1).

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkway.
 - a. Parkway shall be six feet wide continuous planters and flush with the finished sidewalk. Where feasible, parkways shall be designed as infiltration planters with appropriate plant material. An 18 inch wide hardscaped step-out area shall be provided with infiltration planters to allow access from parked vehicles to the sidewalk.
 - b. Where infiltration planters are not feasible parkways shall be landscaped with irrigated turf or native ground covers. Paved walks shall provide access from the sidewalk to the curb and placed at regular intervals not to exceed 50 feet.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required.

F. Front Setbacks

Front setbacks shall be landscaped in conformance with the building Frontage Type standards (see Section 4.6.3).

G. Outdoor Dining

Not permitted.

H. On-Street Parking

Parallel parking shall be provided along both sides of the street.



Infiltration Planter Parkway (Illustrative)



Irrigated Turf Parkway (Illustrative)

3.3.6 Main Street - 1 B
Malmedy Road

A. Design Intent

Malmedy Road is a multi-modal Thoroughfare that connects Lightfighter Drive to Gigling Road. Sidewalks with street trees provide a comfortable pedestrian experience, a Class IV bikeway connects to local FORTAG spurs, and a travel lane and parking lane in both directions serve vehicular users. Like other streets throughout the Specific Plan, the Roadway has slow traffic speeds on a pedestrian-friendly street.

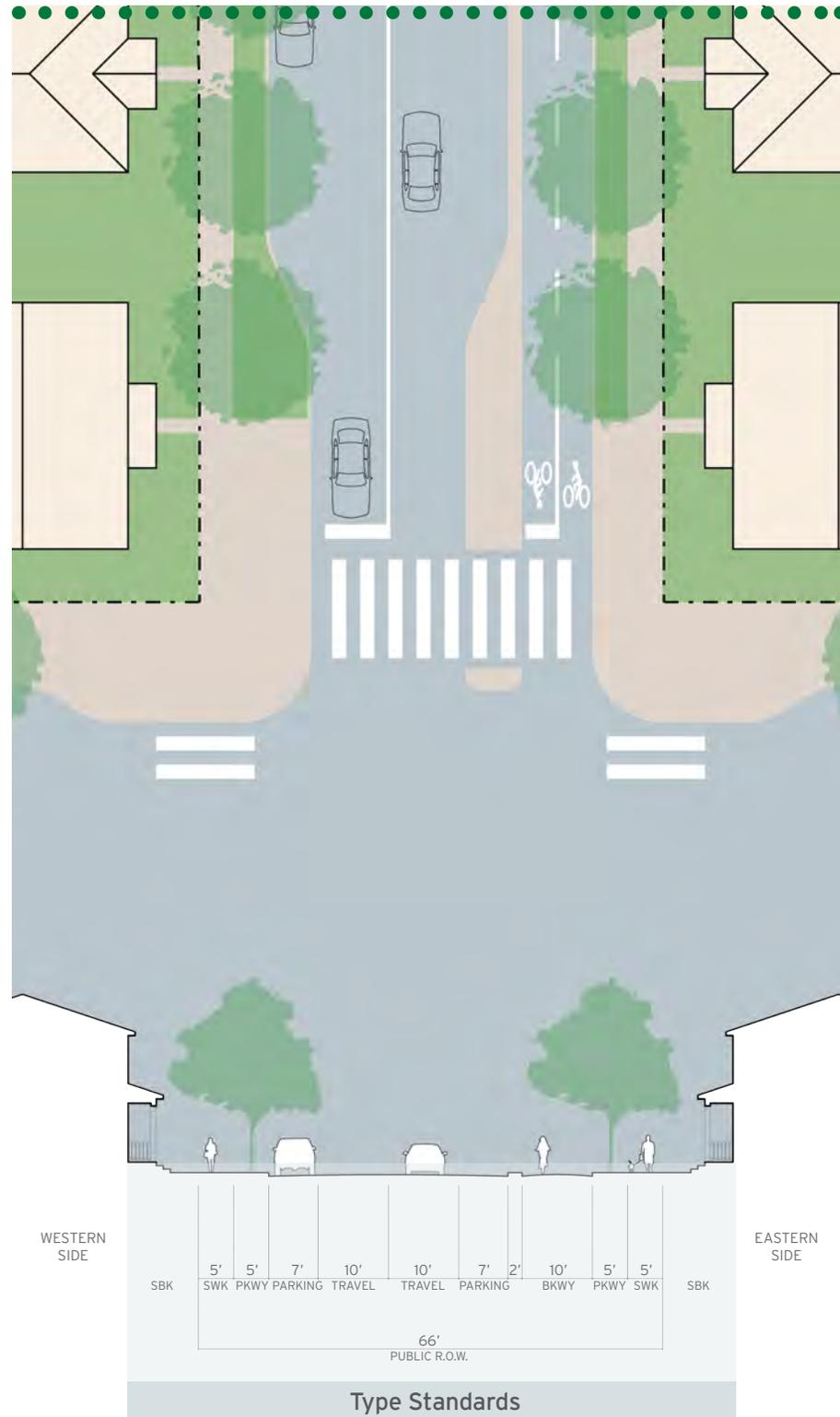


Figure 3.8 - Main Street - 1B Section
Conceptual Diagram (right)
Location Legend (below)



- BKWY Bikeway
- E.T.R. Existing to Remain
- PKWY Parkway
- R.O.W. Right of Way
- SBK Setback
- SHDR Shoulder
- SWK Sidewalk



B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1).

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkway.
 - a. Parkway shall be five feet wide continuous planters and flush with the finished sidewalk. Where feasible, parkways shall be designed as infiltration planters with appropriate plant material. An 18 inch wide hardscaped step-out area shall be provided with infiltration planters to allow access from parked vehicles to the sidewalk.
 - b. Where infiltration planters are not feasible parkways shall be landscaped with irrigated turf or native ground covers. Paved walks shall provide access from the sidewalk to the curb and placed at regular intervals not to exceed 50 feet.

3. Curb Extensions. Landscape planters shall extend into curb extensions and separate sidewalk amenity zones from the Roadway.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required. Curb extensions should be furnished with pedestrian or bicycle amenities or both.

F. Front Setbacks

Front setbacks shall be landscaped in conformance with the Building Frontage Type standards (see Section 4.5.3).

G. Outdoor Dining

Not permitted.

H. On-Street Parking

Parallel parking shall be provided along both sides of the street.



Irrigated Turf Parkway (Illustrative)



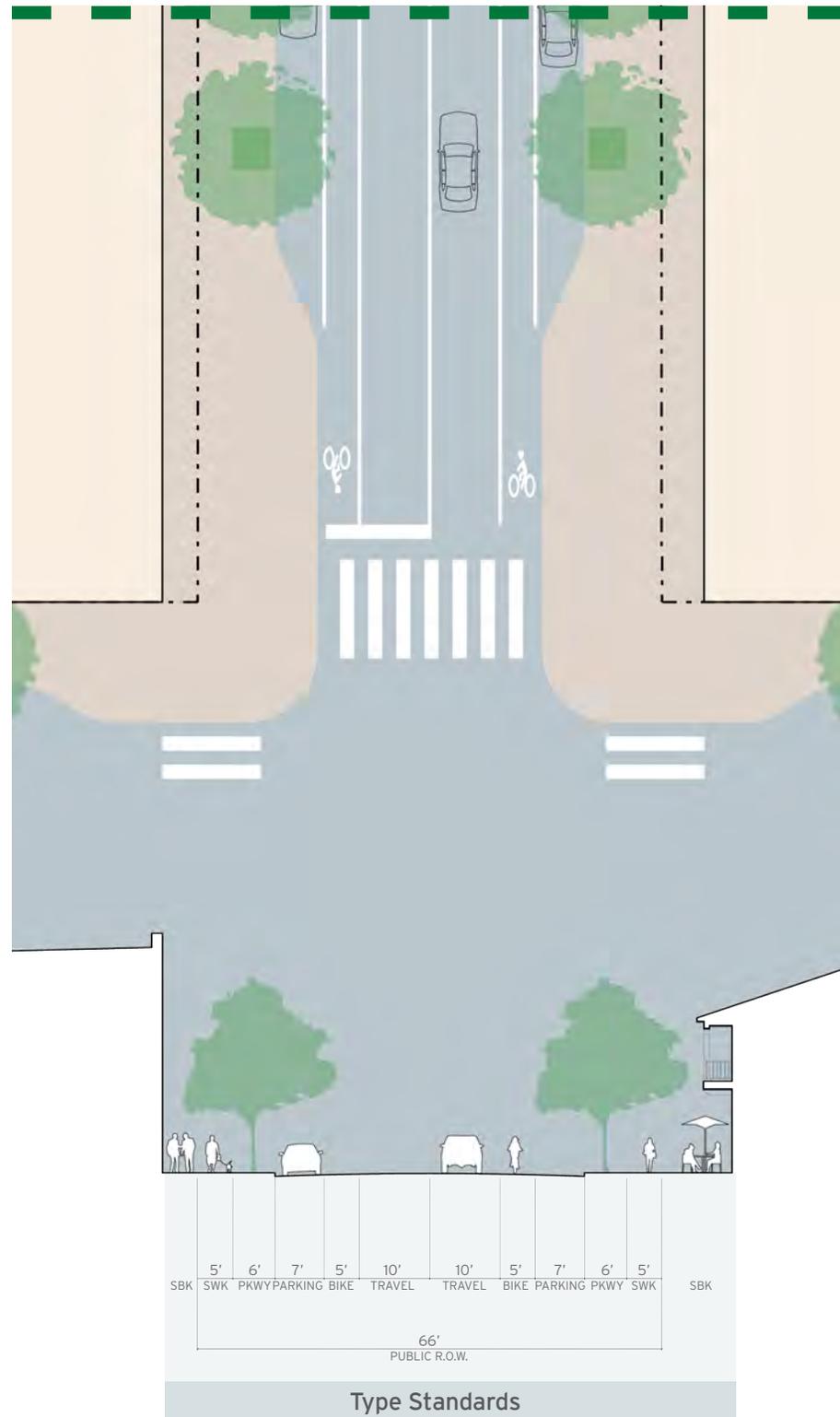
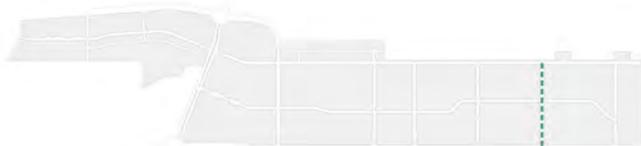
Protected Class IV Bikeway (Illustrative)

3.3.7 Main Street - 1 C
6th Avenue

A. Design Intent

6th Avenue connects the heart of the CSUMB campus to the Campus Town area, and Gigling Road. It is also the “Main Street” of the University Village Sub-Area. Planting and streetscape design will establish a unique identity that fosters a lively environment for pedestrians and bicyclists. 6th Avenue will have one travel lane, a Class II bike lane, and an on-street parking lane on each side of the street. Like other Streets throughout the Specific Plan, the Roadway will have slow traffic speeds on a pedestrian-friendly street. Street trees are provided in grates or in landscape planters.

Figure 3.9 - Main Street - 1C Section
Conceptual Diagram (right)
Location Legend (below)



E.T.R. Existing to Remain
 PKWY Parkway
 R.O.W. Right of Way
 SBK Setback
 SHDR Shoulder
 SWK Sidewalk

Type Standards



B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1), or decorative paving, with integral color.

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkways. Trees shall be planted in pits with 5' x 5' minimum tree grates, separated from the face of curb by a one-foot step out strip.
3. Curb Extensions. Landscape planters shall extend into curb extensions and separate sidewalk amenity zones from the Roadway.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required. Curb extensions should be furnished with pedestrian or bicycle amenities or both.

F. Front Setbacks

1. Paving.
 - a. Where adjacent to retail or Shopfronts, Setbacks shall be at-grade and paved with poured, scored concrete to match the public sidewalk (see Section 3.6.1).
 - b. Where adjacent to ground-floor residential, Setbacks shall be at-grade or raised landscape planters enclosed by a wall no more than 18 inches in height, measured from the adjacent Sidewalk Grade. Planters should be planted with decorative plants which may include small trees and low shrubs. Walks to building entries shall be paved.
2. Landscaping.
 - a. Where adjacent to retail or Shopfronts, small shrubs and trees in movable pots are permitted in the paved Setback areas fronting retail spaces. Landscaped planters or Yards are not permitted in those areas.
 - b. Where adjacent to ground-floor residential, raised landscape planters, walls, and/or landscaped slopes to accommodate site grading/conform conditions may be appropriate at some residential ground floors.

3. Movable Signs and Displays. Where adjacent to retail or Shopfronts, movable Signs and outdoor merchandise displays in conformance with Sign Standards and Guidelines of Section 4.8 are permitted. All such signs and displays shall be approved by the City, as part of the landscape plan submittal. Outside of business hours, signs and displays shall be removed from the setback and stored indoors or stacked and stored at the back of the Setback area.

G. Outdoor Dining

Outdoor dining is permitted within the front setback adjacent to the operating ground-floor use. Outdoor dining areas may encroach into the public right of way and along a paved parkway provided that a minimum five foot pedestrian path is accessible. They shall be enclosed by removable barriers when barriers are required by State licensing regulations. Outside of business hours furniture shall be stored indoors or may be stacked and secured at the back of the Setback area.

H. On-Street Parking

Parallel parking shall be provided along both sides of the street.

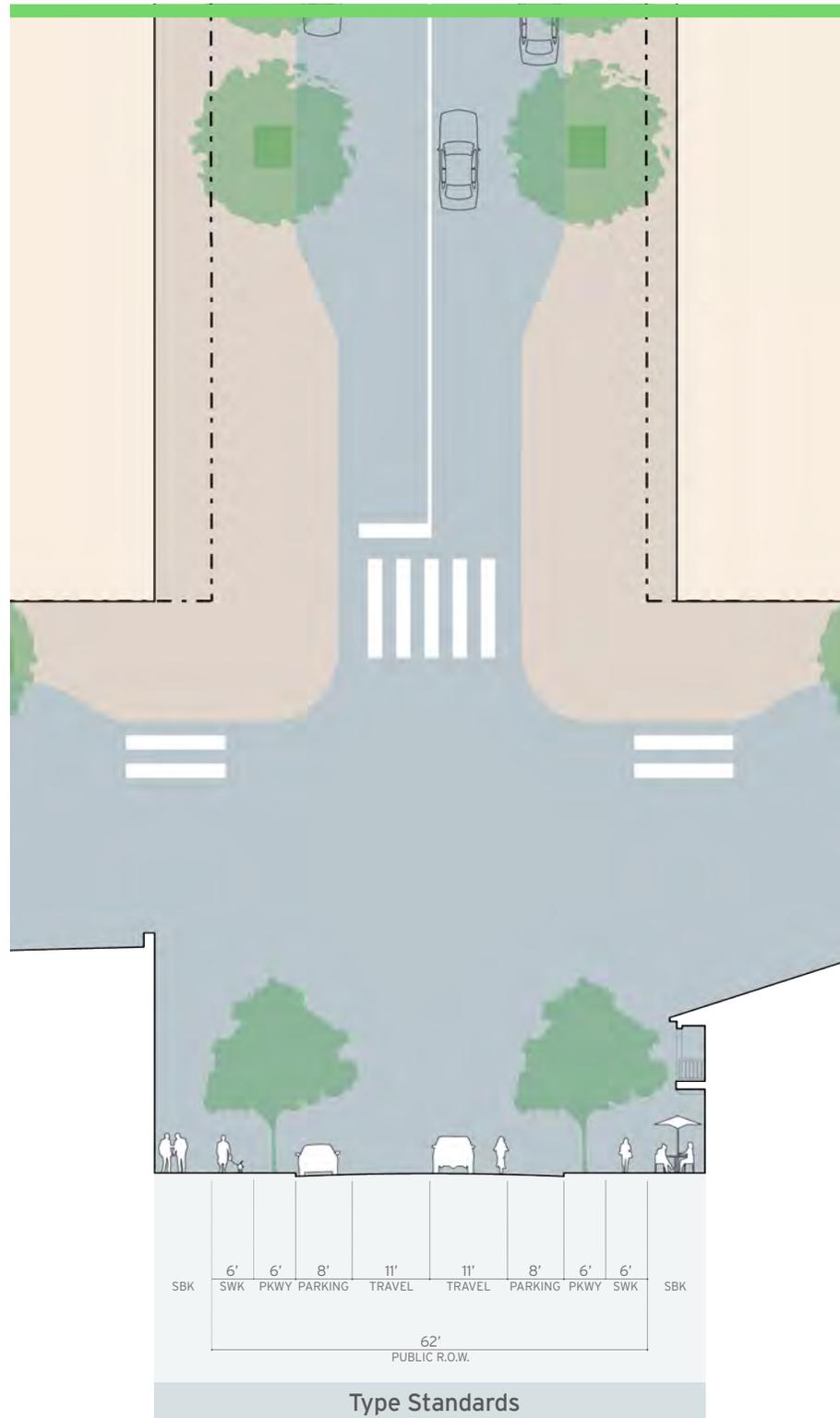


3.3.8 Main Street - 2
2nd Avenue

A. Design Intent

This Street is an extension of 2nd Avenue, south of Lightfighter Drive. It serves as a short block-long “Main Street” for the West-End Sub-Area, with a high volume of pedestrians anticipated. It has one-travel lane in each direction and curb-side parallel parking.

Figure 3.10 - Main Street - 2 Section
Conceptual Diagram (right)
Location Legend (below)



- E.T.R. Existing to Remain
- PKWY Parkway
- R.O.W. Right of Way
- SBK Setback
- SHDR Shoulder
- SWK Sidewalk



B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1), or decorative paving, with integral color.

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkways. Trees shall be planted in pits with 5' x 5' minimum tree grates, separated from the face of curb by a one-foot step out strip.
3. Curb Extensions. Landscape planters shall extend into curb extensions and separate the sidewalk from the Roadway (see Section 3.5.2).

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required. Curb extensions should be furnished with pedestrian or bicycle amenities or both.

F. Front Setbacks

1. Paving. Setbacks shall be at-grade and paved with poured, scored concrete to match the public sidewalk (see Section 3.6.1).
2. Landscaping. Where adjacent to retail or Shopfronts, small shrubs and trees in movable pots are permitted in the paved Setback areas. Landscaped planters or Yards are not permitted in these areas.
3. Movable Signs and outdoor merchandise displays in conformance with Sign Standards and Guidelines of Section 4.8 are permitted. All such signs and displays shall be approved by the City, as part of the landscape plan submittal. Outside of business hours, signs and displays shall be removed from the Setback and stored indoors or stacked and stored at the back of the Setback area.

G. Outdoor Dining

Outdoor dining is permitted within the front Setback adjacent to the operating ground-floor use. Outdoor dining areas may encroach into the public right of way and along a paved parkway provided that a minimum five foot pedestrian path is accessible. They shall be enclosed by removable barriers when barriers are required by State licensing regulations. Outside of business hours furniture should be stored

indoors or may be stacked and secured at the back of the Setback area.

H. On-Street Parking

Parallel parking shall be provided along both sides of the street.



Tree Grate Parkway (Illustrative)

3.3.9 Main Street - 3 A

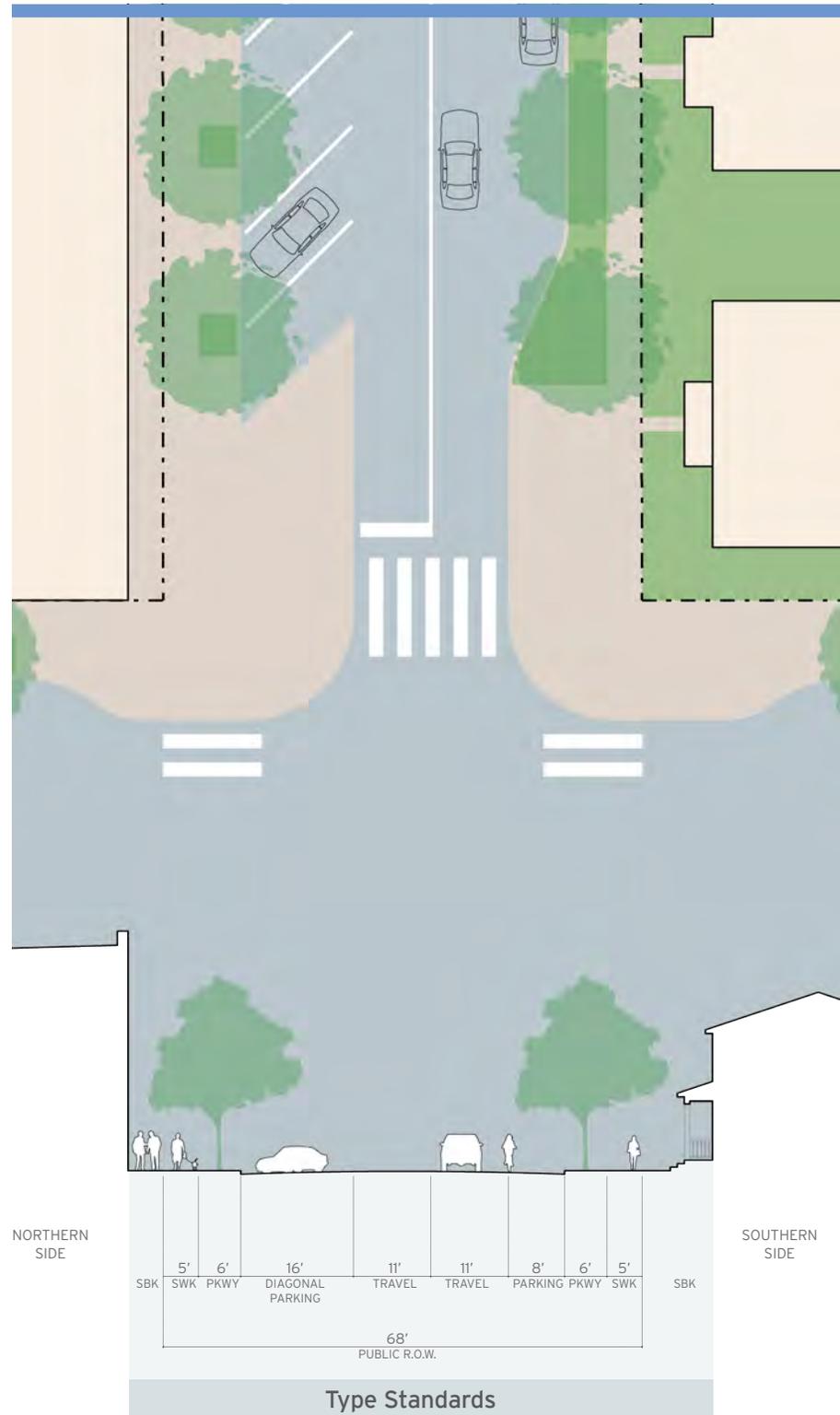
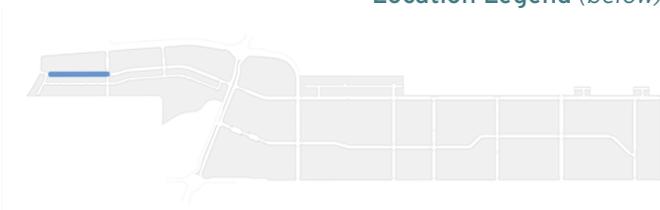
A. Design Intent

This east/west Street at the western edge of the Specific Plan Area extends Colonel Durham Street to within one-block of 1st Avenue. It is designed to accommodate a hotel use on its north side, with slow moving vehicular traffic and bicycles sharing the Roadway, and generous sidewalks on both sides. It has one-travel lane in each direction, diagonal parking on the north side and curb-side parallel parking on the south.

B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1), or decorative paving, with integral color.

Figure 3.11 - Main Street - 3A Section
Conceptual Diagram (right)
Location Legend (below)



E.T.R. Existing to Remain
PKWY Parkway
R.O.W. Right of Way
SBK Setback
SHDR Shoulder
SWK Sidewalk



C. Landscape

1. Street Trees. Street trees shall be planted in conformance with the Street Tree Plan (see Figure 3.24).
2. Parkways. Where feasible, parkways shall be designed as infiltration planters with appropriate plant material. An 18 inch wide hardscaped step-out area shall be provided with infiltration planters to allow access from parked vehicles to the sidewalk. Alternatively, trees may be planted in pits with 5' x 5' minimum tree grates, separated from the face of curb by a one-foot step out strip. Parkways on the southern side of the street shall be landscaped with irrigated turf or native ground covers. Paved walks shall provide access from the sidewalk to the curb and placed at regular intervals not to exceed 50 feet.
3. Curb Extensions. Landscape planters shall extend into curb extensions and separate the sidewalk from the Roadway (see Section 3.5.2).

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required. Curb extensions should be furnished with pedestrian or bicycle amenities or both.

F. Front Setbacks

1. Paving.
 - a. Where adjacent to retail or Shopfronts, Setbacks shall be at-grade and paved with poured, scored concrete to match the public sidewalk (see Section 3.6.1).
 - b. Where adjacent to ground-floor residential, Setbacks shall be at-grade or raised landscape planters enclosed by a wall no more than 18 inches in height, measured from the adjacent sidewalk grade. Planters should be planted with decorative plants which may include small trees and low shrubs. Walks to building entries shall be paved.
2. Landscaping.
 - a. Where adjacent to retail or Shopfronts, small shrubs and trees in movable pots are permitted in the paved Setback areas fronting retail spaces. Landscaped planters or Yards are not permitted in those areas.
 - b. Where adjacent to ground-floor residential, raised landscape planters, walls, and/or landscaped slopes to accommodate site grading/conform conditions may

be appropriate at some residential ground floors.

3. Movable Signs and outdoor merchandise displays in conformance with Sign Standards and Guidelines of Section 4.8 are permitted. All such signs and displays shall be approved by the City, as part of the landscape plan submittal. Outside of business hours, signs and displays shall be removed from the Setback and stored indoors or stacked and stored at the back of the Setback area.

G. Outdoor Dining

Outdoor dining is permitted within the front Setback adjacent to the operating ground-floor use. Outdoor dining areas may encroach into the public right of way and along a paved parkway provided that a minimum five foot pedestrian path is accessible. They shall be enclosed by removable barriers when barriers are required by State licensing regulations. Outside of business hours furniture shall be stored indoors or may be stacked and secured at the back of the setback area.

H. On-Street Parking

Diagonal parking along the northern side of the Street shall be provided. Parallel parking shall be provided along the southern side of the Street.

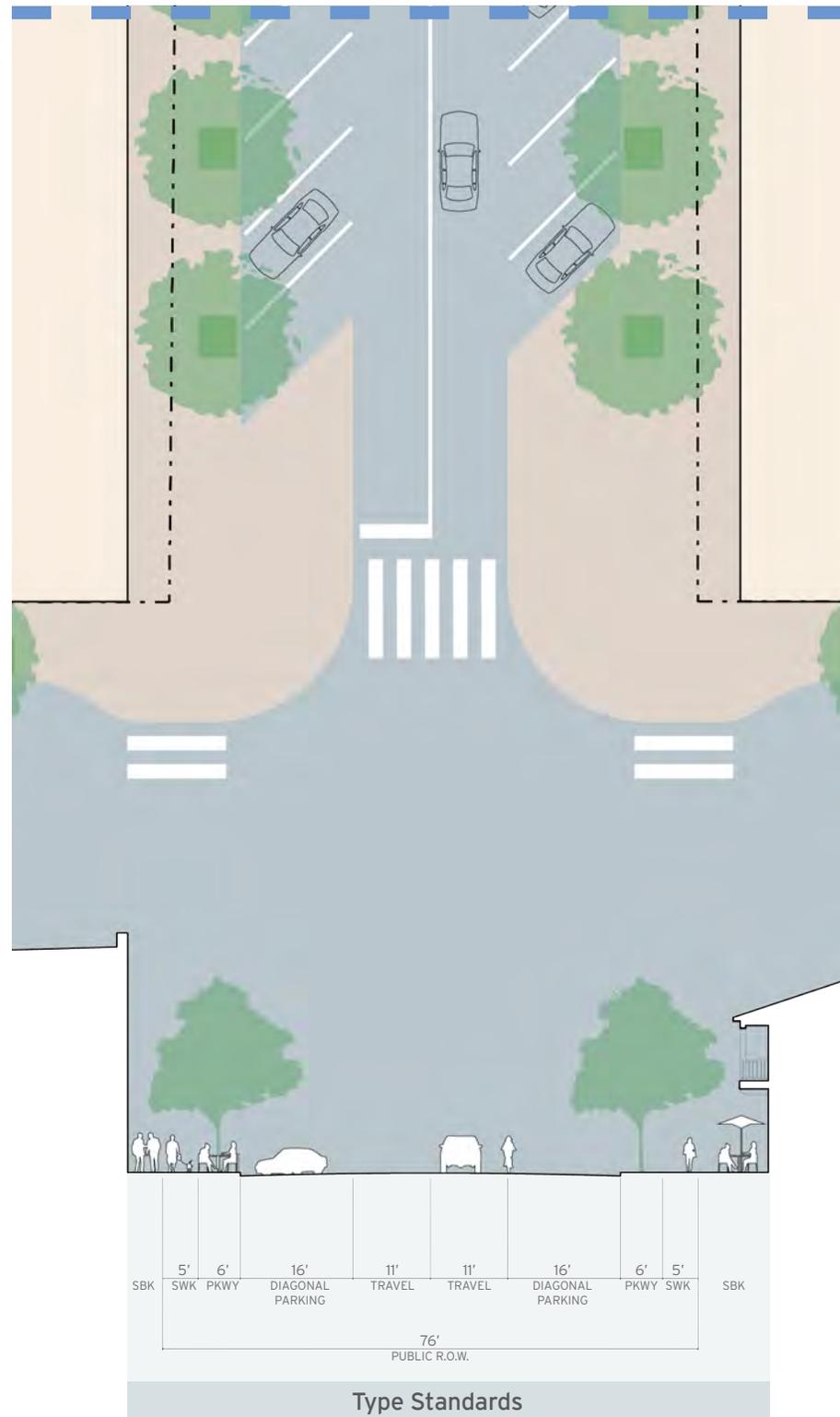


3.3.10 Main Street - 3 B

A. Design Intent

Forming the heart of the Commercial Center Sub-Area, this east-west Street connects General Jim Moore Boulevard at its western terminus to the Campus Town square and to the eastern portion of the Specific Plan Area. It is designed for a mix of uses overall with ground-floor retail. Vehicular traffic will move slowly and will share the Roadway with bicycles. Generous sidewalks are provided on both sides. It has one-travel lane in each direction, and curb-side diagonal parking on both sides of the Street.

Figure 3.12 - Main Street - 3B Section
Conceptual Diagram (right)
Location Legend (below)



E.T.R. Existing to Remain
PKWY Parkway
R.O.W. Right of Way
SBK Setback
SHDR Shoulder
SWK Sidewalk



B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1), or decorative paving, with integral color.

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.24).
2. Parkways. Trees shall be planted in pits with 5' x 5' minimum tree grates, separated from the face of curb by a one-foot step out strip.
3. Curb Extensions. Landscape planters shall extend into curb extensions and separate sidewalk amenity zones from the Roadway (see Section 3.5.2).

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required. Curb extensions should be furnished with pedestrian or bicycle amenities or both.

F. Front Setbacks

1. Paving. Setbacks shall be at-grade and paved with poured, scored concrete to match the public sidewalk (see Section 3.6.1).
2. Landscaping. Small shrubs and trees in movable pots are permitted in the paved Setback areas fronting retail spaces. Landscaped planters or Yards are not permitted in those areas.
3. Movable Signs and outdoor merchandise displays in conformance with Sign Standards and Guidelines of Section 4.8 are permitted. All such signs and displays shall be approved by the City, as part of the landscape plan submittal. Outside of business hours, signs and displays shall be removed from the setback and stored indoors or stacked and stored at the back of the Setback area.

G. Outdoor Dining

Outdoor dining is permitted within the front Setback adjacent to the operating ground-floor use. Outdoor dining areas may encroach into the public right of way and along a paved parkway provided that a minimum five foot pedestrian path is accessible. They shall be enclosed by removable barriers when barriers are required by State licensing regulations. Outside of business hours furniture should be stored

indoors or may be stacked and secured at the back of the Setback area.

H. On-Street Parking

Diagonal parking shall be provided along both sides of the Street.



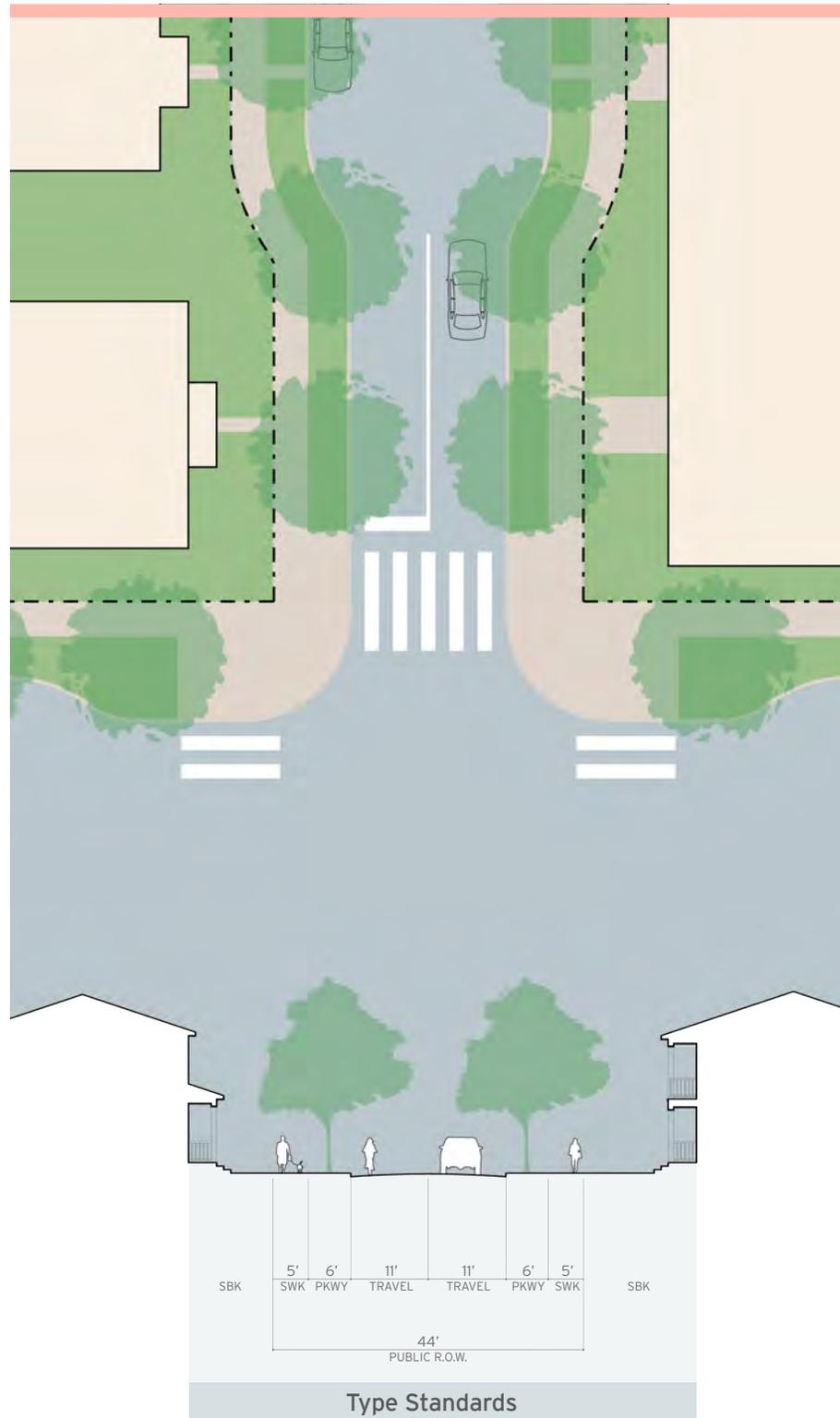
Paved Parkway with Tree Grates (Illustrative)

3.3.11 Local Street Entry

A. Design Intent

These local street entries are small segments of Colonel Durham Street, where it connects to 1st Avenue in the western portion of the Specific Plan, and across General Jim Moore Boulevard. They serve pedestrians, bicycles, and slow-moving vehicles. They have one travel lane in each direction with no curbside parking. Sidewalks provide ample room for pedestrians. Streets are landscaped with street trees and continuous parkways with paved pass-throughs to the sidewalk. Setback standards allow buildings to be set at least ten feet from the Lot Line. The setback areas are landscaped per the building Frontage Type Standards.

Figure 3.13 - Local Street Entry Section
Conceptual Diagram (right)
Location Legend (below)



E.T.R. Existing to Remain
PKWY Parkway
R.O.W. Right of Way
SBK Setback
SHDR Shoulder
SWK Sidewalk

B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1). Walks in parkways shall be paved with permeable pavers.

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkway. Parkway shall be six feet wide continuous planters and flush with the finished sidewalk. Where feasible, parkways shall be designed as infiltration planters with appropriate plant material.

Where infiltration planters are not feasible, parkways shall be landscaped with irrigated turf or native ground covers. Paved walks shall provide access from the sidewalk to the curb and placed at regular intervals not to exceed 50 feet.

3. Curb Extensions. Landscape planters shall extend into curb extensions and separate sidewalk amenity zones from the Roadway (see Section 3.5.2).

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required.

F. Front Setbacks

Front setbacks shall be landscaped in conformance with the Building Frontage Type Standards (see Section 4.5.3).

G. Outdoor Dining

Not permitted.

H. On-Street Parking

Not applicable.



Infiltration Planter Parkway (Illustrative)



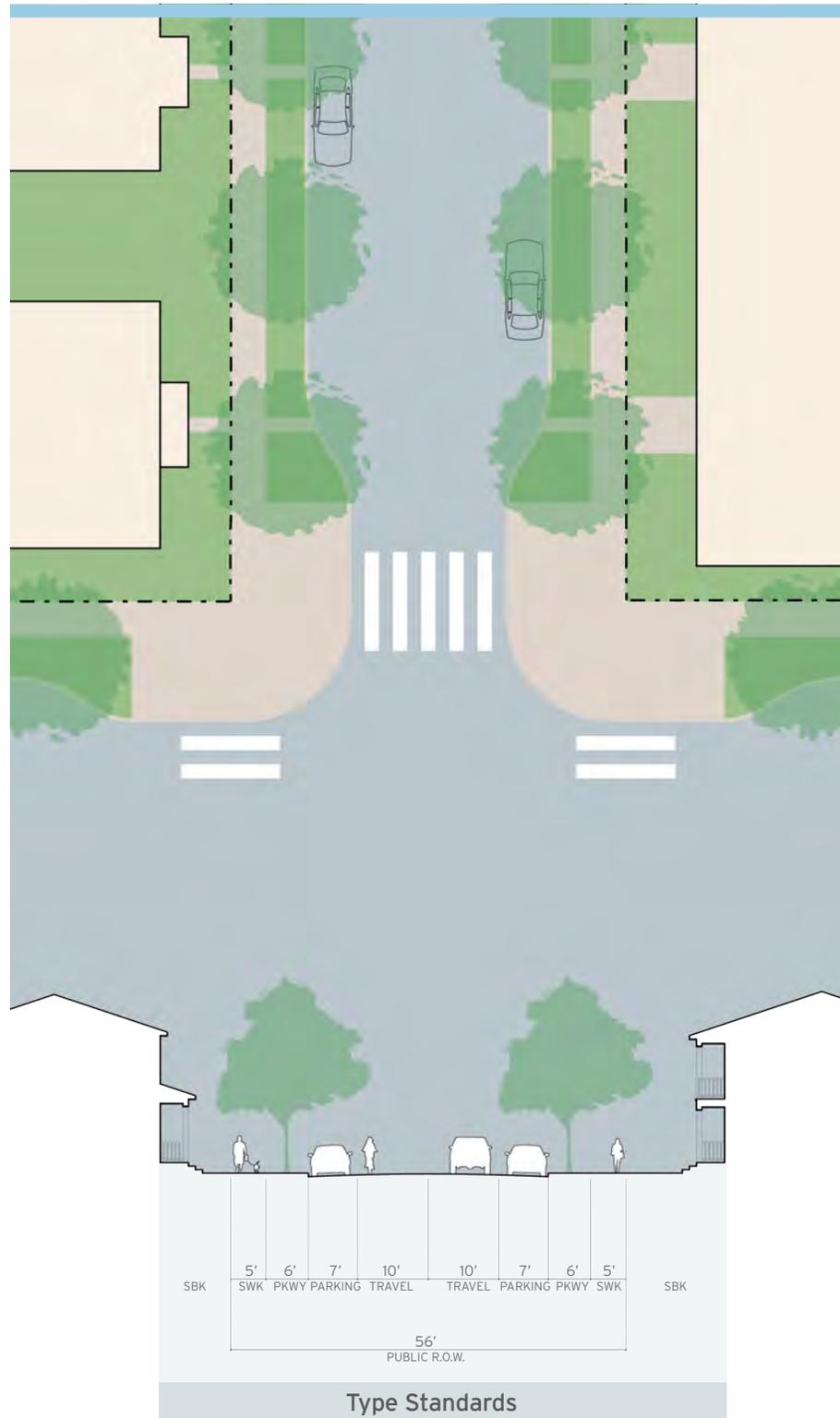
Landscaped Curb Extension (Illustrative)

3.3.12 Local Street - 1

A. Design Intent

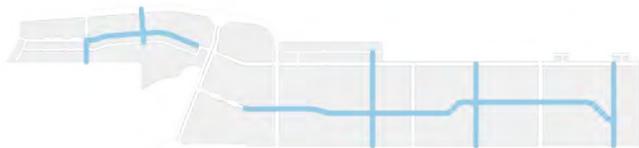
These local Streets are located throughout the Campus Town Specific Plan and include the western extension of Colonel Durham Street, as well as the internal east/west Street that traverses the eastern portion of the Central Avenue. They serve pedestrians, bicycles, and slow-moving vehicles. The Roadways are designed for slow traffic speeds with shared use traffic lanes that accommodate bicycles with one travel lane in each direction and two curbside parking lanes. Sidewalks provide ample room for pedestrians. Streets are landscaped with street trees and continuous parkways with paved pass-throughs to the sidewalk. Setback standards allow buildings between five and fifteen feet from the Lot Line for a livelier street Frontage. The Setback areas are paved or landscaped per the building Frontage Type Standards.

Figure 3.14 - Local Street - 1 Section
 Conceptual Diagram (right)
 Location Legend (below)



E.T.R. Existing to Remain
 PKWY Parkway
 R.O.W. Right of Way
 SBK Setback
 SHDR Shoulder
 SWK Sidewalk

Type Standards



B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1).

C. Landscape

1. Street Trees. Street trees should be planted in conformance with the Conceptual Street Tree Plan (see Figure 3.25).
2. Parkway. Parkway shall be six feet wide continuous planters and flush with the finished sidewalk. Where feasible, parkways shall be designed as infiltration planters with appropriate plant material. An 18 inch wide hardscaped step-out area shall be provided with infiltration planters to allow access from parked vehicles to the sidewalk.

Where infiltration planters are not feasible, parkways shall be landscaped with irrigated turf or native ground covers. Paved walks shall provide access from the sidewalk to the curb and placed at regular intervals not to exceed 50 feet.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required.

F. Front Setbacks

Front setbacks shall be landscaped in conformance with the Building Frontage Type Standards (see Section 4.6.3).

G. Outdoor Dining

Not permitted.

H. On-Street Parking

Parallel parking shall be provided along both sides of the street.



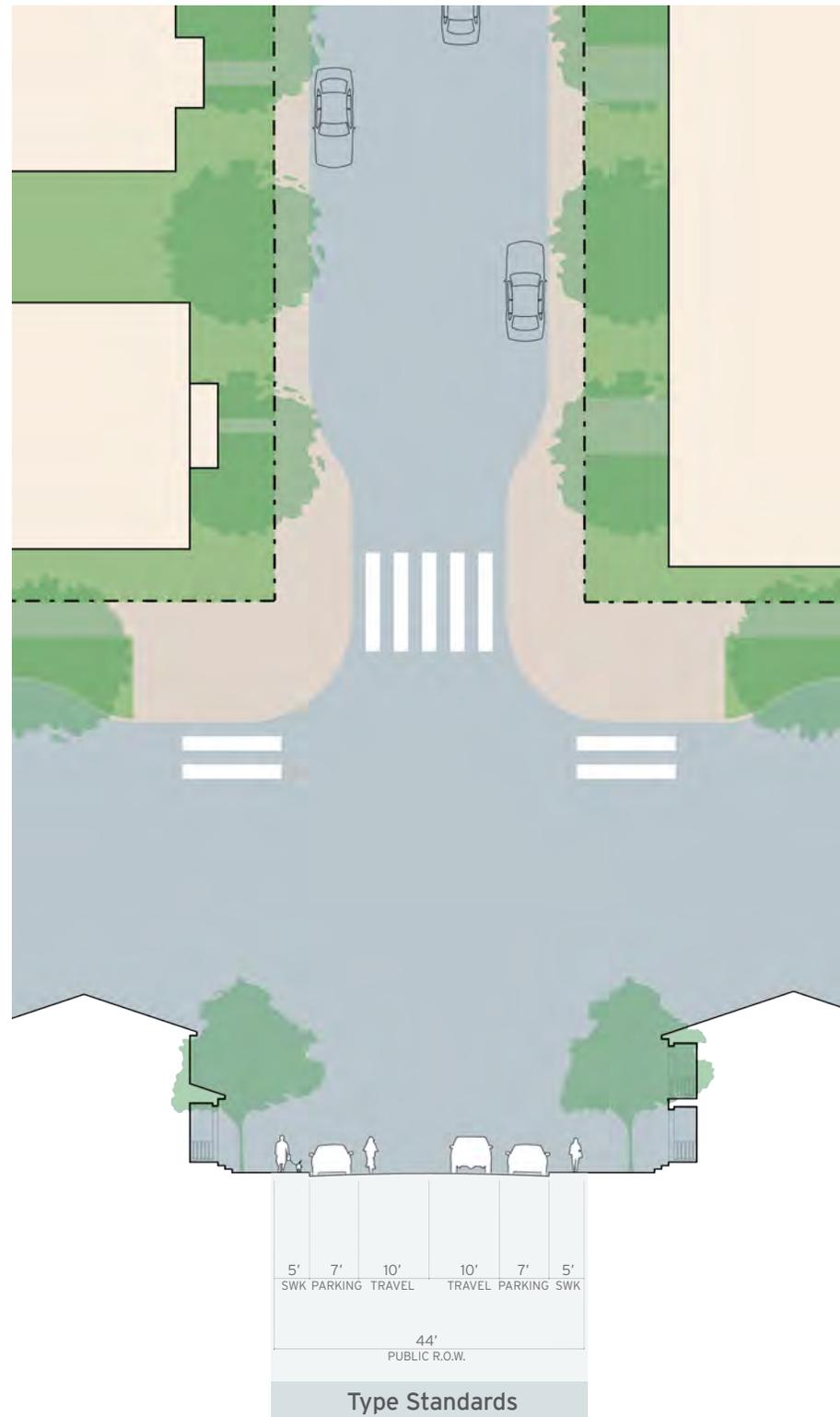
Native Groundcover Parkway (Illustrative)

3.3.13 Local Street - 2

A. Design Intent

This local Thoroughfare type is not designated in the Specific Plan but may be used as an interstitial street. The Roadways are designed for slow traffic speeds with shared use traffic lanes that accommodate bicycles. They have one travel lane in each direction and curbside parking lanes on both sides. Sidewalks provide room for pedestrians. There are no Setback requirements along these streets, except as required per the Building Type or Building Frontage Type Standards.

Figure 3.15 - Local Street - 2 Section Conceptual Diagram



E.T.R. Existing to Remain
 PKWY Parkway
 R.O.W. Right of Way
 SHDR Shoulder
 SWK Sidewalk



B. Sidewalk Paving

The sidewalk shall be paved with poured, scored concrete (see Section 3.6.1).

C. Landscape

Appropriate trees should be landscaped in Setbacks to compensate for the lack of street trees. See Section 3.5.1 Street Trees for a selection of appropriate trees.

D. Streetlights

See Section 3.6.3.

E. Street Furniture

Permitted, not required.

F. Front Setbacks

Front Setbacks shall be landscaped in conformance with the Building Frontage Type Standards (see Section 4.5.3).

G. Outdoor Dining

Not permitted.

H. On-Street Parking

Parallel parking shall be provided along both sides of the street.

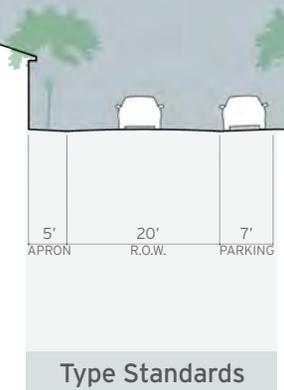
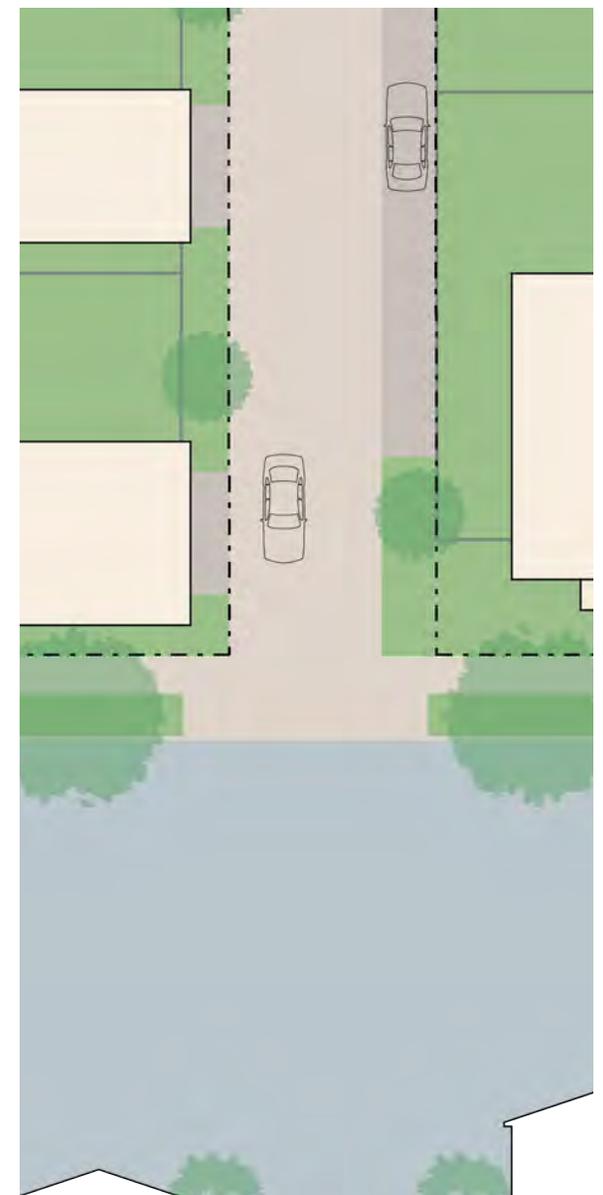
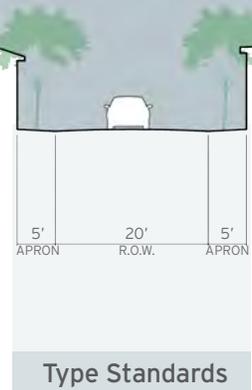
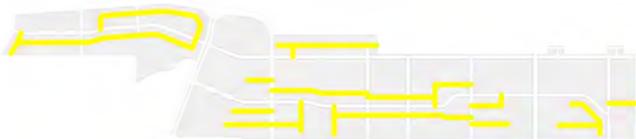


3.3.14 Alleys - 1 and 2

A. Design Intent

Alleys provide access to the interior of Blocks in order to allow for access to garages and surface parking, as well as to provide for service vehicles. Alley widths provide a two-way drive lane for very slow moving traffic mixing with pedestrians and bicyclists. Alley 2 provides for parallel parking on one side, but it is not designated in the Specific Plan. Alleys shall be of durable paving materials suitable for trash and fire access. Where an Alley is Designated, it must conform to the Standards of Alley 1, but may shift or move in accordance with the proposed development on the Lots it would be serving, upon approval during the Substantial Conformance Review process provided in Chapter 6, Implementation.

Figure 3.16 - Alleys 1 & 2 Section
Conceptual Diagrams (right)
Location Legend for Alley Type 1 (below)



E.T.R. Existing to Remain
PKWY Parkway
R.O.W. Right of Way
SHDR Shoulder
SWK Sidewalk



B. Setback Paving

Driveway aprons shall be paved with poured concrete. Other areas between the apron may be landscaped or paved with porous paving materials.

C. Landscape

Not applicable.

D. Streetlights

Not required.

E. Street Furniture

Not permitted.

F. Front Setbacks

Not applicable.

G. Outdoor Dining

Not permitted.

H. On-Street Parking

Dependent on the alley type, parking may be allowed on one side, but not both.



Poured Concrete Driveway Apron at Alley (Illustrative)



Landscaped Buffer at Alley (Illustrative)

3.4 Open Space Network and Type Standards and Guidelines

3.4.1 Open Space Network

The public Open Space network is a defining feature of the Specific Plan Area. A series of Open Spaces link the central east-west street, forming a green network that unites the Specific Plan Area and the community that will evolve within. The Open Space Plan emphasizes access. Neighborhood parks shall be located near residential neighborhoods that in turn connect to mixed-use village centers and CSUMB.

Linear parks along Gigling Road also shall provide a green link that runs from General Jim Moore Boulevard to 7th Avenue, connecting to a variety of Open Spaces along the way through Common Walkways. Buildings should face Open Spaces to encourage safety and activation of the Parks. The Public Open Space Plan, right, depicts the type and location required in the Plan area, features in grey scale are Conceptual.



The location and general configuration of the Designated Public Open Spaces depicted in Figure 3.17 are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.

The proposed Open Space Network includes more than 9 acres for public recreation and is easily accessible to the Seaside community at large.



Figure 3.17 - Open Space Types Plan



3.4.2 Open Space Types

The system of Open Spaces is categorized into seven types, ranging from verdant recreationally-activated Parks to hardscaped civic Plazas capable of hosting community events such as farmers markets and seasonal fairs. Open Spaces may contain a variety of programs, including Playground areas for children, green expanses for Sports Fields, and linear Park connections for passive strolling. Fundamentally, the Open Space system is designed to provide a high level of connectivity throughout the neighborhood and a family of spaces offering a variety of experiences.

Open Spaces shall be defined (bordered) by the Fronts of buildings on at least three sides, except as noted in the type descriptions (right). Garage doors shall not be part of the Frontages that define Open Spaces, except as noted. There may be intervening streets between the Open Spaces and the fronts of buildings bordering the Open Space.

All lighting within the public Open Space network shall be high-efficiency LED lighting or other comparable high-efficiency lighting technology. The public Open Space network shall be equipped with recycling receptacles in addition to waste receptacles. Composting receptacles shall be installed at such time composting service becomes available.

	Type	Description
	Park	An Open Space available for unstructured recreation including hiking and bike trails. A Park must include building Frontages on at least one side. Parks specifically created as “tree saves” (see Section 3.4.2.1.A) are exempt from the minimum building Frontage requirement. The landscape should consist of natural vegetation and trees that should extend the natural habitat for native species. Linear Parks are included in this type.
	Green	An unstructured recreational area with a natural or structured landscape character. The landscape should consist of paths, water bodies, open areas defined by informal tree plantings, and open shelters all naturally disposed.
	Sports Field	An Open Space designed for active recreation. A Sports Field should be large enough to accommodate team sports, such as soccer or softball, but not specifically designed for any single sport such that it precludes its use for other sports or activities. Sports Fields shall include public restroom facilities at the edge or within 500 feet of the Sports Field area.
	Square	An Open Space available for unstructured recreation and civic purposes. A Square is spatially defined by Fronts of Buildings. Its landscape should consist of paths, lawns, and trees with more hardscape elements compared to that of the Green. Squares shall be located at the intersection of important streets.
	Plaza	An Open Space available to civic purposes and commercial activities. A Plaza shall be spatially defined by Fronts of Buildings. It should consist primarily of hardscape with consistent pavement palettes and strategically placed trees and plantings. Plazas shall be located at the intersection of important streets.
	Pocket Park	An Open Space for unstructured recreational use and informal gathering or relaxation. Pocket Parks should be interspersed within each neighborhood to serve as moments of respite with shaded seating along the public space network. Pocket Parks may be equipped with Playgrounds.
	Playground	An Open Space designed and equipped for the recreation of children. A Playground shall be fenced and may include open shelter. Playgrounds should be interspersed within residential areas and may be included within Greens, Parks, and Pocket Parks. The majority of public Open Spaces within the Plan Area could accommodate a Playground.

Table 3.1 - Open Space Types





3.4.2.1.A Park - *Tree-Save*

An existing tree grove to the west of General Jim Moore Blvd will be conserved to protect the natural landscape and local ecosystem. Limited interventions such as walking paths and minimal hardscape will ensure that the Park is publicly accessible for recreation without adversely impacting native wildlife. Necessary storm-water management resources located within the Park will be appropriately designed to maintain public access and recreation.



Figure 3.18 - Tree-Save Park Conceptual Plan (right)
Location Legend (below)





3.4.2.1.B Park - *Linear Park*

A linear Park will run parallel to Central Street to the north, making use of a utility easement that runs along the northern edge of Gigling Road from General Jim Moore Boulevard in the west to 7th Avenue to the east. The string of Parks provides a green alternative to accessing different areas within the Specific Plan Area by linking to a variety public Open Spaces and serves as a link to the existing neighborhoods south of Gigling Road. The long stretch of paths provide an ideal opportunity for a series of public art installations by local artists.

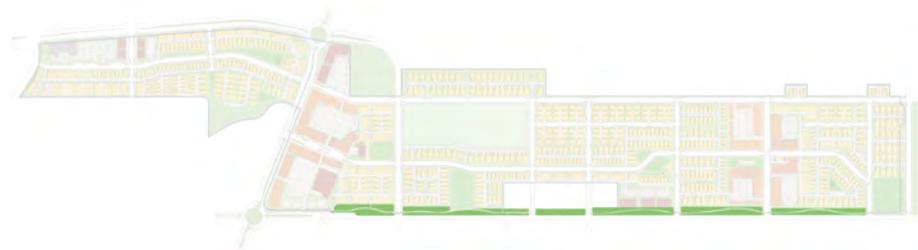
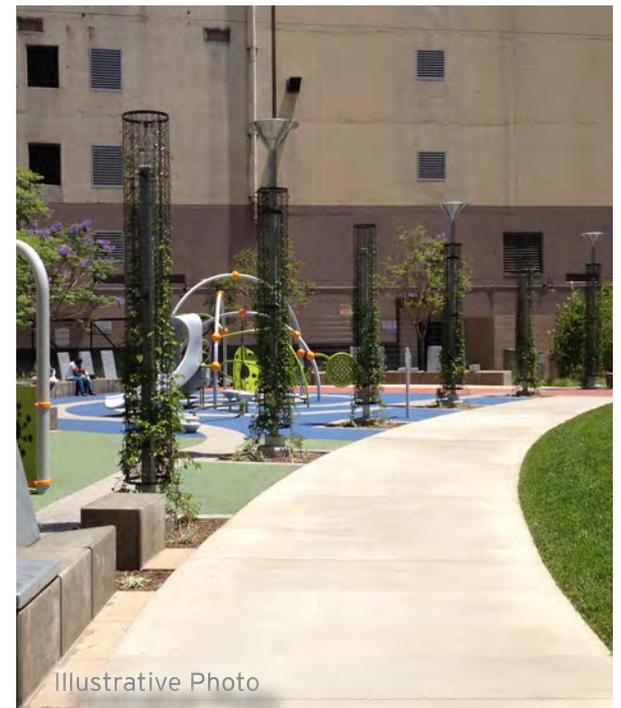


Figure 3.19 - Linear Park Conceptual Plan (below)
Location Legend (above)



Illustrative Photo



Illustrative Photo



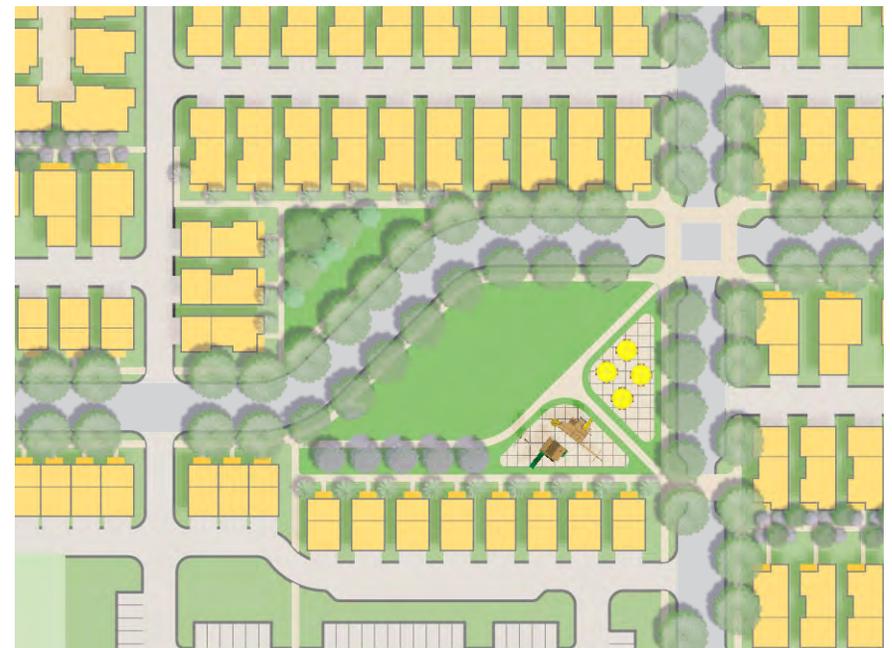
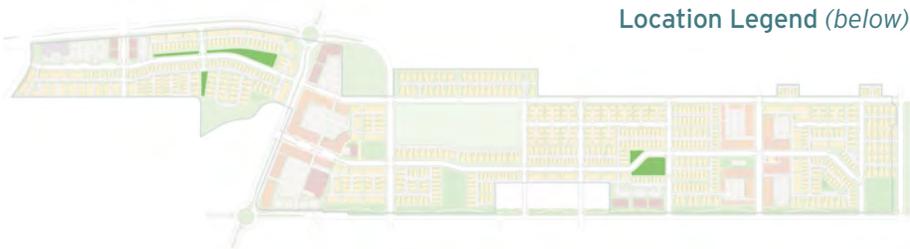


3.4.2.2 Green

Greens function as neighborhood Parks that serve local residents with passive and/or active recreation and should be located within residential areas. Play structures, picnic tables, and open lawns for unstructured play promote active lifestyles and foster community interaction. The majority of public Open Spaces within the Plan area can accommodate a Playground to provide recreational opportunities for children across the Campus Town area.



Figure 3.20 - Green Conceptual Plan (right)
Location Legend (below)





3.4.2.3 Sports Field

In conjunction with neighborhood Playgrounds, Sports Fields encourage healthy living by providing open areas for active recreation. Flexible spaces accommodate different uses, such as informal team sports like soccer and softball and leisure activities like picnicking. Located at the terminus of Central Street, the spine of Campus Town, and along the string of Parks on Gigling Road, Sports Fields are highly accessible to the entire Campus Town community. The Sports Field at Gigling Road and 7th Avenue shall also include a distinctive gateway element to the National Monument and public restroom facilities at the edge or within 500 ft of the Sports Field area.



Illustrative Photo

Figure 3.21 - Sports Field Conceptual Plan (right)
Location Legend (below)



Illustrative Photo



Illustrative Photo



3.4.2.4 Square

Squares at the intersection of important Streets will provide civic gathering spaces for community events. Enclosed by Building Frontages on all sides, Squares become neighborhood outdoor living rooms that are easily accessible to the general public. A mix of landscape and hardscapes allows for a diversity of uses and users ensuring a vibrant Open Space. Squares can also accommodate free-standing cafes or restaurants to anchor and activate the Public Realm.



Figure 3.22 - Square Conceptual Plan (right)
Location Legend (below)





3.4.2.5 Plaza

Directly south of one of the main gateways to CSUMB, the heart of the University Village Sub-Area at Central Street and 6th Avenue is a Plaza bounded by mixed-use buildings. Activated by ground-floor uses such as retail and commercial spaces, the Plaza is a mostly hardscaped area that fosters community interaction between University students, staff, and faculty and the rest of the Seaside population.



Figure 3.23 - Plaza Conceptual Plan (right)
Location Legend (below)





3.4.2.6 Pocket Park

Interspersed throughout the Specific Plan Area, Pocket Parks enhance the pedestrian experience by providing important moments of repose from the bustling built environment with an infusion of natural landscaping and scenic settings. These Open Spaces can easily be incorporated into the urban fabric because their size and shape conform to their surroundings. Playgrounds, community gardens, and other amenities can also be placed in Pocket Parks because their inherent versatility enables them to be deployed in a variety of settings and functions.



Illustrative Photo



Illustrative Photo

Figure 3.24 - Pocket Park Conceptual Plan (right)



Illustrative Photo



3.5 Landscape Standards and Guidelines

Given the unique ecosystem in Monterey Bay, the careful selection of landscaping in the Campus Town Specific Plan Area is linked to the vitality and sustainability of the local natural environment. The appropriate incorporation of street trees and vegetation detailed below will ensure the mutual health of the natural and built environments.

Existing Coast Live Oak trees that have a height of ten feet or more, or a circumference of twenty inches or more measured twenty-four inches above the ground that are removed as part of construction shall be replaced at a ratio of 1:1.5 within the Specific Plan Area. The size of each replacement tree shall be a 15-gallon or larger specimen, measuring one inch or more in diameter at a point of twelve inches above the base, and not less than seven feet in height, measured from the base.





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3.5.1 Street Trees

Throughout the Specific Plan Area, street trees will enhance the streetscapes. They provide highly visible green in the public realm, typically separating the sidewalk from parking and drive lanes. In summer, trees provide shade, reduce the heat island effect, and aid in minimizing stormwater flows through infiltration planters and Swales. Generally, street trees are selected for several features including higher canopies to provide visibility at the street level, ornamental or seasonal aesthetic value, shade and density, and climate suitability. Street trees are located in the parkway, the area between the Common Walkway/sidewalk and the curb.

Vital to the overall health of the natural landscape, species variation protect against the spread of tree diseases and enhance native habitat for local wildlife while providing visual interest.

Along General Jim Moore Boulevard and Lightfighter Drive, larger street trees are appropriate due to the wider street sections and center medians. They create large canopies that provide shade and have the requisite planting space to thrive. Appropriate street tree planting methods and materials should be incorporated to ensure space for maximum root growth.

Along major Local Streets, like Central Street and 6th Avenue, the tree species will underline the streets' significance within the hierarchy of the street network. Furthermore, flowering-accent planter trees should be located at street intersections and other important locations. Accent trees should be located in landscape planters situated in curb extensions at street intersections. Here, curb extensions provide additional space that can help buffer and protect the smaller accent trees from passing vehicular traffic while enriching the streetscape.

Along minor local streets, a variety of climate-appropriate trees should be provided. Minor street trees have been selected for their drought tolerance, growth rate and low maintenance.

Palettes presented in this section provide an overall design intent and may be added to or modified based on City direction during the Substantial Conformance process per Chapter 6, Implementation.

Common Name	Scientific Name	Appropriate Street Type		
		Boulevard	Major	Minor
Street Trees				
Australian Willow	<i>Geijera parviflora</i>		✓	✓
Catalina Ironwood	<i>Lyonothamnus floribunda</i>	✓	✓	
Coast Live Oak	<i>Quercus agrifolia</i>	✓		
Fern Pine	<i>Afrocarpus gracilior</i>		✓	✓
Flax Leaf Paperbark	<i>Melaleuca linariifolia</i>		✓	
Monterey Cypress	<i>Cupressus macrocarpa</i>	✓		
New Zealand Christmas Tree	<i>Metrosideros excelsa</i>	✓	✓	
Paperbark	<i>Melaleuca quinquenervia</i>	✓		✓

Table 3.2 - Street Tree Palette



“Establish a pattern of landscaping of major and minor streets [to] enhance the visual quality and environmental comfort within the community.”

Base Reuse Plan

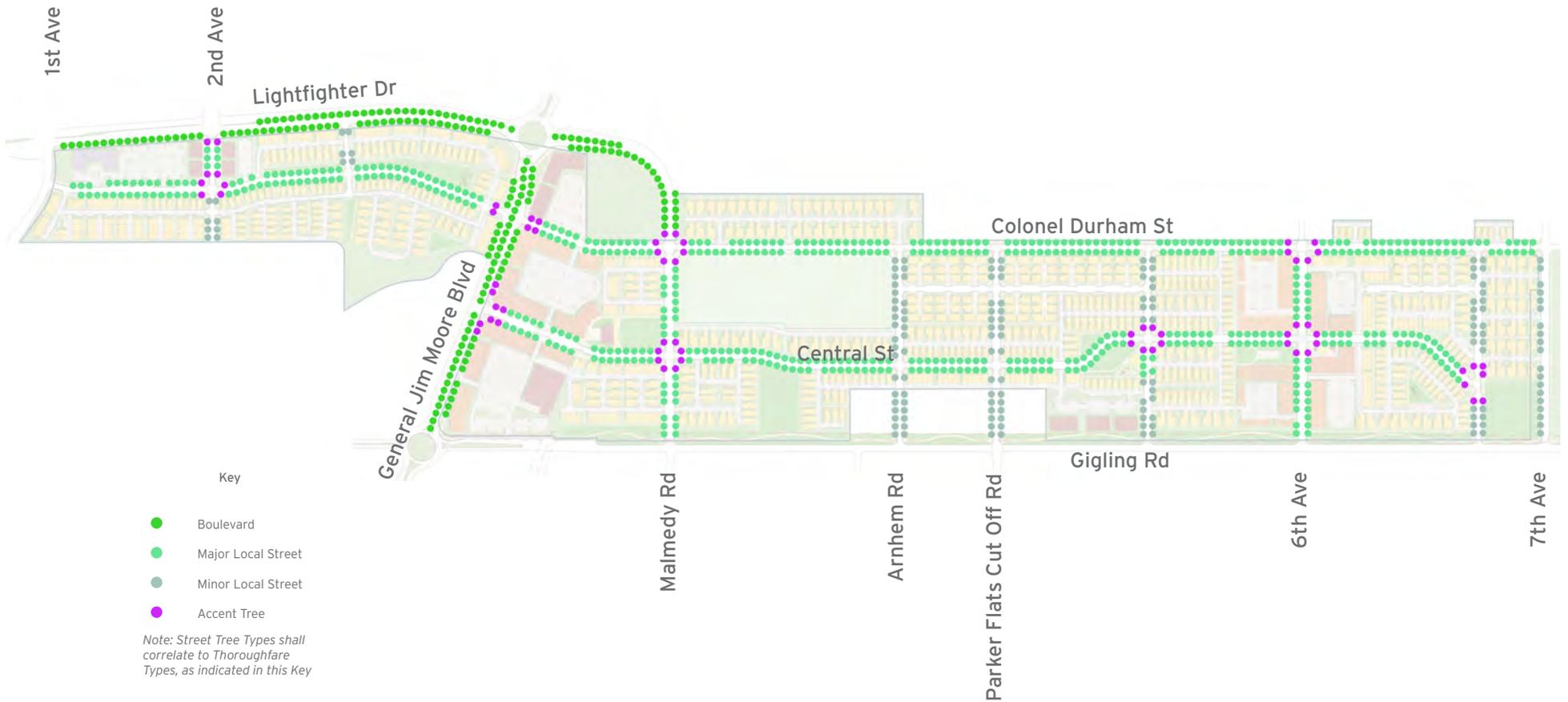


Figure 3.25 - Conceptual Street Tree Plan

The location and general configuration of the Designated Street Tree Types depicted in Figure 3.25 are fixed. All other elements shown on this Figure are Conceptual and subject to change consistent with the Standards in this Specific Plan.





Illustrative Photo

Australian Willow
Geijera parviflora



Illustrative Photo

Catalina Ironwood
Lyonothamnus floribunda



Illustrative Photo

Coast Live Oak
Quercus agrifolia



Illustrative Photo

Fern Pine
Afrocarpus gracilior



Illustrative Photo

Flax Leaf Paperbark
Melaleuca linariifolia



Illustrative Photo

Monterey Cypress
Cupressus macrocarpa



Illustrative Photo

New Zealand Christmas Tree
Metrosideros excelsa



Illustrative Photo

Paperbark
Melaleuca quinquenervia

3.5.2 Landscaping

Like the street trees, the type of planting and parkway are determined by its location and function. At Central Street, for example, paved parkways should be used to extend the usable area for ground-floor commercial activity by providing more space for pedestrians, café seating, bicycle parking, etc, without forgoing street trees. In typical residential Streets, traditional parkways with native grasses and shrubs should enhance the landscape character of the Monterey Bay region environment.

Palettes presented in this section provide an overall design intent and may be added to or modified based on City direction during the Substantial Conformance process per Chapter 6, Implementation. Plants listed in the Landscape Palettes, Tables 3.3 and 3.4, may be planted throughout the Specific Plan Area.



Illustrative Photo

Paved Parkway

Appropriate in heavily trafficked, mixed-use areas, Paved Parkways are mostly hardscaped to extend the pedestrian zone, while providing street trees in 5 ft x 5 ft minimum grated wells.



Illustrative Photo

Infiltration Planter Parkway

Appropriate in moderately trafficked areas, Infiltration Planters allow storm run-off to permeate into the ground, replenishing the water table. Planted with street trees and appropriate vegetation, Infiltration Planters are efficient and add greenery.



Illustrative Photo

Landscaped Parkway

Appropriate in primarily residential areas, Landscaped Parkways are planted with native and climate-appropriate vegetation.

Common Name	Scientific Name	WUCOLS Rating*	California Native	Flowering/Accent
Planter Trees/ Tree-Shrubs				
California Coffeeberry	<i>Rhamnus californica</i>	L	✓	
California Flannel Bush	<i>Fremontodendron californicum</i>	VL	✓	✓
Coast Live Oak	<i>Quercus agrifolia</i>	VL	✓	
Lemonade Berry	<i>Rhus integrifolia</i>	L	✓	✓
Toyon	<i>Heteromeles arbutifolia</i>	L	✓	✓
Planter Shrubs				
Allen Chickering Sage	<i>Salvia 'Allen Chickering'</i>	L		✓
Black Sage	<i>Salvia mellifera</i>	L	✓	✓
Blue Blossom Ceanothus	<i>Ceanothus thyrsiflorus</i>	VL	✓	✓
Bush Lupine	<i>Lupinus arboreus</i>	L	✓	✓
Carmel Ceanothus, Yankee Point	<i>Ceanothus thyrsiflorus ssp. griseus</i>	L	✓	✓
Chamisso Bush Lupine	<i>Lupinus chamissonis</i>	L	✓	✓
Chaparral Currant	<i>Ribes malvaceum</i>	VL	✓	✓
Coast Buckwheat	<i>Eriogonum latifolium</i>	L	✓	✓
Elderberry	<i>Sambucus mexicana</i>	L	✓	✓
Evergreen Huckleberry	<i>Vaccinium ovatum</i>	M	✓	✓
Fortnight Lily	<i>Diets grandiflora</i>	L		✓
Little Sandmat Manzanita	<i>Arctostaphylos pumila</i>	VL	✓	✓
Sea Mallow	<i>Lavatera maritima</i>	L	✓	✓
Silk Tassel	<i>Garrya elliptica</i>	L	✓	✓
Upright Coyote Bush	<i>Baccharis pilularis</i>	L	✓	✓

* WUCOLS (Water Use Classification of Landscape Species) Rating: VL= very low water; L= low water; M= moderate water

Table 3.3 - Landscape Palette - Trees and Shrubs



The California flannel bush is a vibrant native species that requires little water to thrive.



Although not a native species, the Fortnight Lily is perfectly adapted to the Monterey Bay climate and can add visual interest to any streetscape.

Common Name	Scientific Name	WUCOLS Rating*	California Native	Flowering/Accent
Groundcovers				
Carmel Creeper	<i>Ceanothus griseus var. horizontalis</i>	L	✓	✓
Coastal Strawberry	<i>Fragaria chiloensis</i>	VL	✓	✓
Common Yarrow	<i>Achillea millefolium</i>	L	✓	✓
Dwarf Coyote Brush, Pigeon Point	<i>Baccharis pilularis</i>	L	✓	
Hummingbird Sage	<i>Salvia spathacea</i>	L	✓	✓
Mock Heather	<i>Ericameria ericoides</i>	L	✓	✓
Monterey Manzanita	<i>Arctostaphylos hookeri</i>	L	✓	✓
Pacific Coast Iris	<i>Iris douglasiana</i>	L	✓	✓
Sea Pink	<i>Armeria maritima ssp. californica</i>	M	✓	✓
Seaside Daisy	<i>Erigeron glaucus</i>	L	✓	✓
Sword Fern	<i>Polystichum munitum</i>	L	✓	
Grasses				
Blue Bunchgrass	<i>Festuca idahoensis</i>	VL	✓	
California Gray Rush	<i>Juncus patens</i>	L	✓	
Canyon Wild Eye, Canyon Prince	<i>Elymus condensatus</i>	L	✓	
Creeping Wild Rye	<i>Elymus triticoides</i>	L	✓	
Deergrass	<i>Muhlenbergia rigens</i>	L	✓	
June Grass	<i>Koeleria macrantha</i>	L	✓	

* WUCOLS (Water Use Classification of Landscape Species) Rating: VL= very low water; L= low water; M= moderate water

Table 3.4 - Landscape Palette - Groundcovers and Grasses



While the native Seaside Daisy requires little water, it flowers abundantly year round.



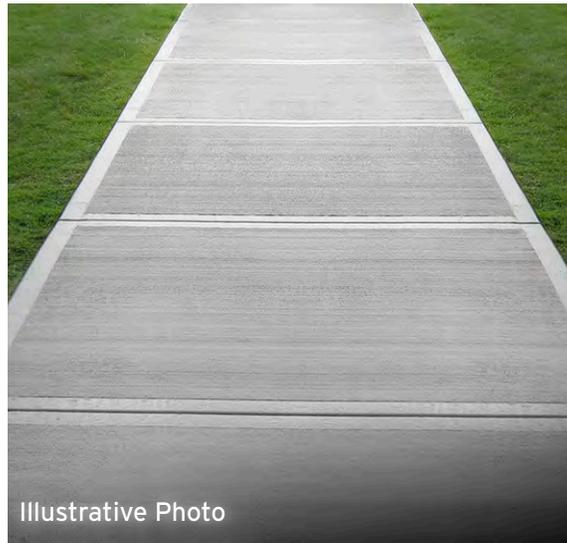
June Grass requires little water and maintenance and hosts natural wildlife like butterflies.

3.6 Streetscape Guidelines

While the Thoroughfare Types lay out the dimensional and functional requirements for Campus Town Streets, this section provides a series of material, street furniture, and palettes that inform the Street design. This section promotes a design unity that supports the Specific Plan Area identity while allowing for options and variety responsive to location-specific needs. Palettes presented in this section provide an overall design intent and may be added to or modified based on City direction during the Substantial Conformance process per Chapter 6, Implementation. The streetscape design elements place a particular emphasis on elements that enhance the pedestrian and cyclist's experience in Campus Town.

3.6.1 Paving Palette

Paving materials for sidewalks and other hardscaped surfaces should be consistent with the intent of this paving palette. Where required by the Thoroughfare Type standards of Section 3.3, paved areas in front Setback should be consistent with this section. Roadway surfaces are not included in this section.



Illustrative Photo

Poured, Scored Concrete



Illustrative Photo

Integral Color Concrete



Illustrative Photo

Permeable Pavers



Illustrative Photo

Decomposed Granite

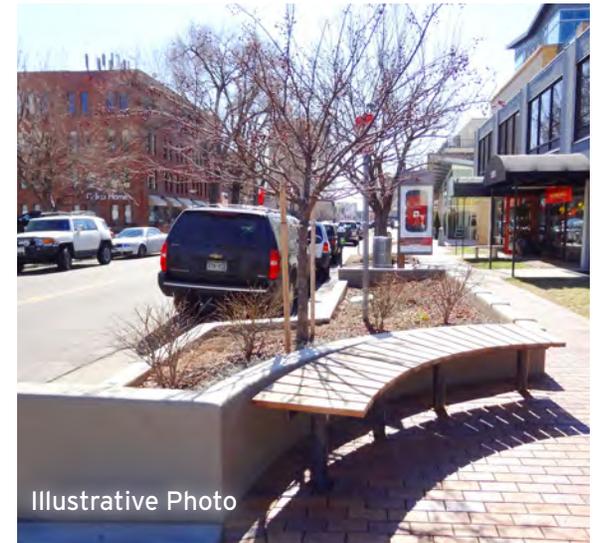
3.6.2 Street Furniture

Street furniture, waste/recycling/composting receptacles, lighting, bike racks, and similar devices, significantly enhance the usability of the Public Realm. A consistent theme of materials and design language in street furniture selections should be used to foster a unifying identity for the Specific Plan Area. The street furniture presented in this section provides an initial palette of appropriate street furniture selections. The selections are based on a clean aesthetic with a high degree of functionality that maintains a respect for the human scale. The City may approve additional items that complement this selection and expand the palette during the Substantial Conformance process provided for in Chapter 6, Implementation.



Illustrative Photo

Waste/Recycling/Composting Receptacles



Illustrative Photo

Bench



Transit Shelter

Wayfinding maps must be provided if shelters incorporate advertisements.



Illustrative Photo

Bike Rack



Illustrative Photo

Bollard

3.6.3 Street Lighting

The street lights presented in Figure 3.26 are illustrative of the appropriate design aesthetic. The City may approve additional or alternate items that complement and expand this selection during the Substantial Conformance process provided for in Chapter 6, Implementation. In accordance with the City's effort to preserve the dark skies in Monterey Bay, street lights shall use full cutoff luminaires. The term full cutoff describes luminaires that have no direct uplight (no light emitted above horizontal) and limiting intensity of light from the luminaire in the region between 80° and 90°. Street lighting levels shall meet City standards.

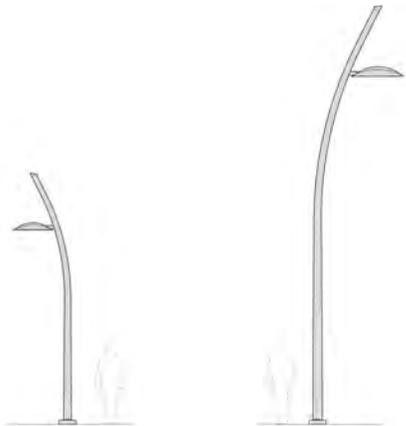


Figure 3.26 - Pedestrian and Roadway Scaled Street Lights (Illustrative)

3.6.4 Public Art

Consideration should be given to the incorporation of public art throughout the Campus Town Specific Plan Area, especially at mixed-use areas and Open Spaces. Partnerships with CSUMB and local arts organizations should be pursued, including installation of art pieces that highlight the creativity and passion of the local artistic community.



A variety of public art types, such as painted sidewalks, murals, and installations should be considered. Specifically, the linear Parks along Gigling Road provide various opportunities for garden sculptures and installations along the paths that stretch from General Jim Moore Boulevard and culminate at the Sports Park at 7th Avenue. Programs of rotating pieces by CSUMB art faculty and students would help create a Campus Town that better integrates the University with the community at large.



Painted sidewalks and garden sculptures animate the public realm and create a sense of place. (Illustrative)



Private Realm Standards and Guidelines

4.1 Purpose and Applicability

The Form-Based Private Realm Standards and Guidelines translate the goals and policies of the Specific Plan into objective and measurable Standards, as well as Guidelines to help clarify the intentions of the Specific Plan. Together, these Standards and Guidelines will ensure that new development in the Private Realm exhibits a high standard of urban design, architecture and landscaping, while addressing the uniqueness of the various conditions within the Campus Town area.

The Sub-Area Plan (Figure 4.1) identifies six sub-areas, all of which are intended as walkable with a diverse mix of Building Types and uses.

Streets and spaces should be appropriately framed by human-scaled buildings with clearly identifiable entries. However, because each Sub-Area is different, the Standards are designed to reinforce the urban context so that all neighboring buildings are harmonious. Consistent with the Specific Plan Goals, the Standards and Guidelines are intended to support the enhancement and expansion of the pedestrian realm by providing connections between Blocks and better integrating a green network.

In order to realize Complete Streets, sidewalk widths should be appropriate to their context and sufficient to support strolling, café seating and other amenities normally found in a pedestrian-oriented environment. Ground floors should be activated by providing shop windows in retail environments, and front doors, Stoops, and porches in residential environments to provide visual interest and an interactive environment for pedestrians. Conflicts between pedestrians and cars should be minimized. Driveways should be relegated to rear Alleys or Auto Courts when present.

Open Spaces should be designed to be:

- easily accessible to all pedestrians and residents;
- programmable;
- compatible with the local climate;
- easily maintained and managed.



Illustrative Pedestrian-Scaled Architecture
Variation in form and color create an enlivened Block Frontage.

In general, the Standards and Guidelines serve the following intentions:

1. Ensure building placement and Frontage along the Street reflects the intended character of each Sub-Areas.
2. Maintain a consistent Street Frontage or “street wall” throughout the Sub-Areas.
3. Utilize building architecture to announce gateways, key intersections, and public spaces.
4. Create architectural variation along a Block Front through diversity of massing, articulation, and architectural detailing.
5. Create a built environment that emphasizes pedestrian scale and variety by activating ground-floor Frontages, using ample Fenestration, Awnings, and frequent building entries.
6. Ensure that Streets and spaces with high volumes of pedestrian traffic are comfortable, protected from the sun, and visually and physically engaging at the ground level.
7. Provide off-street parking in surface lots or garages at the rear of buildings so that parking does not dominate the built environment.
8. Encourage a variety of building and development types within and across the Sub-Areas.



Conceptual Rendering of a Residential Common Walkway
Subject to Change



4.2 Organization of this Chapter

This chapter is organized as follows.

4.3 Regulating Plans

Developments in the Specific Plan Area shall conform to the Land Uses and Urban Standards allowed within the applicable sub-area in which a project is located, and to the front Setback allowed according to a site's location along a specific Street segment identified within the Setback Plan.

- 4.3.1 Sub-Area Plan
- 4.3.2 Setback Plan
- 4.3.3 Parking Placement Plan

4.4 Maximum Allowable Development

The Maximum Allowable Development sets forth the maximum aggregate development allowed in the Specific Plan Area for each land use type.

4.5 Land Use Standards

The Land Use Standards identify the allowable uses, permitted Building Types, permitted Open

Space Types, additional standards and permit requirements for each Sub-Area.

4.6 Urban Standards and Guidelines

The Urban Standards and Guidelines regulate and guide the form of the built environment to create diversity and finely grained development.

- 4.6.1 Large Lot Standards and Guidelines
- 4.6.2 Building Type Standards and Guidelines
- 4.6.3 Frontage Type Standards and Guidelines

4.7 Architectural Standards and Guidelines

Architectural Standards and Guidelines are provided for additional direction for staff in evaluating proposals, and also for architects and developers.

4.8 Signage Standards and Guidelines

This section provides Standards and Guidelines to ensure the successful design of Signs in a pedestrian-oriented environment.

4.3 Regulating Plans

The Regulating Plans consist of the Sub-Area Plan, the Setback Plan, and the Parking Placement Plan. The Sub-Area Plan identifies the applicable land-use and building form standards applicable to each neighborhood, while the Setback Plan identifies the standards for building placement relative the Street segment on which a Building is located. The Parking Placement Plan identifies parking placement relative to the Street segment on which parking is located. Development shall substantially conform to the applicable Standards relative to its position within all three Plans to determine permitted land uses and applicable Urban Standards.

The Sub-areas are envisioned as follows:

Sub-Area WE: West End

Bounded by State Route 1 and the Commercial Center Sub-Area, the West End Sub-Area is characterized by residential uses and a Special District for hospitality close to the Fort Ord Dunes State Park.

Sub-Area CC: Commercial Center

Bounded by the West End and the Central Sub-Areas, the Commercial Center Sub-Area is characterized by multi-use development with that serves the entire Seaside community.

Sub-Area CA: Campus Adjacent

Bounded by CSUMB and the Central Sub-Area, the Campus Adjacent Sub-Area is characterized by residential uses that front the university campus.

Sub-Area CE: Central

Bounded by the Commercial Center and University Village Sub-Areas, the Central Sub-Area is characterized by a variety of residential building types and a Special District for Flex/Space and Makerspace opportunities.

Sub-Area UV: University Village

Bounded the Central and East End Sub-Areas, the University Village Sub-Area is characterized by mixed-use development serving the student, faculty and staff of CSUMB, as well as the city at large.

Sub-Area EE: East End

Bounded by the University Village Sub-Area and undeveloped Monterey County Open Space, the East End is characterized by residential uses and serves as a gateway to the Fort Ord National Monument.



4.3.1 Sub-Area Plan

The Sub-Area Plan divides the Campus Town Specific Plan Area into six Sub-Areas, each with their own land use and unique Urban Standards. Land Use Standards are identified in Section 4.5. Urban Standards are identified in Section 4.6.

Some Sub-Areas include Special Districts which require new development within the respective Sub-Area to comply with site-specific Standards. Permitted land uses and Building Types within a Special District may differ from the rest of the Sub-Area in which it is located.

While uses reserved for the Special District are permitted only in the designated area, other compatible uses permitted within the respective Sub-Area may also be incorporated. All applicable Standards and Guidelines apply within the Special District unless otherwise noted.

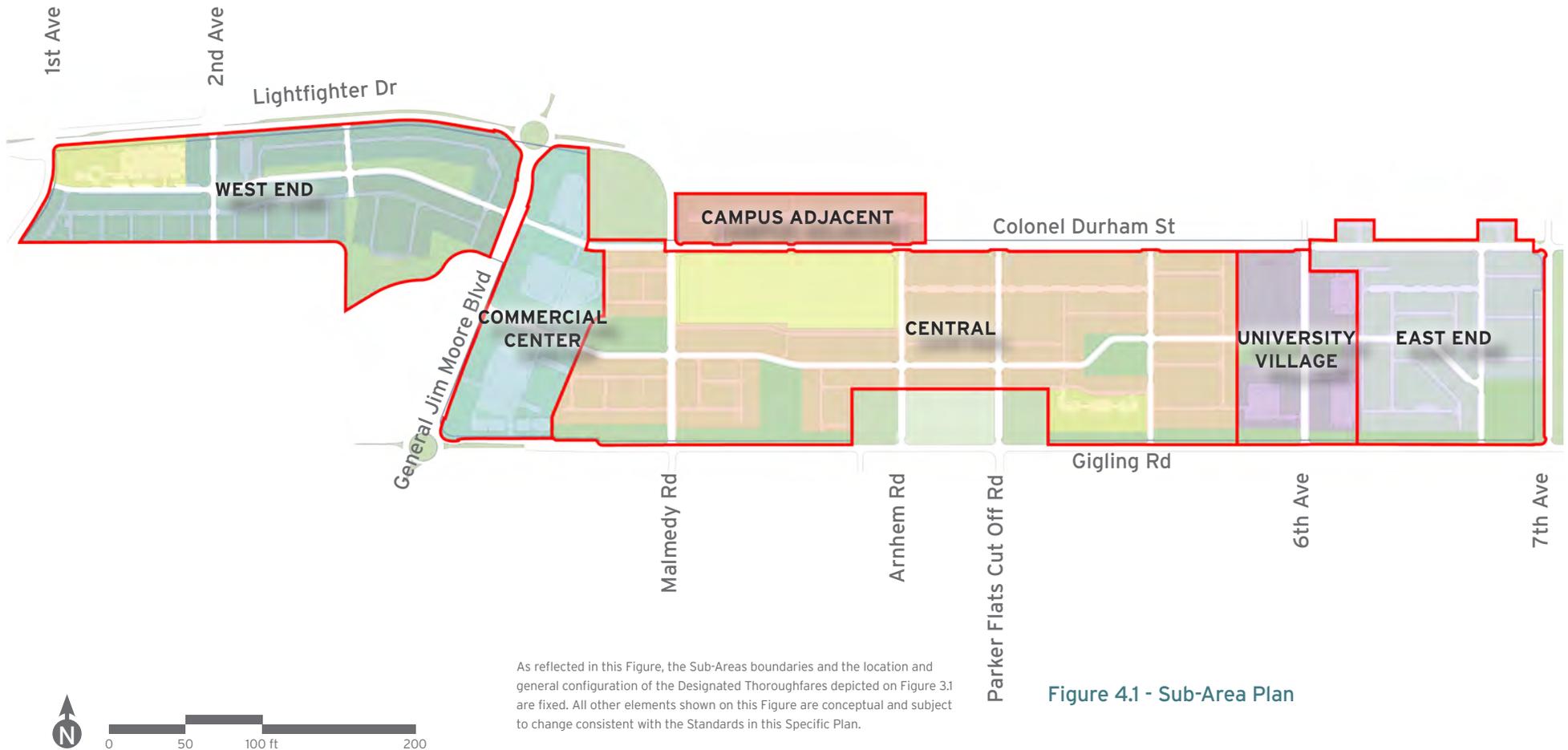


Figure 4.1 - Sub-Area Plan



4.3.2 Setback Plan

The Setback Plan establishes minimum and maximum Setbacks along the Principal Frontage for all Lots within the Specific Plan Area. Street portions without a designation are not regulated.

These Standards are in addition to Setback Standards defined in other sections of the Specific Plan. Designated Thoroughfares are fixed. Other Thoroughfares are subject to change. Setbacks would govern.



Conceptual Perceived Street Space

Front Setbacks are part of the overall perceived street space.

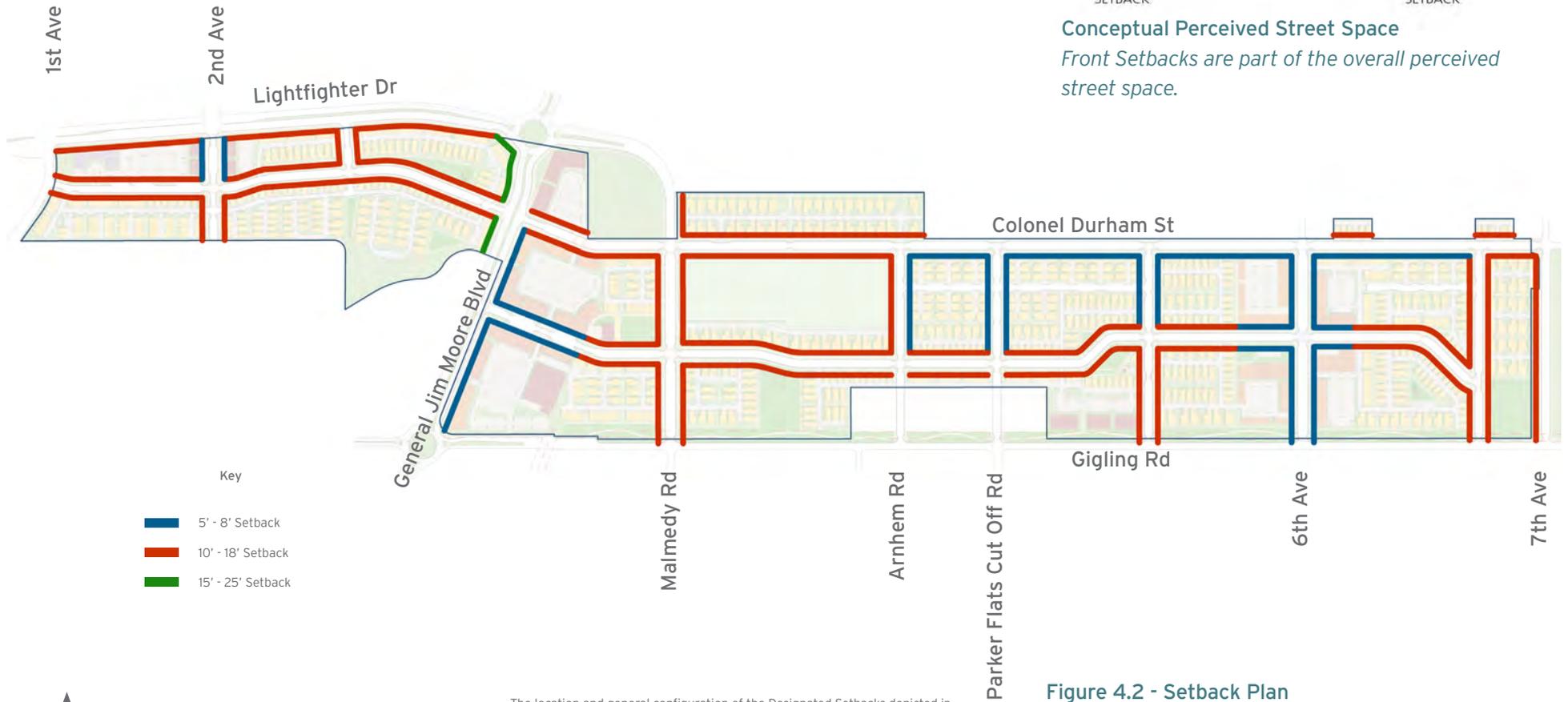


Figure 4.2 - Setback Plan

The location and general configuration of the Designated Setbacks depicted in Figure 4.2 are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.



4.3.3 Parking Placement Plan

The Parking Placement Plan defines parking placement on Lots for specifically identified Frontages. The parking Standards are classified by three layers: first, second, and third. The layer classification identifies where parking is not permitted along a Lot's Frontage. These apply to all parking areas at grade or above grade and include those that are within residential garages or other parking structures.

First Layer: No parking permitted within five feet of the Lot Line.

Second Layer: No parking permitted within 20 feet of the Lot Line.

Third Layer: No parking permitted within 40 feet of the Lot Line.

These Standards are in addition to parking Standards defined in other sections of the Specific Plan. Frontages that do not have a layer classification must still comply with all other relevant parking Standards. Designated Thoroughfares are fixed. Other Thoroughfares are subject to change. Setbacks would govern.



Figure 4.3 - Parking Placement Plan

The location and general configuration of the designated Parking Placement layers depicted in Figure 4.3 are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.



4.4 Maximum Allowable Development

The Specific Plan permits a maximum aggregate amount of development as specified in Table 4.1 below.

Table 4.1 Maximum Allowable Development

LAND USE CATEGORIES	MAXIMUM ALLOWED
Housing Units	1,485*
Hotel Rooms	250
Youth Hostel Beds	75
Retail, Dining and Entertainment	150,000 sf
Office, Flex, "Maker Space," & Light Industrial	50,000 sf

*The exact breakdown of housing unit types is not yet known, but it will be a mix of single-family detached, single-family attached, and multi-family buildings. Within the Maximum Allowed Housing Units specified above, any Development Application that results in the cumulative development of more than 885 single-family units or more than 600 multi-family units within the Plan Area shall be processed as a Minor Deviation or Deviation pursuant to Section 6 of this Specific Plan.





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4.5 Land Use Standards

The Land Use Standards for each Sub-Area are described in this section. In keeping with the Form-Based Code strategy guiding development for Campus Town, the primary elements that will define the character of the Campus Town Specific Plan Area are: Thoroughfare (Street) types, Frontage Types, Building Types and Open Space types. Allowable land-uses enjoy flexibility within limits described in this Specific Plan. The Land Use Standards will be reviewed periodically by the City to ensure a balanced mix of uses and building types.

4.5.1 Allowable Land Uses

All uses in the Campus Town Specific Plan Area shall contribute to the mixed-use character envisioned for Campus Town. **Permitted Uses** identified in Table 4.2 are allowable by right. Uses that require a **Use Permit**, which is discretionary, must identify how the intended use is not detrimental to the vision of the Plan. Uses that are not listed but are similar to the uses in Table 4.2 may be treated the same manner as the similar uses identified in Table 4.2.

4.5.1.1 Specialty Food Retail, including, but not limited to the following: grocery/drug stores (under 15,000 sf); coffee/tea; candy; gourmet foods; ice cream; pastry/desserts; yogurt/dairy; doughnuts/bakery; wine.

4.5.1.2 Specialty Goods Retail, including but not limited to the following: cooking supplies/culinary; general housewares; decorator/

art and design centers (including tile, floor and wall coverings); architectural showrooms and supplies; specialty hardware; specialty gardening supplies; antiques selling previously used; high-quality goods; party supplies; lamps and lighting; household accessories; stationary; books and magazines; musical instruments.

4.5.1.3 Quality Goods and Services, including but not limited to the following: small crafts; art supplies; picture framing; specialty furniture; clothing/shoe stores; thrift/consignment stores; electronics and computers; cameras/photography service and supplies; sporting goods; outdoor/sports clothing and supplies; toys/games; cards/gifts; jewelry/watches; florists.

4.5.1.4 Personal Services, including but not limited to the following: dry-cleaning; shoe repair; seamstress; tailor; minor appliance repair; barber and beauty shops; finance and insurance services; pharmacy and drug stores.

4.5.1.5 Business Services, including but not limited to the following: photocopying services; printing services; shipping and delivery services.

4.5.1.6 Medical and Dental Offices.

4.5.1.7 Eating and Drinking Establishments including but not limited to: restaurants, fast casual, food halls, outdoor dining, fast food without drive-thrus.

4.5.1.8 Light Industrial, including the following and similar uses: bakery; upholstery; tile-making; screen-printing; craft brewery and distillery.

4.5.1.9 Large Format Retail (over 15,000 sf), including but not limited to the following: supermarkets, furniture stores, department stores, and cinemas. No individual use may exceed 75,000 sf on a single story.



- 4.5.1.10 **Civic and Cultural Facilities**, including but not limited to libraries; public recreation facilities; museums; art galleries; auditoriums; police and fire stations.
- 4.5.1.11 **Child Care Facilities**.
- 4.5.1.12 **Health and Exercise Clubs**.
- 4.5.1.13 **Lodging** or similar establishments engaged in the provision of temporary or travel accommodation on a less than monthly basis, including bed and breakfasts, hostels, hotels and inns.
- 4.5.1.14 **Bars and Nightclubs**, including establishments providing entertainment, dancing or alcohol not clearly ancillary to food service.
- 4.5.1.15 **Bank Institutions**.
- 4.5.1.16 **Professional and Government Offices**.
- 4.5.1.17 **Residential**, including detached single-family dwellings, rowhouses, townhouses, multi-family housing for rent or sale, student housing, senior housing, etc.
- 4.5.1.18 **Live/Work**, including the following: an integrated work space within a residence; ground-floor office space with residential living spaces above.
- 4.5.1.19 **Real-Estate Sales and Leasing Office**.
- 4.5.1.20 **Makerspace**, including but not limited to: collaborative workspaces, coworking spaces, and art and dance studios.

Table 4.2 - Allowable Land Uses

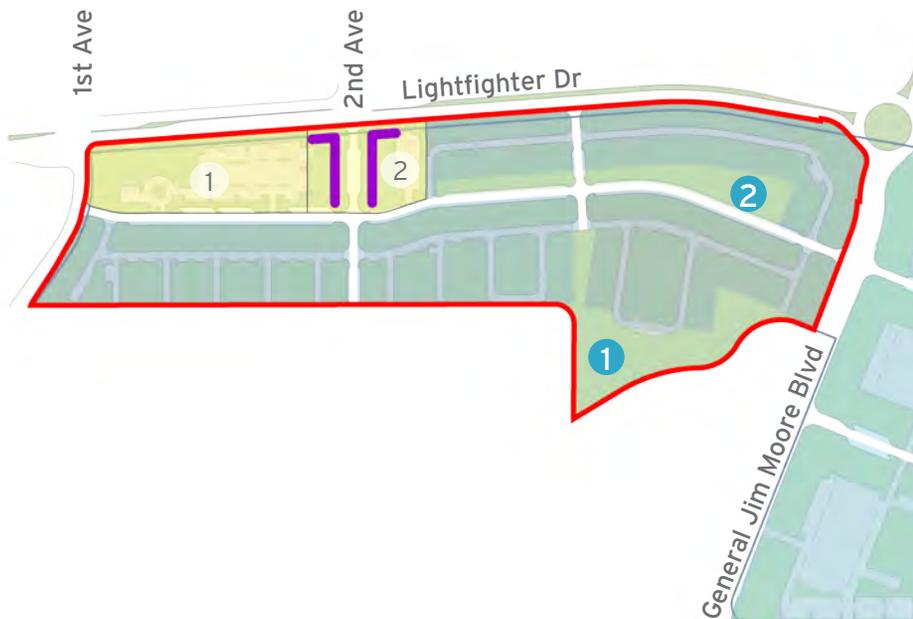
Key			
	Permitted		Limited to Special District 1
	Use Permit		Limited to Special District 2
	Limited to Special Districts		Limited to Flex Loft Building Type

Land Use	Sub-Area					
	WE	CC	CA	CE	UV	EE
Ground-floor Uses						
Specialty Food Retail						
Specialty Goods Retail						
Quality Goods and Services						
Personal Services						
Business Services						
Medical and Dental Offices						
Eating and Drinking Establishments						
Light Industrial						
Large Format Retail						
Civic and Cultural Facilities						
Child Care Facilities						
Health and Exercise Clubs						
Lodging						
Bars and Nightclubs						
Bank Institutions						
Residential						
Live/Work						
Real Estate Sales and Leasing Office						
Makerspace						
Upper Floor Uses						
Light Industrial						
Residential						
Professional and Government Offices						
Medical and Dental Offices						
Child Care Facilities						
Health and Exercise Clubs						
Lodging						
Makerspace						



4.5.2.1 WE: West End

Bounded by 1st Avenue, Lightfighter Drive, and General Jim Moore Boulevard, the West End Sub-Area is characterized by residential uses, with a Special District designation on the northwestern portion of the Sub-Area. The Special District allows for non-residential development with direct access to State Route 1. There are two designated Open Space areas: a Green and a Park.



The location and general configuration of the designated Land Use areas, Sub-Area boundaries, and required ground-floor retail frontages in this Figure are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.

Figure 4.4 - West End Land Use Standards

Land Use Areas		
Developable Area	Designated Open Space	Special District
Sub-Area Boundary	Required Ground-Floor Retail Frontage	

Permitted Building Types			
Minimum Number of Building Types per Sub-Area: 4			
Single-Family Dwelling	✓	Urban Block	SD
Rosewalk	✓	Liner with Garage	SD
Auto Court	✓	Large Format - Type I	
Rowhouse	✓	Large Format - Type II	
Townhouse	✓	Flex Block	✓
Flex Loft	✓	Flex Shed	SD
Carriage House	✓	✓ = permitted; SD = permitted in Special Districts	

Permitted Open Space Types		
Park	Green	Square
Plaza	Pocket Park	Playground

- Additional Standards**
- Special District 1 permits lodging land uses to accommodate development compatible with State Route 1, Fort Ord Dunes State Park, and CSUMB. Surface parking fields are permitted along Lightfighter Drive and its parallel street provided that it is screened by low walls or hedges. See Figure 4.3 Parking Placement Plan.
 - Special District 2 permits local retail. Ground-floor retail Frontages are required at the intersection of Lightfighter Drive and 2nd Avenue.
 - An EV charging area is required within this Sub-Area.

4.5.2.2 CC: Commercial Center

Bounded by General Jim Moore Boulevard, Lightfighter Drive, Central sub-area, and Gigling Road, the Commercial Center sub-area is characterized by mixed-use development that serves the entire Seaside community.



The location and general configuration of the designated Land Use areas, Sub-Area boundaries, and required ground-floor retail frontages in this Figure are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.

Figure 4.5 - Commercial Center Land Use Standards

Land Use Areas	
Developable Area	Designated Open Space
Sub-Area Boundary	Required Ground-Floor Retail Frontage

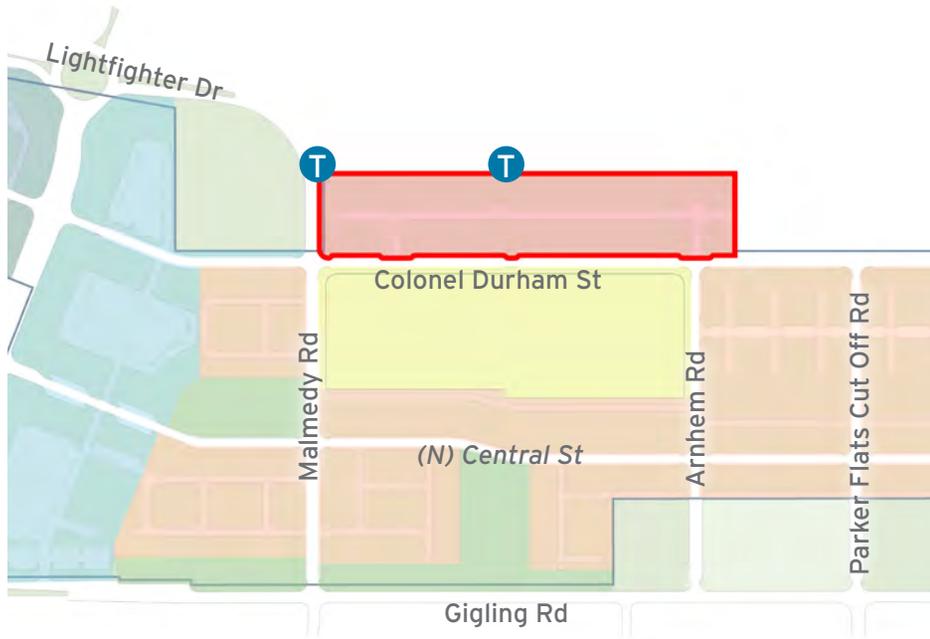
Permitted Building Types			
Minimum Number of Building Types per Sub-Area: 2			
Single-Family Dwelling		Urban Block	✓
Rosewalk		Liner with Garage	✓
Auto Court		Large Format - Type I	✓
Rowhouse	✓	Large Format - Type II	✓
Townhouse	✓	Flex Block	✓
Flex Loft	✓	Flex Shed	✓
Carriage House	✓	✓ = permitted	

Permitted Open Space Types		
<p>2 Green</p>	<p>4 Square</p>	<p>5 Plaza</p>
<p>6 Pocket Park</p>	<p>7 Playground</p>	

- | Additional Standards |
|---|
| <p>1. Ground-floor retail Frontages are required at the intersection of General Jim Moore Boulevard and Central Street. Ground-floor retail Frontages should be accessible from the adjacent sidewalk when practicable, but may be raised no more than 18 inches in height above the sidewalk to accommodate grade.</p> <p>2. At least 60% of the required ground-floor Frontages must have office or residential uses above.</p> <p>3. An EV charging area is required within this Sub-Area.</p> |

4.5.2.3 CA: Campus Adjacent

Bounded by Lightfighter Drive, Colonel Durham Street and CSUMB, the Campus Adjacent Sub-Area is characterized by residential uses that front the University campus.



The location and general configuration of the designated Land Use areas, Sub-Area boundaries, and required FORTAG Spur Connections in this Figure are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.

Figure 4.6 - Campus Adjacent Land Use Standards

Land Use Areas	
 Developable Area	 Designated Open Space
 Sub-Area Boundary	 FORTAG Spur Connection

Permitted Building Types			
Minimum Number of Building Types per Sub-Area: 1			
Single-Family Dwelling	✓	Urban Block	
Rosewalk		Liner with Garage	
Auto Court		Large Format - Type I	
Rowhouse	✓	Large Format - Type II	
Townhouse	✓	Flex Block	
Flex Loft	✓	Flex Shed	
Carriage House		✓ = permitted	

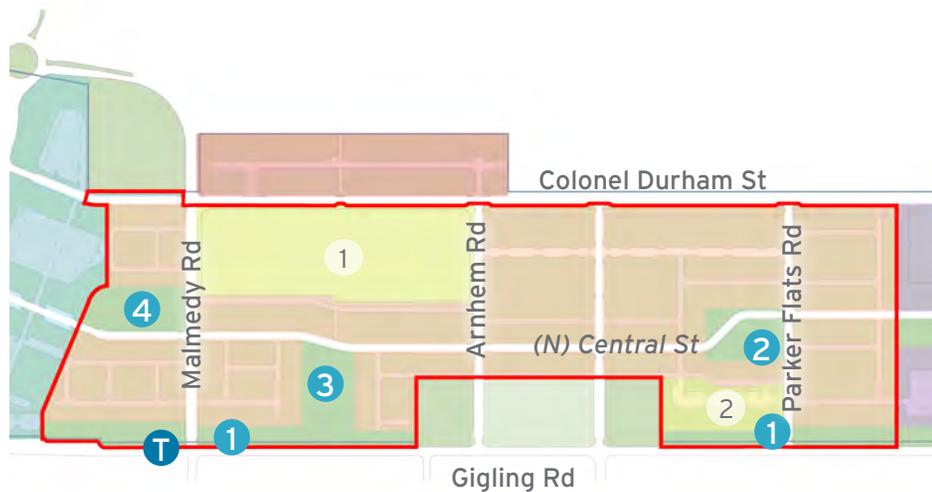
Permitted Open Space Types	
 <p>6 Pocket Park</p>	 <p>7 Playground</p>

Additional Standards

1. Accessible-gradient connections to the two FORTAG trail spurs at the northern edge of the Campus Adjacent Sub-Area shall be provided to link the bicycle network through the Specific Plan Area from the CSUMB campus. Accessibility may require ramps and/or grading on the adjacent CSUMB property, subject to approval by CSUMB.

4.5.2.4 CE: Central

Bounded by the Commercial Center Sub-Area, Colonel Durham Street, the University Village Sub-Area, and Gigling Road, the Central Sub-Area is characterized by mixed-use development of varying residential and commercial opportunities. There are four designated Open Space areas: a Green, a Park, a Sports Park, and a Square.



The location and general configuration of the designated Land Use areas, Sub-Area boundaries, and required FORTAG Spur Connection in this Figure are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.

Figure 4.7 - Central Land Use Standards

Land Use Areas		
Developable Area	Designated Open Space	Special District
Sub-Area Boundary	FORTAG Connection	

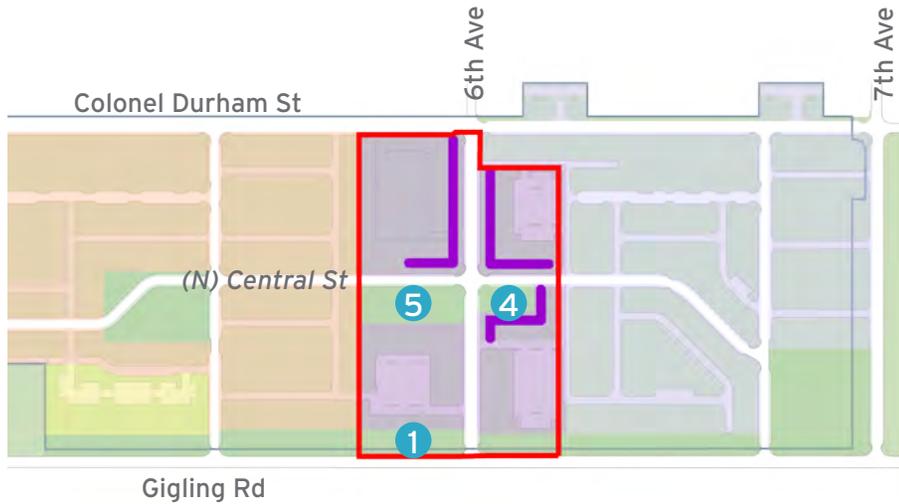
Permitted Building Types			
Minimum Number of Building Types per Sub-Area (excluding SD 1): 4			
Single-Family Dwelling	✓	Urban Block	✓
Rosewalk	✓	Liner with Garage	✓
Auto Court	✓	Large Format - Type I	
Rowhouse	✓	Large Format - Type II	
Townhouse	✓	Flex Block	✓
Flex Loft	✓	Flex Shed	SD 2
Carriage House	✓	✓ = permitted; SD = permitted in Special District	

Permitted Open Space Types			
Park	Green	Sports Park	Square
Plaza	Pocket Park	Playground	

Additional Standards
1. Special District 1 permits institutional and educational development to accommodate such uses compatible with the Monterey College of Law and CSUMB.
2. Special District 2 permits employment to accommodate uses compatible with small-scale entrepreneurial commerce.
3. An EV charging area is required within this Sub-Area.

4.5.2.5 UV: University Village

Centered along 6th Avenue between Colonel Durham Street and Gigling Road, the University Village Sub-Area is characterized by mixed-use development serving the student, faculty, and staff CSUMB community as well as the Seaside community at large. There are three designated Open Space areas: a Park, a Square and a Plaza.



The location and general configuration of the designated Land Use Areas, Sub-Area boundaries, and required ground-floor retail frontages in this Figure are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.

Figure 4.8 - University Village Land Use Standards

Land Use Areas	
Developable Area	Designated Open Space
Sub-Area Boundary	Required Ground-Floor Retail Frontage

Permitted Building Types			
Minimum Number of Building Types per Sub-Area: 2			
Single-Family Dwelling		Urban Block	✓
Rosewalk		Liner with Garage	✓
Auto Court		Large Format - Type I	
Rowhouse	✓	Large Format - Type II	
Townhouse	✓	Flex Block	✓
Flex Loft	✓	Flex Shed	
Carriage House	✓	✓ = permitted	

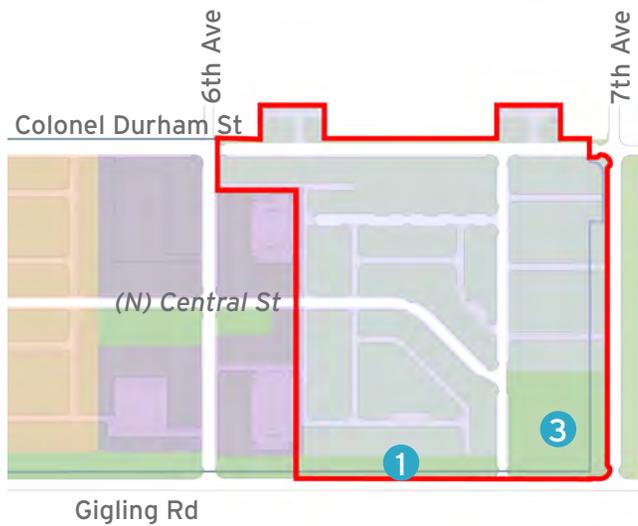
Permitted Open Space Types		
Park	Green	Square
Plaza	Pocket Park	Playground

Additional Standards

1. Ground-floor retail Frontages are required at the designated locations of the intersection at 6th Avenue and the (N) Central Street.
2. An EV charging area is required within this Sub-Area.

4.5.2.6 EE: East End

Bounded by the University Village, Colonel Durham Street, 7th Avenue, and Gigling Road, the East End Sub-Area is characterized by primarily residential development of varying type and intensity. There are two designated Open Space areas: a Park and a Sports Field.



The location and general configuration of the designated Land Use areas, Sub-Area boundaries, and required ground-floor retail frontages in this Figure are fixed. All other elements shown on this Figure are conceptual and subject to change consistent with the Standards in this Specific Plan.

Figure 4.9 - East End Land Use Standards

Land Use Areas	
 Developable Area	 Designated Open Space
 Sub-Area Boundary	

Permitted Building Types			
Minimum Number of Building Types per Sub-Area: 2			
Single-Family Dwelling	✓	Urban Block	
Rosewalk	✓	Liner with Garage	
Auto Court	✓	Large Format - Type I	
Rowhouse	✓	Large Format - Type II	
Townhouse	✓	Flex Block	✓
Flex Loft	✓	Flex Shed	
Carriage House		✓ = permitted	

Permitted Open Space Types			
			
Park	Green	Sports Field	Square
			
Plaza	Pocket Park	Playground	

Additional Standards

4.6 Urban Standards and Guidelines

Urban Standards and Guidelines affect development form and intensity and pertain to the scale of the Sub-Areas. The Urban Standards and Guidelines require buildings to define the Street as a public room, to overlook outdoor spaces, and to create a ground-floor environment that is human-scaled and pedestrian-oriented.

4.6.1 Large Lot Standards

A. Intent Statement

It is anticipated that some development will occur on large Blocks or Lots with one or more Large Format Type buildings as part of the ensemble or with a large assembly of homes. The intent of this section is to ensure that the resulting Primary Streets are pedestrian-friendly and lined with building fronts while still accommodating surface parking lots required by large format uses. It is also intended to ensure an adequate density of intersection spacing throughout the Specific Plan. The Standards require that large development projects be composed of multiple structures and/or be designed to have the appearance of multiple independent buildings and that there are a sufficient number of vehicular and pedestrian connections through the Blocks or Lots. A variation in building height and a mix of Building Types is required and is ensured by requiring multiple Building Types.

B. Applicability

Any lot or lot assemblage with a contiguous gross area of 60,000 square feet or more shall be developed according to the Large Lot Standards of this section.

C. Relationship to Other Urban Standards

Each building within a Large Lot development shall substantially conform to the applicable requirements in the Urban Standards and Building Types Standards and should be consistent with the Urban Guidelines; subject to the following modified Standards for Large Lots:

1. Development on Large Lots between 60,000 gross square feet and 85,000 gross square feet shall be composed of at least two distinct buildings (not connected via interior circulation), which may be of the same or different Building Types, as allowed in the applicable Sub-Area. In addition, they shall have at least one pedestrian connection bisecting the site.
2. Development on Large Lots exceeding 85,000 gross square feet shall be composed of at least three distinct buildings (not connected via interior circulation), which may be the same or different Building Types, as allowed in the applicable Sub-Area.
3. Frontage Types shall be permitted based upon the applicable Sub-Area, in which the project is located.



D. Access

Because there is a direct relationship between walkability and Block size, in cases where Lot sizes exceed 85,000 gross square feet, development shall provide for new internal Streets, Alleys, Common Walkways, or Drive Aisles according to the Standards and Guidelines below.

1. Vehicular and Pedestrian access should include new internal Streets, Alleys or Drive Aisles. With the exception of Auto Court drives, cul-de-sacs and dead end Streets are prohibited except where public utility constraints prohibit through Streets. Alleys may dead-end if they allow for future connection to adjacent Lots. Garage doors shall face Alleys, Driveways, or Drive Aisles.
2. At least one publicly-accessible Street, Alley, or Drive Aisle and at least one additional Common Walkway shall bisect the Lot. Where such Lots border a public Street and an Alley sufficient to service the entire Lot, then the Lot may be partially bisected by a Common Walkway (minimum 15 feet wide between primary building walls) in lieu of a Street, Alley or Drive Aisle. Larger Lots shall be bisected by sufficient Streets, Alleys, or Common Walkways.

3. New Streets should be designed to emphasize the pedestrian and should be the minimum acceptable by the City engineer.
4. New Drive Aisles shall be designed to the same Standards as Streets. An internal pattern of Drive Aisles may substitute for Streets as long as they are designed in a grid that supports infill development at a later date. In such cases, at least 70% of the perimeter of surface parking lots shall be located behind buildings that front Primary Streets. Parking fields should be provided with appropriate safety and comfort enhancements for pedestrians, such as sidewalks and trees.

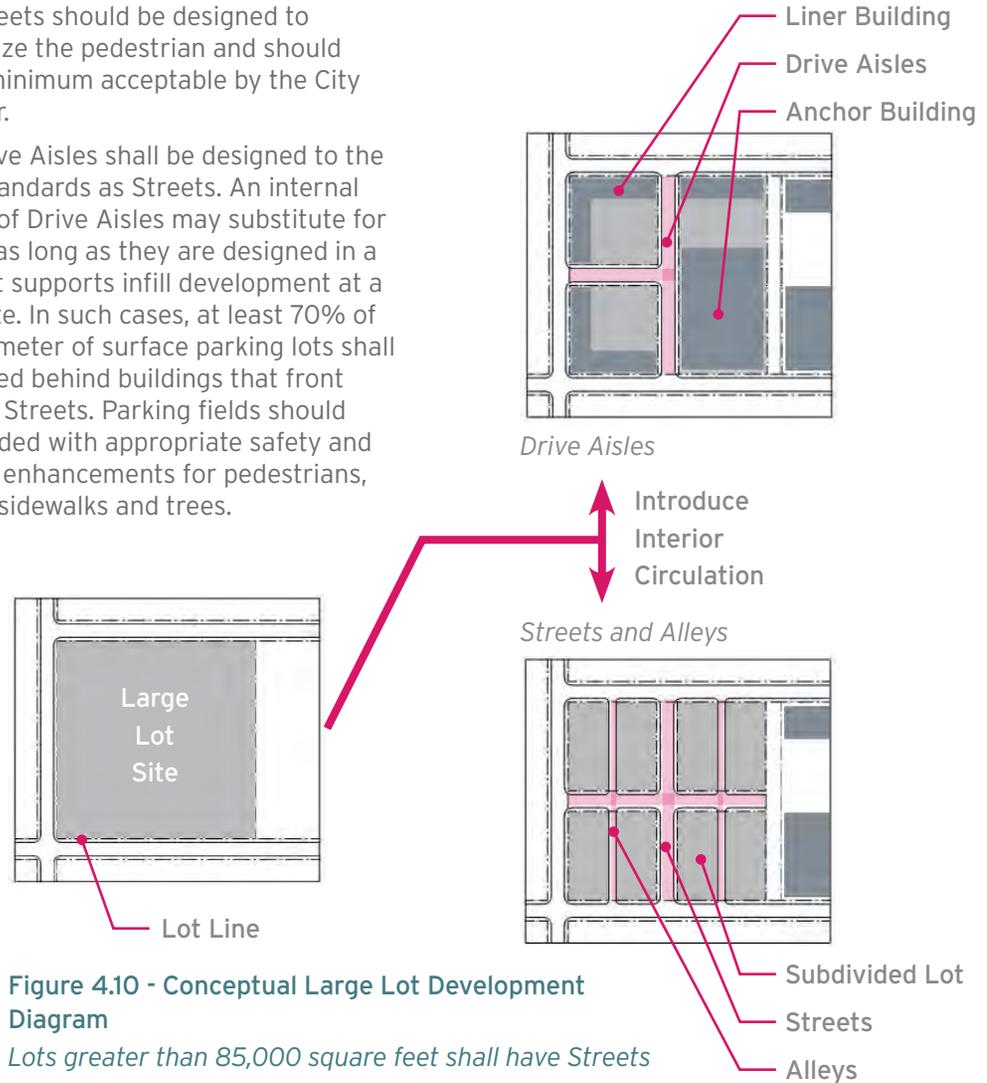


Figure 4.10 - Conceptual Large Lot Development Diagram

Lots greater than 85,000 square feet shall have Streets or Drive Aisles introduced to create a rich urban network.

4.6.2 Building Type Standards

In order to provide for a variety of uses and household types and to create a walkable urban environment, this Specific Plan provides for a diversity of Building Types. Once a particular Building Type is selected, development must adhere to the type-specific Standards and Guidelines.

The selected Building Types for each Sub-Area will be chosen by the developer at the time of Development Application. The Building Types provided in this Specific Plan define the Standards and Guidelines that are applicable to the development. While there is flexibility within the choice of Building Types for each Sub-Area, only certain Building Types may be appropriate for a particular area given adjacent uses and other requirements. Each Sub-Area's Block development diagram specifies appropriate Building Types for the Sub-Area.

In addition to providing photo voltaic panels on all new single-family homes, all new construction is to utilize passive solar techniques to the maximum extent practicable. All new construction shall at a minimum meet the requirements of Title 24 of the California Building Code. In the case of conflict between the Building Type Standards and the Architectural Standards, the Building Type Standards shall prevail.



"Plan the broadest range of building types within Centers. Include a minimum of four building types in every major project."
Regional Urban Design Guidelines



Explanation of Standards

The Building Type Standards and Guidelines cover the following:

A. Intent Statement

This statement describes the development intent and typical characteristics for the respective Building Type. While the Intent Statements below may provide illustrative land uses for certain Building Types, these lists are not intended as Land Use Standards or Guidelines. Such land uses are controlled by other Standards and Guidelines in the Specific Plan, such as those contained in Section 4.5.

B. Lot Size

All buildings shall be designed per the applicable Lot Width and Lot Depth Standards, except as otherwise provided in the Large Lot Standards.

C. Building Height and Massing

Height Standards regulate the maximum building Height in Stories rather than feet, so that interior floor to floor Heights are comfortable and appropriate to their function. For the purposes of determining the Height of a structure, neither rooftop amenities, attics, nor basements are considered a Story. Parking structures may exceed the maximum building

Height Story limit so long as they are not taller than the adjacent habitable structures. Regardless of Building Type, no building shall be taller than 85 feet in Height; excluding rooftop amenities.

Some Building Types require horizontal or vertical Plane Breaks or both (see Figure 4.13). Plane Breaks are only required at Street-facing Façades.

Furthermore, a maximum allowed footprint per Story is presented for certain Building Types. The maximum allowable footprint per Story limits the percentage of occupiable space per building Story in relationship to the building's ground-floor footprint (see Figure 4.11). For example, a four-Story building that limits the maximum allowable footprint of the fourth Story to 65 percent may satisfy this requirement by providing Stepbacks, decks, patios, building articulation, or similar massing strategies that assure that the occupiable space on the fourth Story occupies no more than 65 percent of the building's ground floor footprint. Balconies shall count toward the maximum allowable footprint unless they are open to the sky or at least 18 feet in height. These Standards are intended to articulate new development and avoid monotonous, block-like building designs in favor of more varied building designs with reduced bulk at the upper Stories.

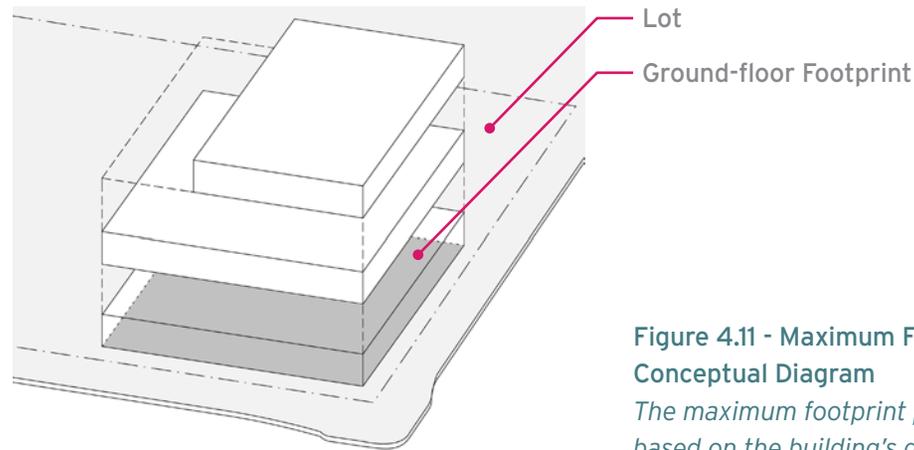


Figure 4.11 - Maximum Footprint per Story Conceptual Diagram

The maximum footprint per Story is computed based on the building's ground-floor footprint, not the overall Lot Area.

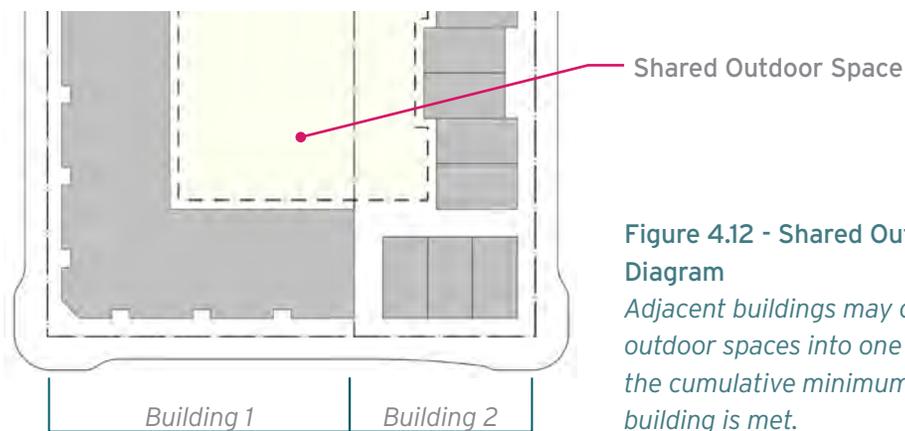


Figure 4.12 - Shared Outdoor Space Conceptual Diagram

Adjacent buildings may combine the required outdoor spaces into one shared space provided the cumulative minimum requirement for each building is met.

D. Access

This Standard regulates the location and orientation of building and Lot entries. Where buildings are accessed from a Common Walkway, the walkway shall connect to a Street or Open Space on at least one end. The Common Walkway may cross through an Alley or Driveway, but must give access to a Street or Open Space.

E. Parking and Service

Building Type parking Standards and Guidelines provide parking ratios and guidelines that are specific to each Building Type. Unless otherwise noted, tandem parking spaces do not count toward fulfilling a parking requirement.

Unless noted in this Specific Plan, parking is not required; this supersedes parking requirements in the SMC. There are no minimum parking requirements for commercial or multi-family uses.

F. Outdoor Space

Each Building Type requires a specific amount of outdoor space to be designated on-site. Such outdoor space may either be private, only accessible to the occupants, or open to the general public. Outdoor space may be located at grade, atop a podium or at the rooftop,

unless the location is restricted by the selected Building Type. Regardless of location, the design of outdoor space should maximize access to daylight and air flow.

Required outdoor space may be shared between adjacent Building Types, as long as the cumulative minimum requirement for each Type is satisfied (see Figure 4.12). Required Setbacks count towards fulfilling this requirement.

G. Frontage

This Standard lists which Frontage Types are permitted for each Building Type. See Section 4.6.3 for applicable Standards per each Frontage Type.

Façade width Standards regulate the maximum width of a Building Façade. If the Frontage length exceeds the maximum Façade Width, the plane must be broken by providing any two of the following techniques:

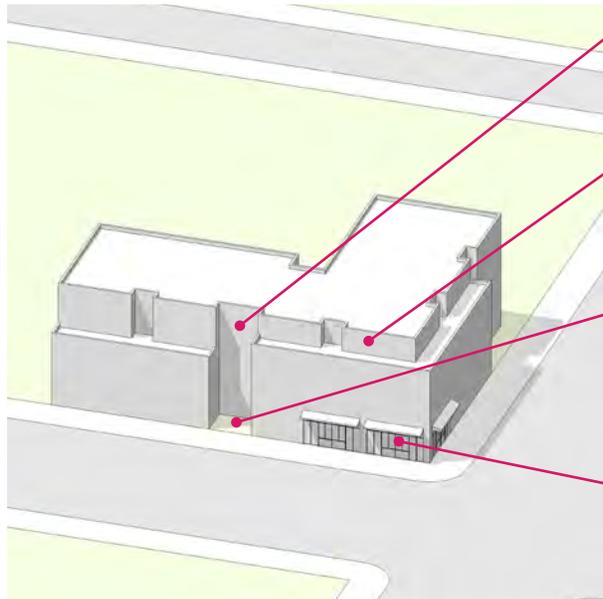
- Provide a horizontal Plane Break with one Façade set behind the other by at least two feet.
- Provide a material change.
- Provide a change in the overall type, size, spacing, or proportion of windows or Fenestration system or change in sill heights and header conditions. This option

is applicable only to vertically proportioned windows.

- Provide a change in Façade compositional strategy, including roof heights and roof types. For example, a symmetrical Façade may be placed next to a Façade with a repetitive bay system that is not symmetrical.
- Provide separate and additional primary entries from the Street.

H. Landscape

The landscape Standards and Guidelines regulate size and the design of outdoor space including the amount of outdoor space that is required to be planted with vegetation.



Plane Break

The area of the building where the plane of the Façade varies in depth. Plane Breaks can be a horizontal or vertical offset.

Maximum Footprint per Story

The occupiable floor area of upper Stories shall be less than the area of the building footprint at grade as indicated by the maximum allowed footprint per Story charts on each Building Type page.

Outdoor Space

Each Building Type requires the Lot Area to be occupied by a certain percentage of outdoor space, which can be accommodated in a variety of ways, for instance through gardens, Yards, patios, courtyards, etc., as described on each Building Type page.

Frontage Type

Each building has certain Façade conditions that are called Frontage Types. Each Frontage interacts differently with the Street and therefore is appropriate for different areas and Building Types (see Section 4.6.3).

Figure 4.13.a - Building Elements Conceptual Diagram

Building Type	Lot Width (min-max)	Maximum Stories	Sub-Area					
			WE	CC	CA	CE	UV	EE
Single Family Dwelling	27 - 60	2	✓		✓	✓		✓
Rosewalk	27 - 50	3	✓			✓		✓
Auto Court	30 - 50	3	✓			✓		✓
Rowhouse	16 - 34	3	✓	✓	✓	✓	✓	✓
Townhouse	16 - 34	3	✓	✓	✓	✓	✓	✓
Flex Loft	16 - 34	4	✓	✓	✓	✓	✓	✓
Carriage House	N/A	3	✓	✓		✓	✓	
Urban Block	N/A	5	SD	✓		✓	✓	
Liner with Garage	N/A	5	SD	✓		✓	✓	
Large Format - Type I	N/A	5		✓				
Large Format - Type II	N/A	5		✓				
Flex Block	N/A	3	✓	✓		✓	✓	✓
Flex Shed	N/A	2	SD	✓		SD2		

✓ = permitted; SD = only permitted in Special Districts; SD2 = only permitted Special District 2

Table 4.3 - Allowable Building Types

Roof line Break

Bay

Dormer

Veranda

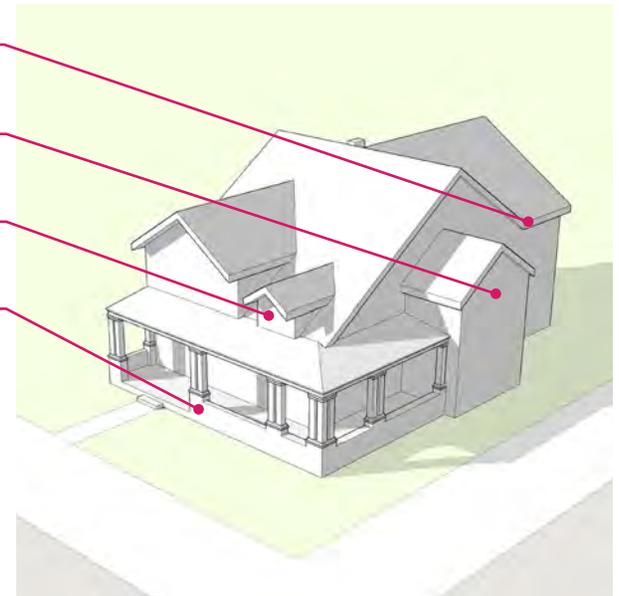


Figure 4.13.b Residential Building Plane Break Conceptual Diagram



Figure 4.14 - Single Family Dwelling Conceptual Diagram

4.6.2.A Single Family Dwelling

A. Intent Statement

A freestanding structure occupied by one primary residence.

B. Lot Size

1. Lot Width: 27 ft min to 60 ft max.
2. Lot Depth: Not regulated
3. Distance between Dwellings: 5 ft min.

C. Building Height and Massing

1. Maximum Height: 2 Stories
2. Maximum building dimension along Primary Street Frontage: 50 feet
3. Side Street building Façades longer than 50 ft in length shall provide at least one Plane Break of at least two feet.
4. A second Story may occupy the full ground-floor footprint area. A second Story may cantilever over the ground-floor footprint area while maintaining Setback requirements.
5. Attic space not exceeding 75% of the ground-floor area may be occupied and not count as a Story.

D. Access

1. The Primary Entrance to the dwelling shall be accessed directly from and face the Street.
2. For a dwelling with no Street Frontage, the Primary Entrance shall be accessed directly from and face a public Common Walkway.
3. If a Common Walkway is present, it shall connect to a Street or Open Space, not to an Alley or Driveway, on at least one end.
4. Primary pedestrian access is not permitted from an Alley.

5. Building fronts shall not face building backs.
6. Required parking and services shall be accessed through the Alley, except that Lots that are 60 ft or wider may provide parking from the Street.

E. Parking and Service

1. Two parking spaces are required. These may be accommodated at grade on the Lot, in a carport, or in a garage.
2. Garages may be attached or detached from the primary dwelling.
3. Garages shall face the Alley.
4. Services, utilities and trash container areas should be located on the Alley, when practicable.

F. Outdoor Space

1. At least 20% of the Lot Area shall be provided as outdoor space. This can be achieved on single-family large lots with minimum of 15 ft Front Yards and a sufficient rear Yard.
2. Outdoor space types that count toward the satisfaction of the required amount of outdoor space include porches, patios, verandas, balconies, Yards, and decks.

3. Required outdoor space can be shared between adjacent Building Types, as long as the cumulative minimum requirements for each type are satisfied (see Figure 4.12).

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontage Types: Yard/Porch and Stoop.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3) except that:
 - porches (unenclosed) may encroach into the Setback area to within two feet of Primary and Side Street Lot Lines; and
 - bay windows and enclosed porches may encroach up to two feet beyond the minimum Setback.
3. Where a front Setback is not designated per the Setback Plan, a five ft minimum Setback line from the Lot Line shall be observed.
4. A dwelling's ground level shall be designed so that the ground-floor living areas, sleeping rooms, and/or home office spaces are oriented toward the fronting Street. Garages may not front the street.

H. Landscape

Outdoor space shall be landscaped with native or adapted landscape or hardscaped.



Single Family Dwelling Illustrative Photo



Figure 4.15 - Rosewalk Conceptual Diagram

4.6.2.B Rosewalk

A. Intent Statement

Four or more Single-Family Dwellings arranged on either side of a Common Walkway accommodated in a public easement from which the Primary Entrances are accessed.

B. Lot Size (Individual Parcels)

1. Lot Width: 27 ft min. to 50 ft max.
2. Lot Depth: Not regulated
3. Distance between dwellings: 5 ft min.

C. Building Height and Massing

1. Maximum Height: 3 Stories
2. Maximum building dimension along Primary Street Frontage or Open Space: 40 feet
3. Side Street building Façades longer than 50 ft in length shall provide at least one Plane Break of at least two feet.
4. Second and third Stories may occupy the full ground-floor footprint area.
5. Attic space not exceeding 75% of the ground-floor area may be occupied and not count as a Story.
6. Basements may occupy the full ground-floor area and not count as a Story.

D. Access

1. The Primary Entrance to the dwelling shall be accessed directly from and face the Street or the Common Walkway.
2. Primary pedestrian access is not permitted from an Alley.
3. The Common Walkway shall connect to a Street or Open Space, not to an Alley or Driveway, on at least one end.
4. The front Setback along the Common Walkway shall be at least 12 ft from the center-line of the sidewalk resulting in a Building Face to Building Face width of at least 24 ft.

5. Building fronts shall not face building backs.
6. Parking and services shall be accessed through an Alley.

E. Parking and Service

1. Two parking spaces per dwelling are required. These may be accommodated at grade on the Lot, in a carport, or in a garage.
2. Garages may be attached or detached from the primary dwelling and they shall face the Alley.
3. Services, utilities and trash container areas should be located on the Alley, when practicable.

F. Outdoor Space

1. At least 10% of the Lot area shall be provided as outdoor space.
2. Outdoor space types that count toward the satisfaction of the required amount of outdoor space include elevated terraces, porches, patios, verandas, balconies, Yards, and decks.
3. Required outdoor space can be shared between adjacent Building Types, as long as the cumulative minimum requirements for each type are satisfied (see Figure 4.12).

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontage Types: Yard/Porch and Stoop.
2. Setbacks along Primary and Side Streets shall comply with the Setback Plan (Section 4.3) except that:
 - porches (unenclosed) may encroach into the Setback area to within two ft of Primary Street and Side Street Lot Lines; and
 - bay windows and enclosed porches may encroach up to two ft beyond the minimum Setback.
3. Where a Setback along Primary Street and Side Streets is not designated per the Setback Plan, a five ft minimum Setback from the Lot Line shall be observed.
4. A dwelling's ground level shall be designed so that the ground-floor living areas, sleeping rooms, and/or home office spaces are oriented toward the fronting Street and/or Common Walkway.

H. Landscape

1. The Common Walkway path shall be a minimum of five ft in width.
2. Outdoor space shall be landscaped with native or adapted landscape or hardscaped.
3. Fences, walls or hedges are allowed along the Common Walkway at fronts of homes, provided they are setback from the walkway by at least two ft and are no taller than 42 inches from the Grade of the Common Walkway.



Rosewalk Illustrative Photo



Figure 4.16 - Auto Court Conceptual Diagram

4.6.2.C Auto Court

A. Intent Statement

A group of up to six detached Single Family Dwellings that share a common Driveway. The primary pedestrian access shall be located from a Thoroughfare, Open Space or Common Walkway.

B. Lot Size (Individual Parcels)

1. Lot Width: 30 ft min. to 50 ft max.
2. Lot Depth: Not regulated

3. Distance between dwellings: 5 ft min.

C. Building Height and Massing

1. Maximum Height: 3 Stories
2. Maximum building dimension along Primary Street Frontage or common green: 40 feet
3. Side Street building Façades longer than 50 ft in length shall provide at least one Plane Break of at least two feet.
4. Second and third Stories may occupy the full ground-floor footprint area.
5. Attic space not exceeding 75% of the ground-floor area may be occupied and not count as a Story.
6. Basements may occupy the full ground-floor area and not count as a Story.

D. Access

1. The Primary Entrance to a dwelling shall be accessed directly from and face the Street or a Common Walkway.
2. Only when necessary, up to two dwellings per auto court may locate their primary pedestrian access from the Common Driveway.
3. If a Common Walkway is present, it shall connect to a Street or Open Space, not to an Alley or Driveway, on at least one end.

4. The front Setback along the Common Walkway shall be at least 10 ft from the center-line of the sidewalk resulting in a Building Face to Building Face width of at least 20 ft.
5. Unenclosed porches may encroach into the Setback by up to seven ft.
6. Enclosed porches or bay windows may encroach into the Setback by up to two ft.
7. Building fronts should not face building backs.
8. Parking and services shall be accessed through the Common Driveway.

E. Parking and Service

1. Two parking spaces are required per dwelling. These may be accommodated at Grade on the Lot, in a carport, or in a garage.
2. Garages may be attached or detached from the primary dwelling and they shall face the Common Driveway.
3. Services, utilities and trash container areas should be located on the Common Driveway, when practicable.

F. Outdoor Space

1. At least 20% of the Lot Area shall be provided as outdoor space.

2. Outdoor space types that count toward the satisfaction of the required amount of outdoor space are elevated terraces, porches, patios, verandas, balconies, Yards, and decks.

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontage Types: Yard/Porch, Stoop and Mews.
2. Setbacks along Primary Streets and Side Streets shall comply with the Setback Plan (Section 4.3) except that:
 - porches (unenclosed) may encroach into the Setback area to within two ft of Primary Street and Side Street Lot Lines; and
 - bay windows and enclosed porches may encroach up to two ft beyond the minimum Setback.
3. Where a Setback along Primary Streets or Side Streets is not designated per the Setback Plan, a six ft minimum Setback from the Lot Line shall be observed.
4. Where there is no fronting Street and/or Common Walkway, a two ft minimum Setback from the Lot Line at the Auto Court Common Driveway shall be observed.
5. A dwelling’s ground level shall be designed so that the ground-floor living

areas, sleeping rooms, and/or home office spaces are oriented toward the fronting Street and/or Common Walkway.

H. Landscape

1. The Common Walkway shall be a minimum of five ft in width.
2. Outdoor space shall be landscaped with native or adapted landscape or hardscaped.
3. Fences, walls or hedges are allowed along the Common Walkway, provided they are Setback from the Common Walkway by at least two ft.



Auto Court Illustrative Photo



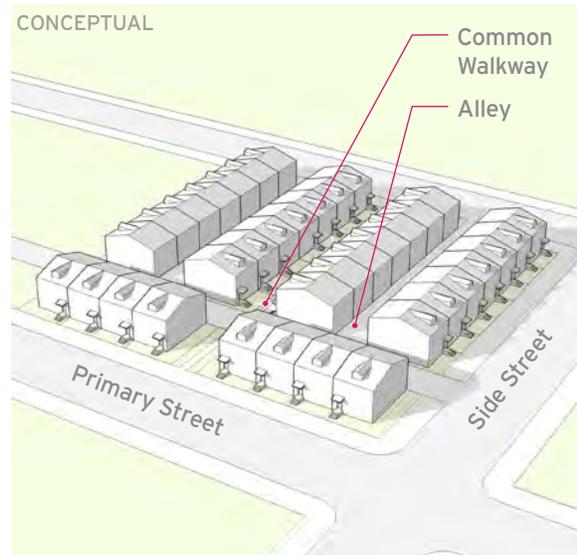


Figure 4.17 - Rowhouse Conceptual Diagram

4.6.2.D Rowhouse

A. Intent Statement

A structure that consists of at least two primary residences with common walls, side by side along the Building Frontage, with access from a Street or Common Walkway. The structure has individual garages for each unit, accessed from an Alley, or may have a shared garage with dedicated parking spaces.

B. Lot Size

1. Lot Width: 16 ft min to 34 ft max.
2. Lot Depth: Not regulated.

C. Building Height and Massing

1. Maximum Height: 3 Stories
2. The second and third Stories may occupy the full ground-floor footprint area.
3. Attic space may be occupied and not count as a Story. Occupiable Attic space shall not exceed 50% of the maximum footprint per Story.
4. Basements may occupy the full ground-floor area and not count as a Story.
5. Façade Strings shall have at least one Encroachment per 100 linear feet, such as a porch, balcony, or Plane Break. The combined length of Plane Breaks shall occupy at least 10% of the Façade length.
6. Building faces abutting Side Streets or Yards shall provide at least one horizontal Plane Break of at least three feet, and one vertical Plane Break of at least two feet.
7. In a three-story building, a two-story Rowhouse may be stacked over a separate ground-floor unit.

D. Access

1. The Primary Entrance to the dwelling shall be accessed directly from and face the Street or a public Common Walkway.
2. If a Common Walkway is present, it shall connect to a Street or Open Space, not to an Alley or Driveway, on at least one end.
3. The front Setback along the Common Walkway shall be at least 15 ft from the center-line of the sidewalk resulting in a Building Face to Building Face width of at least 30 ft.
4. In order to accommodate Grade, front entrances may be raised up to six feet above the Grade of the adjacent sidewalk.
5. Primary pedestrian access is not permitted from an Alley.
6. Building fronts shall not face building backs.
7. Parking and services shall be accessed through the Alley.
8. On a corner Lot without access to an Alley, parking and services shall be accessed from the Side Street, by a 16 ft wide, maximum, Driveway.

E. Parking and Service

1. Two parking spaces are required per unit. These may be accommodated on the surface, in a carport or garage.
2. Tandem Parking spaces are permitted and count toward the parking requirement.
3. Garages may be attached or detached from the primary dwelling.
4. Garages shall face the Alley.
5. Services, utilities and trash container areas should be located on the Alley, when practicable.

F. Outdoor Space

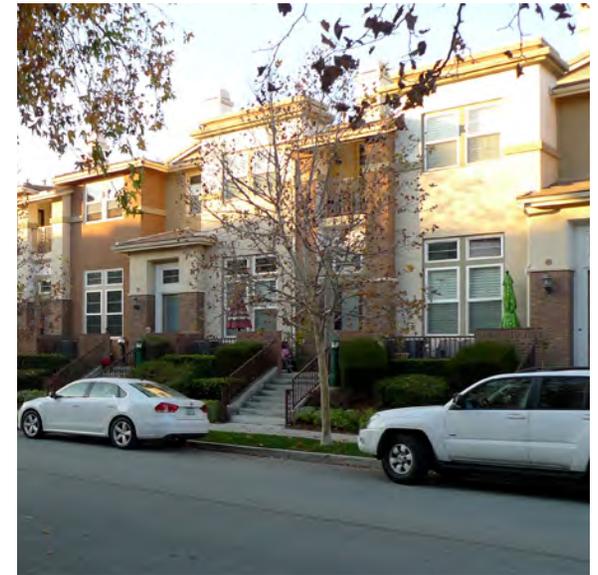
1. At least 10% of the Lot Area shall be provided as outdoor space.
2. Outdoor space types that count toward the satisfaction of the required amount of outdoor space include porches, patios, verandas, balconies, Yards, and decks.
3. Required outdoor space can be shared between adjacent Building Types, as long as the cumulative minimum requirements for each type are satisfied (see Figure 4.12).

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontage Types: Yard/Porch and Stoop.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3) except that:
 - porches (unenclosed) may encroach into the Setback area to within two feet of Primary Street and Side Street Lot Lines; and
 - bay windows and enclosed porches may encroach up to two feet beyond the minimum Setback.
3. Where a Setback along Primary Street and Side Streets is not designated per the Setback Plan, a 10 ft minimum Setback from the Lot Line shall be observed.
4. A dwelling’s ground level shall be designed so that the ground-floor living areas, sleeping rooms and/or home office space are oriented toward the fronting Street. Garages may not front the Street.

H. Landscape

Outdoor space shall be landscaped with native or adapted landscape or hardscaped.



Rowhouses Illustrative Photo

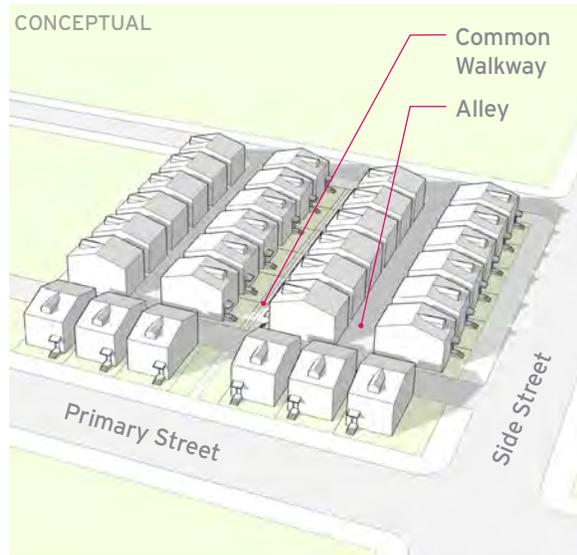


Figure 4.18 - Townhouse Conceptual Diagram

4.6.2.E Townhouse

A. Intent Statement

A Townhouse is a vertically disposed Single Family Dwelling usually with a garage on the ground floor. A Townhouse is freestanding and does not share common walls with adjacent structures.

B. Lot Size

1. Lot Width: 16 ft min to 34 ft max.
2. Lot Depth: Not regulated.

3. Distance between dwellings: 3 ft min.

C. Building Height and Massing

1. Maximum Height: 3 Stories
2. The second and third Stories may occupy 100% of the maximum footprint per Story.
3. Attic space may be occupied and not count as a Story. Occupiable Attic space shall not exceed 50% of the ground-floor footprint area.
4. Basements may occupy the full ground-floor area and not count as a Story.
5. Building faces abutting Side Streets or Yards shall provide at least one horizontal Plane Break of at least three feet, and one vertical Plane Break of at least two feet.
6. In a three Story building, a two Story Townhouse can be stacked over a separate ground-floor unit.

D. Access

1. The Primary Entrance to the dwelling shall be accessed directly from and face the Street or a public Common Walkway.
2. If a Common Walkway is present, it shall connect to a street or Open Space, not to an Alley or Driveway, on at least one end.
3. In order to accommodate Grade, front entrances may be raised up to six feet above the Grade of the adjacent sidewalk

4. Primary pedestrian access is not permitted from an Alley.
5. Building fronts shall not face building backs.
6. Parking and services shall be accessed through the Alley.
7. On a corner Lot without access to an Alley, parking and services shall be accessed from the Side Street, by a 16 ft wide, maximum, Driveway.

E. Parking and Service

1. Two parking spaces are required per unit. These may be accommodated on the surface, in a carport, or garage.
2. Tandem Parking spaces are permitted and count toward the parking requirement.
3. Garages may be attached or detached from the primary dwelling.
4. Garages shall face the Alley.
5. Services, utilities and trash container areas should be located on the Alley, when practicable.

F. Outdoor Space

1. At least 10% of the Lot Area shall be provided as outdoor space.
2. Outdoor space types that count toward the satisfaction of the required amount of

outdoor space includes porches, patios, verandas, balconies, Yards, and decks.

3. The front Setback along the Common Walkway shall be at least 15 ft from the center-line of the sidewalk resulting in a Building Face to Building Face width of at least 30 ft.
4. Required outdoor space can be shared between adjacent Building Types, as long as the cumulative minimum requirements for each type are satisfied (see Figure 4.12).

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontage Types: Yard/Porch and Stoop.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3) except that:
 - porches (unenclosed) may encroach into the Setback area to within two feet of Primary Street and Side Street Lot Lines; and
 - bay windows and enclosed porches may encroach up to two feet beyond the minimum Setback.
3. Where a Setback along Primary Street and Side Streets is not designated per the Setback Plan, a 10 ft minimum Setback from the Lot Line shall be observed.

4. A dwelling's ground level shall be designed so that the ground-floor living areas, sleeping rooms and/or home office spaces are oriented toward the fronting Street. Garages may not front the Street.

H. Landscape

Outdoor space shall be landscaped with native or adapted landscape or hardscaped.



Townhouses Illustrative Photo



Figure 4.19 - Flex Loft Conceptual Diagram

4.6.2.F Flex Loft

A. Intent Statement

An integrated residence and work space, comprising a single unit. Often two or more such units may be arranged side by side along the Primary Street or structurally modified to accommodate joint residential and work occupancy.

B. Lot Size

1. Unit Width: 16 ft min to 34 ft max.
2. Unit Depth: 45 ft min.

C. Building Height and Massing

1. Maximum Height: 4 Stories
2. The upper Stories may occupy 100% of the maximum footprint per Story.
3. Attic space may be occupied and not count as a Story. Occupiable Attic space shall not exceed 50% of the ground-floor footprint area.
4. Basements may occupy the full ground-floor area and not count as a Story.
5. The maximum number of attached Flex Loft units allowed is 10 units per Façade String.
6. Façade Strings shall have at least one Encroachment per 100 linear feet, such as a porch, balcony, or Plane Break. The combined length of Plane Breaks shall occupy at least 10% of the Façade length.
7. Building faces abutting Side Streets or Yards shall provide at least one horizontal Plane Break of at least three feet, and one vertical Plane Break of at least two feet.
8. See Section 4.6.2 for additional techniques that may be implemented to fulfill the Plane Break requirements.

D. Access

1. The Primary Entrance to the building shall be accessed directly from and face the Street except that primary residential entries may be accessed through workspace, through a Common Walkway between units, or from the rear.
2. Entrances to ground-floor work spaces shall be at Grade.
3. Parking and services shall be accessed through the Alley.

E. Parking and Service

1. Two parking spaces are required per unit. These may be accommodated at Grade on the Lot, in a common surface lot, in a carport, in a garage on the Lot, or in a common garage.
2. Tandem Parking spaces are permitted and count toward the parking requirement.
3. Garages may be attached or detached from the primary dwelling.
4. Garages and surface parking lots shall face the Alley and be screened from view from the Side Street by a garden wall, fence or hedge.
5. Services, utilities and trash container areas should be located on the Alley, when practicable.

F. Outdoor Space

1. At least 10% of the Lot Area shall be provided as outdoor space.
2. Permitted outdoor space types that count toward the satisfaction of the required amount of outdoor space include elevated terraces, porches, patios, verandas, balconies, Yards, and roof decks.

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontage Types: Stoop, Shopfront, Terrace, and Industrial Shop.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3) except that:
 - upper story porches or balconies (unenclosed) may encroach into the Setback area up to the Lot Line of Primary Street and Side Street; and
 - bay windows and enclosed porches may encroach up to two feet beyond the minimum Setback.
3. Where a Setback is not designated per the Setback Plan, the building Frontage may be setback a maximum of five feet from the Lot Line.

4. A Flex Loft’s ground level shall be designed so that the ground-floor areas are made up primarily of retail, office or work space, visible in part, to the public through ground-floor windows.

H. Landscape

1. Front Yard space required by Setback requirements shall be hardscaped
2. Other outdoor space shall be landscaped with native or adaptive landscaping or hardscaped. Permeable paving is encouraged.



Flex Loft Illustrative Photo



Figure 4.20 - Carriage House Conceptual Diagram

4.6.2.G Carriage House

A. Intent Statement

A structure comprised of residential units above at least partially-enclosed ground-floor parking spaces. The parking is accessed from an Alley or surface parking lot, while the residential units are accessed from the Street or Common Walkway. The residential space need not be directly connected to the parking. Especially suitable as Liner buildings, Carriage Houses can be used to screen surface parking lots from the Primary Street or a Common Walkway.

B. Lot Size

N/A

C. Building Height and Massing

1. Maximum Height: 3 Stories
2. The second and third Story may occupy 100% of the maximum footprint per story.
3. Building Depth: 20 to 36 ft.
4. Façade Strings shall have at least one Encroachment per 100 linear feet, such as a porch, balcony, or Plane Break. The combined length of Plane Breaks shall occupy at least 10% of the Façade length.
5. Building faces abutting Side Streets or Yards shall provide at least one horizontal Plane Break of at least three feet, and one vertical Plane Break of at least two feet.

D. Access

1. The Primary Entrance to the building shall be accessed directly from and face the Street or Common Walkway.
2. A secondary entrance may be provided from the rear parking spaces.
3. Parking and services shall be accessed through the Alley or surface parking lot.

E. Parking and Service

1. There are no minimum parking requirements for this Building Type. However, adequate parking will be provided for commercial uses.
2. Parking spaces shall face the Alley or surface parking lot and be screened from view from the Side Street by a garden wall, fence or hedge.
3. Tandem Parking spaces are permitted.
4. Parking spaces do not have to correspond with the residential unit above.
5. Parking spaces do not need to be fully enclosed.
6. Services, utilities and trash container areas should be located on the Alley or in the surface parking lot, when practicable.

F. Outdoor Space

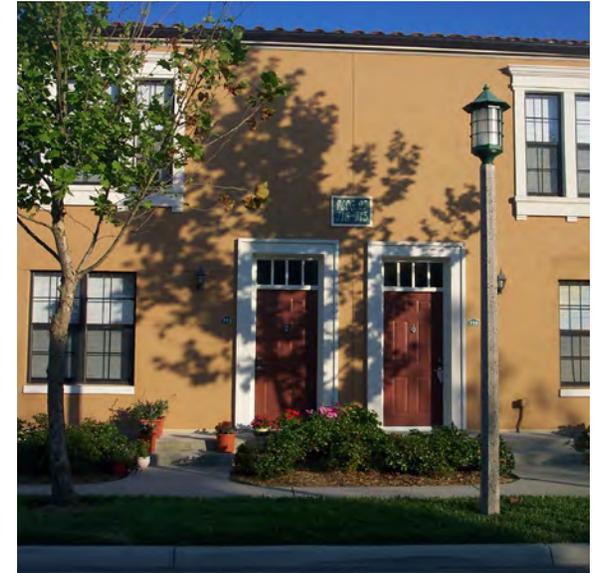
N/A

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontage Types: Yard/Porch, Stoop, and Terrace.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3) except that:
 - upper story porches or balconies (unenclosed) may encroach into the Setback area up to the Lot Line of Primary Street and Side Street; and
 - bay windows and enclosed porches may encroach up to two feet beyond the minimum Setback.
3. Where a Setback along Primary Street and Side Streets is not designated per the Setback Plan, an six ft minimum Setback from the Lot Line shall be observed.
4. A Carriage House’s ground floor should be designed so that the Street-facing side appears to be habitable.

H. Landscape

Outdoor space is not required. However if provided, outdoor space shall be landscaped with native or adaptive landscaping or hardscaped. Permeable paving is encouraged.



Carriage House Illustrative Photo



Figure 4.21 - Urban Block Conceptual Diagram

4.6.2.H Urban Block

A. Intent Statement

A highly-flexible building designed for occupancy by a variety and/or combination of uses such as retail, hotel, service, office and residential uses.

B. Lot Size

N/A

C. Building Height and Massing

1. Minimum Height: 2 Stories
2. Maximum Height: 5 Stories
3. The upper Stories may occupy the full ground-floor footprint area, except for the fifth Story, which may only occupy up to 75% of the ground-floor footprint area.
4. Maximum building dimension along Primary Street and Side Street Frontages: 350 ft
5. Maximum building footprint per floor: 24,000 gross square feet. Proposals that require larger footprints must be considered as multiple buildings with separate lobbies and Facades.
6. Façades greater than 175 ft in length:
 - must have at least one Plane Break of at least 20 ft in length and 10 ft in depth and open to the sky or;
 - must have at least 30 ft of its length Stepback at least five feet from the remainder of the facade, be of a different material, and have at least one additional ground-floor entry.

D. Access

1. Primary access to ground-floor spaces shall be directly from the Street and shall occur at a maximum interval of 60 ft.

2. For non-residential uses, building and retail Primary Entrances should remain accessible and unlocked during regular business hours.
3. Primary Entrances to upper floors shall be accessed through:
 - an interior courtyard.
 - a lobby that is accessed directly from the Street or a surface parking lot in the rear of the building.
 - Stoops entered from the Street.

E. Parking and Service

1. There are no minimum parking requirements for this Building Type. However, adequate parking will be provided for commercial uses.
2. Optional parking is accommodated in an underground garage, surface lot behind the building, tuck-under, a mid-block shared above ground garage or a combination thereof. When provided, at least five percent of parking spaces shall be equipped for charging of electric vehicles, with signage limiting the use of those spaces to electric vehicles only. An electrical conduit shall be installed at the time of construction to facilitate the future installation of EV charging stations to at least 10 percent of parking spaces.

3. Parking lots and garages shall be located at the rear of the building and screened from adjacent Streets with walls, fences, or hedges, except as noted per Sub-Area Standards.
4. Parking garages may exceed the maximum Height Story limit but may not exceed the Height of the adjacent habitable structures.
5. Off-street parking shall be accessed through an Alley, when served by an Alley.
6. Where an Alley is present, services, utilities and trash container areas should be located on the Alley, when practicable.
7. Where an Alley is not present, parking entrances to garages and/or driveways should be located close to the side or rear of the Lot.

F. Outdoor Space

1. At least 10% of the ground-floor footprint area of the Urban Block building shall be provided as outdoor space.
2. Outdoor space types that count toward the satisfaction of the required amount of outdoor space include elevated terraces, porches, patios, verandas, balconies, and decks.
3. Required outdoor space can be shared between adjacent Building Types, as long

as the cumulative minimum requirements for each type are satisfied (see Figure 4.12).

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontages: Stoop, Shopfront and Terrace.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3) except that:
 - upper story porches or balconies (unenclosed) may encroach into the Setback area up to the Lot Line of Primary Street and Side Street; and
 - bay windows and enclosed porches may encroach up to two feet beyond the minimum Setback.
3. Where a Setback is not designated per the Setback Plan, the building Frontage may be Setback a maximum of five feet from the Lot Line.
4. When an Urban Block’s ground level is non-residential, it shall be designed so that the ground-floor areas are made up primarily of retail, office or work space, visible in part, to the public through ground-floor windows.



Urban Block Illustrative Photo

H. Landscape

Front Yard space required by Setback requirements shall be hardscaped except that planters may provide privacy for office or work space.



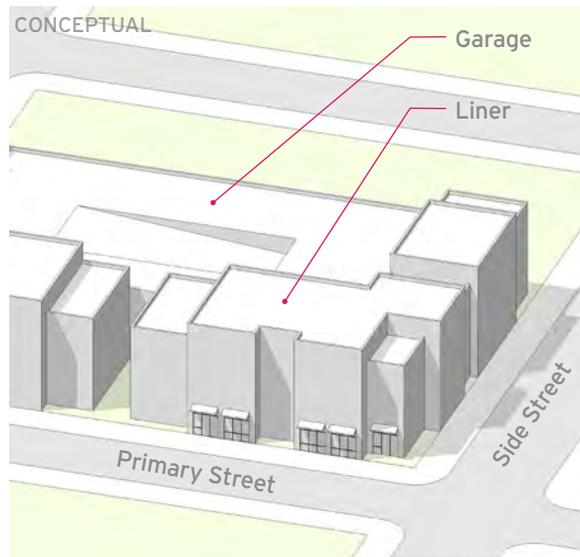


Figure 4.22 - Liner with Garage Conceptual Diagram

4.6.2.1 Liner with Garage

A. Intent Statement

A building and garage ensemble where the building directly fronts the Street and wraps around an above ground garage. The garage can either be attached to or detached from the building.

B. Lot Size

N/A

C. Building Height and Massing

1. Minimum Height: 3 Stories
2. Maximum Height: 5 Stories
3. Maximum building dimension along Primary Street and Side Street Frontages: 250 ft. Proposals that require larger Frontages must be considered as multiple buildings with separate lobbies and Facades.
4. The upper Stories may occupy the full ground-floor footprint area, except for the fifth Story, which may only occupy up to 75% of the maximum footprint per Story.
5. Façades greater than 175 ft in length:
 - must have at least one Plane Break of at least 20 ft in length and 10 ft in depth and open to the sky or;
 - must have at least 30 ft of its length Stepback at least five feet from the remainder of the Façade, be of a different material, and have at least one additional ground-floor entry.

D. Access

1. Primary access to ground-floor spaces shall be directly from the Street and shall occur at a maximum interval of 60 ft.
2. Primary retail entrances shall remain accessible and unlocked during regular business hours.

3. Primary Entrances to upper floors shall be accessed through:
 - an interior courtyard.
 - a lobby that is accessed directly from the Street or the parking garage in the rear of the building.
 - Stoops entered from the Street.

E. Parking and Service

1. There are no minimum parking requirements for this Building Type. However, adequate parking will be provided for commercial uses.
2. Optional parking is accommodated in an underground garage, surface lot behind the building, tuck-under, a mid-block shared above ground garage or a combination thereof. When provided, at least five percent of parking spaces shall be equipped for charging of electric vehicles, with signage limiting the use of those spaces to electric vehicles only. An electrical conduit shall be installed at the time of construction to facilitate the future installation of EV charging stations to at least 10 percent of parking spaces.
3. Parking garages may exceed the maximum Height Story limit but may not exceed the Height of the adjacent habitable structures.

4. Off-street parking shall be accessed through an Alley, when served by an Alley.
5. Where an Alley is present, services, utilities and trash container areas should be located on the Alley, when practicable.
6. Where an Alley is not present:
 - parking shall be accessed from a Side Street, where possible
 - services, above ground equipment, and trash container areas shall be located off of a Driveway accessing the parking garage.

F. Outdoor Space

1. At least 10% of the ground-floor footprint area of the Liner building shall be provided as outdoor space.
2. Outdoor space types that count toward the satisfaction of the required amount of outdoor space includes elevated terraces, porches, patios, verandas, balconies, and decks.

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontages: Stoop, Shopfront and Terrace.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3) except that:
 - upper story porches or balconies (unenclosed) may encroach into the Setback area up to the Lot Line of Primary Street and Side Street; and
 - bay windows and enclosed porches may encroach up to two feet beyond the minimum Setback.
3. Where a Setback is not designated per the Setback Plan, a building Frontage may be setback a maximum of five feet from the Lot Line.

H. Landscape

Front Yard space required by Setback requirements shall be hardscaped, except that planters may provide privacy for office or work space, or be landscaped to provide privacy for ground-floor residential.



Liner Building with Garage Illustrative Photo



Figure 4.23 - Large Format Type I Conceptual Diagram

4.6.2.J Large Format - Type I

A. Intent Statement

The Large Format-Type I building accommodates the need for large-footprint anchor retailers or cinemas while providing Active Frontages in secondary Façades. Ground-floor storefronts or other Liner uses avoid exposing blank walls on Street fronts.

B. Lot Size

N/A

C. Building Height and Massing

1. Maximum Height: 5 Stories.
2. The maximum anchor floor footprint is 60,000 gross square feet. The City may grant an exception to the maximum anchor floor footprint for cinemas, concert halls, or other live performance spaces during Substantial Conformance Review (Section 6.3.3.1.C). This maximum floor footprint does not include parking garages.
3. Façade Width: No limit, except that a maximum of 150 ft of the anchor use may be exposed to a Building Frontage. Anchor buildings that are longer than 150 ft must be lined with other uses for the portion of the Frontage exceeding 150 ft.

D. Access

1. Primary access to each ground-floor anchor shall be directly from the Street and shall occur at a maximum interval of 200 ft. Liner uses shall be accessible directly from the Street and access shall be provided at a maximum interval of 60 ft. All retail spaces should be accessed from a ground-floor, single-tenant entry along a street, courtyard, or Alley.

2. Primary retail entrances should remain accessible and unlocked during regular business hours.
3. In addition to the building's required Primary Entrances, there may be ancillary entrances to the building from parking garages and areas.

E. Parking and Service

1. There are no minimum parking requirements for this Building Type. However, adequate parking will be provided for commercial uses.
2. Optional parking is accommodated in an underground garage, surface lot behind the building, tuck-under, a mid-block shared above ground garage or a combination thereof. When provided, at least five percent of parking spaces shall be equipped for charging of electric vehicles, with signage limiting the use of those spaces to electric vehicles only. An electrical conduit shall be installed at the time of construction to facilitate the future installation of EV charging stations to at least 10 percent of parking spaces.
3. Parking lots and above-grade garages shall be located at the rear of the building and screened from adjacent Streets with walls, fences, or hedges.

4. Parking garages may exceed the maximum Height Story limit but may not exceed the Height of the adjacent habitable structures.
5. Off-street parking shall be accessed through an Alley, when served by an Alley.
6. Where an Alley is present, services, utilities and trash container areas should be located on the Alley, when practicable.
7. Where an Alley is not present, parking entrances to garages and/or Driveway should be located close to the side or rear of the Lot.

F. Outdoor Space

N/A

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontages: Shopfront and Terrace
2. Front Setbacks shall comply with the Setback Plan (Section 4.3)
3. Where a Setback is not designated per the Setback Plan, a building Frontage may be Setback a maximum of five feet from the Lot Line.

4. A Large Format - Type I Building Type should be designed so that the ground-floor area is made up primarily of retail, office or work space, visible, in part to the public, through ground-floor windows.

H. Landscape

Front Yard space required by Setback requirements shall be hardscaped except that planters may provide privacy for office or work space.



Large Format Building Illustrative Photo



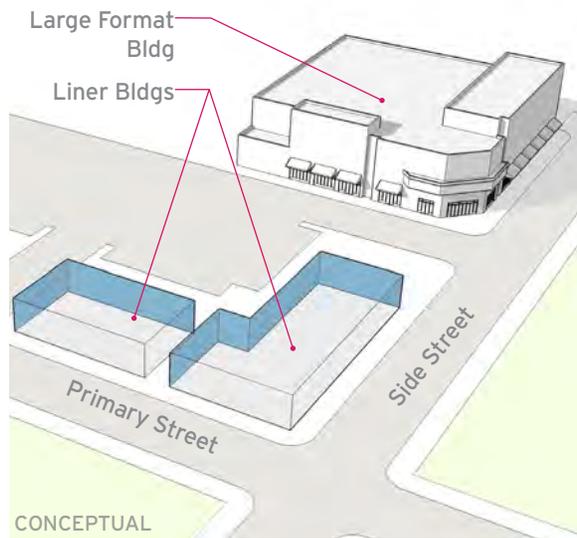


Figure 4.24 - Large Format Type II Conceptual Diagram

4.6.2.K Large Format - Type II

A. Intent Statement

The Large Format-Type II building accommodates large-footprint retailers at the interior of the Lot while providing Active Frontages along Primary Streets in the form of Liner buildings.

B. Lot Size

N/A

C. Building Height and Massing

1. Maximum Height: 5 stories.
2. The maximum anchor floor footprint is 60,000 gross square feet. The City may grant an exception to the maximum anchor floor footprint for cinemas, concert halls, or other live performance spaces during Substantial Conformance Review (Section 6.3.3.1.C). This maximum floor footprint does not include parking garages.
3. Façade Width: No limit

D. Access

1. Primary access to each ground-floor anchor is typically from the parking. Liner buildings may be either attached to the anchor, or detached and align the Primary Street. These shall be accessible directly from the Street and the parking and access shall occur at a maximum interval of 60 ft. All retail spaces along Streets should be accessed from a ground-floor, single-tenant entry along a Street, courtyard, or Alley.
2. Primary retail entrances along Street Frontages should remain accessible and unlocked during regular business hours.
3. In addition to the building's required Primary Entrances, there may be ancillary

entrances to the building from parking garages and areas.

4. Where any portion of the building fronts a Side Street, at least 50% of the ground floor must have entries or shop windows.

E. Parking and Service

1. There are no minimum parking requirements for this Building Type. However, adequate parking will be provided for commercial uses.
2. Optional parking is accommodated in an underground garage, surface lot behind the building, tuck-under, a mid-block shared above ground garage or a combination thereof. When provided, at least five percent of parking spaces shall be equipped for charging of electric vehicles, with signage limiting the use of those spaces to electric vehicles only. An electrical conduit shall be installed at the time of construction to facilitate the future installation of EV charging stations to at least 10 percent of parking spaces.
3. Parking lots may front the Large Format - Type II building provided that at least 70% of the perimeter of the parking is screened by Liner buildings that are accessible from the perimeter Streets.



4. Parking garages may exceed the maximum Height Story limit but may not exceed the Height of the adjacent habitable structures.
5. Where an Alley is present services, above ground equipment, and trash container areas shall be located on the Alley, when practicable.

F. Outdoor Space

N/A

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontages: Shopfront and Terrace.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3).
3. Where a Setback is not designated per the Setback Plan, a building street Frontage may be Setback a maximum of five feet from the Lot Line.
4. A Large Format - Type II Building Type as well as its associated Liner buildings should be designed so that the ground-floor areas are made up primarily of retail, office, or work space visible, in part, to the public through ground-floor windows.

H. Landscape

Front Yard space required by Setback requirements shall be hardscaped except that planters may provide privacy for office or work space.



Large Format Building Illustrative Photo

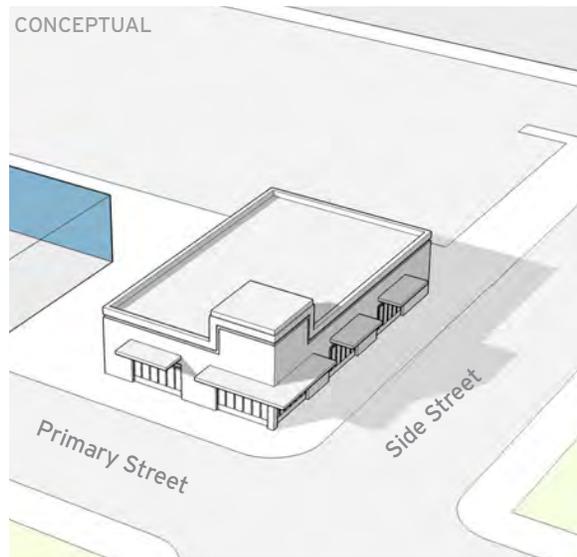


Figure 4.25 - Flex Block Conceptual Diagram

4.6.2.L Flex Block

A. Intent Statement

A small footprint freestanding building designed for commercial use on the ground floor. Flex Blocks are especially suitable as Liner buildings that screen surface parking from the Primary Street.

B. Lot Size

N/A

C. Building Height and Massing

1. Maximum Height: 3 Stories
2. A third Story may occupy up to 90% of the ground-floor footprint area.

D. Access

1. Primary access to ground-floor spaces shall be directly from the Street.
2. Access is also permitted from adjacent surface parking, but not in lieu of Street Frontage access .
3. Building entrances fronting Primary Streets and for non-retail uses Side Streets, should remain accessible and unlocked during regular business hours.

E. Parking and Service

1. There are no minimum parking requirements for this Building Type. However, adequate parking will be provided for commercial uses.
2. Optional parking is accommodated in an underground garage, surface lot behind the building, tuck-under, a mid-block shared above ground garage or a combination thereof. When provided, at least five percent of parking spaces shall be equipped for charging of electric vehicles, with signage limiting the use of those spaces to electric vehicles only. An electrical conduit shall be installed at the time of construction to facilitate the

future installation of EV charging stations to at least 10 percent of parking spaces.

3. Off-street parking shall be accessed through an Alley, when served by an Alley.
4. Where an Alley is present, services, utilities and trash container areas should be located on the Alley, when practicable.

F. Outdoor Space

N/A

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontages: Shopfront, Terrace, and Industrial Shop.
2. Front Setbacks shall comply with the Setback Plan (Section 4.3).
3. Where a Setback is not designated per the Setback Plan, a building Frontage may be setback a maximum of five feet from the Lot Line.

H. Landscape

Front Yard space required by Setback requirements, shall be hardscaped except that planters may provide privacy for office or work space.



Figure 4.26 - Flex Shed Conceptual Diagram

4.6.2.M Flex Shed

A. Intent Statement

A building designed for occupancy by light manufacturing, workshop, and warehouse uses.

B. Lot Size

N/A

C. Building Height and Massing

1. Maximum Height: 2 Stories

2. Maximum building width dimension along Primary Street Frontages: 200 ft.
3. Façades greater than 175 ft in length:
 - must have at least one Plane Break of at least 20 ft in length and 10 ft in depth and open to the sky or;
 - must have at least 30 ft of its length Setback at least five feet from the remainder of the Façade, be of a different material, and have at least one additional ground-floor entry.

D. Access

Primary access to ground-floor spaces shall be directly from the Street or from adjacent surface parking.

E. Parking and Service

1. There are no minimum parking requirements for this Building Type. However, adequate parking will be provided for commercial uses.
2. Optional parking is accommodated in an underground garage, surface lot behind the building, tuck-under, a mid-block shared above ground garage or a combination thereof. When provided, at least five percent of parking spaces shall be equipped for charging of electric vehicles, with signage limiting the use

- of those spaces to electric vehicles only. An electrical conduit shall be installed at the time of construction to facilitate the future installation of EV charging stations to at least 10 percent of parking spaces.
3. Parking lots should be screened from view from adjacent Streets with walls, fences, or hedges where an Alley is not present, parking entrances shall be located close to the side or rear of the Lot.

F. Outdoor Space

N/A

G. Frontage

1. Applicable Frontage Standards apply per Section 4.6.3. Permitted Frontages: Shopfront, Terrace and Industrial Shop
2. Front Setbacks shall comply with the Setback Plan (Section 4.3).
3. Where a Setback is not designated per the Setback Plan, a building Frontage may be setback a maximum of five feet from the Lot Line.

H. Landscape

Where optional outdoor space is provided it shall be landscaped with native or adaptive landscaping or hardscaped. Permeable paving is encouraged.

4.6.3 Frontage Standards and Guidelines

A building's Frontage is the interface between the public realm and private development. This Specific Plan recognizes that the successful design of this interface significantly contributes to the realization of an active and engaging urban environment.

Buildings within the Specific Plan Area should have ground-floor Frontages that are human-scaled, provide visual interest, and access to ground-floor uses. This section provides a palette of prototypical Frontage Types that are permitted. Standards include dimensional criteria, criteria for openings, as well as criteria for the ground plane immediately adjacent to the Frontage, such as minimum glazing (see Figure 4.27).



Explanation of Standards and Guidelines

The Building Type Standards and Guidelines cover the following:

A. Intent Statement

This statement describes the building-to-street relationship that each Frontage Type is meant to achieve.

B. Entries

These Standards and Guidelines address entries at the blockfronts, not those that are internal to the Lot.

C. Dimensions

Specific dimensions of features like massing, entry Height, openings, and Setbacks are delineated here.

D. Paving and Landscaping

These Standards and Guidelines addresses the area between the Lot Line and Building Face.

E. Furnishing Zone

These Standards and Guidelines address furnishing within front Setbacks.

F. Additional Standards and Guidelines

These Standards and Guidelines provide additional direction in shaping the appropriate building to Street relationship. They address glazing at the ground-floor, Frontages, and entries.

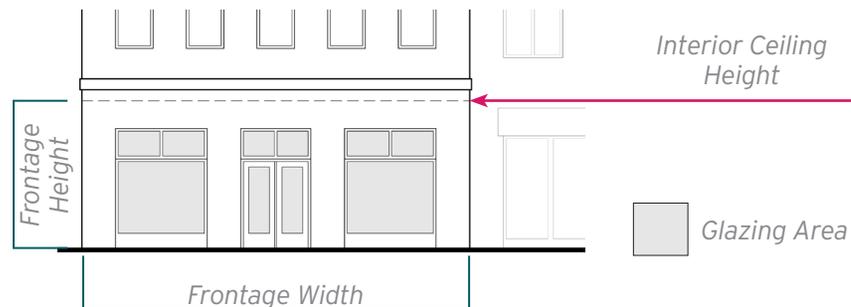


Figure 4.27 - Minimum Frontage Glazing Conceptual Diagram
The Frontage glazing area shall be measured from the finished floor to the bottom of ceiling of the ground floor.

4.6.3.A Yard/Porch



Yard/Porch Illustrative Photo



Figure 4.28 - Yard/Porch Conceptual Diagram

A. Intent Statement

Yards provide a physical transition from the sidewalk/public realm to the private residence. Porches are highly encouraged for community building by facilitating neighbor-to-neighbor interaction and visibility, and activating Street Frontages with residents that provide “eyes on the street.”

B. Entries

1. Single Family Dwellings should have Primary Entries accessible directly from the sidewalk/public realm.
2. When porches are incorporated, they shall be placed on the primary Façade and entrance of a dwelling.

C. Dimensions

1. Porch Width: 6 ft min.
2. Porch Depth: 6 ft min.

D. Paving and Landscaping

1. Yards shall be landscaped with native or adaptive landscaping, or hardscaped.
2. Walks shall be paved.

E. Furnishing Zone

N/A

F. Additional Standards and Guidelines

1. Porches may encroach into Front Yard Setbacks.
2. Optional fences, low walls, or hedges may be used to define Front Yard space and shall not exceed 36 inches in height.

4.6.3.B Stoop



Stoop Illustrative Photo



Figure 4.29 - Stoop Conceptual Diagram

A. Intent Statement

Stoops are elevated entry stairs, placed close to the Frontage line with the ground Story elevated from the sidewalk, securing privacy for the windows and front rooms. This type is suitable for ground-floor residential uses with small Setbacks and may be covered.

B. Entries

Where provided, Stoops shall be placed on the primary Façade and entrance of a dwelling.

C. Dimensions

1. Stoop Width: 4 ft min.
2. Stoop Depth: 4 ft min.

D. Paving and Landscaping

1. Yards shall be landscaped with native or adaptive landscaping, or hardscaped.
2. Walks shall be paved.

E. Furnishing Zone

N/A

F. Additional Standards and Guidelines

1. Stoops may encroach into Front Yard Setbacks.
2. Awnings, canopies, and shed roofs may cover Stoops.

4.6.3.C Shopfront



Shopfront Illustrative Photo



Figure 4.30 - Shopfront Conceptual Diagram

A. Intent Statement

Shopfronts provide direct access to ground-floor spaces that are located adjacent to the sidewalk. Shopfronts are typically associated with retail uses but may accommodate other uses. Where permitted, Shopfront Frontages may provide outdoor seating areas and outdoor displays.

B. Entries

Entries shall be set at the adjacent sidewalk or within an alcove that is adjacent to a sidewalk.

C. Dimensions

1. Shopfronts shall be between 12 to 25 ft high, measured from the finished floor to the bottom of the ceiling of the Shopfront space.
2. Shopfront spaces shall be at the Grade of the adjacent sidewalk at the Primary Entrance. Where grading conditions make this infeasible, Shopfronts may be raised no more than 18 inches in height, measured from the adjacent Sidewalk grade, provided there is ADA accessibility into the space and subject to the review and approval of the Zoning Administrator.

D. Paving and Landscaping

The area between the Lot Line and the Building Face shall be paved per Section 3.6.1.

E. Furnishing Zone

1. Where permitted, outdoor seating may be provided in front Setbacks.
2. Product displays (e.g. flowers, food, merchandise displays) are encouraged near Shopfront entries.

F. Additional Standards and Guidelines

1. At least 60% of the Shopfront Façade area at the ground floor shall be glazed. Glazing should be transparent and clear; opaque, highly reflective, and dark tinting are not permitted. The sill height of a Shopfront window shall be no more than 30 inches high measured from the adjacent finished sidewalk.
2. Unoccupied Shopfronts may be temporarily covered from the inside with white or light colored paper, fabric or film, which may contain a graphic image or otherwise permitted Signs.
3. The maximum length of blank walls facing the Street is limited to 15 horizontal feet for any one stretch of wall length.

4.6.3.D Terrace



Terrace Illustrative Photo



Figure 4.31 - Terrace Conceptual Diagram

A. Intent Statement

Terraces provide outdoor dining and seating for ground-floor commercial uses. Terraces also buffer residential use from the sidewalk and protect private Yard from public encroachment.

B. Entries

Terraces may be accessed from the building or directly from the adjacent sidewalk.

C. Dimensions

1. Terraces may be raised up to 3 feet above the adjacent sidewalk.
2. Terraces shall be at least 7 feet deep.
3. Terraces may be recessed from the Frontage line up to 8 feet.
4. The Terrace Frontage shall be designed to be subdivided into 30 foot wide bays for commercial leasing flexibility and functionality.

D. Paving and Landscaping

N/A

E. Furnishing Zone

1. Outdoor furniture on Terraces should be durable.
2. Terraces should be clearly delineated with permeable fencing, low walls or landscaping.

F. Additional Standards and Guidelines

1. Terraces must be Setback sufficiently from the curb to accommodate the pedestrian right-of-way and Street trees.
2. Terraces may encroach into required Setbacks.
3. Awnings, signs, etc. shall be located at least 8 feet above the terrace floor level measured from the Grade of the Terrace.
4. ADA accessibility standards apply.

4.6.3.E Industrial Shop



Industrial Shop Illustrative Photo

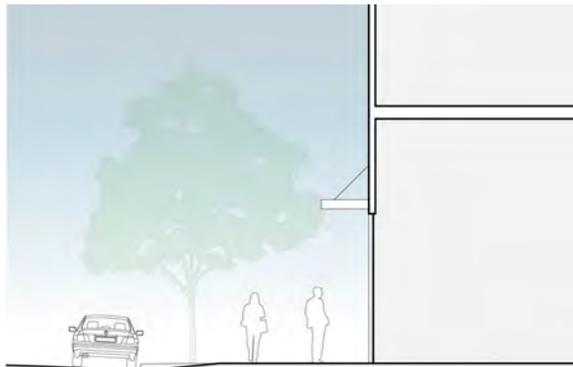


Figure 4.32 - Conceptual Industrial Shop Diagram

A. Intent Statement

Industrial Shop Frontages are intended for urban or industrial settings where large street-facing openings are needed or desired.

B. Entries

Industrial Shops may be accessed from the building or directly from the adjacent sidewalk.

C. Dimensions

1. Industrial Shops may be raised up to 3 feet above the adjacent sidewalk; except when used as part of a Flex Loft Building Type, in such cases they must be within 12 inches of Grade.
2. Industrial Shops shall be at least 7 feet deep.
3. Industrial Shops may be recessed from the Frontage line up to 8 feet.

D. Paving and Landscaping

Industrial Shops on Grade with the sidewalk shall be hardscaped with a paving palette consistent with the overall streetscape.

E. Furnishing Zone

Outdoor furniture should be durable.

F. Additional Standards and Guidelines

N/A

4.6.3.F Mews



Mews Illustrative Photo



Figure 4.33 - Mews Conceptual Diagram

A. Intent Statement

Mews Frontages provide access to buildings at the interior lots of an Auto Court that cannot directly be accessed from a Thoroughfare, Common Walkway or Open Space.

B. Entries

Mews entries must have distinct pedestrian and garage entries.

C. Dimensions

N/A

D. Paving and Landscaping

1. Driveway aprons shall be paved with poured concrete.
2. Walks shall be paved.
3. Areas between the Driveway apron and pedestrian entry walk may be landscaped with native or adaptive landscaping, hardscaped, or paved with porous paving materials.

E. Furnishing Zone

N/A

F. Additional Standards and Guidelines

N/A

4.7 Architectural Standards and Guidelines

The Architectural Standards and Guidelines of this section apply to all development in the Specific Plan Area. They address the composition of buildings as well as functional aspects of building, parking, and outdoor space design. The goal of this section is to ensure that development within Campus Town is consistent with the goal of creating a human-scale mixed-use environment in which each individual building furthers the overall Specific Plan vision.

The images in this section are for illustrative purposes, only provided to illustrate intent.



Illustrative Photo

4.7.1 Building Massing, Scale, and Architecture

The massing, scale, and Architectural Style of proposed buildings in the Specific Plan Area should be varied to create a unique and attractive Campus Town and avoid a uniform and monotonous urban form. By incorporating a diversity of building scales and massing, the Specific Plan Area should appear as a neighborhood that grows over time and is blended with the surrounding neighborhood.

Buildings should have Fenestration that establishes a clear pattern on the Façade (with special attention paid to Façades that are visible from a public Street) and that provides depth and additional articulation. The design of all buildings should be of a quality and character that improves community appearance. Buildings should be composed of a variety of forms and contrasting shapes and should employ attractive and complementary building materials and architectural features.

Variation in Building Mass, Material, and Color create a Sense of Depth and Visual Interest

In general, the overall scale, massing, roof form, materials, and architectural style of new structures should provide a variety of forms, depth and texture, and encourage a cohesive neighborhood character by building new structures at a scale that is appropriate to the Street and the surrounding neighborhood context. Building massing should include a variation in wall planes and Height as well as roof forms to reduce the perceived scale of the building.

The architecture of the building should clearly delineate an Architectural Style, and should not appear as a simplified version thereof, with appropriate Fenestration patterns, architectural features, proportions, and material consistent with the style.

The specific criteria included throughout these Architectural Standards and Guidelines have been included to achieve a consistent design and architectural vision with respect to the general massing, scale and architectural criteria.

4.7.2 Building Composition

Base, Middle, and Top. Building design concepts of human scale references, such as base, middle, and top, should be incorporated to reduce perceived building mass. The tripartite distinction can be achieved through massing, Fenestration, and materials.

1. The building base should “ground” the building and activate the Street through Active Frontages supported by generous windows, building entrances, and outdoor spaces at the Street level.
2. The building middle should define the primary Façade and should be differentiated from the base and top through materials, color, or offsets of building volume.
3. The building top should define the roofline.
4. Architectural elements and techniques should be employed to articulate the Building Façade to express the divisions between the base, middle and top, reduce the building mass, and strengthen the building character and identity.



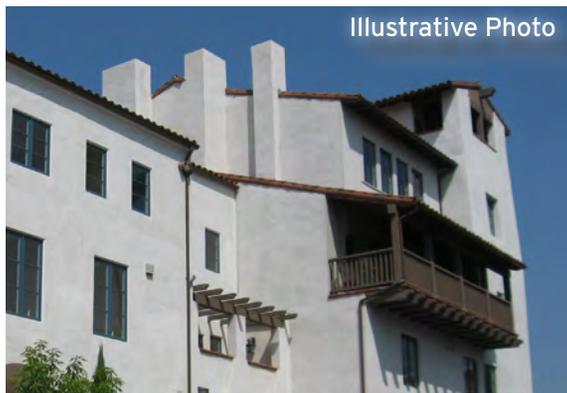
Illustrative Photo

The Ground-Floor Pedestrian Zone at the Base, the Upper Stories with Balconies in the Middle, and the Window Differentiation at the Top Articulate a Tripartite Building Composition

4.7.3 Roof Guidelines

1. When used, “Cool Roofs” should be designed using white or other lightly colored surface to reflect the sun and reduce building temperatures and need for cooling.
2. Roof forms and materials that complement the character of the building design should be used.
3. The roof edge should be defined with a parapet, cornice, overhang, or some other architectural element.
4. Roofline should be broken up by varying the height of building segments of long buildings to create a more human scale.
5. Green roofs should be considered for their ability to treat stormwater, as well as to provide a visual amenity.
6. Rooftop mechanical equipment should be clustered away from the edge of the building and behind a parapet wall or within an enclosure so as not to be visible from the Street and improve building appearance from surrounding taller buildings.
7. Rooftop equipment not within an enclosure should be painted to match the rooftop, if feasible.

8. Unenclosed shade structures, and similar amenities should be considered to encourage rooftop use and to provide visual relief. Shade structures should not exceed 16 feet in height from the roof deck floor level and do not count as a Story.
9. Rooftop amenities such as swimming pools are permitted; rooftop amenities do not count as a Story.
10. Hotels are encouraged to provide publicly accessible rooftops, such as sitting areas, restaurants, bars and similar amenities.



Illustrative Photo

Architectural Elements break up the Mass of the Building and Reinforce the Human Scale

4.7.4 Building Façades

1. “Human scale” proportions and architectural building details that emphasize and reflect the presence and importance of people are encouraged.
2. Massing offsets, Fenestration, varied textures, openings, recesses, and design accents are encouraged to avoid unarticulated walls and monolithic roof forms.
3. Blank walls (defined as having no Active Frontage, glazing, or doorway) should be limited to 20% of the building Façade, but in no case exceeding 40 feet.
4. Architectural elements such as Stepbacks, overhangs, balconies, verandas, and porches that add architectural character are encouraged.
5. Buildings are encouraged to use shade and shadow by reveals, surface changes, overhangs and/or sunshades to provide visual interest on Façades exposed to the sun.
6. One-Story architectural elements and massing should be incorporated into two and three Story building designs.



Illustrative Photo

Roofline Articulation and Green Roof



Illustrative Photo

Roof Form and Materials Complement the Architectural Character of the Building

4.7.5 Colors and Materials

1. Changes of exterior color, texture, or material should be accompanied by changes in plane so that buildings appear substantial and integral.
2. Color and material changes at the outside of corners of a building or plane change that give a thin veneer appearance should be avoided.
3. Quality materials and detailing on the base of the building that are durable, rich in color and texture, and enhance the pedestrian experience should be utilized.
4. Materials that have demonstrated their durability in similar climates, sustainably manufactured, harvested, and/or sourced should be considered.



4.7.6 Entrances

1. The Primary Entrance to buildings should be oriented to the Street front, rather than to the parking lot, Alley, or interior of Lot.
2. Buildings with long Facades are encouraged to provide frequent building entrances along the Street.
3. Side or rear building entrances should always be accompanied by a front, street-facing entrance.
4. On corner Lots, residential lobby entries should be located on the Side Street of the two Streets, where practical, to allow uninterrupted retail on the Primary Street.
5. Residential entries should be well marked and easy to find. Entry doors should be recessed to articulate the entrance, add depth to the Façade, and ensure that doors do not swing into the Sidewalk.
6. Residential and/or hotel entries for pedestrians may be accessed from courtyards, which provide additional richness to the streetscape experience.
7. Special paving and landscaping should be included at entrances to enhance the overall building design.

Complementary Colors and Materials Enhance the Building Mass



Easily-identifiable Pedestrian Entries to Buildings, whether to Shopfronts or Residences, Promote Walkability and Enhance the Streetscape

4.7.7 Shopfronts

1. Shopfronts may have special paving to differentiate the building entrance, but should be designed with primary consideration of the public right-of-way.
2. Shopfronts should provide large windows and window display boxes along ground floor commercial spaces to activate the Street and allow pedestrians to view the merchandise for sale, restaurant interiors, dance classes, art galleries, artists at work, etc.
3. Where multiple retail tenants occupy one building, the same height of awnings across and entire building should be maintained.
4. Building or Shopfront entries along the ground floor should be at regular intervals if feasible. Ideally, Shopfronts should be designed to be subdivided at 30 foot intervals for commercial leasing flexibility and functionality and to create a fine-grained rhythm along the Street.
5. Shopfronts should avoid blank walls on large tenant spaces by lining Street Frontage with smaller, in-line retail shops on either side of a prominent entry for the large tenant.
6. A transition between Shopfronts with a defined edge treatment, such as a change in plane, column, or a vertical trim

element between Shopfronts should be provided.

7. Multiple entrances should be incorporated for large stores along a Street front whose length spans the width of more than two typical shops (2 X 30 ft), or which front on more than one Street, to enliven the Street by providing more access opportunities.
8. Rather than shallow window box displays, at least 65% of a retail Frontage should include transparent glazing and at least 70% of the glazing should allow views into the store.
9. The base below windows should be between 18-30 inches in height to protect glazing from foot traffic while maintaining the characteristic of height and openness of the glazing.
10. Transoms should be incorporated above entry doors to accommodate business addresses.
11. Doors and entryways to stores should be recessed to articulate the entrance, add depth to the Façade, and ensure that doors do not swing into the Sidewalk.
12. Commercial grade entry doors with clear glazing framed in metal or wood should be used.

13. Shopfronts should use awnings, canopies, architectural lighting, and pedestrian signage to articulate shop entrances.
14. Shopfront materials and colors that complement the overall building and adjoining Shopfronts should be selected.



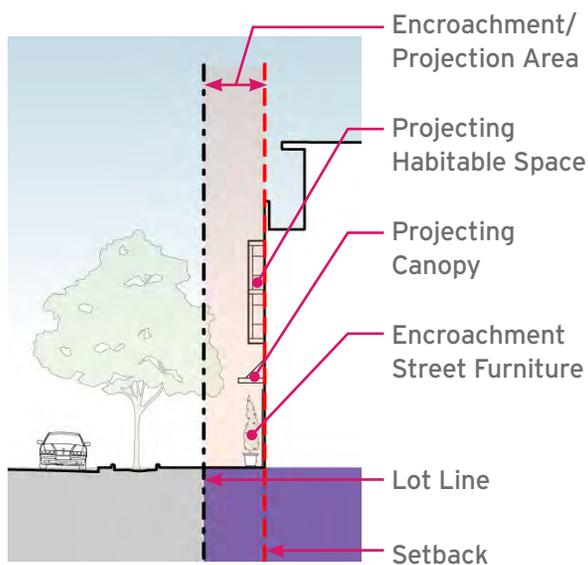
Architecturally-consistent Detailing from Signage and Lighting Fixtures to Colors and Materials enhance the Character of the Built Environment

4.7.8 Encroachments and Projections

The following are the permitted Encroachments and/or Projections into the public right-of-way and/or Setback as indicated:

1. Awnings and canopies may project up to eight feet or 33% of the distance between the Building Face and the curb, whichever is less, with the exception of a canopy projecting over the Primary Entrance of a multi-family residential lobby of no greater than eight feet in width, which may project to the curb line. Support for the canopy's structure in the form of two posts no greater than four inches in diameter may be provided at least two feet from the back of the curb.
2. Minimum vertical clearance for awnings and canopies shall be eight feet if it is removable or retractable and 12 ft if it is fixed or permanent.
3. Awnings should not obscure storefront Signage.
4. Canvas and high-quality fabric should be used, rather than vinyl or other materials.
5. No individual Habitable Encroaching Space shall exceed 15 ft in length.

6. Habitable Projecting Space/ Habitable Encroaching Space may project up to four feet from the Building Face but shall not extend over the Lot Line, unless noted otherwise per a Building Type's standards.
7. Minimum vertical clearance of habitable Projecting Spaces above the ground floor shall be 21 ft from the Sidewalk Grade on Shopfront and nine feet on other Frontage Types.



The Encroachment/Projection area is dependent on Building Type and Setback requirements.

8. Non Habitable Projecting Space/ Non Habitable Encroaching Space shall extend no more than six feet from the Building Face into the public right-of-way.
9. Minimum vertical clearance of Projections is nine feet from Sidewalk Grade.
10. Balconies are encouraged on Buildings facing major public spaces such as Parks, Playgrounds, and Plazas.
11. Balconies are permitted on internal courtyard spaces.
12. Recessed balconies should be used occasionally, so as not to be a dominant accoutrement.
13. Residential balconies should be designed to have a minimum occupiable depth of at least five feet and may encroach into the Front Yard Setback up to eight feet from a Building Face, but shall not encroach within two feet from the Lot Line.
14. Balcony railings should be as transparent as feasible.
15. Stoops may encroach into the Front Yard Setback up to eight feet from a Building Face, but shall not extend beyond the Lot Line.
16. Street furniture such as outdoor seating, product displays, and portable Signs may encroach into Setbacks.

4.7.9 Passageways

1. Common Walkways should be introduced to increase access within and across the Blocks.
2. Common Walkways may be open or roofed, and may go between or through buildings, to courtyards, parking areas or public Open Spaces.



Passageways Increase Connectivity and Promote Walkability



Balconies add Variety, Amenity, and Architectural Character



Pocket Parks Can Provide Shade and Common Walkways through Urban Blocks

4.7.10 Windows

1. Windows should overlook public areas to allow for increased safety.
2. Highly-reflective, mirrored, heavily-tinted and opaque glazing are not permitted (except that opaque glazing may be used as spandrel glass). Window glazing should be transparent with clear or limited UV tint so as to provide views to and from the inside of the building and the Street.
3. Outer surface of window frames facing the public realm set within masonry, stucco or simulated masonry or stucco walls shall be recessed from the wall or trim surface by at least two inches.
4. Regardless of architectural style, windows should be located in such a way so as to help avoid blank walls.
5. If exterior shutters are used, they should be sized and mounted appropriately to fit the window.
6. Windows and related architectural treatments should be designed, arranged and sized to be appropriate in style, scale, proportion and purpose to the overall architectural form.
7. When architecturally appropriate, window openings should be recessed so that they are not flush with exterior walls, to create a sense of depth and shadow along the streetwall.

8. Glazing that has limited UV tinted glazing should be used so as to provide views into the building from the street.
9. Upper-story windows should be designed to be operable. Typically, upper-story windows should be smaller than ground floor windows.
10. Lintels, transoms, sills, shutters, trim detailing and mullions should be considered to enhance window elements.

4.7.11 Private Open Space

1. Outdoor dining areas should have special paving and/or a row of planters, bollards or a permeable fence to delineate the dining space.
2. Interior courtyards should include seating and planting areas. Low walls and steps may be used as alternative forms of seating.
3. Interior courtyard landscaping should include shade trees or shading devices, where space permits.
4. Lighting should be provided that illuminates the courtyard, but does not negatively impact surrounding buildings.
5. Blank walls should be avoided inside the perimeter of the courtyard.



Illustrative Photo

Interior Courtyards and Shared Open Spaces should be Activated with Entries and Windows



Illustrative Photo

4.7.12 Ventilation

1. Air ventilation from outdoors is encouraged to improve indoor air quality for occupant comfort and wellbeing.
2. Windows, vents, and courtyards should be placed and oriented to enhance cross-ventilation and cooling.
3. Operable transom windows are highly encouraged.



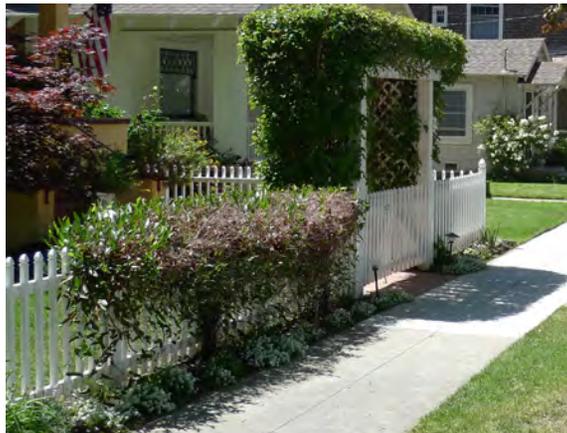
Illustrative Photo

Operable Transom Windows enhance Ventilation

4.7.13 Garden Walls, Perimeter Walls, Hedges, and Fences

1. Garden walls, perimeter walls, hedges, and fences may be used to define the edge between adjoining private properties. Walls, hedges, and fences facing the public Street shall also comply with the Frontage Type standards (see Section 4.6.3).
2. No fence, wall, or hedge shall exceed three feet in height in Front Yards and Street facing side Yards or six feet in height in rear Yards.
3. Garden walls, perimeter walls, hedges, and fences shall be built at least 18 inches from the Lot Line, to allow room for footings and planting.
4. Walls and fences shall not be used at Shopfronts, except where they are necessary to accommodate grade changes.
5. Solid perimeter walls should be constructed of high quality enduring construction materials such as masonry or ornamental metal. Concrete block and interlocking concrete pavers (such as keystone) are not permitted when visible from the Street.
6. No plastic, vinyl or chainlink fencing shall be permitted in Front Yards or Street facing Side Yards.

7. In general, fences, walls, and hedges should complement the architecture of the building that they enclose and be compatible with the land use intensity. For example, residential uses should incorporate a softer texture of enclosure such as wood fences and landscaped hedges, whereas commercial buildings should use masonry or concrete walls.
8. Walls and fences should be architecturally enhanced and complemented by adjoining landscaping. Tiered planting should be provided adjacent to perimeter walls to soften their appearance from surrounding areas.



Illustrative Photo

Fences and Walls should be Architecturally Complementary to the Buildings it encloses



4.7.14 Retaining Walls

1. No individual wall shall exceed three feet in height in Front Yards. Retaining wall height is defined as the vertical distance from the finished grade on the high side to the finished grade on the low side of the wall.
2. Other than a retaining wall at the perimeter of the Specific Plan Area, an individual wall in or along a street-facing Yard shall not exceed six feet in Height unless the design is approved as part of the Substantial Conformance Review provided in Chapter 6, Implementation, in which case the retaining wall shall not exceed ten feet in Height. Minor Deviations and Deviations exceeding these limits may be authorized as provided in Chapter 6, Implementation.
3. An individual wall at the perimeter of the Specific Plan Area shall not exceed six feet in Height unless the design is approved as part of the Substantial Conformance Review provided in Chapter 6, Implementation, in which case the perimeter retaining wall shall not exceed 14 feet in Height. Minor Deviations and Deviations exceeding these limits may be authorized as provided in Chapter 6, Implementation.

4. Retaining walls should be masonry, stone, or finished concrete when they are visible from the Street. Concrete block and interlocking concrete pavers (such as keystone) are not permitted when visible from the Street.
5. Wall heights can be up to six feet in areas that do not have street frontage. This standard does not include perimeter walls, which are addressed in Section 4.7.13.
6. Retaining walls may be placed on the Lot Line. Fencing up to six feet in height (unless other Standards dictate in the Thoroughfare Standards) may be placed on top of the wall, and does not factor into the overall height of the retaining wall.

7. Down-turned or up-turned foundations do not qualify as retaining walls.
8. Where multiple retaining walls are proposed in a series, the combined retaining heights shall not exceed the individual wall heights described above.
9. Project developer(s) shall make reasonable efforts to minimize retaining wall usage and height.

4.7.15 Parking Area Standards

1. Parking requirements are determined by Building Type, see Section 4.6.2.
2. Surface parking areas.
 - a. Surface parking areas with two rows of parking or fewer shall be landscaped with a minimum of one tree per 20 spaces with a minimum of one landscaped island (between spaces) for every 10 spaces.
 - b. Surface parking areas with more than two rows of parking shall include a landscaped median for every other row of parking that extends the entire length of the row. The landscaped median shall be planted with large shade trees at least every 45 feet and include a Common Walkway for access to and from the parked vehicles. Where landscaped medians are



Front Yard Retaining Walls shall not exceed three feet

present, landscaped islands shall also be placed a minimum of every 15 spaces.

- c. Surface parking areas covered in solar panels are encouraged and are not subject to minimum tree planting standards stated above.
- 3. When unrequired parking spaces are provided, at least five percent of parking spaces shall be equipped for charging of electric vehicles, with signage limiting the use of those spaces to electric vehicles only. An electrical conduit shall also be installed at the time of construction to facilitate the future installation of EV charging stations to at least 10 percent of parking spaces.
- 4. Permeable surfaces for parking and maneuvering areas are encouraged. Stormwater requirements may identify the need for permeable surfaces. Permitted permeable surfaces include: pervious concrete and pervious pavers. Other permeable surfaces may be permitted during Substantial Conformance Review (Section 6.3.3.1.C).
- 5. Parking areas shall be accessed from Alleys, where feasible. Where Alleys do not exist, parking areas may be accessed from Side Streets or narrow Driveways that are perpendicular to the Street.

4.7.16 Service and Auxiliary Equipment

1. Service, utility, and mechanical functions, including retail loading, shall be located in Alleys whenever present. When Alleys are not present, service, utility and mechanical functions shall be placed behind buildings and provisions for access shall be made.
2. Service, utility, and mechanical equipment that is visible from the Street shall be screened from view with landscaping or enclosures. Backflow preventers and fire standpipes, along with utility box transformers shall be screened.
3. All screening devices should be compatible with the architecture, materials and colors of adjacent buildings.
4. Chain link fencing with slats or mesh screen and unpainted wood are discouraged.
5. Trash areas that are visible from public Streets or other properties shall be enclosed by walls. Trash area entrances shall be enclosed by a door.
6. Service enclosures and retail loading areas should be sited to minimize nuisance to adjacent properties.
7. Roof vent penetrations and mechanical equipment should be located at least 10 feet from any exterior Building Face.



Trash Enclosures and Utilities should be Screened from View



Illustrative Photo

4.7.17 Architectural Lighting

1. Lighting should encourage a pedestrian-friendly environment and enhance both community safety and business exposure.
2. Lighting on buildings should be oriented to pedestrians in terms of scale, design, and location.
3. All exterior lighting shall use full cutoff luminaires and be directed toward the areas to be lit to limit spill-over onto off-site uses.
4. Light quality should not be harsh, glaring, blinking or shed beyond Lot Lines.
5. Building lighting may include low-level exterior lights adjacent to buildings and along pathways for security and wayfinding purposes and low-level accent lighting features and landscape elements.
6. Alleys shall have lights mounted on outbuildings or garages.
7. Lights should use LED and other technologies to maximize energy efficiency.
8. High-pressure sodium lights are prohibited.



Lighting should be Architecturally Compatible

4.8 Sign Standards and Guidelines

The Campus Town Specific Plan Sign Standards and Guidelines constitute the applicable development standards and regulations for signage within the Campus Town Specific Plan Area. The provisions in the SMC regarding Signs apply only to matters not covered by this Specific Plan Chapter 4.8. If there is any conflict or inconsistency between this chapter of the Specific Plan and the SMC regarding Signs, the provisions of this Specific Plan shall take precedence.

The intent of the Sign Standards and Guidelines is to enhance the pedestrian experience in Campus Town, prevent visual clutter, and promote successful Sign design that contributes to the area’s economic health.

4.8.1 Special Sign Districts

This Specific Plan recognizes that the urban environment envisioned for Campus Town is unique with the context of Seaside. In order to accommodate Signs that may be appropriate for portions of the new Campus Town, this Specific Plan provides for two special Sign districts: Sign District 1: Village Centers and Sign District 2: Campus Town General.

1. Sign District 1: Village Centers
The mix of uses that will populate the village centers, Sub-Areas Commercial Center and University Village, will also accommodate a greater variety of Sign types. These centers will be definitively multi-modal in nature and therefore, the expanded variety of permitted Sign types will appropriately address the intended viewer while exhibiting a coherent and unified aesthetic.
2. Sign District 2: Campus Town General
Campus Town General encompasses the remaining portions of the Specific Plan Area not within the village centers. While mostly residential, there are opportunities for commercial development and the Sign types permitted within Sign District 2 shall respect the residential nature of its surroundings.

Sign Type	Sign District	
	1	2
Awning Sign	✓	✓
Banner Sign	✓	✓
Channel Letter Sign	✓	✓
Inflatable Sign	✓ ⁽¹⁾	
Marquee	✓	
Monument Sign	✓	✓
Neon Sign	✓	
Parapet/Elevated Sign	✓	✓ ⁽³⁾
Pole Sign	✓	
Portable Sign	✓	
Projecting Sign	✓	✓
Shingle Sign	✓	✓
Wall Sign	✓	✓
Window Foil	✓ ⁽²⁾	
Window Graphic	✓	

✓ = Permitted; ✓^(x) = Permitted with conditions

Notes:
 (1) Inflatable Signs are only permitted for temporary installations.
 (2) Window foil Signs may not obscure the majority of the glazing of a storefront and prevent visibility into the interior. Window foils may completely cover storefront glazing when the storefront is vacant.
 (3) Parapet/Elevated Signs are only permitted within Special District 1 of the West End Sub-Area within Sign District 2.

Table 4.4 - Permitted Sign Types



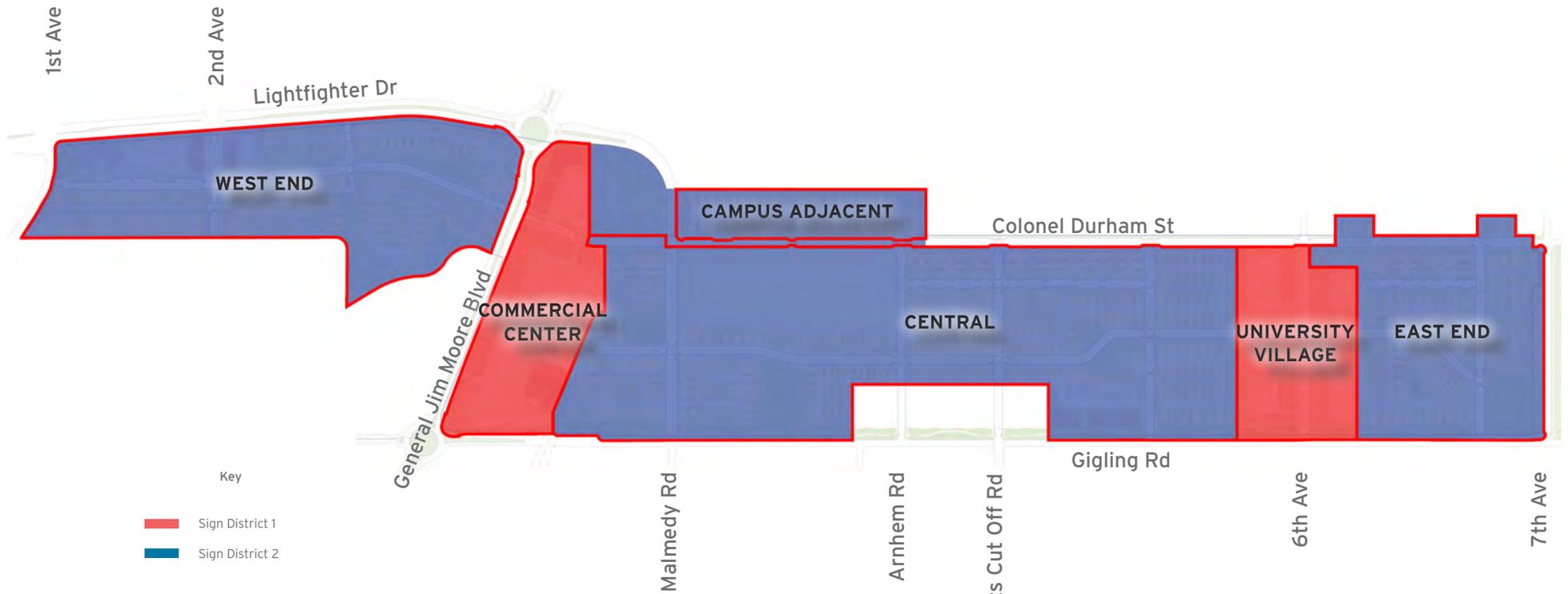


Figure 4.34 - Sign Districts Plan



4.8.2 Sign Type Standards

1. Awning Signs. Any sign painted on, or applied to, an awning.
 - a. Limitation in Number. One Sign per awning.
 - b. Maximum Area. Awning Signs shall not exceed one line of text.
 - c. Restrictions, Additions, Clarifications, and Exceptions. Only non-illuminated or externally illuminated letters shall be used.
2. Banner Signs. Any cloth, bunting, plastic, paper, or similar non-rigid material attached to any structure, staff, pole, rope, wire, or framing that is anchored on two or more edges. Banner Signs are temporary in nature, and do not include flags.
 - a. Limitation in Number. One Sign per building. Two Signs per light pole.
 - b. Maximum Area. Banner signs shall not exceed 8 square feet.
 - c. Restrictions, Additions, Clarifications, and Exceptions. Banner Signs affixed to an exterior building Facade shall not extend above the roofline.
3. Channel Letter Signs. A Sign consisting of fabricated or formed three-dimensional letters, individually applied to the wall, which may accommodate a light source.
 - a. Limitation in Number. One Sign per street Frontage for each ground-floor tenant not to exceed two Frontages. An additional building identification Sign per street Frontage is allowed.
 - b. Maximum Area. The greater of 30 square feet, or two square feet of Sign area for each lineal foot of building or tenant Frontage, not to exceed 100 square feet in area.
 - c. Restrictions, Additions, Clarifications, and Exceptions. Light fixtures may be used for direct illumination of Channel Letter signs.
4. Inflatable Signs. A Sign that is an air-inflated object, which may be of various shapes, made of flexible fabric, resting on the ground or structure, and equipped with a portable blower motor that provides air flow into the device.
 - a. Limitation in Number. One Sign per each ground-floor tenant.
 - b. Restrictions, Additions, Clarifications, and Exceptions. A four foot clearance along the right-of-way must be maintained when locating the Sign on the sidewalk. Inflatable Signs may only be used by each tenant for up to five consecutive days, twice a year.
5. Marquee. A Sign that is attached to or otherwise made part of a permanent roof-like structure that projects beyond the building wall.
 - a. Limitation in Number. One Marquee per street Frontage not to exceed two Frontages.
 - b. Restrictions, Additions, Clarifications, and Exceptions. Marquees may project to the curb line.
6. Monument Signs. An independent, freestanding structure supported on the ground having a solid base as opposed to being supported by poles or open braces.
 - a. Limitation in Number. None.
 - b. Maximum Area. Monument Signs may not exceed 100 square feet.
 - c. Restrictions, Additions, Clarifications, and Exceptions. None.



7. Neon Signs. A Sign illuminated by a neon tube, or other visible light-emanating gas tube, that is bent to form letters, symbols, or other graphics.
 - a. Limitation in Number. One Sign per Street Frontage for each ground-floor tenant not to exceed two Frontages. An additional building identification Sign per street Frontage is allowed.
 - b. Maximum Area. Neon Signs may not exceed 8 square feet.
 - c. Restrictions, Additions, Clarifications, and Exceptions. Neon signs may not flash unless incorporated into a Marquee sign.
8. Parapet/Elevated Signs. Any Sign attached or applied to the face of a building above the highest occupied floor.
 - a. Limitation in Number. One Sign per Street Frontage.
 - b. Maximum Area. Parapet/Elevated Signs may not exceed 100 square feet in area.
 - c. Restrictions, Additions, Clarifications and Exceptions. The specific Sign type, (ie. Wall Sign, Channel Letter Sign, etc.) must be permitted in the applicable Sign District for use as a Parapet/Elevated Sign.
9. Pole Signs. An elevated, freestanding Sign, typically supported by one or two poles.
 - a. Limitation in Number. One Sign per Street Frontage not to exceed two Frontages.
 - b. Maximum Area. Pole signs may not exceed 8 square feet.
 - c. Restrictions, Additions, Clarifications, and Exceptions. Light fixtures may be used for direct illumination of Pole Signs. Pole Signs may not exceed 8 feet in height measured from the ground plane.
10. Portable Signs. A Sign that is not permanently affixed to a structure or the ground.
 - a. Limitation in Number. One double-sided Sign is permitted per each ground-floor tenant to be located adjacent to the store Frontage.
 - b. Maximum Area. Portable Signs may not exceed 8 square feet in area per face.
 - c. Restrictions, Additions, Clarifications and Exceptions. A four foot clearance along the right-of-way must be maintained when locating the Sign on the sidewalk. Portable Signs must be removed and stored indoors during non-business hours.
11. Projecting Signs. A Sign, other than a Wall Sign, which is suspended from, or supported by, a structure and projecting outward.
 - a. Limitation in Number. One double-sided Sign is permitted per each ground-floor tenant to be located adjacent to the store Frontage. One Projecting Sign is permitted per each second-floor tenant.
 - b. Maximum Area. Projecting Signs may not exceed 8 square feet in area.
 - c. Restrictions, Additions, Clarifications and Exceptions. Light fixtures may be used for direct illumination of Projecting Signs. Projecting Signs located on the second-floor must project off the building at a 90-degree angle.
12. Shingle Signs. A Sign that hangs from a projecting structure that is perpendicular to the face of the building.
 - a. Limitation in Number. One Sign per Street Frontage for each ground-floor tenant to be located adjacent to the store Frontage not to exceed two Frontages.



- b. Maximum Area. Shingle Signs may not exceed 8 square feet in area.
 - c. Restrictions, Additions, Clarifications and Exceptions. Light fixtures may be used for direct illumination of Shingle Signs.
13. Wall Signs. A Sign that is attached to or painted on the exterior wall of a structure with the display surface of the Sign approximately parallel to the building wall, and that does not project more than 12 inches from the wall.
- a. Limitation in Number. One wall Sign per street Frontage for each ground-floor tenant not to exceed two Frontages. An additional building identification Sign per Street Frontage is allowed.
 - b. Maximum Area. The greater of 30 square feet, or two square feet of Sign area for each lineal foot of building or tenant Frontage, not to exceed 100 square feet in area.
 - c. Restrictions, Additions, Clarifications, and Exceptions. Light fixtures may be used for direct illumination of Wall Signs.
14. Window Foil Signs. Any Sign that is applied or attached to a window that obscures visibility into the building.
- a. Limitation in Number. Window Foil Signs shall not cover more than half of Street fronting glazing.
 - b. Restrictions, Additions, Clarifications, and Exceptions. Window Foil Signs may completely cover storefront glazing when the storefront is vacant.
15. Window Graphic Signs. A Sign that is applied, painted, or affixed to a window, facing the outside of the building whose text, symbols, or graphics are individually applied to the window.
- a. Limitation in Number. Two window graphics per Street Frontage not to exceed two Frontages.
 - b. Maximum Area. The greater of 30 square feet, or two square feet of sign area for each lineal foot of building or tenant Frontage, not to exceed 100 square feet in area.

“Ensure wayfinding signage is clear and readable to the intended audience (i.e. pedestrians, cyclists, equestrians and motorists).”

Regional Urban Design Guidelines



4.8.3 Additional Sign Standards

1. Prohibited Sign Types

The following Sign types are not allowed within the Campus Town area:

- a. Cabinet Signs. A Sign with its text and/or logo symbols and artwork on a translucent face panel that is mounted within a metal frame or cabinet that contains the lighting fixtures which illuminate the Sign face from behind.
- b. Flashing Light Sign. A sign that contains an intermittent or sequential flashing light source and that exhibits changes in light, color, direction, or animation.
- c. LED Signs. Any Sign that utilizes LEDs (light emitting diodes) as pixels.
- d. Revolving Signs. A Sign that revolves in a circular motion, rather than remaining stationary on its supporting structure.

2. Residential Signage

- a. Limitation in Number. One non-illuminated Identification Sign located above the building entrance is permitted for residential buildings.
 - b. Maximum Area. Identification Signs may not exceed 40 square feet in area.
 - c. Restrictions, Additions, Clarifications, and Exceptions. The Sign may not project above the roofline of the building to which the Sign is attached. Mixed-use buildings with residential units are included in this Standard.
3. Temporary Signs advertising model homes are permitted within both Sign Districts.

4.8.4 Sign Guidelines

1. General

- a. Signs should be of a character and scale that relates to the pedestrian.
- b. Signs should be conceived as an integral part of the design so as not to appear as an afterthought application.
- c. The location, size, and appearance of building identification signs should complement the building and overall character of the Campus Town.
- d. Signs should be located and designed for maximum visibility and legibility.
- e. Signs should generally face the centerline of the Street or the direction of pedestrian traffic.
- f. Signs should exhibit quality and contribute to the character of the Specific Plan Area.
- g. Illuminated Signs should limit glare upon adjacent properties, sensitive uses, and Roadways.



2. Colors and Design

- a. Colors should be selected that enhance Sign legibility taking into consideration the color of the building wall or awning to which the Sign is to be attached. Dark letters on light colored background and light colored letters on dark backgrounds are encouraged.
- b. Sign colors should be selected that complement the colors of the building and related architectural features. Sign colors and finishes should be compatible with the Building as a whole. Self-illuminated Signs that emit light from within themselves are discouraged.

3. Guidelines for Ground-Floor Tenants

- a. Signs should be placed in locations that complement the building's architectural design. The rhythm of Storefronts and openings should be considered.
- b. Primary signing opportunities on a building, awning, and canopy should be reserved for the identification of the business name, logo, or both.
- c. Secondary signing opportunities on a building and shop windows should be reserved for identification of business products and services

offered on the premises, when such identification is desired. Such service and product identification should use a smaller font than in the primary business identification signing.

- d. Hours of operation and other operational information important to shoppers should be considered on entry door or near entry doors, scaled for viewing by pedestrians, not motorists.

4. Illumination

- a. Level of brightness of Sign lighting on buildings that include a residential component should be reduced by limiting external illumination to shielded or full cut-off fixtures such as gooseneck fixtures and recessed under canopy lighting.
- b. Exterior Sign lighting should be placed above the Sign in a manner that does not obscure the text and graphics. Only as many fixtures as are needed to adequately light the sign should be used.
- c. Exterior lights should be directed onto Signs so as not to create off-site glare or hot spots.

- d. Indirectly illuminated signs, which do not produce light from within, but are illuminated by spotlights, are preferred.
- e. No sign should be permitted which, by virtue of the intensity, direction, or color of its lighting or illumination, interferes with or causes confusion to traffic in public streets.

5. Materials and Workmanship

- a. Signs should convey professionalism and high-quality workmanship, and should be crafted by a professional.
- b. High-quality, durable, and low maintenance materials, such as aluminum, brass, copper, stainless steel, and finished wood should be used. If wood is used, it should be properly sealed to keep moisture from soaking into the wood and causing the Sign's lettering to deteriorate.
- c. Materials that complement the design of the building, the type of business being promoted, and the building material on which they are placed should be used.
- d. Materials, colors, graphic style, and lighting fixtures that contribute to Sign legibility should be used.



Sign Types Allowed in Sign District 1: Village Centers

The following are examples of permitted signage for use in Sign District 1 based upon the Sign Standards in Section 4.8.1.

Illustrative Images Only



Marquee and Neon Sign



Window Graphics Sign



Parapet/Elevated Sign



Portable Sign



Pole Sign

Sign Types Allowed in Sign Districts 1 and 2

The following are examples of permitted signage for use in the entire Specific Plan Area based upon the Sign Standards in Section 4.8.1.

Illustrative Images Only



Wall Sign



Projecting Sign



Channel Letter Sign



Awning Sign



Shingle Sign



Banner Sign

Conditional Sign Types

The following are examples of signage that may be used within prescribed parameters.

Illustrative Images Only



Window Foil Sign
(see 4.8.2)



Inflatable Sign
(See 4.8.2)

Prohibited Sign Types

The following are examples of signage prohibited throughout the entire Campus Town area based upon the Sign Standards in Section 4.8.1.

Illustrative Images Only



Cabinet Sign



Flashing Light Sign



LED Sign



Revolving Sign

Infrastructure

5.1 Introduction

Build out of the Campus Town Specific Plan Area requires provision of new and upgraded utility infrastructure to meet the needs of Specific Plan Area residents and tenants. Improvements include water, sewer, storm drain, electrical, natural gas, and communications infrastructure as well associated connections necessary to serve Campus Town buildings. New utility lines constructed within the Campus Town Specific Plan Area will be placed underground in public street rights-of-way or within easements and will be publicly owned or owned in common.

Conceptual locations for infrastructure improvements are identified on the exhibits in this section; alternate locations may be implemented. Changes to the proposed infrastructure location or service providers may be implemented, if approved by the appropriate jurisdiction, without amending the Specific Plan.

5.2 Conceptual Water System

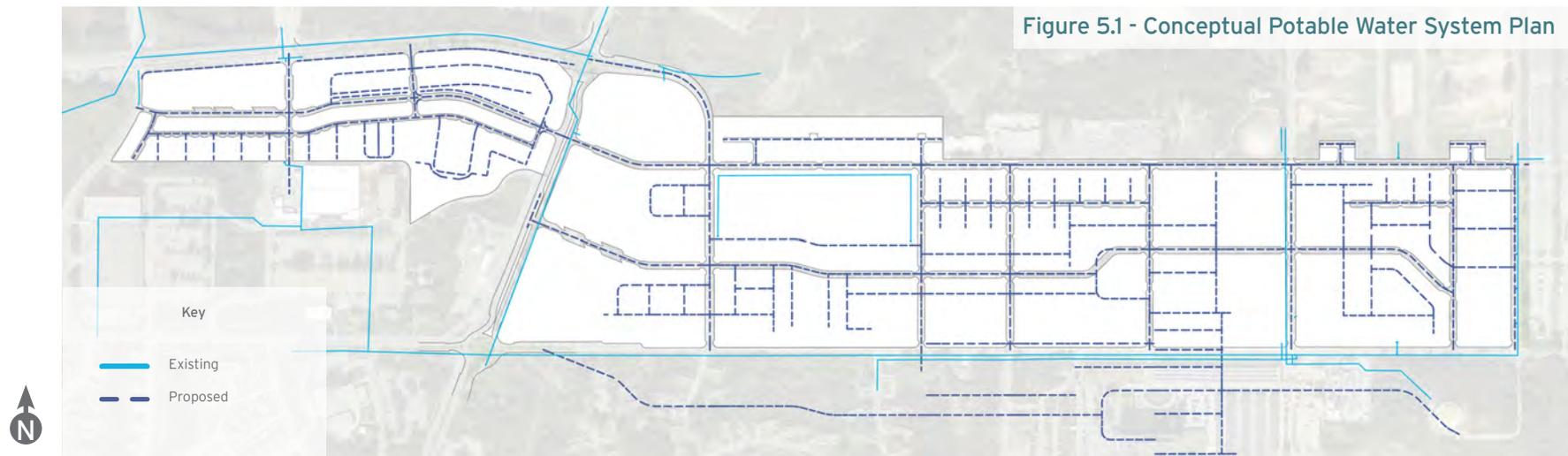
5.2.1 Potable Water

The Conceptual Potable Water System Plan, Figure 5.1, consists of three pressure zones within the Specific Plan Area. The water system will be sized to meet both potable water and firefighting demand requirements. Existing facilities within the Specific Plan Area that are to remain (e.g. those serving the Monterey College of Law) will be connected to the new system.

The existing water mains within Lightfighter Drive, General Jim Moore Boulevard, Gigling Road, 6th Avenue, and 7th Avenue will remain. All other existing water lines within the Specific Plan Area will be removed and replaced with new potable water mains that will be installed in all public and private roads within the Specific Plan Area. The remaining existing water mains and new mains will be tied together to create a gridded, redundant potable water system within each pressure zone.

5.2.2 Recycled Water

Marina Coast Water District recently installed a recycled water main in General Jim Moore Boulevard. Campus Town infrastructure will include installation of a recycled water main in Lightfighter Drive from 1st Avenue to General Jim Moore Boulevard, and adjacent to Gigling Road from General Jim Moore Boulevard to 7th Avenue. Within Specific Plan Area, recycled water will be used to irrigate public street landscape medians, public Open Space, landscaping for commercial/flex sites and landscaping for residential Front Yards. Recycled water may be provided for toilets, floor sinks, and other applicable recycled water uses allowed under the California Building Code.



5.3 Conceptual Storm Water System

5.3.1 Drainage System

The Specific Plan Area generally slopes to the north and west. In keeping with the existing site topography, drainage basins are proposed to be located at the low points of the Specific Plan Area: at 1st Avenue; in a portion of the “tree save” area; and at the General Jim Moore Boulevard/Lightfighter Drive intersection.

The conceptual Storm Drainage System Plan, Figure 5.2, will collect runoff from all internal residential streets and convey stormwater to these basin areas, which will be designed to provide retention up to the 100-year storm event.

5.3.2 Low Impact Development

Development within Campus Town will employ Low Impact Development techniques and stormwater control measures for residential and commercial uses that manage rainfall at the source. This includes compliance with

applicable MS4 permit requirements and applicable Municipal Code regulations, such as Urban Storm Water Control Management and Discharge Control, as well as the requirements discussed in Section 6.4.6. Examples could include: on-lot treatment/retention; pervious pavement; minimizing impervious footprints such as narrowed alley and road widths; providing vegetated drainage Swales/Open Spaces to pre-treat site run-off; preserving natural on-site areas; and disconnecting impervious surfaces.

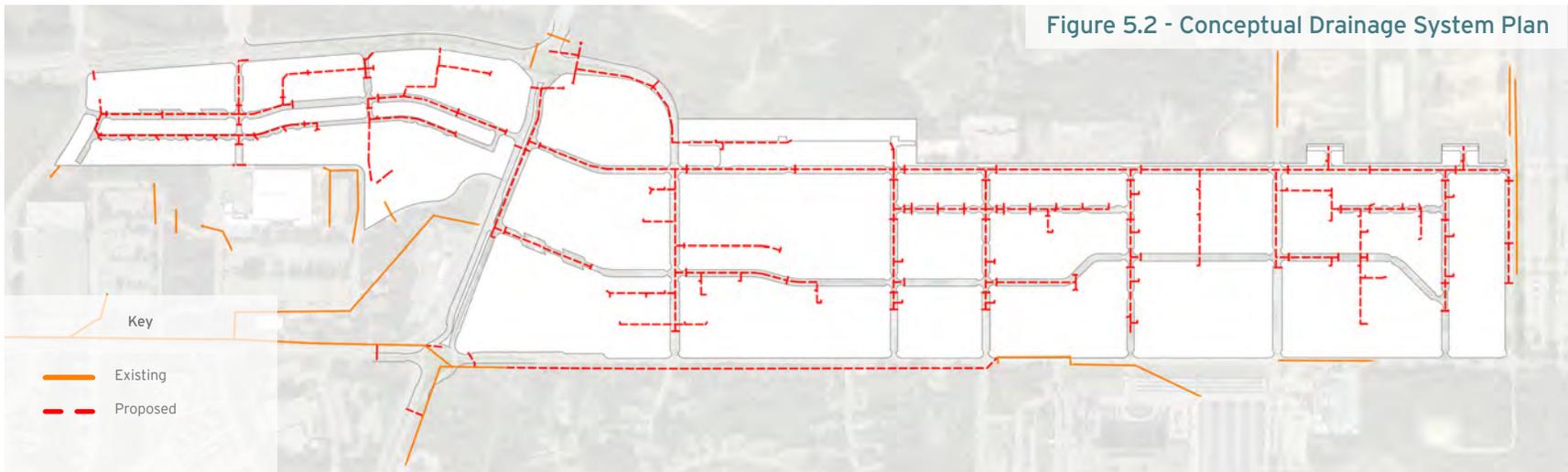


Figure 5.2 - Conceptual Drainage System Plan

5.4 Conceptual Sanitary Sewer System

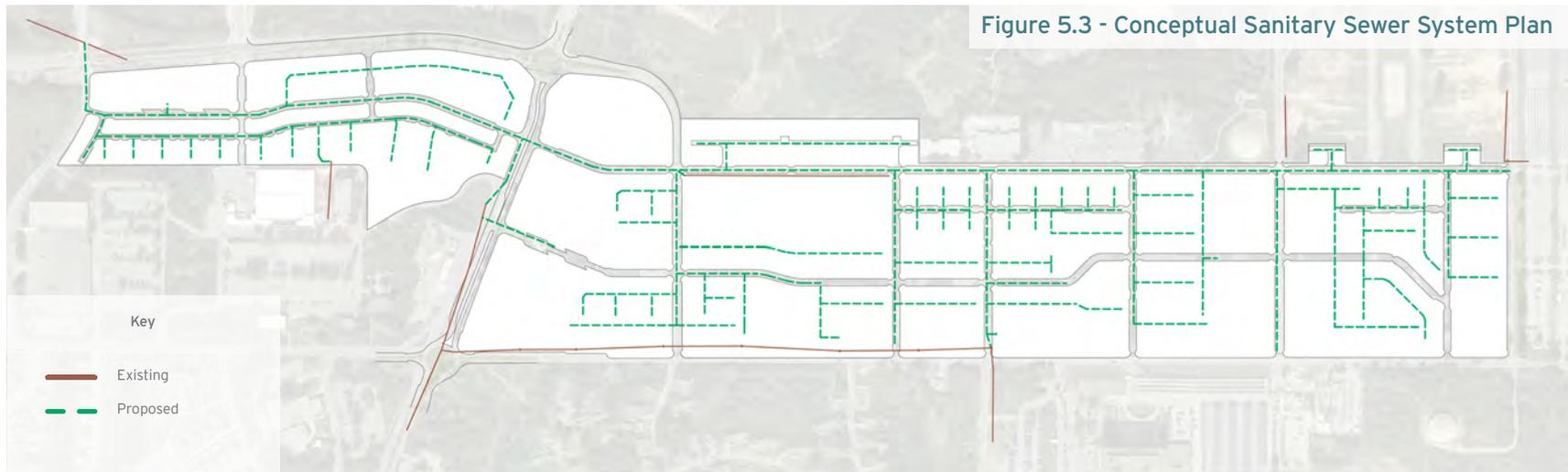
The Conceptual Sanitary Sewer System, Figure 5.3, will connect to an existing trunk line in 1st Avenue north of Lightfighter Drive. Effluent will be conveyed in a gravity-fed system and no pump stations are proposed. Sanitary sewer mains will be sized to accommodate the proposed development and placed in Street/

Alley rights-of-way, replacing the old pipe network that includes several “cross-country” alignments.

Gravity-fed mains that once connected to CSUMB’s sanitary system in 6th Avenue and 7th Avenue will be disconnected from this system, and will be joined to the new pipe network that feeds to 1st Avenue. Existing facilities within the Specific Plan Area that are to remain (e.g. those that serve the Monterey College of Law) will be connected to the new system. Existing sewer mains along the General Jim Moore Boulevard

Frontage will remain and be tied to the new system.

The Specific Plan Area’s sewer network will also include the connections of the existing mains from outside the Specific Plan Area that serve the U.S. Army Main Exchange and the Defense Department complex.



5.5 Conceptual Dry Utilities Plan

Power, telephone, cable, and natural gas plans will be submitted concurrent with the final tract map and improvement plan per phase. Existing dry utilities (see Figure 5.4) will be tied into once the improvement plans are submitted.

Dry utilities currently serve existing structures within the Campus Town Specific Plan Area. Gas lines are located underground. Site observations reveal that the electricity and telephone networks are composed of both above-ground and underground facilities; the most prominent of these is the row of transmission towers located immediately north of, and parallel to, Gigling Road. Overhead lines area also located along the easterly side of General Jim Moore Boulevard, and along a portion of Colonel Durham Street.

Future dry utilities including gas, electric, telephone, and fiber optic cable facilities will be networked throughout the Campus Town area. These utilities may be placed within streets, or located in a Public Service Easement (PSE) adjacent to rights-of-way and/or in Pedestrian Paths. Gas lines will be placed underground. Utility boxes will be placed within the proposed PSE.

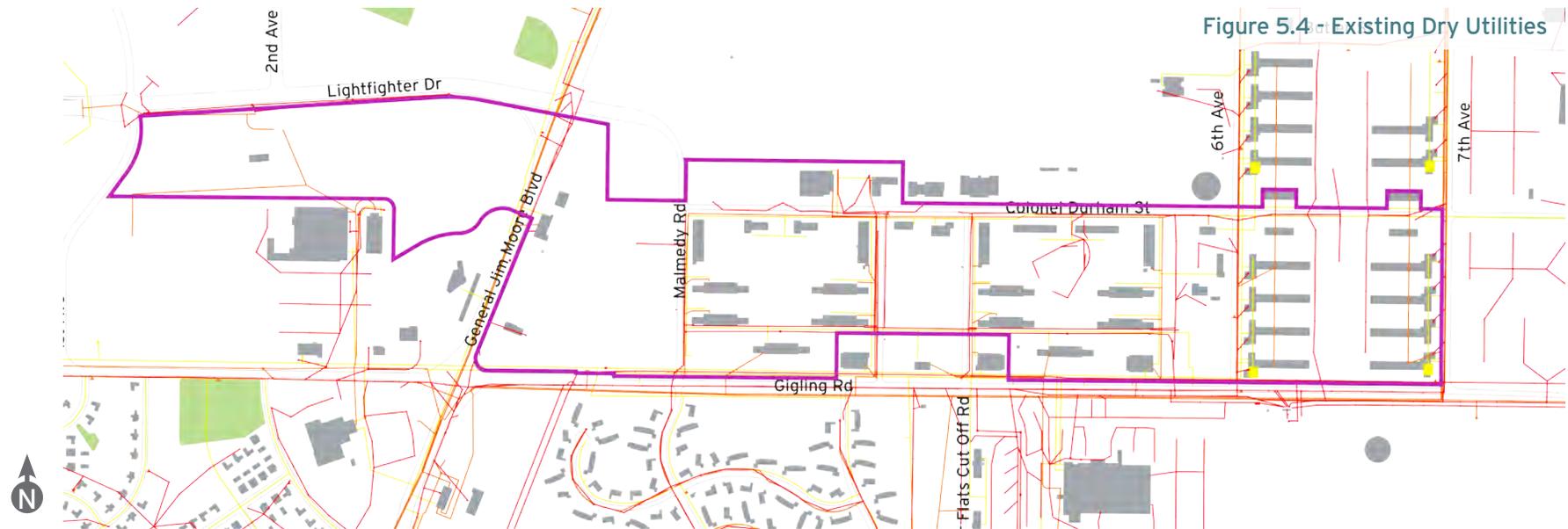


Figure 5.4 Existing Dry Utilities

5.6 Conceptual Solid Waste Plan

The proposed distribution, location, and extent intensity of major components of public and private solid waste disposal, energy, and other essential facilities proposed to be located within the Specific Plan Area and needed to support the land uses described in the Specific Plan will be submitted concurrent with the final tract map and improvement plan per phase.

5.7 Infrastructure Financing

The development of public facilities and the provision of public services associated with the Specific Plan Area may occur through a combination of financing programs. Different financing methods may be used for different types of infrastructure.

Some of the possible funding mechanisms for infrastructure improvements include, but are not limited to:

- Conventional private development financing
- Per unit hook up charges
- Reimbursement agreements
- Landscape and lighting districts
- Special Benefit Assessments
- Community Facilities Districts (e.g. Mello Roos)
- State and/or Federal grants and loans (e.g. Federal Transportation funds and various infrastructure financing programs)
- Development Impact fees, taxes, and processing fees
- Homeowner's Associations



Implementation

6.1 Purpose and Applicability

The Campus Town Specific Plan is intended to guide and regulate development within the Specific Plan Area. The Plan anticipates new development in the Specific Plan Area facilitated by a significant investment in infrastructure and public amenities. This chapter provides the framework for the development review process, including a detailed implementation program.

6.2 Specific Plan Adoption and Required Actions

This Specific Plan has been prepared and adopted in a manner consistent with California Government Code section 65451, as well as all pertinent sections of the Seaside Municipal Code. Adoption of this Specific Plan is by Ordinance (Section 65453) of the City Council.

It is anticipated that the following approvals/actions may be required to implement the Specific Plan:

- Certification of the EIR and Adoption of the Mitigation Monitoring and Reporting Program
- Specific Plan Approval/Adoption
- Zoning Map and Text Amendment
- FORA Consistency Determination
- Vesting Tentative Map(s) and Final Map(s)
- Conditional Use Permit(s)
- Development Application Approval(s)
- Minor Deviations and/or Deviation(s)
- Improvement Plans
- Development Agreement

- Disposition and Development Agreement with Successor Agency
- Affordable Housing Plan
- Building Permits
- Grading Permits
- Tree Removal Permits
- Encroachment Permits

6.3 Development Process

This section outlines the development review and approval process for all proposed development within the Campus Town Specific Plan Area.

By approving the Campus Town Specific Plan, the City has established the legislative policies applicable to the Specific Plan Area. In order to implement those policy decisions, the City will review Development Applications for development within the Specific Plan Area pursuant to this Chapter. The scope of review of Development Applications will consist of review for substantial conformance with the Specific Plan, as provided in Section 6.3.3.1.C. The development review process for projects proposed within the Campus Town Specific Plan Area is streamlined based on required conformance with the policies, Standards, and Guidelines established by the Specific Plan.



6.3.1 Administration and Interpretation

Approval of development within the Campus Town Specific Plan Area shall be subject to the review process set forth in this section.

A. Relationship to Seaside Municipal Code

The Campus Town Specific Plan constitutes the applicable zoning, including development standards and regulations, for development within the Specific Plan Area. Title 17 (Zoning) of the Seaside Municipal Code (SMC) shall apply only to matters not covered in this Specific Plan. If there is any conflict or inconsistency between the provisions of this Specific Plan and the SMC (including without limitation Municipal Code Section 8.54.070 (Trees), Title 17 (Zoning) and/or Title 16 (Subdivisions)), the provisions of this Specific Plan shall take precedence. All applicable state and federal standards and codes also apply to development within the Specific Plan Area. The Zoning Administrator or designee shall prepare a report on trees and views based on these plans and site inspection of the land.

1. Tree Removal. Each Development Application shall specify any tree to be removed or altered and shall demonstrate compliance with Coast Live Oak tree replacement

policy in this Specific Plan. Such application shall identify the Lot on which the tree is located, or to be planted, provide a perimeter outline of an existing or proposed building on the Lot, specify the location of the tree, and furnish a brief statement of the reason for the request. The following finding must be made if it is determined that any of the trees recommended for preservation should be removed: the preservation of the tree(s) would impair the implementation of the Base Reuse Plan, the General Plan, and/or the Specific Plan on the site in question, and/or the preservation is not feasible due to the health of the tree (including disease or pests). Final approval of a Conformance Determination with respect to any Development Application indicating any trees to be removed shall constitute a permit to remove or alter any trees so designated. Appeal of such permit shall be subject to the same provisions as appeal of a Conformance Determination, as provided herein.

2. Subdivisions. Unless otherwise specifically provided for in this Specific Plan, the regulations set

forth in Title 16 (Subdivision) of the SMC, entitled "Subdivisions," shall apply to all divisions of land hereafter made of property within the Campus Town Specific Plan Area. Subdivision will be accomplished through recordation of multiple phased maps to facilitate the full implementation and buildout of the Specific Plan Area. Phasing plans shall provide all infrastructure necessary to support each phase in substantial conformance with the Specific Plan.

B. Administration of Specific Plan

The primary administrator of this Specific Plan is the City's Zoning Administrator. Following review of each Development Application as provided herein, the Zoning Administrator shall have authority over approval of Development Applications, as provided in Section 6.3.3.C. The Zoning Administrator shall have the responsibility to interpret the provisions of the Specific Plan. If an issue or situation arises that is not sufficiently provided for in the Specific Plan, the Zoning Administrator shall be guided by the purpose, goals, and policies of the Specific Plan and the interpretation provisions of the Title 17 (Zoning) of the SMC.



6.3.2 Peer Review

All Development Applications shall be subject to review by a Peer Reviewer appointed by the Zoning Administrator. The Peer Reviewer shall at a minimum be a licensed architect, doing business within 75 miles of the City of Seaside.

6.3.3 Development Applications

6.3.3.1 Application Procedures

The applicant shall submit a Development Application to the Planning Division consistent with the SMC.

A. Submittal Requirements

For each Development Application, the applicant, at its election, may submit a Development Application for Substantial Conformance Review in either a single step or a two-step process, pursuant to Section 6.3.2.1.C below. If an applicant selects a single step process, the Development Application submittal requirements shall be those set forth in Section 6.3.2.1.A.2 for Final Substantial Conformance Review; if an applicant selects a two-step process, the Development Application submittal requirements shall be those set forth in Section 6.3.2.1.A.1 for Preliminary Substantial Conformance Review and

those set forth in section 6.3.2.1.B.2 for Final Substantial Conformance Review.

1. Submittal Requirements for Preliminary Substantial Conformance Review are as follows:
 - a. CD or other electronic format-Containing digital files of all materials listed below:
 - b. APPLICATION - One (1) copy
 - c. SCHEMATIC DESIGN PLANS - One (1) full-size 24"X 36" set and two (2) 11"X 17" reductions.
 - d. COVER SHEET
 - Applicant name, address and phone number.
 - Project site address, or map of all sites where this building is to be constructed.
 - Vicinity Map 1/2 mile radius street system with project site(s) highlighted.
 - Total minimum area of site in square feet.
 - Applicable Building Types and Frontage Types per Sections 4.6.2 and 4.6.3, respectively.
 - Number of proposed new buildings with total square footage for each (including parking structures).

- Square footage for each building's footprint and the maximum percentage of the site covered by the building(s).
- Number of stories for each building.
- Number of parking and loading spaces and garage tiers, if provided in a structure.
- Landscaped areas and percentage of the total site dedicated for landscaping (typical).
- Paved area and percentage of the total site dedicated for hardscape/paving (i.e., driveways, walkways, courtyards, and trash storage) (typical).
- Existing Regulating Plan designation, Sub-Area designation, and Sign District designation.
- Number of pages included in the plan set.
- e. SITE PLAN: Proposed site plan, including existing buildings within one block of the Lot Line, in all directions.

f. SCHEMATIC PROJECT SITE PLAN

- Lot Lines.
- Setbacks (Front Yard and Side Street Setbacks measured from face of curb).
- Internal and external rights-of-way and any vehicular access or other easements, including new Streets and Alleys.
- Sidewalk/planter dimensions (from face of curb to back of sidewalk).
- Existing buildings (if applicable) to be retained and proposed building locations and dimensions with uses labeled.
- Location and purpose of proposed open space.
- Location of structures on adjacent properties and their uses (if applicable).
- Topography and/or any existing site-specific conditions that must be addressed.
- Dimensions of driveway widths, existing and proposed curb cuts, parking spaces (use arrows to indicate traffic circulation).

- Sidewalks and walkways (include existing and proposed).
- Walls and fences (include materials and dimensions for existing and proposed). Indicate walls and fences to be removed (where applicable).
- Preliminary location of all exterior mechanical equipment.
- Street lights (where applicable).
- Trees: show existing trees with accurate canopies and overlap, if any, between proposed building footprints and canopy/root system of existing street trees.
- Preliminary joint trench and utility plan that includes conceptual location of electrical vault (where applicable), gas and electrical meters, fire sprinkler valves, backflow preventer, HVAC condensers, etc.

g. SCHEMATIC FOOTPRINTS: Existing (if to be retained) and proposed building footprints, including perimeter dimensions and openings in exterior walls.

h. SCHEMATIC ROOF PLAN: Roof pitch and material.

i. SCHEMATIC ELEVATIONS

- Building elevations (in color and black and white)
- Building heights in stories
- Proposed materials
- Proposed locations of signs

j. SCHEMATIC SITE/BUILDING SECTIONS

- In the case of a housing unit to be repeated - provide examples representing the range of Thoroughfare types or public spaces it is intended to front.

k. PRELIMINARY LANDSCAPE PLAN

- Softscape (including trees and shrubs) showing typical conditions.
- Hardscape (identify proposed materials) showing typical conditions.
- Preliminary Landscape Palette.

l. FIRE MASTER PLAN to show how the proposed project reduces the risk from wildfires.



- m. CONCEPTUAL GRADING PLAN:
Show all typical cases and cuts and fills needed
- n. DIGITAL MASSING MODEL simple digital building form/massing model of the project. Internal building elements, landscaping, people, cars, etc. not required.
2. Submittal Requirements for Final Substantial Conformance Review are as follows:
- a. RESPONSE TO PRELIMINARY SUBSTANTIAL CONFORMANCE REVIEW COMMENTS (if applicable). Point by point reference guide citing to the specific plan sheets that respond to comments provided at the conclusion of the Preliminary Substantial Conformance Review process.
 - b. FINAL DESIGN PLANS - One (1) full-size 24"X 36" set and two (2) 11"X 17" reductions.
 - c. CD or other electronic format, APPLICATION, COVER SHEET, SITE PLAN, PROJECT SITE PLAN, FLOOR PLANS, ROOF, PLAN, ELEVATIONS, SITE SECTIONS, FIRE MASTER PLAN, GRADING PLAN, DIGITAL MASSING MODEL, as required for Preliminary Substantial Conformance Review, except that all schematic items for Preliminary Substantial Conformance Review must be in final form for Final Substantial Conformance Review.
 - d. BUILDING SECTIONS (cross and longitudinal)
 - Building walls
 - Freestanding walls
 - Floor-to-floor dimensions
 - e. DESIGN DETAILS
 - Gutters and downspouts (indicate the location and colors of gutters and downspouts on the building elevations).
 - Mechanical plans showing locations of exterior equipment, through-the-wall vents, and ventilation shafts.
 - f. LANDSCAPE PLAN
 - Planting plan showing location, spacing, common name, botanical name, container size, quantity of all proposed new plant material with distinct plant symbols for each specimen.
 - Landscape layout plan for construction, including dimensions, materials, and finishes (drawings, catalog cuts, and/or photographs and an existing installation).
 - Hardscape details (including paving, trash enclosure, raised planters, water features, fences, walls, site furniture, etc.).
 - Exterior lighting (including type of fixture and catalog cut sheets)
 - Tree inventory of all trees with a 4-inch diameter or greater.
 - g. EXTERIOR SIGNS AND OTHER GRAPHICS
 - Location, dimensions, type (including illuminated or non-illuminated) and use (i.e., building sign, tenant sign, wayfinding sign).
 - Include light fixtures, electrical raceways (conduit), and all other designs, materials, lettering font, graphics, and colors used as identification or for calling attention to the project or tenant. Include color

- and material samples on the color and material board.
- h. COLOR AND MATERIAL BOARD - One (1) 11"x 17" copy.
- Identify board with project address, architect name, address, telephone number, email address, and date. Include samples of all proposed exterior materials, paint colors, and materials for glazing and paving (except clear glass).
 - Identify all samples with manufacturer name and item specification number (including catalog cut - photographs of existing installation may be accepted in place of project literature).
- i. FIRE MASTER PLAN to show how the proposed project reduces the risk from wildfires
- j. GRADING PLAN
- k. PHASING PLAN (for multi-phased projects)
- l. DIGITAL MASSING MODEL Simple digital building form/massing model of the project. Internal building elements, landscaping, people, cars, etc. not required.

B. Completeness Review

The Zoning Administrator shall review each Development Application for completeness in accordance with the Permit Streamlining Act and the SMC provisions regarding review for completeness.

C. Substantial Conformance Review

Following a determination that the Development Application is complete, the Zoning Administrator and his or her designee shall conduct "Substantial Conformance Review." Substantial Conformance Review shall consist of an evaluation by the Zoning Administrator with input from the City's Peer Reviewer of whether each complete Development Application is (i) consistent with the General Plan; (ii) in Substantial Conformance (as defined below) with this Specific Plan; and (iii) in compliance with applicable law. A Development Application is in "Substantial Conformance" with the Specific Plan if it substantially conforms with the Standards of the Specific Plan (including the applicable Land Use and Sub-Area Standards) and does not materially conflict with the applicable Guidelines of the Specific Plan.

During Substantial Conformance Review, the Zoning Administrator, Peer Reviewer and the applicant shall work collaboratively to address any comments that the Zoning Administrator and the Peer Reviewer may have on the Development Application with respect to Substantial Conformance, such that the applicant is provided with opportunities to respond to the comments and to facilitate the Substantial Conformance Review within the time periods provided for in this Section.

At the end of the Substantial Conformance Review, and no later than 30 days after the a Development Application is deemed complete in accordance with Section 6.3.31.B, the Zoning Administrator shall provide the applicant with a written determination ("Conformance Determination") of whether the Development Application is (i) consistent with the General Plan; (ii) in Substantial Conformance with this Specific Plan; and (iii) in compliance with applicable law. A Conformance Determination by the Zoning Administrator shall not require notice or a public hearing.



If the Conformance Determination indicates that the Development Application is (i) consistent with the General Plan; (ii) in Substantial Conformance with this Specific Plan; and (iii) in compliance with applicable law, the Zoning Administrator shall approve the Development Application. Notice of the Zoning Administrator's approval shall be provided as an informational item on the agenda for the next regularly scheduled public meeting of the Zoning Administrator. The public's right to appeal and procedures to do so shall be posted on the agenda for the public meeting.

If the Conformance Determination indicates that the Development Application is not (i) consistent with the General Plan, (ii) in Substantial Conformance with this Specific Plan, and/or (iii) in compliance with applicable law, the Conformance Determination shall include an explanation of the Zoning Administrator's findings regarding any such inconsistency, lack of Substantial Conformance (including findings regarding qualification for a Minor Deviation pursuant to Section 6.3.3.2.A or Deviation pursuant to Section 6.3.3.2.B), and/or noncompliance, and

a description of changes that would be necessary to bring the Development Application into consistency/Substantial Conformance/compliance. In such case, the Zoning Administrator shall disapprove the Development Application and the applicant may (1) revise the Development Application to achieve the required consistency/Substantial Conformance/compliance and resubmit the Development Application for another Conformance Determination following the process described above; (2) apply for a Minor Deviation or Deviation; or (3) appeal the Zoning Administrator's denial of the Development application. The Conformance Determination of the Zoning Administrator may be appealed to the Planning Commission and subsequently to the City Council pursuant to the procedures of the City's Zoning Code, regarding Appeals.

Any approval of a Development Application not exercised within two years after final approval shall expire and become void, except where an extension of time is approved as specified in the SMC or as otherwise provided for in a Development Agreement. After it has been exercised (meaning that

construction plans have been submitted and/or construction has commenced), a Development Application approval shall remain valid and run with the land in accordance with the Specific Plan.

6.3.3.2 Deviation Procedures

The Specific Plan is a policy and planning framework for the ultimate buildout of the Specific Plan Area. Implementation of the Specific Plan requires a degree of flexibility to respond to site, market, and other factors that may arise in connection with specific development proposals. In order to ensure that implementation of the Specific Plan is feasible and to preserve the opportunity for variations that will result in superior development, the City may approve deviations from Specific Plan Standards and Guidelines as set forth in this Section 6.3.3.2.

A deviation will be considered a Minor Deviation or a Deviation, as defined below.

A. Minor Deviations.

1. Minor Deviations are variations to the Specific Plan Standards and Guidelines that do not materially affect the objectives of this Specific Plan and are minor in light of the Specific Plan as a whole. Minor



Deviations do not constitute or require an amendment to the Specific Plan. If the Zoning Administrator deems a deviation to not be a Minor Deviation, the Development Application shall be processed as a Deviation pursuant to Section 6.3.3.2.B below.

2. Upon written request by an applicant, the Zoning Administrator may approve, conditionally approve, or deny a Minor Deviation without public notice or a public hearing, in which case the decision of the Zoning Administrator shall be reported as an information item on the agenda for the next regularly scheduled public meeting of the Zoning Administrator. Alternatively if the Zoning Administrator determines that the Minor Deviation is of significant public interest or raises significant policy issues, the Zoning Administrator may refer the Minor Deviation to the Planning Commission for a noticed public hearing. The following constitute examples of Minor Deviations:
 - a. To allow walls or fences to exceed the Height limit described herein by a maximum of two feet.
 - b. To allow a decrease not exceeding 10 percent of the required area for landscape coverage.
 - c. To allow a decrease not exceeding 10 percent of the minimum lot dimensions described in Chapter 4.6.2.
 - d. To allow an increase or decrease not exceeding 10 percent of minimum and maximum required front setbacks.
 - e. To allow an increase not exceeding 10 percent of maximum height.
 - f. To allow a decrease in required number of parking spaces based on quantifiable information provided by the applicant regarding shared parking, reduced demand, or alternative mobility strategies.
 - g. Deviations of a similar nature to those listed above that are deemed minor by the Zoning Administrator that are in keeping with the intent and objectives of this Specific Plan.
3. In approving a Minor Deviation, the Zoning Administrator or the Planning Commission, as applicable, may impose conditions as deemed necessary to protect the public's health, safety and welfare, and to assure compliance with the objectives of the Specific Plan.
4. The Zoning Administrator's determination on a Minor Deviation may be appealed to the Planning Commission pursuant to the appeal procedures of the Zoning Code. A decision of the Planning Commission on a Minor Deviation may be appealed to the City Council pursuant to the appeal procedures in the Zoning Code.

B. Deviations

1. Deviations are variations from the Standards or Guidelines of this Specific Plan that do not constitute Minor Deviations, but nonetheless do not materially affect the objectives of this Specific Plan. Applications for a Deviation shall be subject to review and approval by the Planning Commission following a noticed public hearing, and subject to appeal to the City Council in accordance with the appeal procedures of the City's Zoning Code. The Planning Commission (or the City Council on appeal) shall



approve a Deviation if it makes the following findings:

- a. The Deviation is consistent with the goals and policies of the Specific Plan.
 - b. The Deviation is not detrimental to public health, safety or welfare.
 - c. The Deviation does not create a nuisance or hazard.
 - d. The Deviation does not have a significant adverse effect on adjoining properties or the immediate neighbors.
2. In approving a Deviation, the Planning Commission (or the City Council on appeal) may impose conditions as deemed necessary to protect the public's health, safety and welfare, and to ensure compliance with the objectives of the Specific Plan.

shall be followed for hearing, notice, and decision of a Specific Plan amendment.

6.3.4 Fees and Taxes

The City requires the payment of development impact fees and certain taxes to finance public improvements. The City also requires the payment of processing fees to offset the cost of processing Development Applications. Fees for development within the City apply to projects within the Campus Town Specific Plan Area, subject to any applicable Development Agreement. These fees include:

- Public Land Dedication
- School Land Dedication
- Park Development Fee
- Water and Sewer Tap Fees
- Potable Irrigation

6.3.3.3 Amendment Procedures

This Specific Plan can be amended pursuant to the provisions of the California Government Code (Sections 65453-65454). Procedures of the SMC



6.4 Implementation Measures

The infrastructure required for the development of the Campus Town Specific Plan Area includes Streets, utilities, public Open Spaces, and shared public parking facilities for non-residential development. The Specific Plan is designed to allow infrastructure to be built incrementally over time as the area develops. Certain major Streets, public Open Space facilities and utility mains that serve the entire Specific Plan Area will be constructed by the City and repaid through assessments or taxes over time, subject to any applicable Development Agreement. It is expected that tax revenue generated as a result of development within the Specific Plan Area will support any new fire and police services that are necessary.

6.4.1 Conceptual Phasing of Construction

The Specific Plan may be implemented in phases over time at the discretion of each applicant, subject to any applicable requirements of any Development Agreement. Except as specifically provided in this Specific Plan, any existing infrastructure and facilities are permitted to remain in place and continue in use while the development allowed under the Specific Plan is constructed.

The development of the Specific Plan Area is divided into two major phases of horizontal construction as delineated by Malmedy Road, see Table 6.1. Phase 1 consists of the portion of the Plan Area west of Malmedy Road, while Phase 2 consists of the portion of the Plan Area east of Malmedy Road.

Within these major phases, vertical development may occur in sub phases. The anticipated phasing of construction is shown in Figures 6.1 and 6.2, Conceptual Phasing Plan. This phasing is subject to change at the discretion of each applicant, subject to any applicable Development Agreement.

In connection with the subdivision of the Specific Plan Area, phasing plans shall provide all infrastructure necessary to support each phase in Substantial Conformance with the

Sub-Area	Land Area (sf)
Phase 1	
West End	1,047,159
Commercial Center	531,258
Central (partial)	330,723
Phase 1 Total	1,909,140
Phase 2	
Central (partial)	1,935,495
Campus Adjacent	238,456
University Village	431,893
East End	718,516
Phase 2 Total	3,324,360

Table 6.1 - Conceptual Phasing Areas

Specific Plan. As each phase of the Specific Plan with public infrastructure is built, the completed public infrastructure will be dedicated to the City or other applicable public agency or utility for ownership and maintenance.

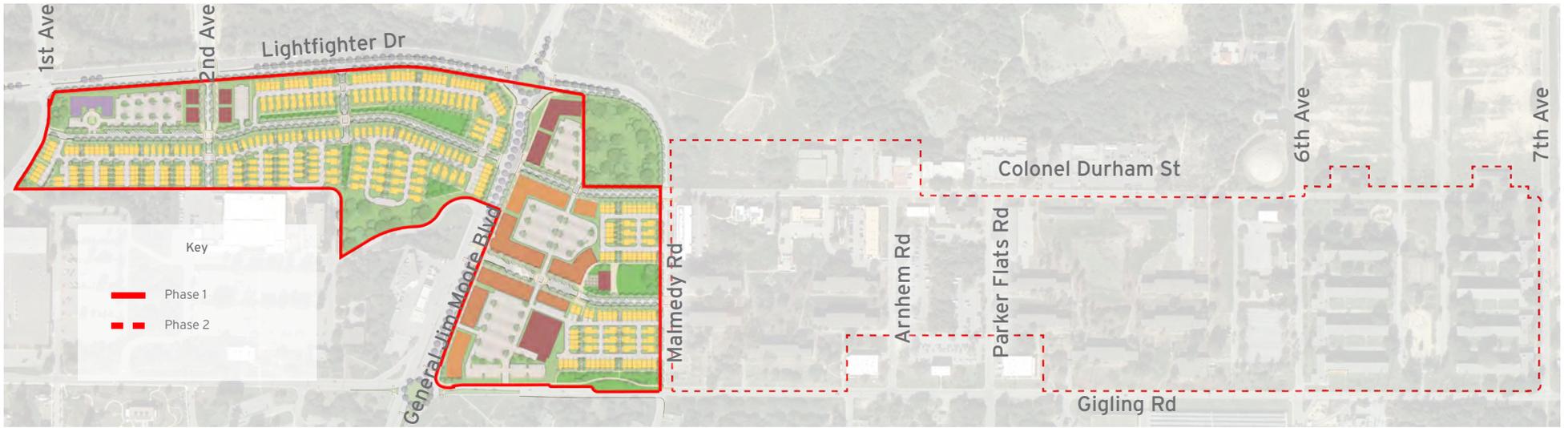


Figure 6.1 - Conceptual Phase 1 Buildout Plan

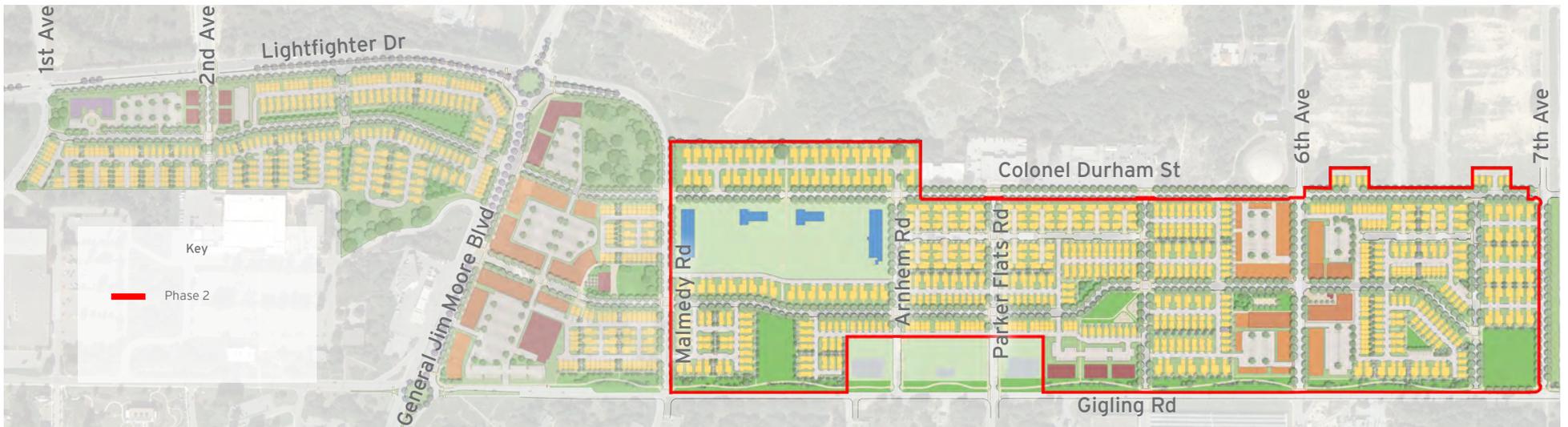


Figure 6.2 - Conceptual Phase 2 Buildout Plan



6.4.2 Financing, Construction, and Maintenance of Improvements

The improvements contemplated for the Specific Plan Area consist of elements for use by the public, as well as for exclusive use of the property owners and their guests. Once constructed, long-term maintenance of improvements will be required, and the party responsible for maintaining those improvements may vary depending on whether they are dedicated for public use or privately owned. Currently, it is anticipated that the City will form a Community Facilities District to fund the maintenance of the City public improvements within the Specific Plan Area, and that a master owner's association (with sub-associations for different portions of the Plan Area) will maintain private improvements within the Specific Plan Area.

6.4.3 Traffic Mitigation

Traffic mitigation measures will be identified in the Mitigation Monitoring Program of the Campus Town Draft EIR.

6.4.4 Thoroughfares

The introduction of an expanded Thoroughfare grid, including sidewalks and landscaping, within the Specific Plan Area will be constructed as private development occurs and will be maintained as City rights-of-way.

Alleys and auto courts, including sidewalks, shall be privately owned and maintained by one or more homeowners' associations.

6.4.5 Water and Sanitary Sewer

All water mains shall be public and shall be maintained by Marina Coast Water District.

All sanitary sewer mains in privately-owned Alleys and auto courts shall be private and maintained by a homeowners' association. All sanitary sewer mains in public streets shall be owned and maintained by the Marina Coast Water District.

All storm drain mains in privately-owned Alleys and auto courts shall be private and maintained by a homeowners' association. All storm drain mains in public streets shall be owned and maintained by the City of Seaside.

Recycled water service is encouraged for public and private landscaping throughout the Specific Plan Area.

6.4.6 Storm Water Retention

The Specific Plan Area is located in the Central Coast Regional Water Quality Control Board (CCRWQCB) jurisdiction. Storm water runoff management shall adhere to the criteria identified in the resolution no. R3-2013-0032 "Post-Construction Stormwater Management Requirements for Development Projects in the Central Coast Region" dated July 12, 2013, as it may be amended for time to time.

Stormwater facilities shall be designed per the guidelines in the Fort Ord Reuse Authority (FORA) "Stormwater Master Plan" dated March 2005, as it may be amended from time to time, which stipulates runoff produced from the 100-year, 24-hour storm event shall be infiltrated.

6.5 Implementation Program

Implementation of the vision for Campus Town will be achieved through regulatory actions and infrastructure improvements. Table 6.2 outlines the expected actions and improvements necessary to achieve build-out of the Campus Town. These actions will occur incrementally, with expected timeliness noted in the table, and will be coordinated by the City or other public agencies. Specific infrastructure improvements will be implemented in concert and negotiation with private development as it occurs.

Phase	Action	Description	Coordinating City Department or Public Agency	Timeline
0	Prepare and Adopt Specific Plan	Approval of this document by Planning Commission and City Council will fulfill Implementation items in the General Plan	Department of Community Development	Near Term
0	Prepare and Adopt EIR for Specific Plan	This is prepared in coordination with the Specific Plan	Department of Community Development	Near Term
0	Adoption of Tentative Map	This is submitted by the contract purchaser coincident with the Specific Plan	Department of Public Works	Near Term
0	Approve Development Agreement		Department of Economic Development	Near Term
0	Approve Affordable Housing Agreement		Department of Economic Development	Near Term
0	Secure Appropriate Amount of Water Rights		Department of Economic Development	Short Term
1	Developer satisfies all conditions of approval for Phase 1		Department of Economic Development	Short Term
1	Approve Final Map and Improvement Plan for Phase 1		Department of Public Works	Short Term
1	Approve Dry Utilities Plan for Phase 1		Department of Public Works	Short Term
1	Approve Solid Waste Plan for Phase 1		Greenwaste Recovery	Short Term
1	Developer closes on sale of Phase 1		Department of Economic Development	Short Term
1	Site Preparation	Grading and Grubbing	Department of Public Works	Short Term

Table 6.2 - Implementation Program Matrix



Phase	Action	Description	Coordinating City Department or Public Agency	Timeline
1	Construction of Horizontal Infrastructure	Grading, installation of utilities and construction of streets, curbs, gutters and sidewalks for Stage 1	Department of Public Works	Short - Medium Term
1	Parcel, lot and home sales, and construction	This will include the sale of certain parcels to other developers	Building Department	Medium Term
1	Final streetscape construction	This includes street tree planting, and irrigation	Department of Public Works	Medium Term
	Public Space construction and deeded to City of Seaside	This includes landscaping, hardscaping and irrigation.	Parks Department	Medium Term
2	Developer satisfies all conditions for approval for Phase 2			Medium - Long Term
2	Improvements to General Jim Moore	This includes new intersection and traffic signal, provision of on-street parking, and construction of two-roundabouts, among other things	Department of Public Works	Medium - Long Term
2	Approve Final Map and Improvement Plan for Phase 2		Department of Public Works	Medium - Long Term
2	Approve Dry Utilities Plan for Phase 2		Department of Public Works	Medium - Long Term
2	Approve Solid Waste Plan for Phase 2		Greenwaste Recovery	Medium - Long Term
2	Developer closes on sale of Phase 2		Department of Economic Development	Medium - Long Term
2	Site Preparation	Grading and Grubbing	Department of Public Works	Medium - Long Term
2	Construction of Horizontal Infrastructure	Grading, installation of utilities and construction of streets, curbs, gutters and sidewalks for Stage 2	Department of Public Works	Long Term

Table 6.2 - Implementation Program Matrix
(continued)



Phase	Action	Description	Coordinating City Department or Public Agency	Timeline
2	Parcel, Lot and home sales, and construction	This will include installation of new traffic signal, on-street parking and construction of roundabouts	Department of Public Works	Long Term
2	Final streetscape construction	This includes street tree planting, and irrigation	Department of Public Works	Long Term
2	Public Space construction and deeded to City of Seaside	This includes landscaping, hardscaping and irrigation.	Parks Department	Long Term

Table 6.2 - Implementation Program Matrix
(continued)



Glossary

A

Active Frontages: the front edge of a building or space that has frequently occurring windows and doors as opposed to blank walls, fences and garages. Active Frontages usually exhibit a vertical rhythm to the buildings; articulation to building Facades. If ground floor uses are residential, an Active Frontage would include frequent entry doors and/or Stoops, as well as an articulation of the Frontage with proximate architectural or landscape detail to engage the eye of a pedestrian walking by.

Access Point: a point of entry on a Block Front providing access to parking or service facility areas.

Alley: a Thoroughfare Type as defined in Section 3.3 of the Thoroughfare Types and Standards.

Architectural Style: the characteristic form and detail of buildings from a particular historical period or school of architecture.

Arterial Street: a high-capacity urban road that primarily delivers traffic from collector roads to freeways.

Attic: the area located between the uppermost plate and the roof or ridge of a structure.

Auto Court: a Building Type as defined in Section 4.6.2 of the Urban Standards.

B

Block: the primary bounded areas defined for the purpose of site organization used to regulate the land uses, Heights, and design requirements in the Specific Plan Standards and Guidelines.

Block Front: the plane of the edge of each side of a Block or section of a Block facing a public or private right-of-way or Open Space.

Build to Line: a line, parallel to the Lot Line, shown in the Setback Plan as illustrated in Section 4.3.2, that must be occupied by a specified percentage of the Façade. The Build to Line is measured as a distance from the Lot Line. For example, a five foot Build to Line would

be located five feet from the Lot Line within the Lot. Where a Build to Line is specified along an Open Space, Lots fronting the Open Space shall conform to the Build to Line.

Building Face: the exterior wall of a building.

Building Type: a structure category determined by function, disposition on the Lot, and configuration, including Frontage and Height. There are 13 Building Types within the Specific Plan Area: Single Family Dwelling, Rosewalk, Auto Court, Rowhouse, Townhouse, Flex Loft, Carriage House, Urban Block, Liner with Garage, Large Format - Types I & II, Flex Block and Flex Shed.

C

Carriage House: a Building Type as defined in Section 4.6.2 of the Urban Standards.

City: the City of Seaside, state of California, referred to in this Specific Plan as the “City.” For the purposes of this Specific Plan, “City” includes the Seaside City Council, agents, employees, employees, and officers of the City of Seaside.

Common Driveway: a shared driveway in the Auto Court Building Type that provides access to multiple garages.

Common Walkway: a paved passageway providing access into or through a block.

Complete Streets: streets that are planned, designed, operated, and maintained to enable safe, convenient and comfortable travel and access for users of all ages and abilities regardless of their mode of transportation.

Conceptual: examples depict one possible design that would substantially conform to the Standards and be materially consistent with the Guidelines, but are not determinative.

Curb Radius (Radii): the radius defined by two sidewalks on perpendicular Streets that come together at a corner.

D

Designated Open Space: a public Open Space that is fixed in its general location and type as described in the Open Space Types Plan.

Designated Thoroughfare: a Thoroughfare that is fixed in its general location and type as described in the Thoroughfare Types Plan.

Development Agreement: an agreement for the development of real property within the Specific Plan Area entered into between the City and any person having a legal or equitable interest in

real property in the Specific Plan Area, pursuant to California Government Code Section 65864.

Development Application: an application filed by the owner of the proposed project site, or other person with the written consent of the property owner, for a land use permit, Deviation, amendment, or other land use approval pertaining to development within the Specific Plan Area.

Deviation: variations from the Standards or Guidelines of this Specific Plan that do not constitute Minor Deviations, but nonetheless do not materially affect the objectives of this Specific Plan.

Drive Aisle: a vehicular lane within a Lot that may give access to parking spaces, service and/or utility areas.

Driveway: a vehicular lane within a Lot, usually leading to a garage.

E

Elevation: an exterior wall of a building.

Enclosed Porch: a portion of a building that protrudes from the primary massing of a building and is fully enclosed by walls and a roof.

Encroachment: any structural element (including architectural features) that extends from the Building Face into the public right-of-way or Setback. The permitted Encroachments are provided in Section 4.7.8 of the Architectural Standards and Guidelines.

Encroachment Area: the area of land between the Building Face and the back of the curb or Setback, where Encroachments may be located.

F

Façade: a Building Face that is along a Frontage.

Façade String: a series of Rowhouse or Flex Loft units attached together in a single building.

Façade Width: the horizontal distance of a single building Façade.

Fenestration: the arrangement and design of windows and other openings on a building's Façade.

Flex Block: a Building Type as defined in Section 4.6.2 of the Urban Standards.

Flex Loft: a Building Type as defined in Section 4.6.2 of the Urban Standards.

Flex Shed: a Building Type as defined in Section 4.6.2 of the Urban Standards.

Form Based Code: A land development regulation that fosters predictable built results and a high-quality public realm by using physical form (rather than separation of uses) as the organizing principle for the code.

Fort Ord Base Reuse Authority Base Reuse Plan (BRP): The Fort Ord Reuse Authority adopted the Fort Ord Reuse Authority Base Reuse Plan in June 1997. The BRP, as amended as of the date of this Specific Plan, prepared by FORA to guide the development of the Former Military Reservation.

Fort Ord National Monument: a national conservation land under the Bureau of Land Management that provides abundant recreational, scientific, ecological, and cultural resources.

Fort Ord Recreational Trails and Greenway (FORTAG): a proposed 30-mile regional network of paved recreational trails and greenways connecting communities to Open Space.

Fort Ord Reuse Authority Regional Urban Design Guidelines (RUDG): the Regional Urban Design Guidelines the Fort Ord Reuse Authority adopted the Fort Ord Reuse Authority Regional Urban Design Guidelines on June 10, 2016. The RUDG as amended as of the date of this Specific

Plan, provides guidance on matters of visual importance to the former Fort Ord Reuse.

Frontage: the privately held area between the Façade of a building and the Lot Line.

Frontage Occupancy: the minimum percentage of the Block Front that must contain a building. Frontage Occupancy requirements shall apply to the first three stories of a building.

Frontage Type: as defined in Section 4.6.3 of the Urban Standards.

Fronts of Buildings: the face of a building where the primary pedestrian entrance is located. Unless otherwise noted, the Fronts of Buildings shall align with Streets (not Alleys), Rosewalks, Common Walkways or public Open Spaces.

Front Yard: the privately held area between the building and the sidewalk or Street, typically landscaped.

Furnishing Zone: a multi-purpose area within a Setback between the sidewalk and the building Frontage that provides space for planting strips, street furniture, café seating, product displays, and retail signage.



G

Grade: the mid-point of elevation of the finished surface of the ground and the sidewalk adjacent.

Green: a type of Open Space as defined in Section 3.4.2 of the Open Space Network and Type Standards.

Ground Plane: a horizontal plane of reference from which vertical measurements can be taken. Usually the ground plane refers to the adjacent Grade at the sidewalk.

Guidelines: recommended practices that allow greater discretion in their interpretation, implementation, or use. Development in the Specific Plan Area should not be materially inconsistent with Specific Plan Guidelines, subject to Minor Deviations and Deviations as provided in Chapter 6, Implementation. "Guidelines" are identified in this Specific Plan by use of the terms "should" or "may."

H

Habitable Encroaching Space: the portion of the building containing Habitable Space enclosed by walls and a roof that projects beyond the Building Face along the ground floor.

Habitable Projecting Space: the portion of the building containing Habitable Space enclosed by walls and a roof that projects beyond the Building Face and is raised a minimum of nine feet from the sidewalk, such as bay windows.

Habitable Space: space in a structure that is occupiable and is used primarily for living, sleeping, eating, selling of goods, or cooking. Bathrooms, closets, halls, storage areas, and utility spaces are not considered Habitable Spaces.

Hardscape: non-living elements of landscaping that primarily consist of paving materials such as brick, stone, wood, and concrete.

Height: the vertical distance between the highest point of the adjacent Grade and the average roof plate of building, not including rooftop amenities or mechanical equipment.

I

Identification Sign: a sign that identifies the building on which it is affixed, either by building name, address, or logo.

Illustrative: examples that illustrate one possible design that would substantially conform to the Standards and be materially consistent with Guidelines, but are not determinative.

Industrial Shop: a Frontage Type as defined in Section 4.6.2 of the Urban Standards.

Infiltration Planter Parkway: a Parkway that allows storm run-off to permeate into the ground.

L

Land Use Standards: the Standards that identify the allowable uses, permitted Building Types, permitted Open Space types, and additional Standards and permit requirements for each Sub-Area, see Section 4.5.

Landscaped Parkway: a Parkway that is planted with native and climate-appropriate vegetation.

Large Format Type I: a Building Type as defined in Section 4.6.2 of the Urban Standards.

Large Format Type II: a Building Type as defined in Section 4.6.2 of the Urban Standards.

Large Lot: a Lot with a contiguous area of 60,000 gross square feet or more.

LEED: Leadership in Energy and Environmental Design. A green building rating system developed by the US Green Building Council that provides a suite of standards for the environmentally sustainable design, construction and operation of buildings and neighborhoods.

Liner Building: a building that fronts a Primary Street to screen parking from the Public Realm.

Liner with Garage: a Building Type as defined in Section 4.6.2 of the Urban Standards.

Lot: a recorded lot or parcel of real property under single ownership, lawfully created as required by applicable Subdivision Map Act and applicable City ordinance requirements.

Lot Area: the total gross area included within the Lot Lines of a Lot, exclusive of adjacent dedicated street rights-of-way.

Lot Assemblage: an group of multiple contiguous Lots that are planned as a cohesive development.

Lot Depth: the average linear distance between the front and the rear Lot Lines or the intersection of the two side Lot Lines if there is no rear line. The Zoning Administrator shall determine Lot Depth for Lots of irregular configuration.

Lot Line: any recorded boundary of a Lot.

Lot Width: the horizontal distance between the side Lot Line, measured at right angles to the Lot Depth at a point midway between the front and rear Lot Lines. The Zoning Administrator shall determine Lot Width for parcels of irregular configuration.

M

Makerspace: a collaborative workspace that provides a variety of resources to foster entrepreneurship and business startups.

Maximum Allowable Development: the maximum aggregate amount of development allowed in the Specific Plan Area as specified in Section 4.4.

Minor Deviation: variations to the Specific Plan Standards and Guidelines that do not materially affect the objectives of this Specific Plan and are minor in light of the Specific Plan as a whole.

N

Non-Habitable Encroaching Space: the portion of the building containing Non Habitable Space that extends beyond the Building Face along the

ground floor, which is not enclosed by walls and a roof, such as a Stoop.

Non-Habitable Projecting Space: the portion of the building containing Non Habitable Space that extends beyond the Building Face, which is not enclosed by walls and a roof and is raised a minimum of nine feet from the ground floor, such as a balcony.

Non-Habitable Space: the portion of a building which is not enclosed by walls and a roof, such as a Stoop, balcony, or roof deck.

O

Open Space: public outdoor space reserved for active and passive recreation.

P

Park: a type of Open Space as defined in Section 3.4.2 of the Open Space Network and Type Standards.

Parking Placement Plan: the Regulating Plan that defines parking placement on Lots for specifically identified Frontages as defined in Section 4.3.3.

Parkway: the strip of land within the public right-of-way between the Sidewalk and the curb.



Paved Parkway: a Parkway that is hardscaped to extend the usable pedestrian area within the public right-of-way.

Plane Break: a vertical or horizontal offset in a Building Face used to create articulation and break up long wall planes.

Plaza: a type of Open Space as defined in Section 3.4.2 of the Open Space Network and Type Standards.

Playground: a type of Open Space as defined in Section 3.4.2 of the Open Space Network and Type Standards.

Pocket Park: a type of Open Space as defined in Section 3.4.2 of the Open Space Network and Type Standards.

Primary Entrance or Principal Entrance: the main point of access for pedestrians into a building.

Primary Street: a Street at which the majority of Lots on that Street have their Primary Entrance.

Principal Frontage: the Frontage designated to bear the addresses of and Principal Entrances to the individual units of a Rowhouse or Flex Loft Building, or other building.

Private Open Space: Open Space that is provided for the use of tenants or owners of a property, not the general public.

Projecting Space: a portion of the building that extends beyond the Building Face into the public right-of-way or Setback that is raised a minimum of nine feet from the sidewalk or Open Space.

Projection: an architectural element or portion of the building that extends beyond the Building Face into the public right-of-way or Setback that is raised a minimum of nine feet from the sidewalk or Open Space.

Public Realm: composed of public rights of way and private Front Yards, the public realm is the communal social setting of urban life. The term Public Realm is not intended to provide general public with any legal access rights to private property.

R

Regulating Plans: plans that regulate development within the Specific Plan Area as defined in Section 4.3.

Roadway: the area in the right of way as measured from curb to curb intended for vehicular travel, as well as bicycle travel, in designated areas.

Rosewalk: a Building Type as defined in Section 4.6.2 of the Urban Standards.

Rowhouse: a Building Type as defined in Section 4.6.2 of the Urban Standards.

S

Setback: the distance between the Lot Line and a building providing a transition between the public or semi-public domain and the private uses on the Lot.

Setback Plan: the Regulating Plan that defines Setback requirements on parcels for specifically identified Frontages as defined in Section 4.3.2.

Shared Parking: the use of a parking space to serve two or more individual land uses.

Shopfront: a Frontage Type as defined in Section 4.6.2 of the Urban Standards.

Side Street: a Street at which the minority of Lots on that Street have their Primary Entrance.

Side Yard: the Private (or semi-private) Open Space on a Lot located on the sides of buildings.

Sidewalk Grade: a level plane along the top of the sidewalk pavement.

Sign: any display board, wall, screen, projected image, object, or any other material or medium used to announce, declare, demonstrate, display or otherwise present a message and attract the attention of the public.

Single Family Dwelling: a Building Type as defined in Section 4.6.2 of the Urban Standards.

Special District: a designated district within a Sub-Area that requires new development to comply with site-specific standards that may differ from the Sub-Area in which it is located.

Specific Plan: a tool authorized by State law that enables a jurisdiction to define a specific vision for the future evolution of a specified planning area.

Specific Plan Area (Plan Area): the specified area that is subject to the jurisdiction of the Specific Plan as defined in Section 1.8.

Sports Field: a type of Open Space as defined in Section 3.4.2 of the Open Space Network and Type Standards.

Square: a type of Open Space as defined in Section 3.4.2 of the Open Space Network and Type Standards.

Standards: Established rules for objective measures to which development must substantially conform, subject to deviations as provided in Chapter 6, Implementation. “Standards” are identified in this Specific Plan by use of the terms “will” or “shall.”

Stepback: the horizontal distance between the building Façade of one Story and the building Façade of the Story below.

Stoop: a Frontage Type as defined in Section 4.6.2 of the Urban Standards.

Story: a floor level of Habitable Space within a building, excluding attics, raised basements, and rooftop amenities.

Street: a public or private Thoroughfare, which affords principal means of access to the abutting Lot. See Street Types in Section 3.2.

Sub-Area: a planning category that designates the character and form of development as defined in Section 4.3.1 of the Regulating Plans.

Sub-Area Plan: the Regulating Plan that defines the boundaries for the six different Sub-Areas as defined in Section 4.3.1.

Swale: a low or slightly depressed natural area for drainage.

T

Tandem Parking: a parking configuration in which two cars park one in front of the other, rear to front, which requires the car in the back to be moved in order to get the car in front out of the parking space.

Terrace: a Frontage Type as defined in Section 4.6.2 of the Urban Standards.

Thoroughfare: a public or private Street, which affords principal means of access to the abutting Lot. See Thoroughfare Types in Section 3.3.

Townhouse: a Building Type as defined in Section 4.6.2 of the Urban Standards.

U

Unenclosed Porch: a covered outdoor space attached to a building that is open on at least one side.

Urban Block: a Building Type as defined in Section 4.6.2 of the Urban Standards.



Urban Standards: the Standards that define Building Types, Frontage Types, and development on Large Lots, see Section 4.6.

Y

Yard: an area between a Lot Line and a structure, unobstructed and unoccupied from the ground upward, except for Projections and Encroachments permitted by this Specific Plan.

Yard/Porch: a Frontage Type as defined in Section 4.6.3 of the Urban Standards.



